

# NOTES

THE COST OF THE BARRIER RAIL ON THE APPROACH SLAB SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE BID FOR BRIDGE APPROACH SLABS.

APPROACH SLAB SHALL NOT BE CONSTRUCTED PRIOR TO COMPLETION OF THE BRIDGE DECK.

FOR REINFORCED BRIDGE APPROACH FILL INCLUDING FABRIC, IMPERMEABLE GEOMEMBRANE, 4" Ø DRAINAGE PIPE, #78M STONE, AND SELECT MATERIAL, SEE ROADWAY PLANS.

TEMPORARY DRAINAGE AND TEMPORARY BERM AND SLOPE DRAINS WILL BE PAID FOR UNDER THE LUMP SUM PRICE FOR BRIDGE APPROACH SLAB.

AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE.

THE 6" COMP. A.B.C. SHALL EXTEND 10'-0" BEYOND THE END OF THE APPROACH SLAB AND 1'-0" OUTSIDE OF EACH EDGE OF THE SLAB.

THE CONTRACTOR MAY USE 4" TYPE B-25.0B ASPHALT CONCRETE BASE COURSE IN LIEU OF 6" COMP. A.B.C. IF THIS OPTION IS USED, THE BASE COURSE SHALL EXTEND 1'-0" BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB.

THE CONTRACTOR MAY USE 5" CLASS "A" CONCRETE BASE IN LIEU OF 6" COMP. A.B.C. IF THIS OPTION IS USED, THE CONCRETE BASE SHALL EXTEND 1'-0" BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB. THE CONCRETE SHALL BE FINISHED TO A SMOOTH SURFACE AND A LAYER OF 30 LB ROOFING FELT SHALL BE PLACED BETWEEN THE CONCRETE BASE AND THE APPROACH SLAB TO PREVENT BOND. THE APPROACH SLAB SHALL NOT BE CAST UNTIL THE CONCRETE BASE HAS REACHED AN AGE OF THREE CURING DAYS.

THE JOINT SHALL BE SAWS PRIOR TO THE CASTING OF THE BARRIER RAIL.

WITH EVAZOTE JOINT SEAL

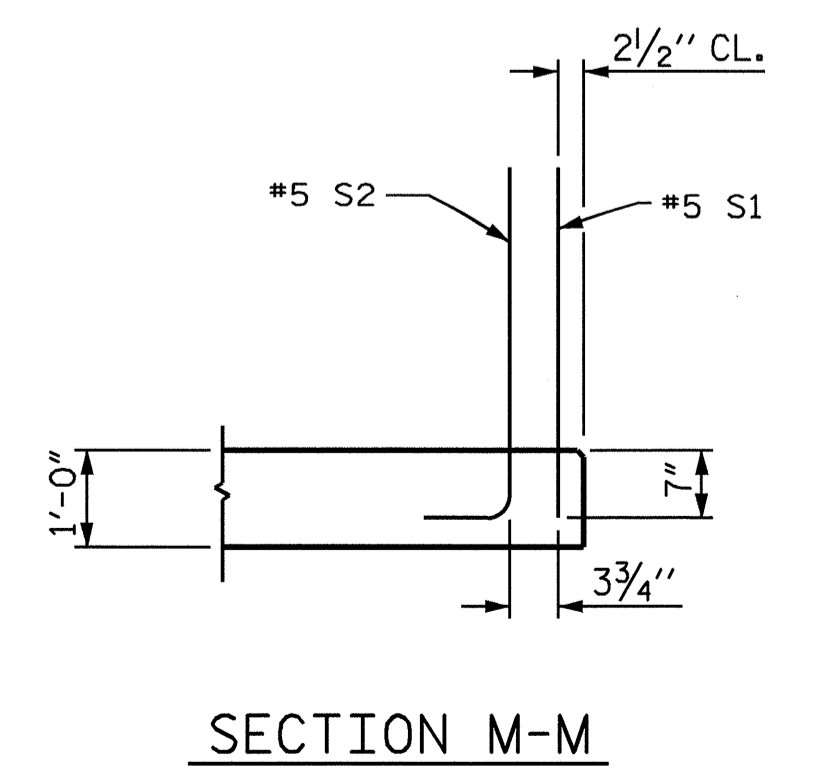
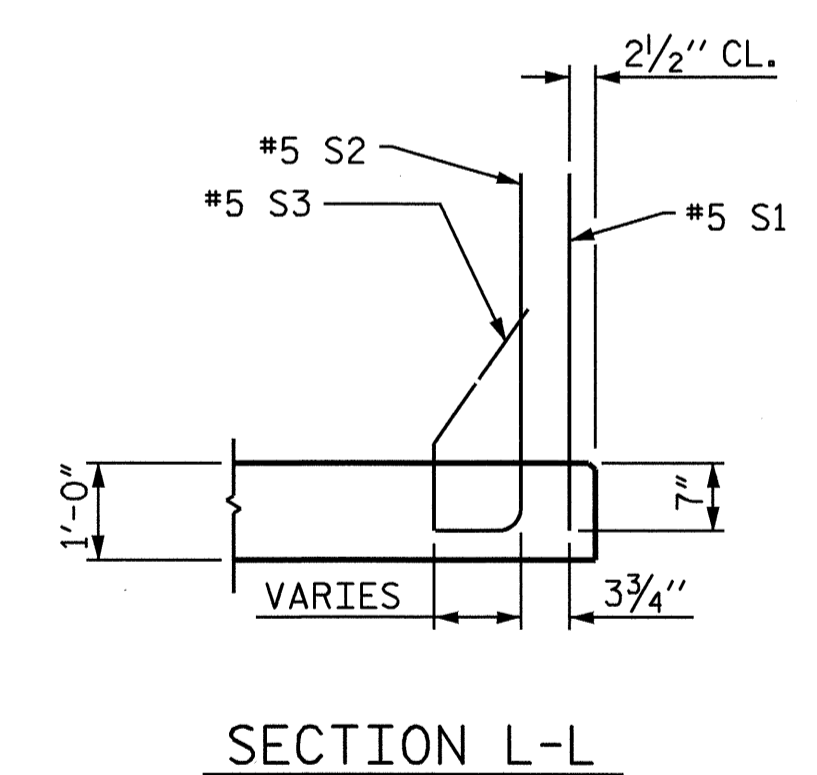
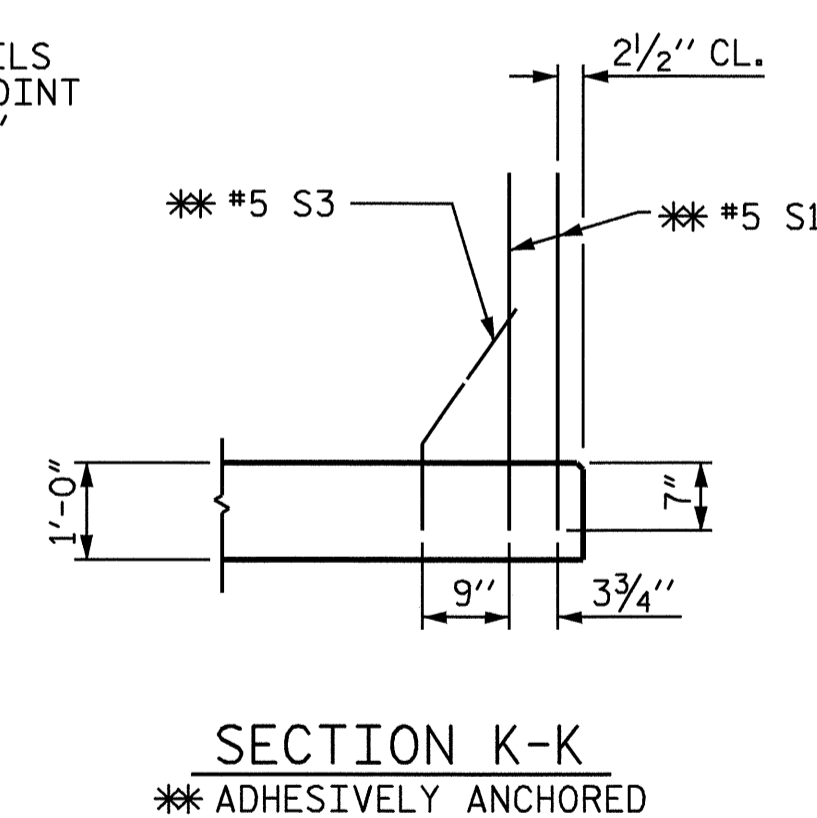
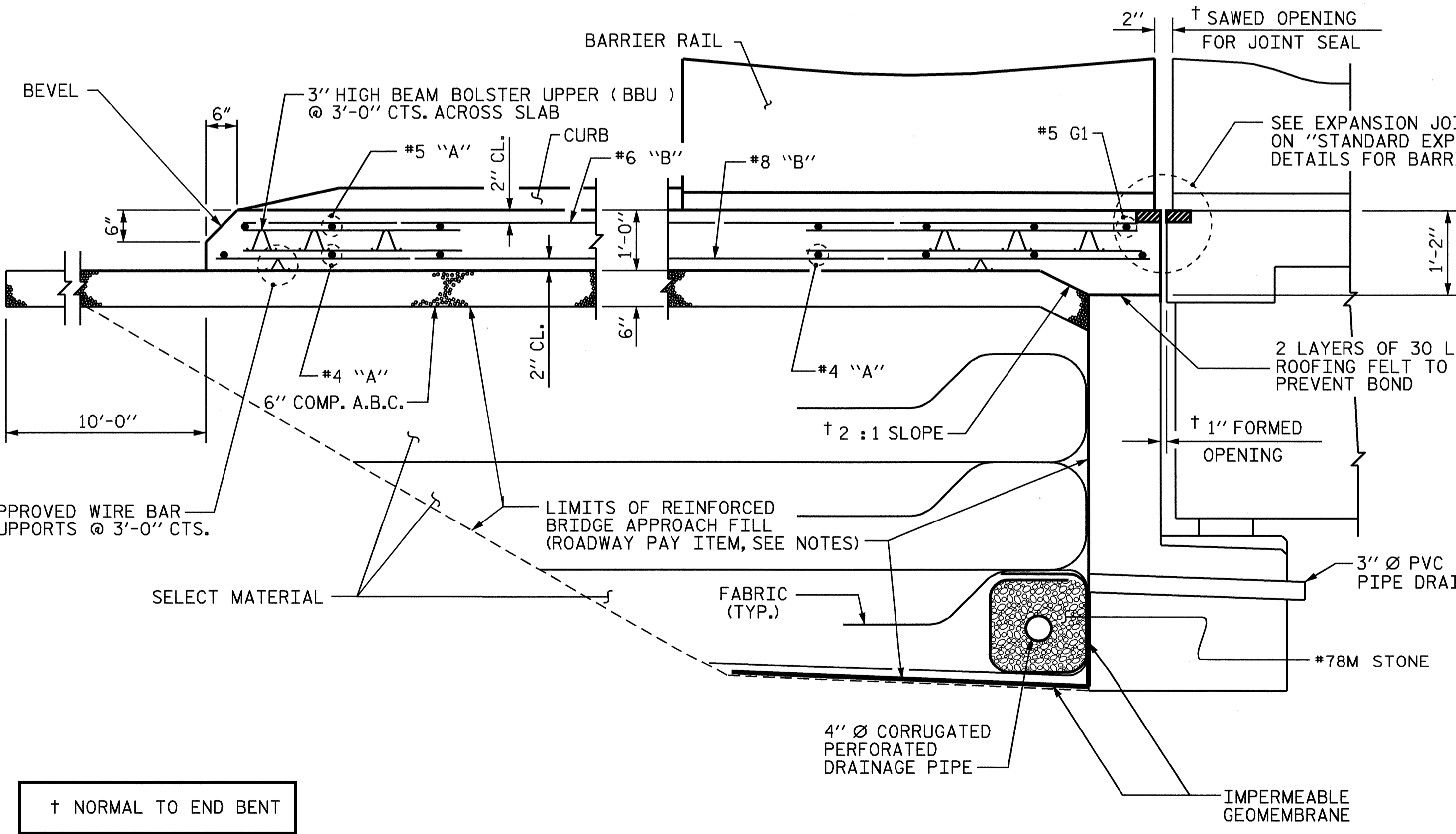
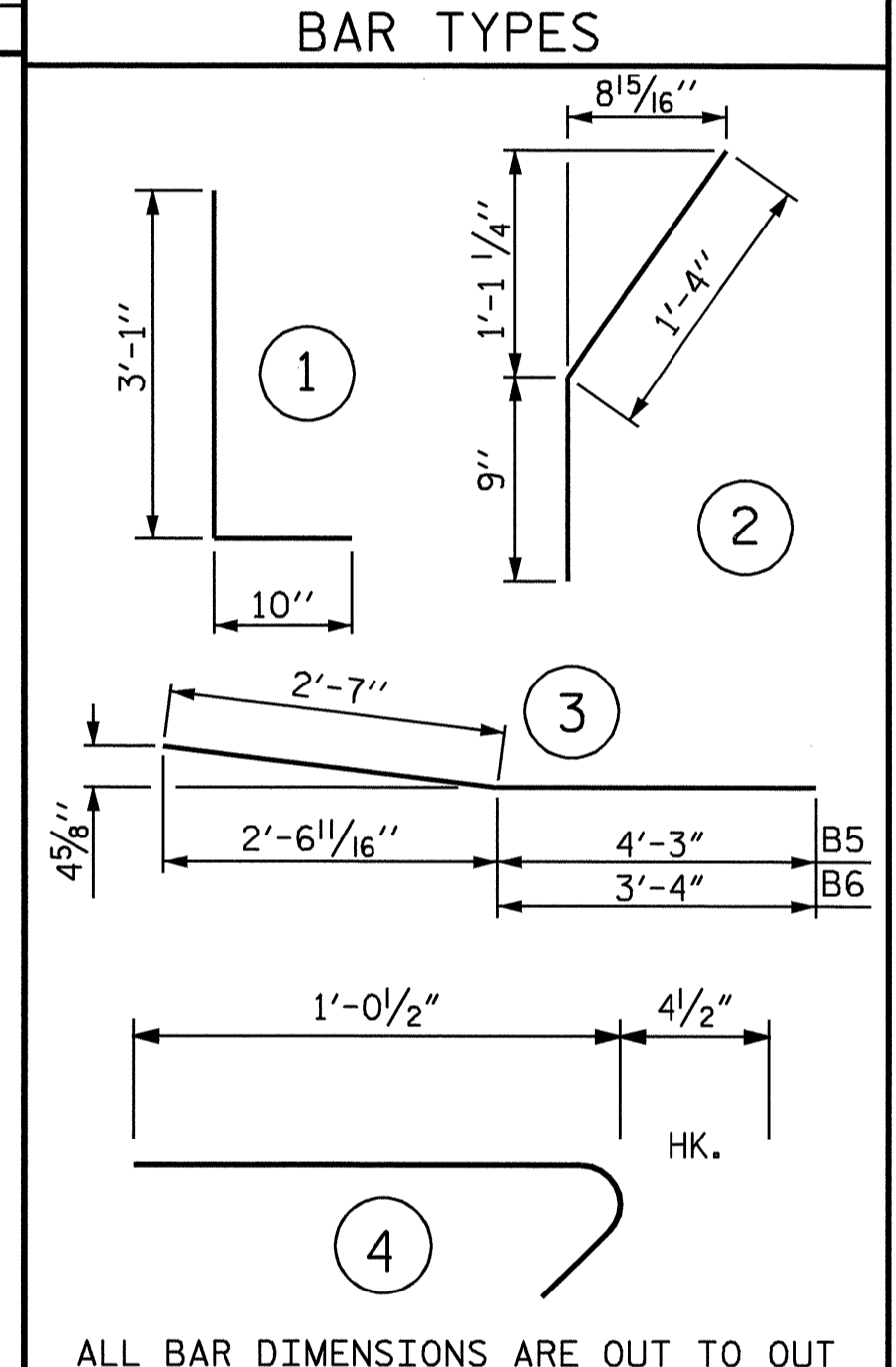
FOR EVAZOTE JOINT SEALS, SEE SPECIAL PROVISIONS.

THE NOMINAL UNCOMPRESSED SEAL WIDTH OF THE EVAZOTE JOINT SEAL SHALL BE 2 1/2".

# BILL OF MATERIAL FOR ONE APPROACH SLAB (2 REQ'D)

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT	BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
*A1	50	#5	STR	36'-6"	1903	A2	100	#4	STR	19'-2"	1280	*B101	3	#6	STR	24'-9"	112	B201	3	#8	STR	24'-9"	198	*B3	7	#5	STR	11'-9"	86
*A101	1	#5	STR	35'-6"	37	A201	2	#4	STR	18'-8"	25	*B102	3	#6	STR	25'-3"	114	B202	3	#8	STR	25'-3"	202	*B4	7	#5	STR	11'-5"	83
*A102	1	#5	STR	34'-2"	36	A202	2	#4	STR	18'-0"	24	*B103	3	#6	STR	25'-10"	116	B203	3	#8	STR	25'-10"	207	*B5	1	#5	3	6'-10"	7
*A103	1	#5	STR	32'-9"	34	A203	2	#4	STR	17'-4"	23	*B104	3	#6	STR	26'-4"	119	B204	3	#8	STR	26'-4"	211	*B6	1	#5	3	5'-11"	6
*A104	1	#5	STR	31'-7"	33	A204	2	#4	STR	16'-9"	22	*B105	3	#6	STR	26'-11"	121	B205	3	#8	STR	26'-11"	216						
*A105	1	#5	STR	30'-3"	32	A205	2	#4	STR	16'-0"	21	*B106	3	#6	STR	27'-6"	124	B206	3	#8	STR	27'-6"	220	*G1	2	#5	STR	39'-3"	82
*A106	1	#5	STR	28'-10"	30	A206	1	#4	STR	28'-10"	19	*B107	3	#6	STR	28'-0"	126	B207	3	#8	STR	28'-0"	224	*J1	36	#4	4	1'-5"	34
*A107	1	#5	STR	27'-6"	29	A207	1	#4	STR	27'-6"	18	*B108	3	#6	STR	28'-7"	129	B208	3	#8	STR	28'-7"	229						
*A108	1	#5	STR	26'-1"	27	A208	1	#4	STR	26'-1"	17	*B109	3	#6	STR	29'-1"	131	B209	3	#8	STR	29'-1"	233	*S1	56	#5	STR	3'-1"	180
*A109	1	#5	STR	24'-9"	26	A209	1	#4	STR	24'-9"	17	*B110	3	#6	STR	29'-8"	134	B210	3	#8	STR	29'-8"	238	*S2	40	#5	1	3'-11"	163
*A110	1	#5	STR	23'-4"	24	A210	1	#4	STR	23'-4"	16	*B111	3	#6	STR	30'-2"	136	B211	3	#8	STR	30'-2"	242	*S3	20	#5	2	2'-1"	43
*A111	1	#5	STR	22'-0"	23	A211	1	#4	STR	22'-0"	15	*B112	3	#6	STR	30'-9"	139	B212	3	#8	STR	30'-9"	246						
*A112	1	#5	STR	20'-7"	21	A212	1	#4	STR	20'-7"	14	*B113	3	#6	STR	31'-3"	141	B213	3	#8	STR	31'-3"	250						
*A113	1	#5	STR	19'-3"	20	A213	1	#4	STR	19'-3"	13	*B114	3	#6	STR	31'-10"	143	B214	3	#8	STR	31'-10"	255						
*A114	1	#5	STR	17'-10"	19	A214	1	#4	STR	17'-10"	12	*B115	3	#6	STR	32'-5"	146	B215	3	#8	STR	32'-5"	260						
*A115	1	#5	STR	16'-6"	17	A215	1	#4	STR	16'-6"	11	*B116	3	#6	STR	32'-11"	148	B216	3	#8	STR	32'-11"	264						
*A116	1	#5	STR	15'-1"	16	A216	1	#4	STR	15'-1"	10	*B117	3	#6	STR	33'-6"	151	B217	3	#8	STR	33'-6"	268						
*A117	1	#5	STR	13'-9"	14	A217	1	#4	STR	13'-9"	9	*B118	3	#6	STR	33'-6"	151	B218	3	#8	STR	34'-0"	272						
*A118	1	#5	STR	12'-4"	13	A218	1	#4	STR	12'-4"	8	*B119	3	#6	STR	34'-7"	156	B219	3	#8	STR	34'-7"	277						
*A119	1	#5	STR	11'-0"	11	A219	1	#4	STR	11'-0"	7	*B120	3	#6	STR	35'-1"	158	B220	3	#8	STR	35'-1"	281						
*A120	1	#5	STR	9'-7"	10	A220	1	#4	STR	9'-7"	6	*B121	3	#6	STR	35'-8"	161	B221	3	#8	STR	35'-8"	286						
*A121	1	#5	STR	8'-3"	9	A221	1	#4	STR	8'-3"	6	*B122	3	#6	STR	36'-2"	163	B222	3	#8	STR	36'-2"	290						
*A122	1	#5	STR	6'-10"	7	A222	1	#4	STR	6'-10"	5	*B123	3	#6	STR	36'-9"	166	B223	3	#8	STR	36'-9"	294						
*A123	1	#5	STR	5'-6"	6	A223	1	#4	STR	5'-6"	4	*B124	3	#6	STR	37'-4"	168	B224	3	#8	STR	37'-4"	299						
*A124	1	#5	STR	4'-1"	4	A224	1	#4	STR	4'-1"	3	*B125	1	#6	STR	37'-10"	57	B225	1	#8	STR	37'-10"	101						
*A125	1	#5	STR	2'-9"	3	A225	1	#4	STR	2'-9"	2																		

REINFORCING STEEL	LBS.	7670
*EPOXY COATED REINFORCING STEEL	LBS.	6500
CLASS AA CONCRETE BREAKDOWN		
POUR 1 SLAB & CURBS	C. Y.	44.6
POUR 2 RAIL	C. Y.	1.8
CLASS AA CONCRETE	C. Y.	46.4



PROJECT NO. B-4010  
 ASHE COUNTY  
 STATION: 20+93.60 -L-

SHEET 2 OF 4

STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH

BRIDGE APPROACH SLAB  
 FOR RIGID PAVEMENT  
 WITH BARRIER RAIL

REVISIONS				SHEET NO.
NO.	BY:	DATE:	NO.	DATE:
1			3	
2			4	

TOTAL SHEETS: 37



ASSEMBLED BY: CR LEWIS DATE: 04/04  
 CHECKED BY: L.L. MURPHY DATE: 04/04  
 DRAWN BY: LES 8/01 REV. 5/7/03 RWW/JTE  
 CHECKED BY: RDR 8/01

## SECTION THRU SLAB