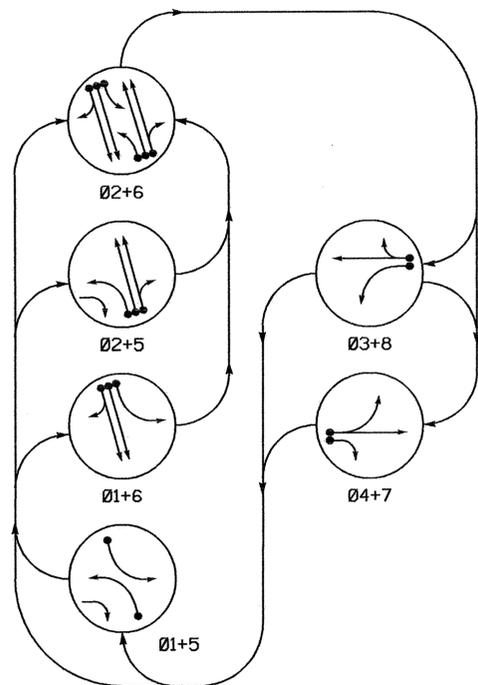


**PHASING DIAGRAM**



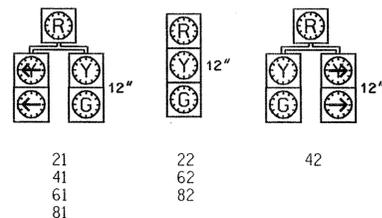
**PHASING DIAGRAM DETECTION LEGEND**

- ← ● ← DETECTED MOVEMENT
- ← ○ ← UNDETECTED MOVEMENT (OVERLAP)
- ← ○ ← UNSIGNALIZED MOVEMENT
- ← ○ ← PEDESTRIAN MOVEMENT

SIGNAL FACE	PHASE							FLASH
	01+5	01+6	02+5	02+6	03+8	04+7		
21	R	R	G	G	R	R	Y	
22	R	R	G	G	R	R	Y	
41	R	R	R	R	R	R	G	
42	R	R	R	R	R	R	G	
61	R	R	R	R	R	R	Y	
62	R	R	R	R	R	R	Y	
81	R	R	R	R	R	R	G	
82	R	R	R	R	R	R	G	

**SIGNAL FACE I.D.**

○ Denotes L.E.D.

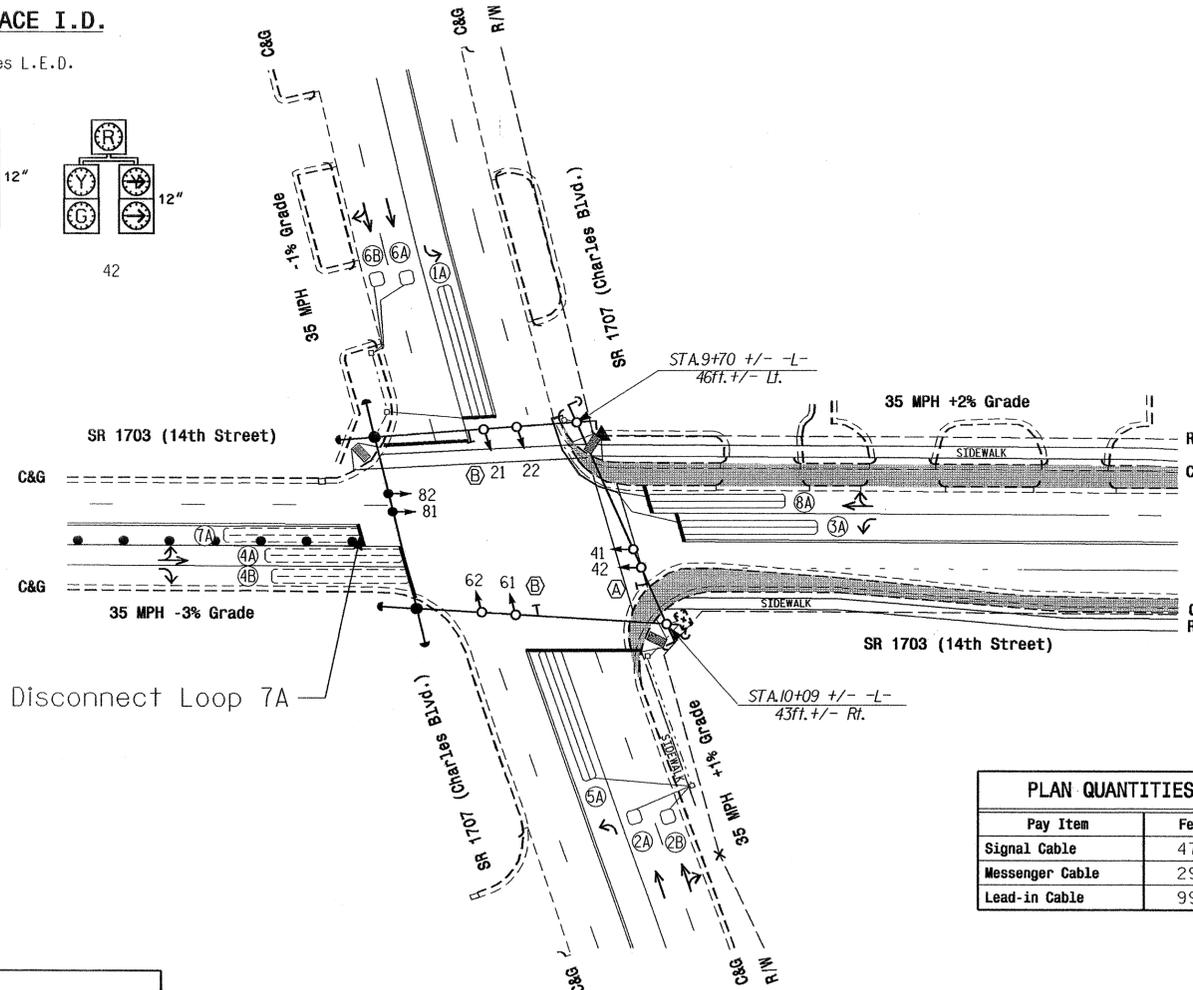


LOOP & DETECTOR UNIT INSTALLATION CHART NEMA CONTROLLER WITH TS-2 CABINET											
INDUCTIVE LOOPS					DETECTOR UNITS						
LOOP NO.	SIZE (ft)	TURNS	DIST. FROM STOPBAR (ft)	NEW	NEMA PHASE	EXISTING	FEATURE	TIME	PLACE CALL DURING PHASE	INHIBIT DELAY DURING GREEN?	
1A	6X60	2-4-2	0	X	1	X	DELAY	15 SEC.	ALL	YES	
2A	6X6	4	70	X	2	X	-	- SEC.	ALL	NO	
2B	6X6	4	70	X	2	X	-	- SEC.	ALL	NO	
3A	6X60	2-4-2	0	X	8	X	DELAY	3 SEC.	ALL	YES	
4A	6X60	2-4-2	0	X	4	X	DELAY	3 SEC.	ALL	YES	
4B	6X60	2-4-2	0	X	4	X	DELAY	15 SEC.	ALL	YES	
5A	6X60	2-4-2	0	X	5	X	DELAY	15 SEC.	ALL	YES	
6A	6X6	4	70	X	6	X	-	- SEC.	ALL	NO	
6B	6X6	4	70	X	6	X	-	- SEC.	ALL	NO	
8A	6X60	2-4-2	0	X	8	X	DELAY	10 SEC.	ALL	YES	

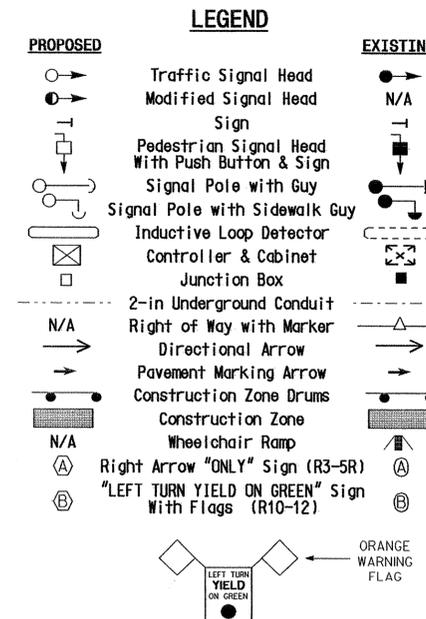
**6 Phase Fully Actuated (Greenville City System)**

**NOTES**

- Refer to "Roadway Standard Drawings NCDOT" dated January 2002 and "Standard Specifications for Roads and Structures" dated January 2002.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Pavement markings are existing.
- Omit phase 1 during phase 2 on.
- Omit phase 5 during phase 6 on.
- Program controller to clear from phase 2+6 to phase 1 and/or 5 by progressing through phase 4+7 (see Electrical Details).
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values shall supersede these values.
- During coordination, the order of phase 4+7 and phase 3+8 may be reversed.
- Thirty days after implementation of the revised signal operation, signs B and/or orange flags may be removed at the discretion of the Regional Traffic Engineer.
- Set all detector units to presence mode.
- Intersection Zone Number: 3  
System address number: 86



PLAN QUANTITIES	
Pay Item	Feet
Signal Cable	475
Messenger Cable	295
Lead-in Cable	995



TIMING CHART NEMA CONTROLLER								
PHASE	01	02	03	04	05	06	07	08
MINIMUM GREEN	7 SEC.	10 SEC.	7 SEC.	7 SEC.	7 SEC.	10 SEC.	7 SEC.	7 SEC.
PASSAGE GAP	1.0 SEC.	3.0 SEC.	1.0 SEC.	1.0 SEC.	1.0 SEC.	3.0 SEC.	1.0 SEC.	1.0 SEC.
YELLOW CHANGE INT.	4.0 SEC.	4.0 SEC.	4.0 SEC.	4.0 SEC.	4.0 SEC.	4.0 SEC.	4.0 SEC.	4.0 SEC.
RED CLEARANCE	2.5 SEC.	2.5 SEC.	2.0 SEC.	2.0 SEC.	2.5 SEC.	2.5 SEC.	2.0 SEC.	2.0 SEC.
MAX. I	20 SEC.	50 SEC.	20 SEC.	50 SEC.	20 SEC.	50 SEC.	20 SEC.	50 SEC.
RECALL POSITION	NONE	MIN. RECALL	NONE	NONE	NONE	MIN. RECALL	NONE	NONE
VEHI. CALL MEMORY	NONLOCK	LOCK	NONLOCK	NONLOCK	NONLOCK	LOCK	NONLOCK	NONLOCK
WALK	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.
FLASHING DON'T WALK	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.	- SEC.
VOLUME DENSITY	OFF	OFF	OFF	OFF	OFF	OFF	OFF	OFF

**Signal Upgrade-Temporary Design**

122 N. McDowell St., Raleigh, NC 27603

**SR 1707 (Charles Blvd.)  
at  
SR 1703 (14th Street)**

Division 2 Pitt County Greenville

PLAN DATE: August 2004 REVIEWED BY:

PREPARED BY: R W Wouberly REVIEWED BY:

REVISIONS: INIT. DATE

SEAL

8/26/04

SIG. INVENTORY NO. 02-0019 T