

6/16/19

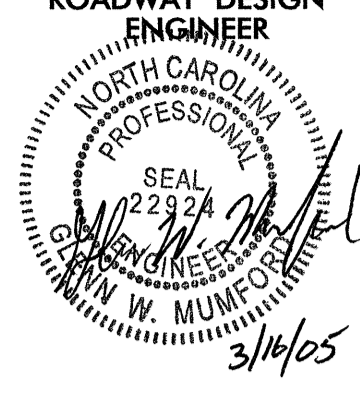
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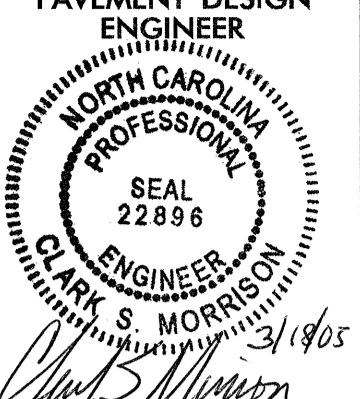
PAVEMENT SCHEDULE

A1	360mm PCCP
A2	CONCRETE SHOULDERS ADJACENT TO 360mm PAVEMENT
C1	30mm TYPE SF9.5A
C2	35mm TYPE SF9.5A
C3	50mm TYPE SF9.5A
C4	70mm TYPE SF9.5A
C5	VAR. DEPTH TYPE S9.5C
C6	100mm TYPE S12.5C
C7	VAR. DEPTH TYPE S12.5C
C8	13mm TYPE S4.75A
D1	65mm TYPE I19.0C
D2	80mm TYPE I19.0C
D3	100mm TYPE I19.0C
D4	VAR. DEPTH TYPE I19.0C
E1	100mm TYPE B25.0B
E2	120mm TYPE B25.0B
E3	VAR. DEPTH TYPE B25.0B
E4	100mm TYPE B25.0C
E5	140mm TYPE B25.0C
E6	VAR. DEPTH TYPE B25.0C
J1	200mm ABC
J2	250mm ABC
J3	270mm ABC
J4	VAR. DEPTH ABC
K	SUBGRADE STABILIZATION
P	PRIME COAT
R	SINGLE FACED BARRIER
T	EARTH MATERIAL
U	EXISTING PAVEMENT
W	WEDGING

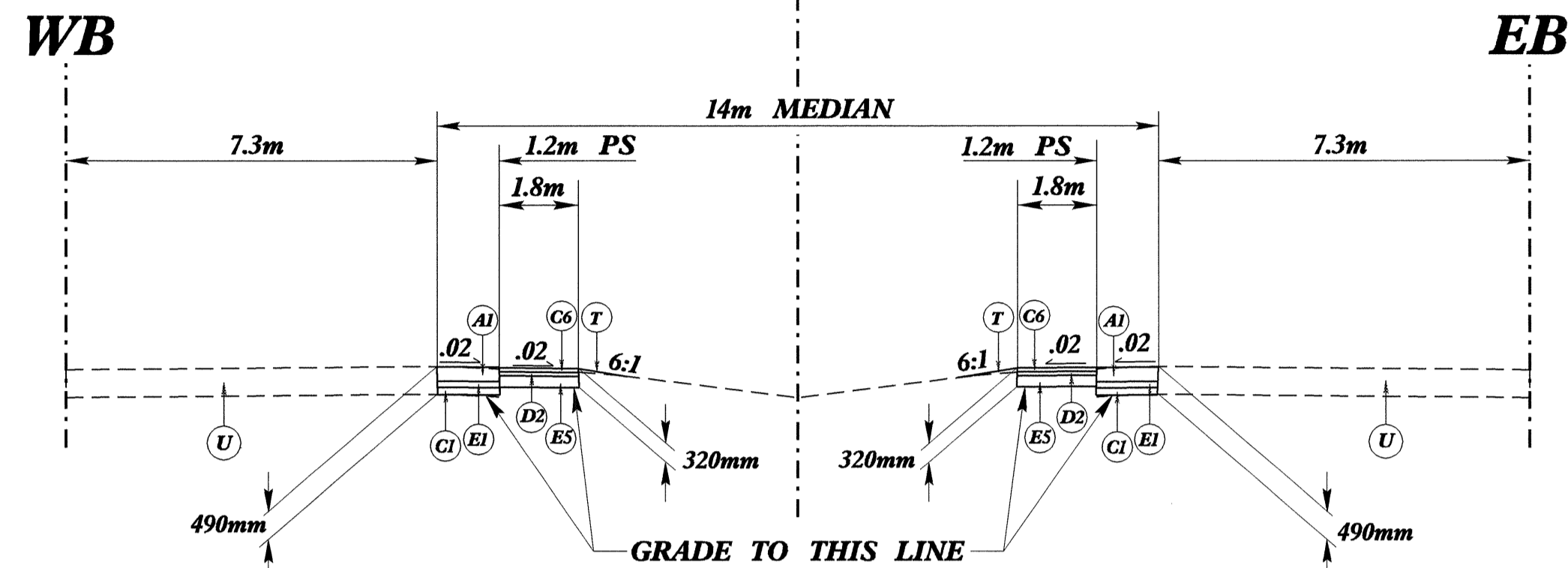
METRIX

PROJECT REFERENCE NO. **R-2552AA** SHEET NO. **2-C**

ROADWAY DESIGN ENGINEER

 W. W. MUMFORD
 3/16/25

PAVEMENT DESIGN ENGINEER

 CLARK S. MORRISON
 3/16/25

-I1Y1- Temporary 1.8m Widening
 (See Traffic Control Plans for Phasing)

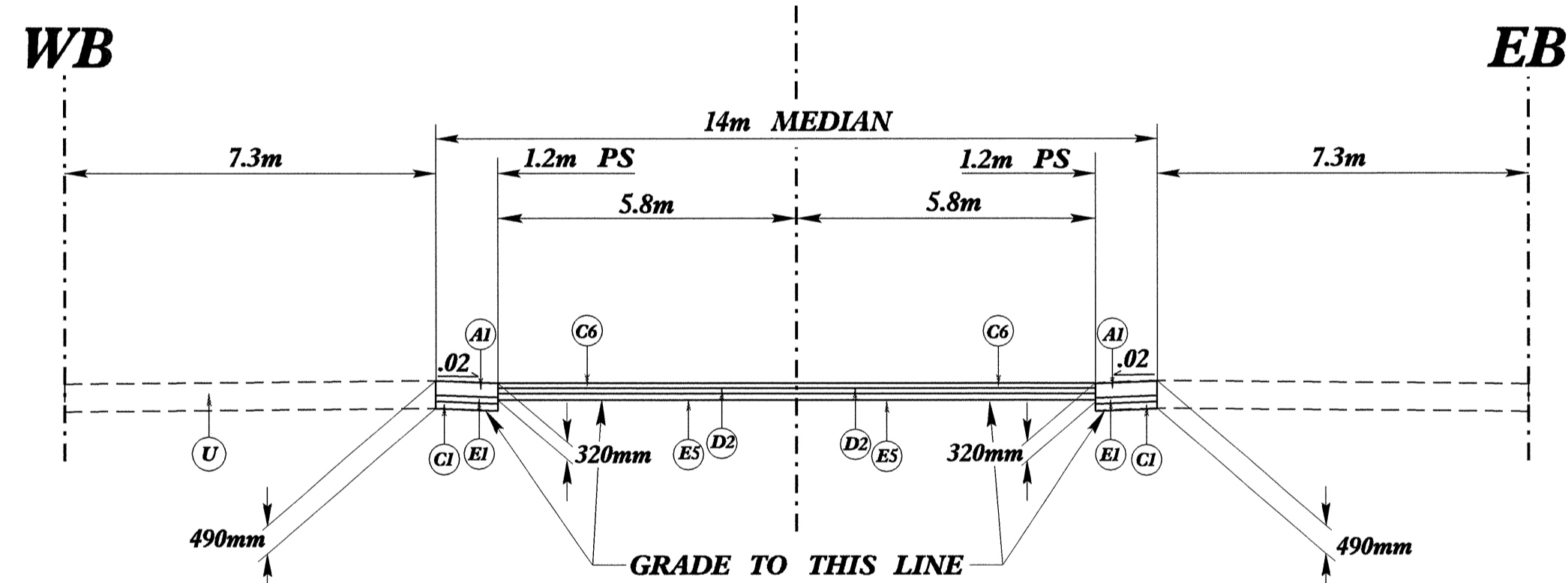


PARTIAL TYPICAL SECTION NO. 2B

USE TYPICAL SECTION NO. 2B IN CONJUNCTION WITH TYPICAL SECTION NO. 2 AS FOLLOWS:

- I1Y1- STA. 9+20.000 TO 13+74.000
- I1Y1- STA. 15+54.000 TO 23+34.000
- I1Y1- STA. 25+14.000 TO 30+26.000
- I1Y1- STA. 32+06.000 LT. TO 42+80.000 LT.
- I1Y1- STA. 32+06.000 RT. TO 37+60.000 RT.

-I1Y1- Temporary Crossovers
 (See Traffic Control Plans for Phasing)



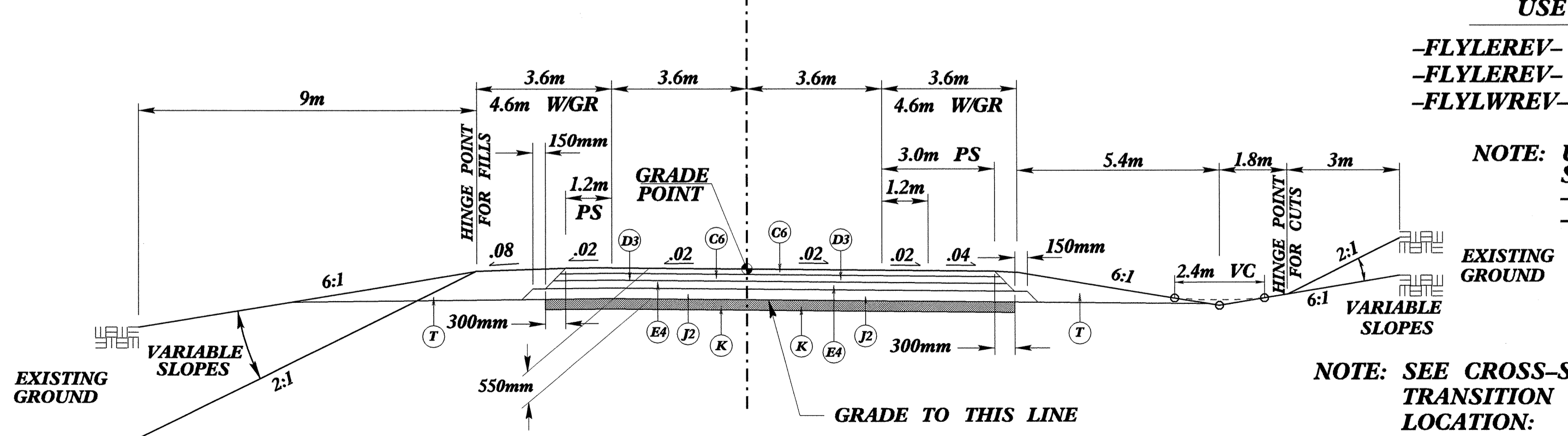
PARTIAL TYPICAL SECTION NO. 2C

USE TYPICAL SECTION NO. 2C IN CONJUNCTION WITH TYPICAL SECTION NO. 2 AS FOLLOWS:

- I1Y1- STA. 13+74.000 TO 15+54.000
- I1Y1- STA. 23+34.000 TO 25+14.000
- I1Y1- STA. 30+26.000 TO 32+06.000

NOTE: SEE DETAIL SHEET 2-O FOR CROSSOVER DESIGN.

**-FLYLEREV-
-FLYLWREV-**



TYPICAL SECTION NO. 3

USE TYPICAL SECTION NO. 3 AS FOLLOWS:

- FLYLEREV- STA. 10+00.000 TO STA. 22+03.308 (BEGIN BRIDGE)
- FLYLEREV- STA. 24+85.308 (END BRIDGE) TO STA. 35+14.173
- FLYLWREV- STA. 10+00.000 TO STA. 26+77.182

NOTE: USE -I1Y1- PAVEMENT DESIGN (SEE TYPICAL SECTION NO. 2) AT THE FOLLOWING LOCATIONS:
 -FLYLEREV- STA. 33+16.415 TO 35+14.173
 -FLYLWREV- STA. 25+10.230 TO 26+77.182

NOTE : REVERSE TYPICAL SECTION FOR -FLYLEREV-

NOTE: SEE CROSS-SECTIONS AND STD. NO. 225.09 FOR SLOPE TRANSITION UNDER BRIDGE AT THE FOLLOWING LOCATION:
 -FLYLEREV- STA. 32+18.843 LT. TO STA. 34+42.240 LT.