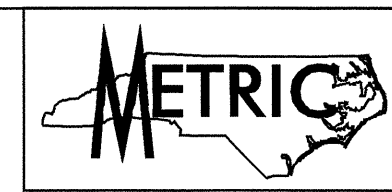
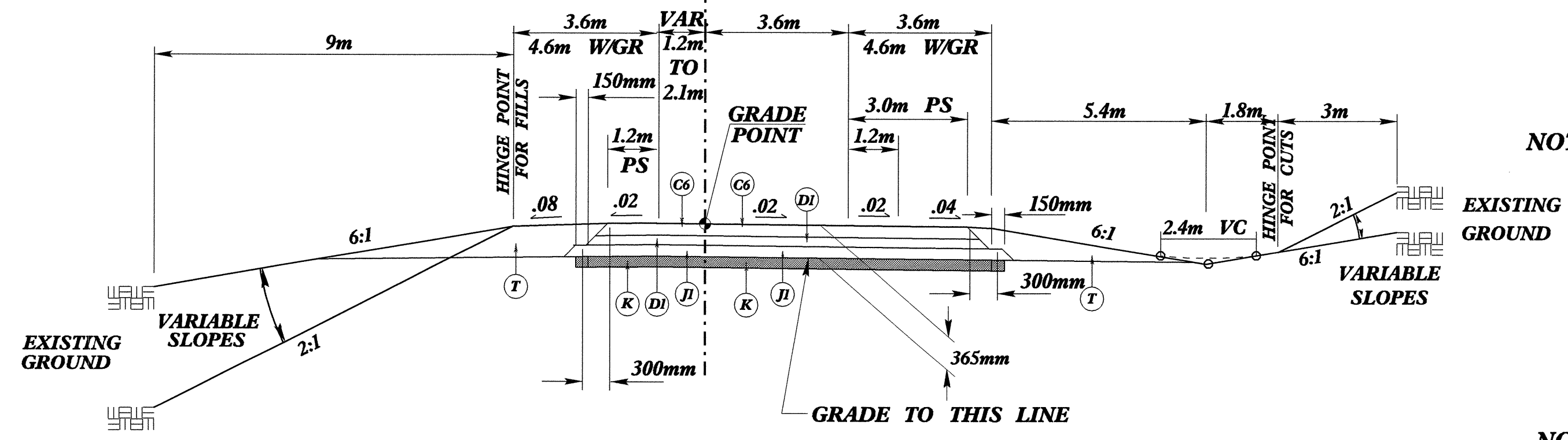


| PAVEMENT SCHEDULE | |
|-------------------|---|
| A1 | 360mm PCCP |
| A2 | CONCRETE SHOULDERS ADJACENT TO 360mm PAVEMENT |
| C1 | 30mm TYPE SF9.5A |
| C2 | 35mm TYPE SF9.5A |
| C3 | 50mm TYPE SF9.5A |
| C4 | 70mm TYPE SF9.5A |
| C5 | VAR. DEPTH TYPE S9.5C |
| C6 | 100mm TYPE S12.5C |
| C7 | VAR. DEPTH TYPE S12.5C |
| C8 | 13mm TYPE S4.75A |
| D1 | 65mm TYPE I19.0C |
| D2 | 80mm TYPE I19.0C |
| D3 | 100mm TYPE I19.0C |
| D4 | VAR. DEPTH TYPE I19.0C |
| E1 | 100mm TYPE B25.0B |
| E2 | 120mm TYPE B25.0B |
| E3 | VAR. DEPTH TYPE B25.0B |
| E4 | 100mm TYPE B25.0C |
| E5 | 140mm TYPE B25.0C |
| E6 | VAR. DEPTH TYPE B25.0C |
| J1 | 200mm ABC |
| J2 | 250mm ABC |
| J3 | 270mm ABC |
| J4 | VAR. DEPTH ABC |
| K | SUBGRADE STABILIZATION |
| P | PRIME COAT |
| R | SINGLE FACED BARRIER |
| T | EARTH MATERIAL |
| U | EXISTING PAVEMENT |
| W | WEDGING |



| | |
|---|--|
| PROJECT REFERENCE NO. R-2522AA | SHEET NO. 2-D |
| ROADWAY DESIGN ENGINEER <i>[Signature]</i> | PAVEMENT DESIGN ENGINEER <i>[Signature]</i> |
| SEAL 22894 W. W. MUMFORD | SEAL 22896 CLARK S. MORRISON |

CL -LPB- & -RPD-



TYPICAL SECTION NO. 4

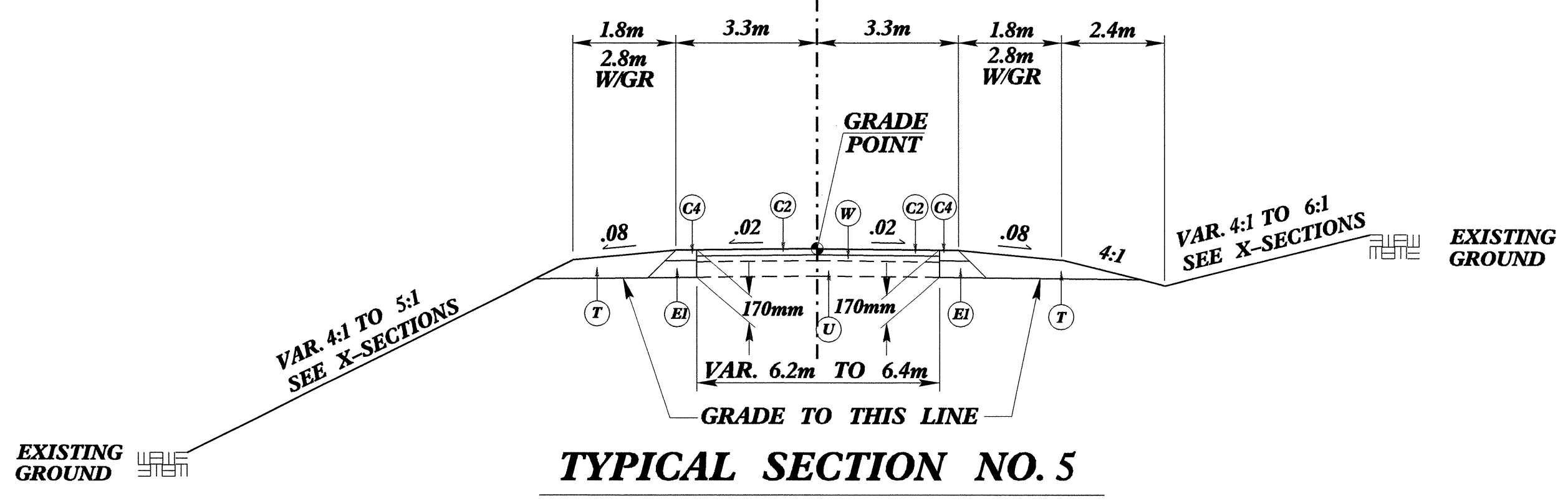
USE TYPICAL SECTION NO. 4 AS FOLLOWS:
 -LPB- STA. 10+00.000 TO STA. 16+58.307 (BEGIN BRIDGE)
 -LPB- STA. 18+91.307 (END BRIDGE) TO STA. 24+01.877
 -RPD- STA. 10+00.000 TO STA. 17+60.652

NOTES: USE -I1Y1- PAVEMENT DESIGN (SEE TYPICAL SECTION NO. 2) AT THE FOLLOWING LOCATIONS:
 -LPB- STA. 23+26.899 TO 24+01.877
 -RPD- STA. 16+37.899 TO 17+60.652

USE -FLYLWREV- PAVEMENT DESIGN (SEE TYPICAL SECTION NO. 3) AT THE FOLLOWING LOCATION:
 -LPB- STA. 10+00.000 TO 12+50.594
 USE -FLYLREVV- PAVEMENT DESIGN (SEE TYPICAL SECTION NO. 3) AT THE FOLLOWING LOCATION:
 -RPD- 10+00.000 TO 11+75.065

NOTE: REVERSE TYPICAL SECTION FOR -RPD-

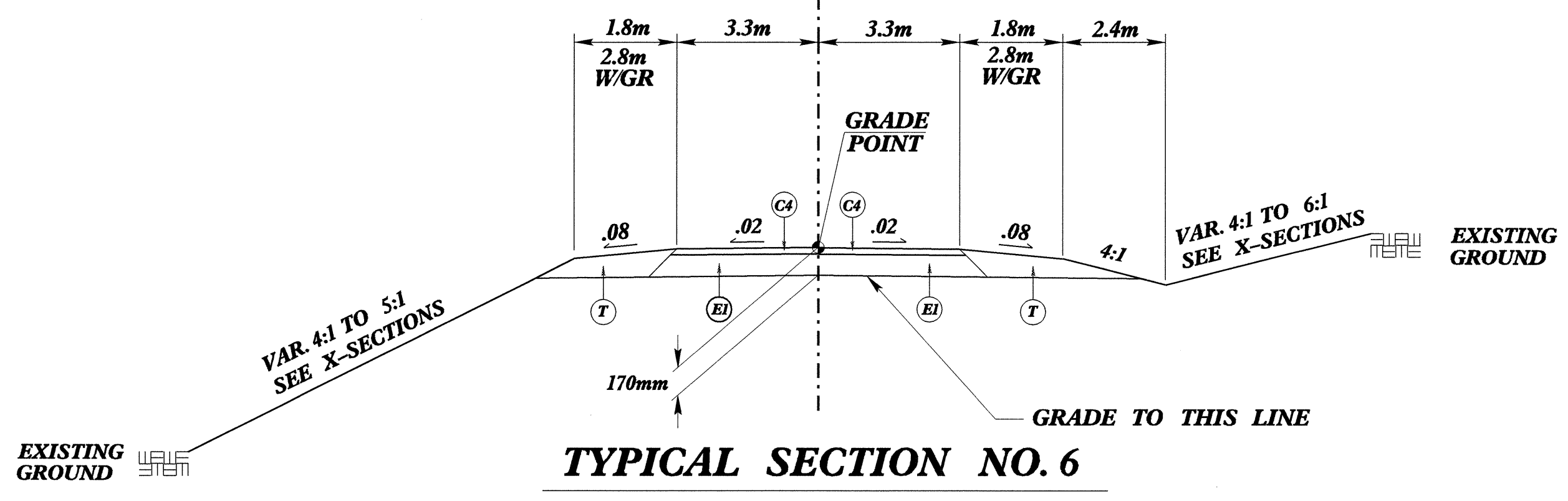
CL -Y2- & -Y2B-



TYPICAL SECTION NO. 5

USE TYPICAL SECTION NO. 5 AS FOLLOWS:
 -Y2- STA. 10+60.000 TO STA. 10+80.000
 TRANSITION FROM EXISTING @ -Y2- STA. 10+40.000 TO TYPICAL SECTION NO. 5 @ STA. 10+60.000
 -Y2- STA. 12+60.000 TO STA. 13+00.000
 TRANSITION FROM TYPICAL SECTION NO. 5 @ -Y2- STA. 13+00.000 TO EXISTING @ STA. 13+20.000
 -Y2B- STA. 10+00.000 TO STA. 11+00.000
 TRANSITION FROM EXISTING @ -Y2B- STA. 9+80.000 TO TYPICAL SECTION NO. 5 @ STA. 10+00.000

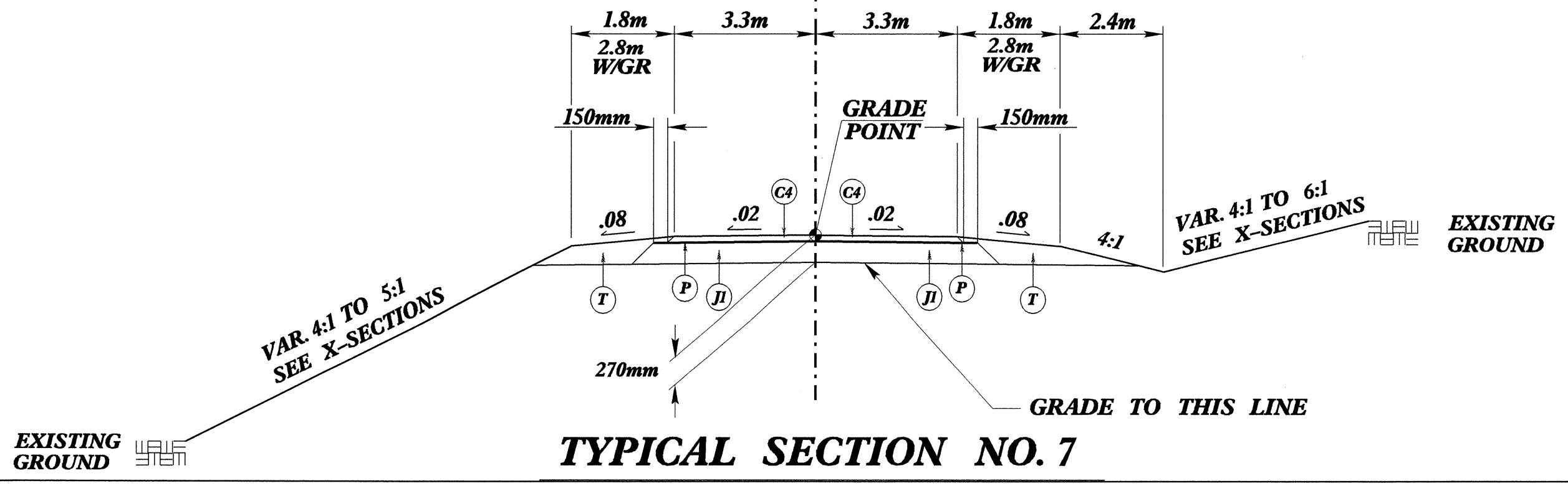
CL -Y2-



TYPICAL SECTION NO. 6

USE TYPICAL SECTION NO. 6 AS FOLLOWS:
 -Y2- STA. 10+80.000 TO STA. 10+96.275 (BEGIN BRIDGE)
 -Y2- STA. 12+05.075 (END BRIDGE) TO STA. 12+60.000

CL -Y2B-



TYPICAL SECTION NO. 7

USE TYPICAL SECTION NO. 7 AS FOLLOWS:
 -Y2B- STA. 11+00.000 TO STA. 19+28.404

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 BCFowler - AT - RD2522AA\intermasa.txd
 3/16/05