

**NOTES**

APPROACH SLAB SHALL NOT BE CONSTRUCTED PRIOR TO COMPLETION OF THE BRIDGE DECK.

FOR REINFORCED BRIDGE APPROACH FILL INCLUDING FABRIC, IMPERMEABLE GEOMEMBRANE, 102mm Ø DRAINAGE PIPE, \*78M STONE, AND SELECT MATERIAL, SEE ROADWAY PLANS.

TEMPORARY DRAINAGE AND TEMPORARY BERM AND SLOPE DRAINS WILL BE PAID FOR UNDER THE LUMP SUM PRICE FOR BRIDGE APPROACH SLAB.

AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE.

THE 150mm COMP. A.B.C. SHALL EXTEND 3m BEYOND THE END OF THE APPROACH SLAB AND 300mm OUTSIDE OF EACH EDGE OF THE SLAB.

THE CONTRACTOR MAY USE 100mm TYPE B-25.0B ASPHALT CONCRETE COURSE IN LIEU OF 150mm COMP. A.B.C. IF THIS OPTION IS USED, THE BASE COURSE SHALL EXTEND 300mm BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB.

THE CONTRACTOR MAY USE 125mm CLASS "A" CONCRETE BASE IN LIEU OF 150mm COMP. A.B.C. IF THIS OPTION IS USED, THE CONCRETE BASE SHALL EXTEND 300mm BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB. THE CONCRETE SHALL BE FINISHED TO A SMOOTH SURFACE AND A LAYER OF 13.6 kg ROOFING FELT SHALL BE PLACED BETWEEN THE CONCRETE BASE AND THE APPROACH SLAB TO PREVENT BOND. THE APPROACH SLAB SHALL NOT BE CAST UNTIL THE CONCRETE BASE HAS REACHED AN AGE OF THREE CURING DAYS.

THE JOINT SHALL BE SAWED PRIOR TO THE CASTING OF THE BARRIER RAIL.

THE COST OF THE BARRIER RAIL ON THE APPROACH SLAB SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE BID FOR BRIDGE APPROACH SLABS.

WITH EVAZOTE JOINT SEAL

FOR EVAZOTE JOINT SEALS, SEE SPECIAL PROVISIONS.

THE NOMINAL UNCOMPRESSED SEAL WIDTH OF THE EVAZOTE JOINT SEAL SHALL BE 64mm.

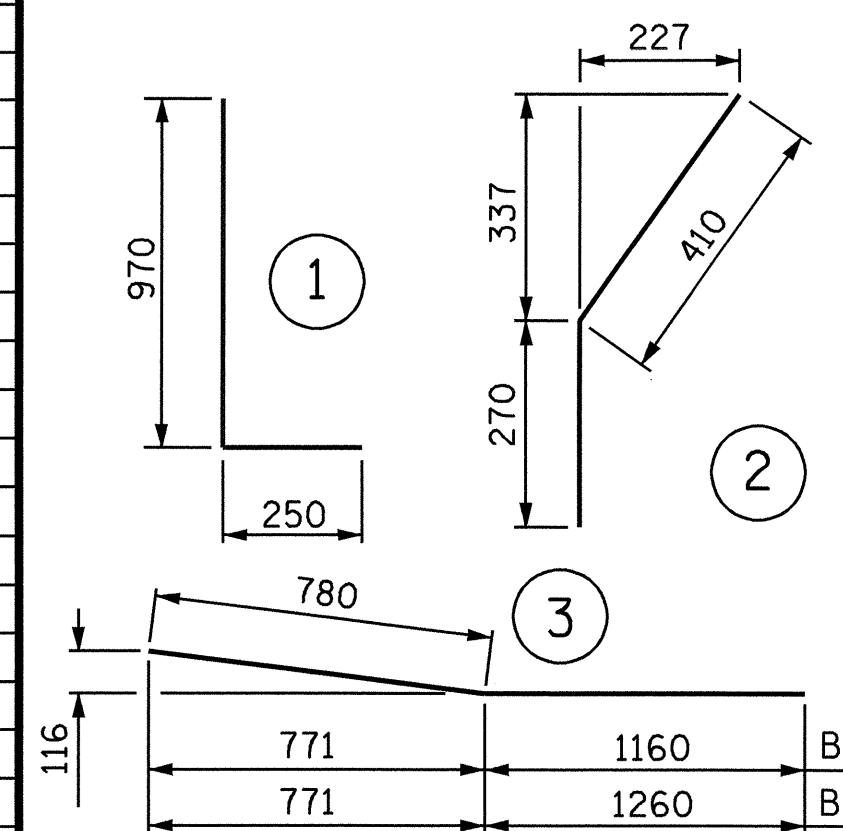
FOR ELASTOMERIC CONCRETE, SEE SPECIAL PROVISIONS.

**BILL OF MATERIAL**  
**FOR ONE APPROACH SLAB (2 REQ'D)**

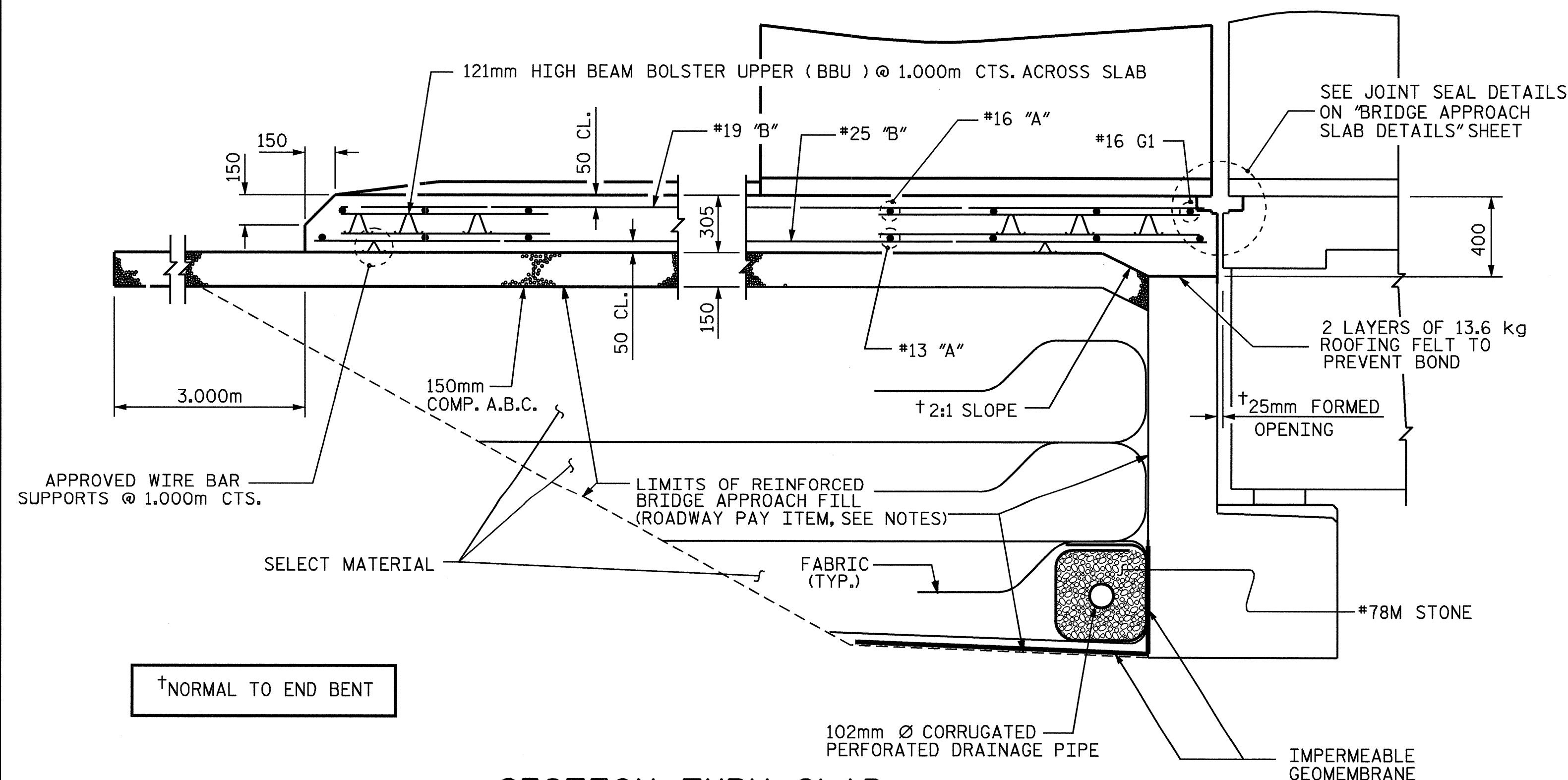
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
*A1	108	#16	STR	5440	912	*B102	6	#19	STR	7600	102
A2	108	#13	STR	5320	571	*B103	6	#19	STR	7840	105
*A100	1	#16	STR	4780	7	*B104	6	#19	STR	8080	108
*A101	1	#16	STR	4220	7	*B105	6	#19	STR	8320	112
*A102	1	#16	STR	3660	6	*B106	6	#19	STR	8560	115
*A103	1	#16	STR	3100	5	*B107	6	#19	STR	8800	118
*A104	1	#16	STR	2540	4	*B108	6	#19	STR	9040	121
*A105	1	#16	STR	1980	3	*B109	6	#19	STR	9280	124
*A106	1	#16	STR	1420	2	*B110	6	#19	STR	9520	128
*A107	1	#16	STR	860	1	*B111	6	#19	STR	9760	131
*A108	1	#16	STR	4960	8	*B112	1	#19	STR	3620	8
*A109	1	#16	STR	4400	7						
*A110	1	#16	STR	3840	6	B200	1	#25	STR	3500	14
*A111	1	#16	STR	3280	5	B201	6	#25	STR	7500	179
*A112	1	#16	STR	2720	4	B202	6	#25	STR	7740	185
*A113	1	#16	STR	2160	3	B203	6	#25	STR	7980	190
*A114	1	#16	STR	1600	2	B204	6	#25	STR	8220	196
*A115	1	#16	STR	1040	2	B205	6	#25	STR	8460	202
*A116	1	#16	STR	480	1	B206	6	#25	STR	8700	207
						B207	6	#25	STR	8940	213
A200	1	#13	STR	4670	5	B208	6	#25	STR	9180	219
A201	1	#13	STR	4120	4	B209	6	#25	STR	9420	225
A202	1	#13	STR	3560	4	B210	6	#25	STR	9660	230
A203	1	#13	STR	2980	3	B211	6	#25	STR	9920	236
A204	1	#13	STR	2420	2	B212	1	#25	STR	3620	14
A205	1	#13	STR	1860	2						
A206	1	#13	STR	1300	1	*G1	1	#16	STR	10380	16
A207	1	#13	STR	740	1						
A208	1	#13	STR	4960	5	*S1	58	#16	STR	980	88
A209	1	#13	STR	4400	4	*S2	42	#16	STR	1220	80
A210	1	#13	STR	3840	4	*S3	20	#16	STR	680	21
A211	1	#13	STR	3280	3						
A212	1	#13	STR	2720	3	*B1	7	#16	STR	3600	39
A213	1	#13	STR	2160	2	*B2	7	#16	STR	3520	38
A214	1	#13	STR	1600	2	*B3	1	#16	STR	1940	3
A215	1	#13	STR	1040	1	*B4	1	#16	STR	2040	3
A216	1	#13	STR	480	1						
*B100	1	#19	STR	3500	8						
*B101	6	#19	STR	7340	98						

REINFORCING STEEL	kg.	2928
* EPOXY COATED REINFORCING STEEL	kg.	2551
CLASS AA CONCRETE BREAKDOWN		
POUR 1 SLAB	C. M.	28.2
POUR 2 RAIL	C. M.	1.6
CLASS AA CONCRETE	C. M.	29.8

**BAR TYPES**



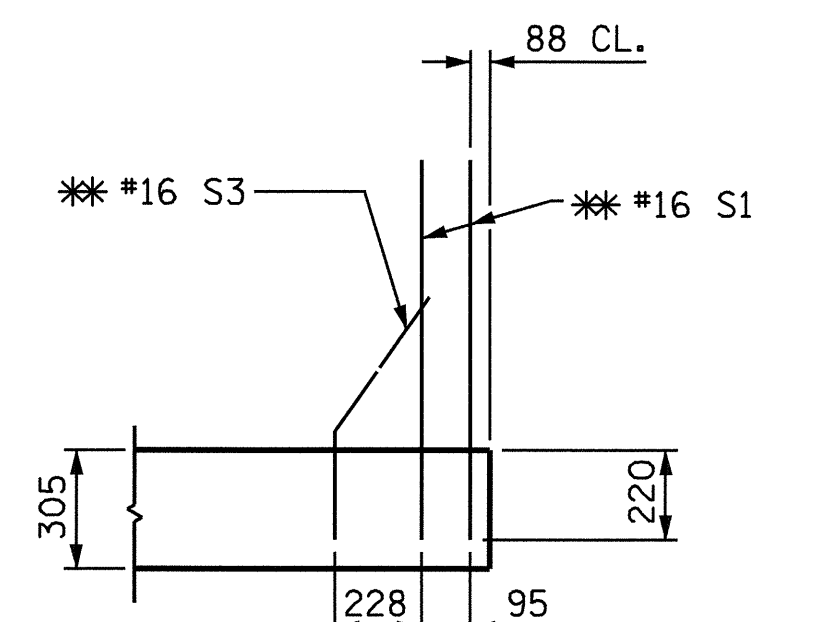
ALL BAR DIMENSIONS ARE OUT TO OUT



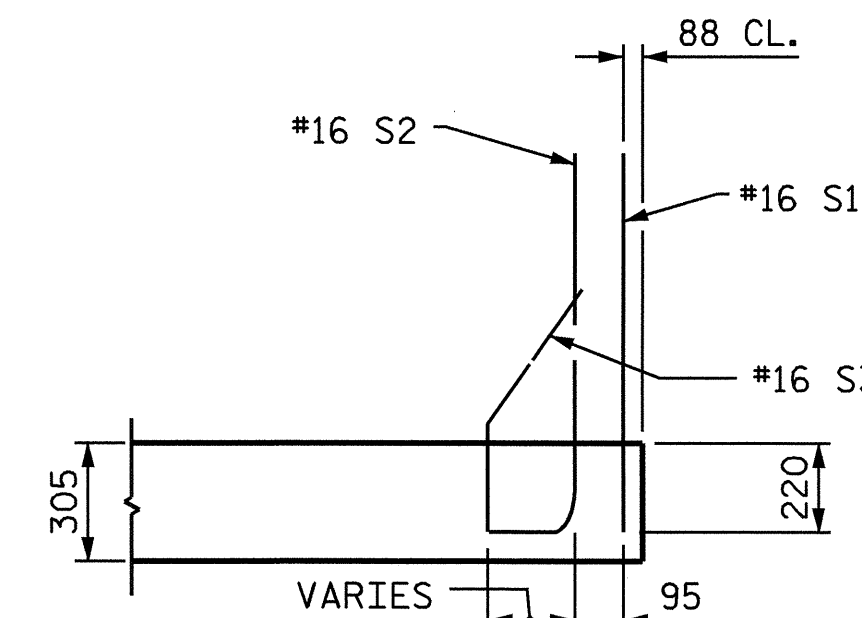
**SECTION THRU SLAB**

DRAWN BY: R.D. MARTIN DATE: 5/13/04  
CHECKED BY: H.A. LUCAS DATE: 2/18/05

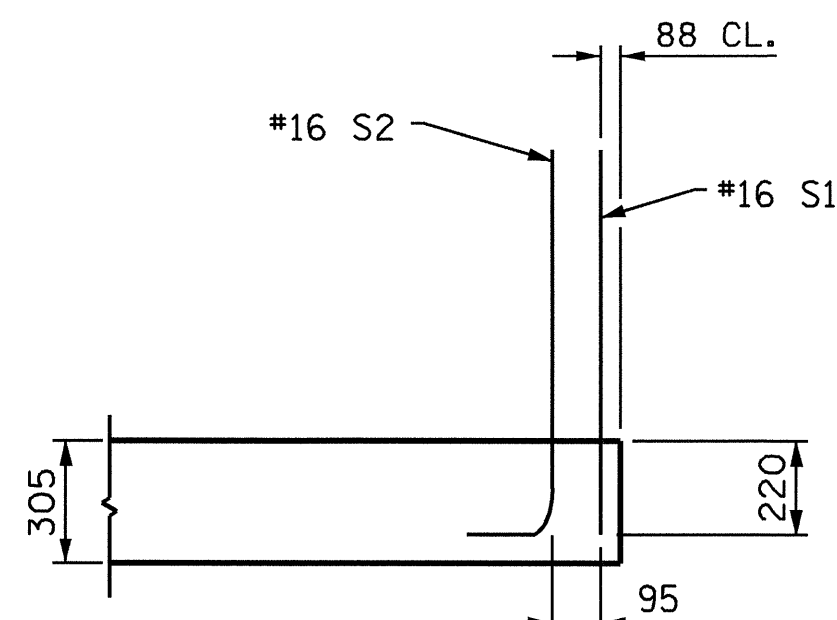
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**SECTION K-K**  
\* ADHESIVELY ANCHORED



**SECTION L-L**



**SECTION M-M**



PROJECT NO. R-2552B  
JOHNSTON COUNTY  
STATION: 14+72.000-Y11-REV.

SHEET 2 OF 4

STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION  
RALEIGH

BRIDGE APPROACH SLAB  
FOR FLEXIBLE PAVEMENT  
WITH BARRIER RAIL

REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	178	
1			3			TOTAL SHEETS	
2			4			431	

STR #4

REV. STD. NO. BAS2SM