

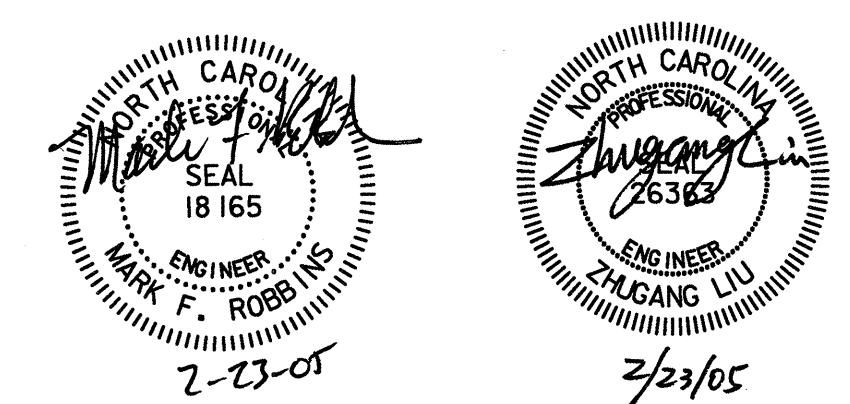
NOTES:

- VALUES ARE SHOWN IN METERS, EXCEPT "REQUIRED CAMBER" WHICH IS GIVEN IN MILLIMETERS.
- SLOPE FOR THE ZERO CAMBER BASE LINE VARIES.
- FOR GIRDER DESIGNATIONS, SEE "FRAMING PLAN AND GIRDER DETAILS" SHEETS.
- DEFLECTIONS IN THE DOWNWARD DIRECTION ARE POSITIVE. A REQUIRED CAMBER IN THE UPWARD DIRECTION IS POSITIVE.
- EFFECTS OF CAMBER DISSIPATION ARE NEGLIGIBLE.

		1.00	1.01	1.02	1.03	1.04	1.05	1.06	1.07	1.08	1.09	1.10	1.11	1.12	1.13	F.S.	1.14	1.15	1.16	1.17	1.18	1.19	1.20
GIRDER 1	DEFLECTION DUE TO WT. OF STEEL	0.000	0.001	0.001	0.001	0.002	0.002	0.002	0.001	0.001	0.001	0.000	0.000	-0.001	-0.001	-0.001	-0.001	-0.002	-0.002	-0.002	-0.001	-0.001	0.000
	DEFLECTION DUE TO WT. OF SLAB ▲	0.000	0.003	0.007	0.009	0.012	0.013	0.014	0.014	0.013	0.012	0.010	0.007	0.005	0.002	0.002	-0.001	-0.003	-0.004	-0.005	-0.004	-0.003	0.000
	DEFLECTION DUE TO WT. OF RAIL	0.000	0.000	0.001	0.001	0.001	0.001	0.001	0.001	0.001	0.001	0.001	0.001	0.001	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000
	TOTAL DEAD LOAD DEFLECTION	0.000	0.004	0.009	0.011	0.015	0.016	0.017	0.016	0.015	0.014	0.011	0.008	0.005	0.001	0.001	-0.002	-0.005	-0.006	-0.007	-0.005	-0.004	0.000
	VERTICAL CURVE ORDINATE	0.000	0.002	0.004	0.006	0.007	0.008	0.009	0.010	0.010	0.011	0.011	0.012	0.011	0.011	0.010	0.010	0.009	0.007	0.006	0.004	0.002	0.000
	REQUIRED CAMBER	0	6	13	17	22	24	26	26	25	25	22	20	16	12	11	8	4	1	-1	-1	-2	0
GIRDER 2	DEFLECTION DUE TO WT. OF STEEL	0.000	0.001	0.002	0.002	0.003	0.003	0.004	0.004	0.004	0.003	0.003	0.002	0.002	0.001	0.001	0.000	0.000	-0.001	-0.001	-0.001	-0.001	0.000
	DEFLECTION DUE TO WT. OF SLAB ▲	0.000	0.006	0.011	0.015	0.019	0.022	0.024	0.025	0.025	0.024	0.022	0.019	0.015	0.012	0.010	0.008	0.004	0.001	-0.001	-0.002	-0.002	0.000
	DEFLECTION DUE TO WT. OF RAIL	0.000	0.000	0.001	0.001	0.002	0.002	0.002	0.002	0.002	0.002	0.002	0.002	0.001	0.001	0.001	0.001	0.000	0.000	0.000	0.000	0.000	0.000
	TOTAL DEAD LOAD DEFLECTION	0.000	0.007	0.014	0.018	0.024	0.027	0.030	0.031	0.031	0.029	0.027	0.023	0.018	0.014	0.012	0.009	0.004	0.000	-0.002	-0.003	-0.003	0.000
	VERTICAL CURVE ORDINATE	0.000	0.003	0.005	0.007	0.008	0.009	0.011	0.012	0.012	0.013	0.013	0.012	0.012	0.012	0.012	0.011	0.010	0.009	0.006	0.005	0.003	0.000
	REQUIRED CAMBER	0	10	19	25	32	36	41	43	43	42	40	35	30	26	24	20	14	9	4	2	0	0
GIRDER 3	DEFLECTION DUE TO WT. OF STEEL	0.000	0.001	0.003	0.004	0.005	0.005	0.006	0.006	0.006	0.006	0.005	0.005	0.004	0.003	0.002	0.002	0.001	0.000	0.000	0.000	0.000	0.000
	DEFLECTION DUE TO WT. OF SLAB ▲	0.000	0.008	0.015	0.022	0.027	0.032	0.035	0.036	0.037	0.036	0.034	0.030	0.026	0.021	0.017	0.016	0.011	0.007	0.003	0.001	-0.001	0.000
	DEFLECTION DUE TO WT. OF RAIL	0.000	0.001	0.001	0.002	0.002	0.002	0.003	0.003	0.003	0.003	0.003	0.002	0.002	0.002	0.001	0.001	0.001	0.001	0.000	0.000	0.000	0.000
	TOTAL DEAD LOAD DEFLECTION	0.000	0.010	0.019	0.028	0.034	0.039	0.044	0.045	0.046	0.045	0.042	0.037	0.032	0.026	0.020	0.019	0.013	0.008	0.003	0.001	-0.001	0.000
	VERTICAL CURVE ORDINATE	0.000	0.002	0.004	0.007	0.009	0.010	0.011	0.012	0.013	0.013	0.014	0.014	0.013	0.013	0.012	0.012	0.011	0.009	0.007	0.006	0.003	0.000
	REQUIRED CAMBER	0	12	23	35	43	49	55	57	59	58	56	51	45	39	32	31	24	17	10	7	2	0
GIRDER 4	DEFLECTION DUE TO WT. OF STEEL	0.000	0.002	0.003	0.005	0.006	0.007	0.008	0.008	0.008	0.008	0.008	0.007	0.006	0.005	0.004	0.004	0.003	0.002	0.001	0.000	0.000	0.000
	DEFLECTION DUE TO WT. OF SLAB ▲	0.000	0.010	0.019	0.027	0.035	0.040	0.045	0.047	0.048	0.047	0.045	0.041	0.036	0.031	0.024	0.024	0.018	0.013	0.007	0.003	0.001	0.000
	DEFLECTION DUE TO WT. OF RAIL	0.000	0.001	0.002	0.002	0.003	0.003	0.004	0.004	0.004	0.004	0.004	0.003	0.003	0.003	0.002	0.002	0.002	0.001	0.001	0.000	0.000	0.000
	TOTAL DEAD LOAD DEFLECTION	0.000	0.013	0.024	0.034	0.044	0.050	0.057	0.059	0.060	0.059	0.057	0.051	0.045	0.039	0.030	0.030	0.023	0.016	0.009	0.003	0.001	0.000
	VERTICAL CURVE ORDINATE	0.000	0.002	0.005	0.007	0.009	0.011	0.013	0.013	0.014	0.014	0.015	0.015	0.015	0.013	0.013	0.013	0.012	0.010	0.008	0.006	0.004	0.000
	REQUIRED CAMBER	0	15	29	41	53	61	70	72	74	73	72	66	60	52	43	43	35	26	17	9	5	0
GIRDER 5	DEFLECTION DUE TO WT. OF STEEL	0.000	0.002	0.004	0.006	0.008	0.009	0.010	0.011	0.011	0.011	0.010	0.009	0.008	0.007	0.005	0.005	0.004	0.003	0.002	0.001	0.000	0.000
	DEFLECTION DUE TO WT. OF SLAB ▲	0.000	0.012	0.023	0.033	0.042	0.049	0.054	0.057	0.059	0.058	0.056	0.051	0.046	0.039	0.032	0.030	0.024	0.017	0.011	0.006	0.002	0.000
	DEFLECTION DUE TO WT. OF RAIL	0.000	0.001	0.002	0.003	0.003	0.004	0.004	0.005	0.005	0.005	0.005	0.004	0.004	0.003	0.003	0.003	0.002	0.002	0.001	0.001	0.000	0.000
	TOTAL DEAD LOAD DEFLECTION	0.000	0.015	0.029	0.042	0.053	0.062	0.068	0.073	0.075	0.074	0.071	0.064	0.058	0.049	0.040	0.038	0.030	0.022	0.014	0.008	0.002	0.000
	VERTICAL CURVE ORDINATE	0.000	0.002	0.006	0.008	0.010	0.013	0.014	0.015	0.015	0.017	0.017	0.016	0.016	0.015	0.014	0.013	0.013	0.011	0.008	0.005	0.003	0.000
	REQUIRED CAMBER	0	17	35	50	63	75	82	88	90	91	88	80	74	64	54	51	43	33	22	13	5	0

▲ DENOTES INCLUSION OF SLAB, BUILDUPS AND STAY-IN-PLACE FORMS.

SCHEMATIC CAMBER ORDINATES - SPAN "A"



PROJECT No. R-2552C
 JOHNSTON COUNTY
 STATION: POS 16+52.599 -Y4-
 POC 9+28.021 -FLYOVER-

SHEET 1 OF 2

STATE OF NORTH CAROLINA	
DEPARTMENT OF TRANSPORTATION	
RALEIGH	
CAMBER AND DEFLECTIONS	
REVISIONS	SHEET NO. 412
NO. BY DATE	TOTAL SHEETS 43
1 1 2	

W RALPH WHITEHEAD ASSOCIATES, INC.
 CONSULTING ENGINEERS
 P.O. BOX 35624 CHARLOTTE, N.C. 28235
 DRAWN BY LGH, DDL DATE: 8-04 DWG. NO. D-1748.JT
 CHECKED BY ZAL DATE: 12-04

02/23/2005 10:50:45 AM n:\p\o\3109-300\ustation\struet\find\3109-300_camber_a.dgn