

GENERAL NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

A) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES (50mm) ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES (75mm) ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

B) DO NOT EXCEED A DIFFERENCE OF 1.5 inches (40mm) IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT (150m) IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

C) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- D) PROVIDE PERMANENT SIGNING.
- E) PROVIDE DETOUR SIGNING OFF THE PROJECT LIMITS.
- F) COVER OR REMOVE ALL DETOUR SIGNS OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- G) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

H) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

PAVEMENT MARKINGS AND MARKERS

I) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
-L- (SR 1779)	THERMOPLASTIC	SNOWPLOWABLE

J) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

PHASING

PHASE I

STEP 1) USING RDWY. STD. DWG. 1101.03 SHEET 1 OF 9 AND SHEET TCP-3, INSTALL ALL ROAD CLOSURE AND DETOUR SIGNING.

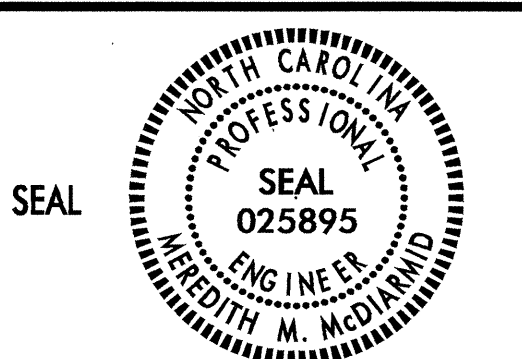

STEP 2) CLOSE SR 1779 (PAYNE ROAD) FROM -L- STA. 10+75 +/- TO -L- STA. 21+25 +/- . PLACE TRAFFIC ONTO OFF-SITE DETOUR AS SHOWN ON TCP-3. MAINTAIN ACCESS TO ALL DRIVEWAYS WITHIN THE PROJECT LIMITS.

PHASE II

STEP 1) REMOVE EXISTING BRIDGE #156 AND APPROACHES. CONSTRUCT THE PROPOSED STRUCTURE AND APPROACHES UP TO AND INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM -L- STA. 10+75 +/- TO -L- STA. 21+25 +/- . INSTALL FINAL PAVEMENT MARKINGS (SEE PAVEMENT MARKING SCHEDULE ON TCP-1).

STEP 2) REMOVE ALL TRAFFIC CONTROL SIGNING AND DEVICES AND RE-OPEN SR 1779 (PAYNE ROAD) TO A TWO-LANE TWO-WAY PATTERN.

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APPROVED: <i>MM</i> DATE: 4/23/05		GENERAL NOTES AND PHASING	
			
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