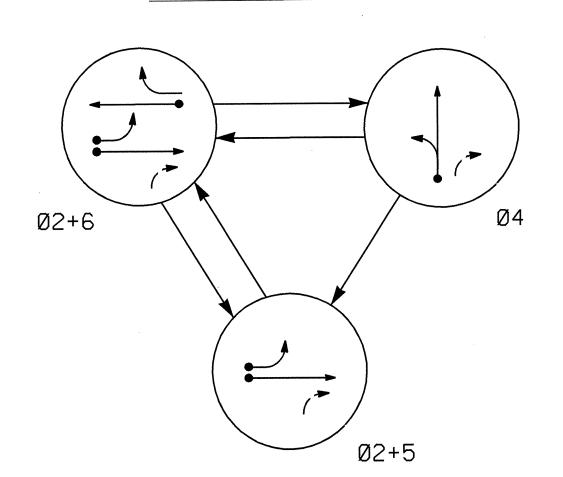


PROJECT REFERENCE NO. SIG.8

PHASING DIAGRAM



PHASING DIAGRAM DETECTION LEGEND

DETECTED MOVEMENT

UNSIGNALIZED MOVEMENT

PEDESTRIAN MOVEMENT

UNDETECTED MOVEMENT (OVERLAP)

TABLE OF OPERATION									
	PHASE								
SIGNAL FACE	Ø2+5	Ø2+6	FLASH						
21	\G ₽	G	R	Υ					
22	G	G	R	Υ					
41,42	R	R	G	R					
61,62	R	G	R	Υ					

PER	ATI	ON	
	F	PHAS	SE.
Ø2+5	Ø2+6	Ø 4	FLAOI
\C ₽	G	R	Υ
G	G	R	Υ
R	R	G	R
R	G	R	Υ
	Ø2+5 G R	Ø Ø 2 + 6 G G G R R	+ + 4 5 6 R G G R R R G

SIGNAL FACE	I.D.													
		2070	L LO	OP 8	DET	Έ(СТОІ	R	II	NS	T/	\LLA	TION	1
() Denotes L.	F.D.	INDUCTIVE LOOPS						DETECTOR PROGRAMMING						
	(R)	LOOP	SIZE (M)	TURNS	DISTANCE FROM STOPBAR (M)	NEW LOOP	PHASE	CALLING	EXTENSION	FULL TIME DELAY	SYSTEM LOOP	STRETCH TIME	DELAY TIME	NEW CARD
	(Y)	2A	1.8XI.8	5	21	Υ	2	Υ	Υ	-	_			_
		4 A	1.8X18	2-4-2	0	Υ	4	Υ	Υ	_	_			_
300mm	300mm		1 0 1 1 0	2 4 2	_	_	2	Υ	Υ	_	_			-
		5A	5A 1.8X18 2	1.8XI8 2-4-2	0		5	Y	Υ	-	_		15	-
21	22 41,42 61,62	6A	1.8XI.8	4	21	Y	6	Y	Υ	-	-			Ŀ
	61,62	S2	1.8X1.8	4	+55	Υ		_	_	_	Υ	A1.45.15.15.15.15.15.15.15.15.15.15.15.15.15		Υ
		S3	1.8X1.8	4	+55	Υ		_	_	_	Υ			Y
		54	1.8X1.8	4	+55	Y		_	_	_	Y			Y

3 Phase Fully Actuated (Closed Loop Signal System)

NOTES

- 1. REFER TO "ROADWAY STANDARD DRAWINGS NCDOT", DATED JANUARY 2002 AND "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" DATED JANUARY 2002.
- 2. MAXIMUM TIMES SHOWN IN TIMING CHART ARE FOR FREE-RUN OPERATIONS ONLY. COORDINATED SIGNAL SYSTEM TIMING VALUES SHALL SUPERSEDE THESE VALUES.

 3. REPOSITION EXISTING SIGNAL HEADS
- NUMBERED 21,22,41,42.
 4. SET ALL DETECTOR UNITS TO PRESENCE MODE.
 5. CLOSED LOOP SYSTEM DATA: INTERSECTION
 CONTROLLER ASSET # 0431.

PLAN QUANT	ITIES
Pay Item	Meters
Signal Cable	
Messenger Cable	
Lead-in Cable	330

	LEGEND	
<u>PROPOSED</u>		EXISTIN
○ ➤	Traffic Signal Head	
O ->	Modified Signal Head	N/A
	Sign	\dashv
	Pedestrian Signal Head With Push Button & Sign	
0	Signal Pole with Guy	•
\circ	Signal Pole with Sidewalk Guy	
	Inductive Loop Detector	C
	Controller & Cabinet	K×3
	Junction Box	
	50mm Underground Conduit -	
N/A	Right of Way with Marker	
\longrightarrow	Directional Arrow	\longrightarrow
$\langle \overline{\mathbb{A}} \rangle$	"YIELD" Sign (R1-2)	(A
(B)	No Right Turn Sign (R3-1)	Œ
\bigcirc	No Left Turn Sign (R3-2)	(

56 KPH (35 MPH) GRADE -6% NC 146 (LONG SHOALS ROAD) NC 146 (LONG SHOALS ROAD) 56 KPH (35 MPH) GRADE +7%

2070L TIMING CHART									
	PHASE								
FEATURE	2 4 5								
Min Green 1 *	12	7	7	12					
Extension 1 *	3	1	2	3					
Max Green 1 *	50	25	20	50					
Yellow Clearance	4.2	4.0	4.0	4.2					
Red Clearance	2.5	1.5	1.0	2.5					
Walk 1 *		***	_	_					
Don't Walk 1	-		_	_					
Seconds Per Actuation *		_	-	-					
Max Variable Initial *	· <u>-</u>	_	-	-					
Time Before Reduction *	-	-		. –					
Time To Reduce *	-		-	-					
Minimum Gap	•••			-					
Recall Mode	MIN RECALL	_	-	MIN RECALL					
Vehicle Call Memory	YELLOW	_	_	YELLOW					
Dual Entry	_	-	_	-					
Simultaneous Gap	ON	ON	ON	ON					

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what

is shown. Min Green for all other phases should not be lower than 4 seconds

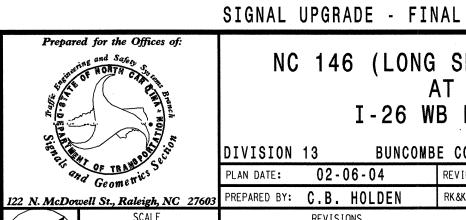
PLANS PREPARED BY :

RUMMEL KLEPPER & KAHL, LLP

consulting engineers 5800 FARINGDON PLACE SUITE 105 RALEIGH, NORTH CAROLINA 27609-3960

FOR

DIVISION OF HIGHWAYS





I-26 WB RAMPS

BUNCOMBE COUNTY ASHEVILLE DIVISION 13 REVIEWED BY: D. MORTON 02-06-04 22 N. McDowell St., Raleigh, NC 27603 PREPARED BY: C.B. HOLDEN RK&K PROJECT NO. 302-079-SIG4

