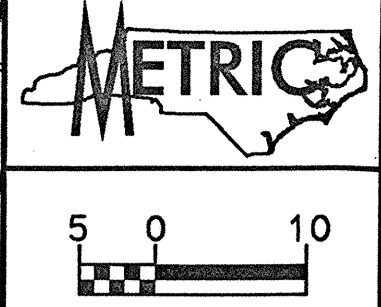


REVISIONS



PROJECT REFERENCE NO. R-977A	SHEET NO. 4
HIGHWAY DESIGN ENGINEER	HYDRAULICS ENGINEER
CONST. REV.	
R/W REV.	

-Y-
 PI Sta 10+37.809
 $\Delta = 12' 19" 51.9" (RT)$
 L = 75.326
 T = 37.809
 R = 350.000
 SE = EXIST.

-Y-
 PIs Sta 11+0.399
 $\Theta s = 8' 35" 39.7"$
 Ls = 105.000
 LT = 70.083
 ST = 35.075

PRELIMINARY PLANS
DO NOT USE FOR CONSTRUCTION

INCOMPLETE PLANS
DO NOT USE FOR R/W ACQUISITION

-L- POT Sta. 9+15.000

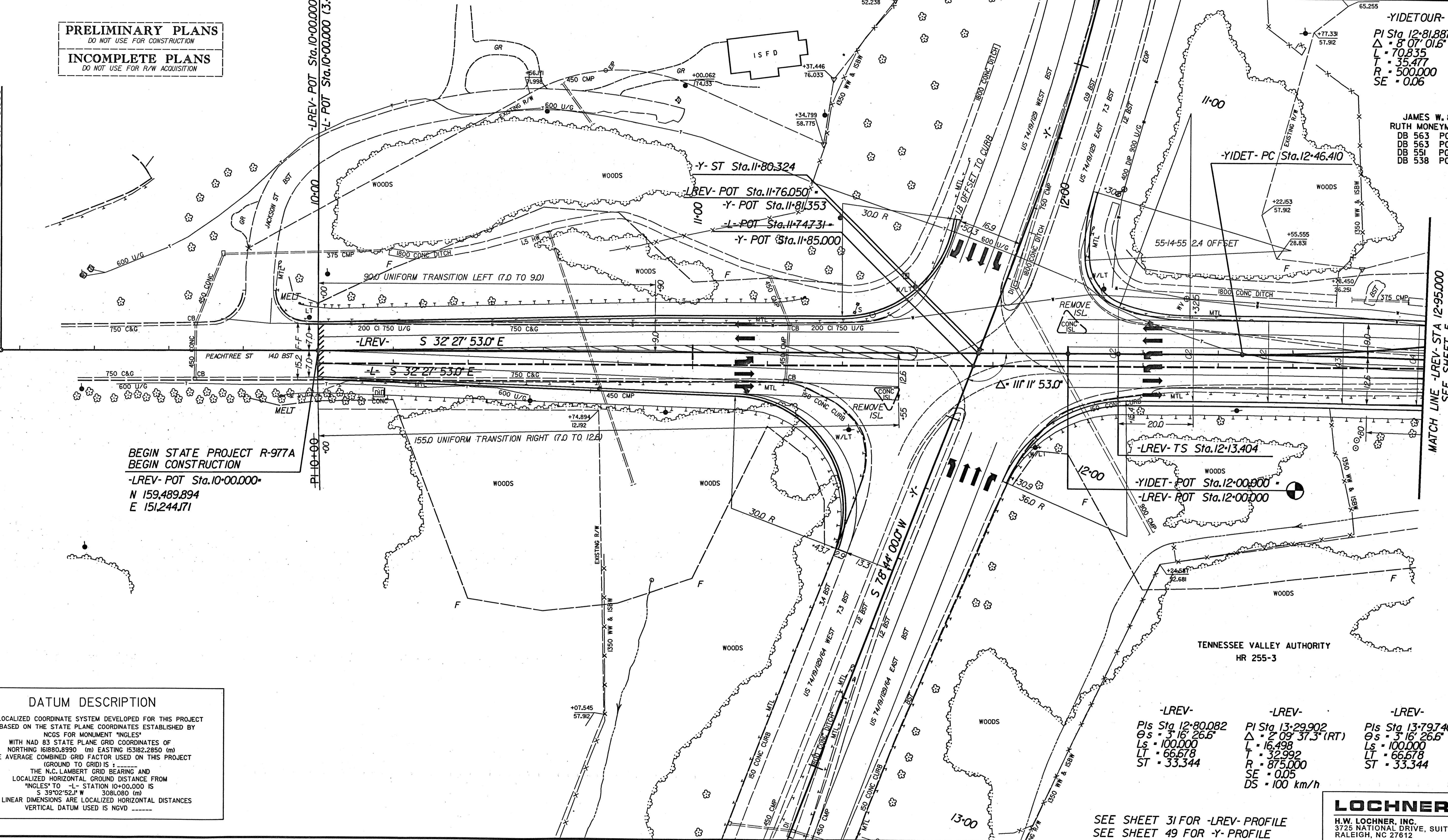
-LREV- POT Sta. 10+00.000
-L- POT Sta. 10+00.000 (3.4 RT)

-Y- CS Sta. 10+75.324

-YIDETOUR-
 PI Sta 12+81.887
 $\Delta = 8' 07" 01.6" (LT)$
 L = 70.835
 T = 35.477
 R = 500.000
 SE = 0.06

JAMES W. & RUTH MONEYMAKER
 DB 563 PG 20
 DB 563 PG 19
 DB 551 PG 77
 DB 538 PG 42

-YIDET- PC Sta. 12+46.410



BEGIN STATE PROJECT R-977A
 BEGIN CONSTRUCTION
 -LREV- POT Sta. 10+00.000
 N 159,489.894
 E 151,244.171

DATUM DESCRIPTION

THE LOCALIZED COORDINATE SYSTEM DEVELOPED FOR THIS PROJECT IS BASED ON THE STATE PLANE COORDINATES ESTABLISHED BY NCGS FOR MONUMENT "INGLES"

WITH NAD 83 STATE PLANE GRID COORDINATES OF NORTHING 161880.8990 (m) EASTING 15382.2850 (m)

THE AVERAGE COMBINED GRID FACTOR USED ON THIS PROJECT (GROUND TO GRID) IS: -----

THE N.C. LAMBERT GRID BEARING AND LOCALIZED HORIZONTAL GROUND DISTANCE FROM "INGLES" TO -L- STATION 10+00.000 IS

INGLES TO -L- STATION 10+00.000 IS S 39°02'52.1" W 3081.000 (m)

ALL LINEAR DIMENSIONS ARE LOCALIZED HORIZONTAL DISTANCES

VERTICAL DATUM USED IS NGVD -----

-LREV-
 PIs Sta 12+80.082
 $\Theta s = 3' 16" 26.6"$
 Ls = 100.000
 T = 66.678
 R = 875.000
 SE = 0.05
 DS = 100 km/h

-LREV-
 PI Sta 13+29.902
 $\Delta = 2' 09" 37.3" (RT)$
 L = 16.498
 T = 32.992
 R = 875.000
 SE = 0.05
 DS = 100 km/h

-LREV-
 PIs Sta 13+79.740
 $\Theta s = 3' 16" 26.6"$
 Ls = 100.000
 T = 66.678
 R = 875.000
 SE = 0.05
 DS = 100 km/h

SEE SHEET 31 FOR -LREV- PROFILE
 SEE SHEET 49 FOR -Y- PROFILE

LOCHNER
 H.W. LOCHNER, INC.
 3725 NATIONAL DRIVE, SUITE 123
 RALEIGH, NC 27612

MATCH LINE -LREV- STA 12+95.000
SEE SHEET 5