

NOTES:

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.

HOOKS ON "V" BARS MAY BE TURNED AS NECESSARY FOR PLACING REINFORCING STEEL.

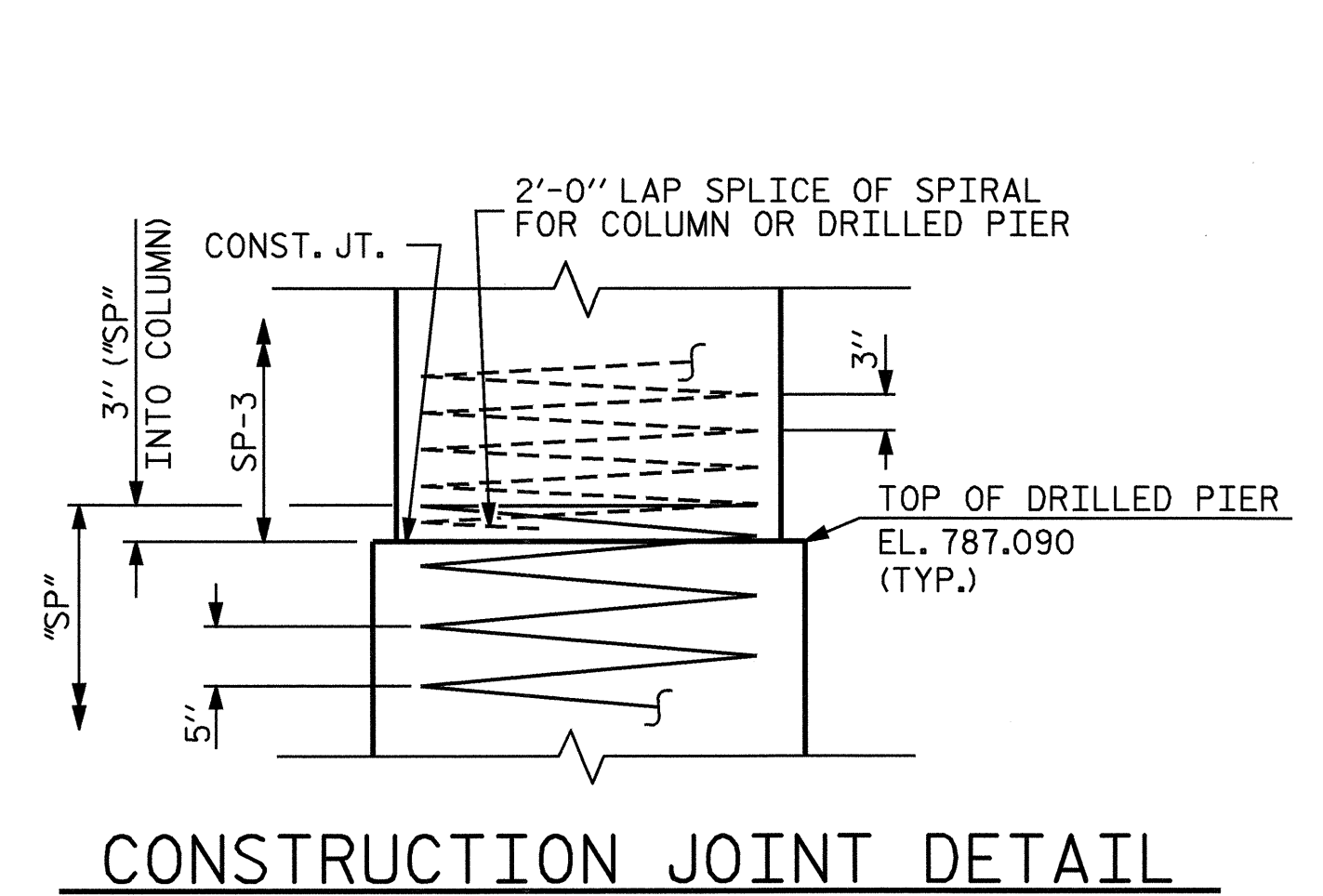
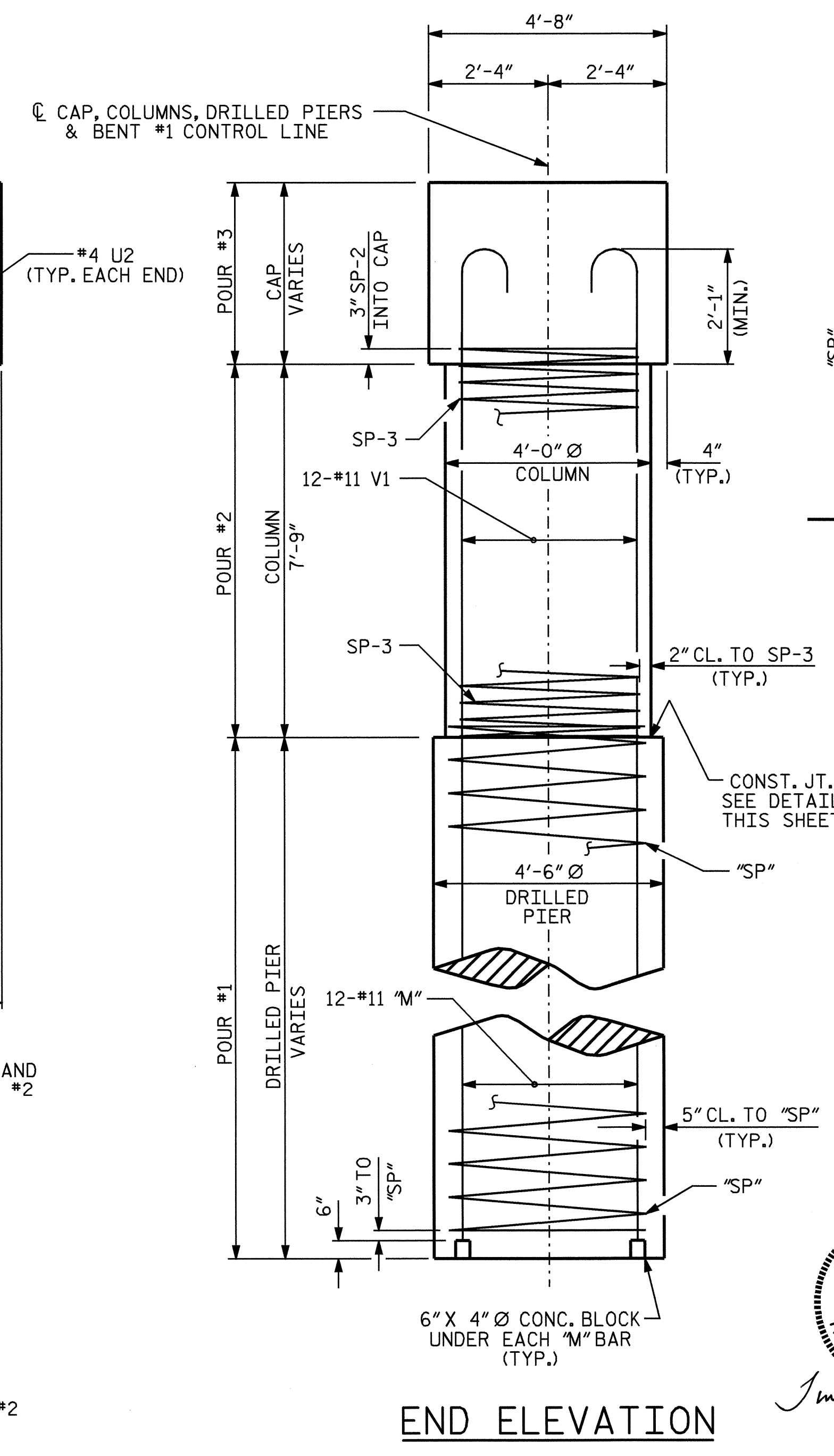
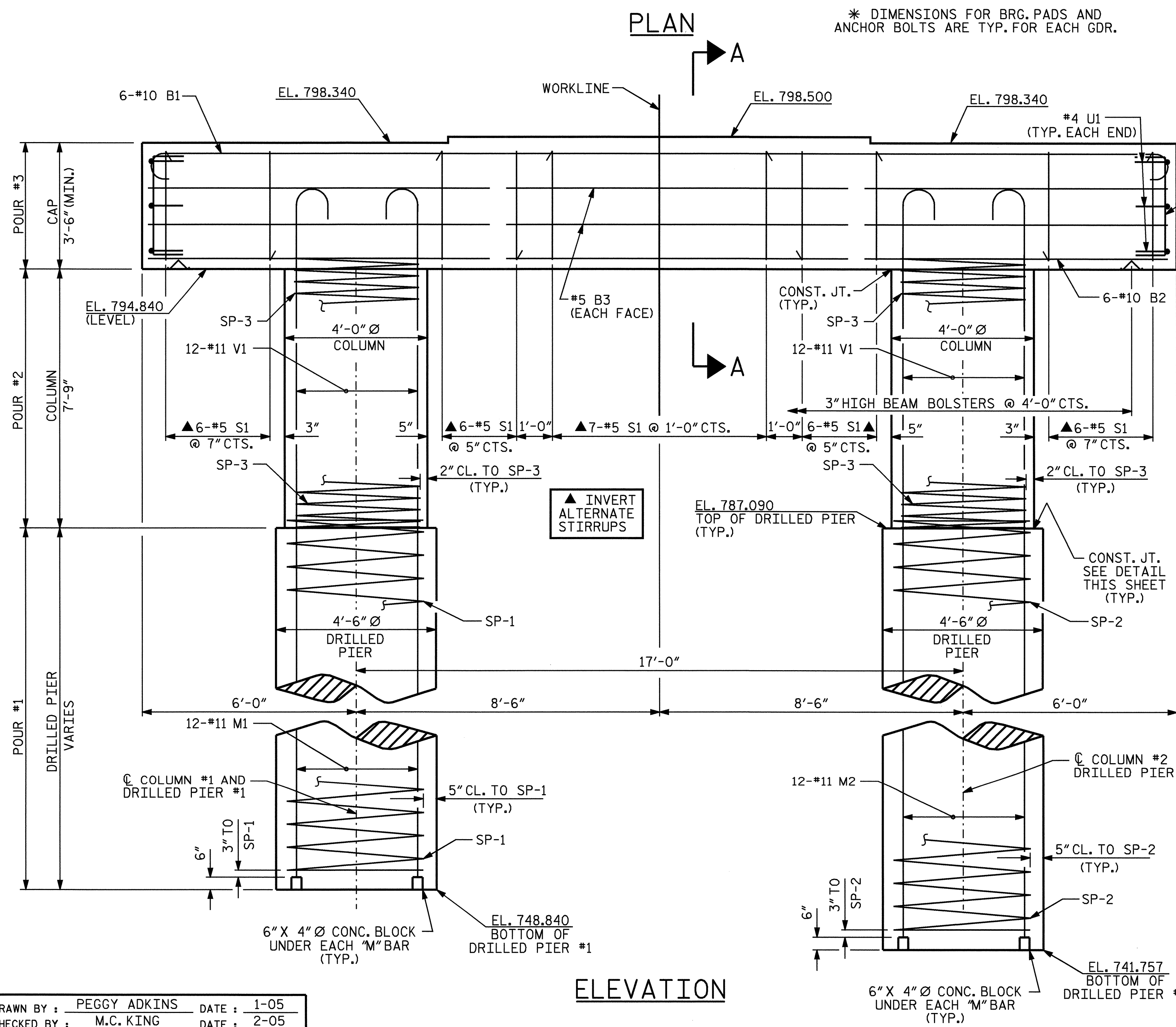
ALL STEEL IN THE DRILLED PIERS IS INCLUDED IN THE PAY ITEMS FOR "REINFORCING STEEL" AND "SPIRAL COLUMN REINFORCING STEEL."

SPLICING OF THE LONGITUDINAL BARS IN THE DRILLED PIER WILL NOT BE PERMITTED.

THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE LONGITUDINAL REINFORCEMENT FOR THE DRILLED PIERS IS DETAILED WITH 3'-0" OF EXTRA LENGTH.

MECHANICAL COUPLERS SHALL BE USED TO JOIN THE LONGITUDINAL DRILLED PIER REINFORCING STEEL TO THE COLUMN REINFORCING STEEL. THE HEIGHT OF THE COUPLERS SHALL BE STAGGERED ON ALTERNATING BARS BY 1 FOOT AND THE DRILLED PIER AND COLUMN STEEL SHALL BE CUT ACCORDINGLY. SEE SPECIAL PROVISIONS FOR MECHANICAL BUTT SPLICING FOR REINFORCING STEEL.

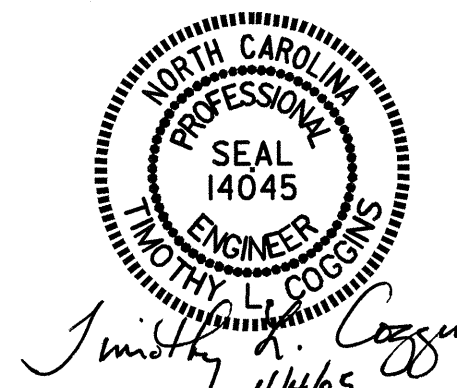
* DIMENSIONS FOR BRG. PADS AND ANCHOR BOLTS ARE TYP. FOR EACH GDR.



PROJECT NO. B-3828
CLEVELAND COUNTY
 STATION: 14+65.00 -L-

SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUBSTRUCTURE
 BENT #1



DRAWN BY: PEGGY ADKINS DATE: 1-05
 CHECKED BY: M.C. KING DATE: 2-05

REVISIONS						SHEET NO.	
NO.	BYs	DATEs	NO.	BYs	DATEs	S-18	
1			3			TOTAL SHEETS	
2			4			30	