

NOTES:

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.

FOR EPOXY PROTECTIVE COATING, SEE SPECIAL PROVISIONS.

PIPE DRAINS MAY BE SHIFTED SLIGHTLY AS NECESSARY TO CLEAR REINFORCING STEEL AND ANCHOR BOLTS.

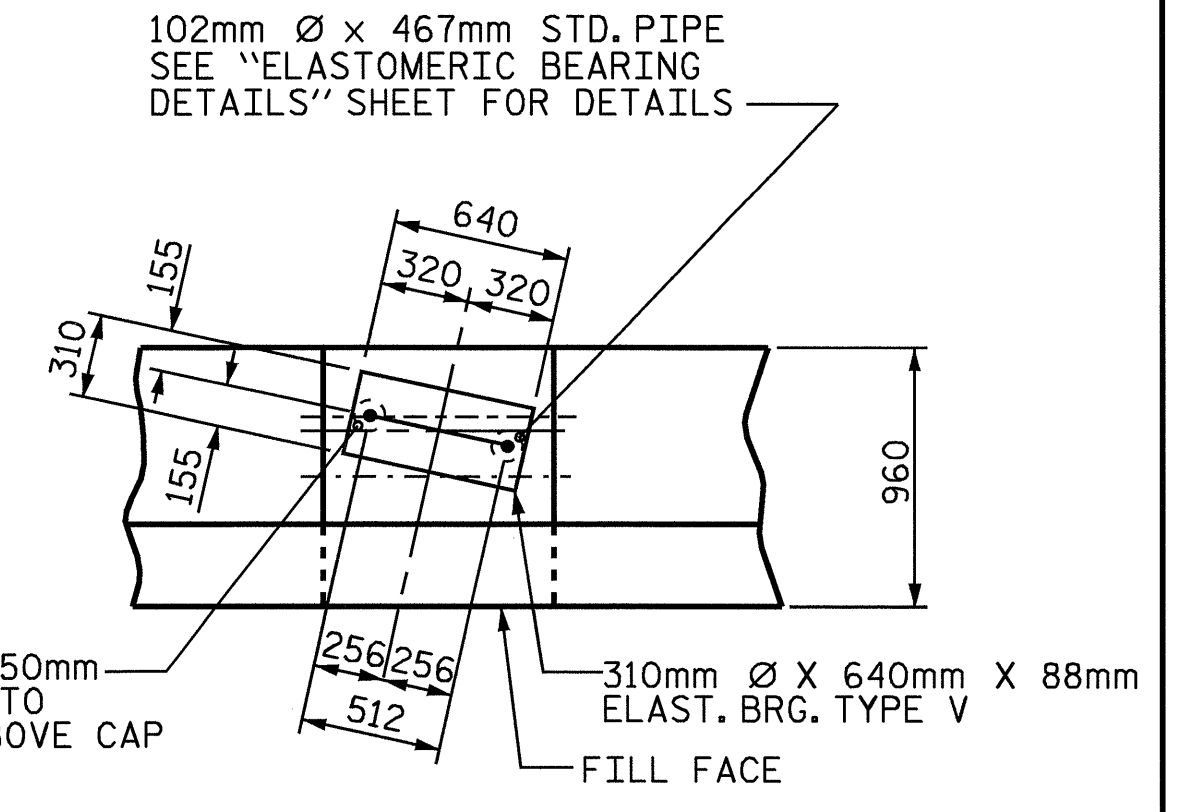
FOR PIPE DRAIN DETAILS, SEE SHEET 3 OF 3.

FOR PILE SPLICE DETAILS, SEE SHEET 3 OF 3.

BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.

THE TOP SURFACE AREAS OF THE END BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.

THE TOP SURFACE OF THE CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2 %.



PROJECT NO. R-2911A
IREDELL COUNTY
 STATION: 39+62.402 -L-

SHEET 1 OF 3

REVISIONS				SHEET NO.
NO.	BY:	DATE:	NO.	DATE:
1			3	
2			4	

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**SUBSTRUCTURE
 END BENT #1
 LEFT LANE**

TOTAL SHEETS: 106



DRAWN BY: QT NGUYEN DATE: 2-04
 CHECKED BY: L.L. MURPHY DATE: 3-04

ELEVATION
 * FOR LOCATION OF ELEVATION BETWEEN BRIDGE SEAT BUILDUPS, SEE SECTION A-A, SHEET 3 OF 3.

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STR. #1