

**PHASE I**

**CONSTRUCTION OF PROPOSED INSIDE AND OUTSIDE SHOULDERS OF I-40 (-EBL-/-WBL-).**

- STEP 1: - INSTALL ALL ADVANCE WORK ZONE WARNING SIGNS ALONG I-40 (-EBL-/-WBL-) AND ALL RAMPS, AS SHOWN ON SHEETS TCP-10 THRU TCP-12. [NOTE: ENSURE THAT THE "\$250 PENALTY" SIGNS (W20-1JSP) ARE INSTALLED, AS SHOWN, ON SHEETS TCP-10 THRU TCP-11. ALL SPEED LIMIT SIGNS WILL BE "FLIP SIGNS," AS SHOWN ON SHEET TCP-8. COVER EXISTING SPEED LIMIT SIGNS, AS DIRECTED BY THE ENGINEER, WHEN REDUCED SPEED LIMITS ARE IN EFFECT.]
- PLACE CHANGEABLE MESSAGE SIGNS (CMS), IN ADVANCE OF THE "BEGIN WORK ZONE" SIGNS ON I-40 (-EBL-/-WBL-), AS DIRECTED BY THE ENGINEER.
  - FURNISH AND INSTALL ALL DUAL MOUNTED "NO PARKING-TOW AWAY ZONE" SIGNS, ON I-40 (-EBL-/-WBL-) EVERY 3,000 FEET, OR AS DIRECTED BY THE ENGINEER AS SHOWN ON SHEET TCP-9.

CONTRACTOR MAY WORK IN PHASE I, STEPS 2 & 3, PHASE II AND PHASE III OR PHASE IIIA (OPTION) INDIVIDUALLY IN SEQUENCE, OR SIMULTANEOUSLY AS DESCRIBED BELOW:

- A. CONTRACTOR SHALL COMPLETE CONSTRUCTION OF APPLICABLE STEP 2 OR 3 OF PHASE I, IN THOSE SECTIONS OF I-40 OF HIS CHOOSING, INCLUDING ALL ON/OFF RAMPS IF REQUIRED, BEFORE PROCEEDING TO PHASE II. (CONTRACTOR MAY CONSTRUCT CONCRETE PAVEMENT REPLACEMENTS AND ADJACENT SHOULDERS BEHIND PORTABLE CONCRETE BARRIER SIMULTANEOUSLY.)
- B. CONTRACTOR SHALL COMPLETE CONSTRUCTION OF PHASE II IN THOSE SECTIONS OF I-40 OF HIS CHOOSING, INCLUDING ALL ON/OFF RAMPS, IF REQUIRED, BEFORE PROCEEDING TO PHASE III OR PHASE IIIA (OPTION).

NOTE: THE CONTRACTOR SHALL MAINTAIN TRAFFIC ON I-40 AND ALL ON/OFF RAMPS DURING THE ABOVE CONSTRUCT SEQUENCE AS STATED IN THE GENERAL NOTES ON SHEETS TCP-2. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO GENERAL NOTES A, B, C, D AND F.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE I, STEPS 2 THRU 3. CONTRACTOR MAY WORK PHASE I, STEP 3 THEN STEP 2, AS DIRECTED BY THE ENGINEER.

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

STEP 2: - USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 7 AND 1101.04, SHIFT TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANES OF I-40 (-EBL-/-WBL-). (SEE SHEET TCP-5, DETAIL 1).

- CONSTRUCT THE PROPOSED INSIDE SHOULDERS AND 280-FOOT TAPERS UP TO THE EDGE AND ELEVATION OF EXISTING I-40 (-EBL-/-WBL-), AS SHOWN IN THE CONSTRUCTION PLANS AND SHEET TCP-5, DETAIL 1.

NOTE: THE CONTRACTOR SHALL BACKFILL/SAFE-UP ALL SHOULDERS BY THE END OF EACH DAILY WORK PERIOD AS FOLLOWS:

BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS THAT EXCEED 2" ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL WITH ABC OR SUITABLE COMPACTED MATERIAL, APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- MAY BEGIN CONSTRUCTION OF PROPOSED SIGNING STRUCTURES. (SEE SIGNING PLANS.)

- RETURN TRAFFIC TO THE EXISTING 4-LANE, 2-WAY PATTERN BY NOON EACH FRIDAY AND REPEAT PHASE I, STEP 2 UNTIL THE INSIDE SHOULDERS ARE COMPLETED FOR THE ENTIRE PROJECT. (SEE SHEET TCP-5, DETAIL 2).

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

NOTE: THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE REST AREAS AT ALL TIMES. HOWEVER, THE CONTRACTOR SHALL CLOSE THE REST AREA TO PERFORM THE PAVING OPERATIONS IN PHASE I, STEP 3 INSTEAD OF USING THE "STOP OR YIELD" SIGNS AS SHOWN ON ROADWAY STANDARD DRAWINGS NO. 1101.02, SHEET 6 OF 7. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE REST AREA FOR ONLY ONE WORK PERIOD (FROM MIDNIGHT SUNDAY TO NOON FRIDAY). ALL REQUIRED PAVING ON I-40 AND THE ACCELERATION/DECELERATION LANES IN THE REST AREA SHALL BE COMPLETED IN THIS WORK PERIOD. THE REST AREA IS DEFINED AS FROM THE BEGINNING OF THE DECELERATION LANE TO THE END OF THE ACCELERATION LANE.

STEP 3: - USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3, 6 & 7 OF 7 AND 1101.04, SHIFT TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE INSIDE LANES OF I-40 (-EBL-/-WBL-). (SEE SHEET TCP-5, DETAIL 3).

- CONSTRUCT THE PROPOSED OUTSIDE SHOULDERS AND 280-FOOT TAPERS UP TO THE EDGE AND ELEVATION OF EXISTING I-40 (-EBL-/-WBL-), AS SHOWN IN THE CONSTRUCTION PLANS AND SHEET TCP-5, DETAIL 3.

NOTE: THE CONTRACTOR SHALL BACKFILL/SAFE-UP ALL SHOULDERS BY THE END OF EACH DAILY WORK PERIOD, AS FOLLOWS:

BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS THAT EXCEED 2" ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL WITH ABC OR SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- MAY BEGIN CONSTRUCTION OF PROPOSED SIGNING STRUCTURES. (SEE SIGNING PLANS.)

- RETURN TRAFFIC TO THE EXISTING 4-LANE, 2-WAY PATTERN BY NOON EACH FRIDAY AND REPEAT PHASE I, STEP 3, UNTIL THE OUTSIDE SHOULDERS ARE COMPLETED FOR THE ENTIRE PROJECT. (SEE SHEET TCP-5, DETAIL 4).

**PHASE II**

**CONSTRUCTION OF PROPOSED CONCRETE PAVEMENT REMOVAL ON I-40 (-EBL-/-WBL-).**

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

NOTE: THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE REST AREAS AT ALL TIMES. HOWEVER, THE CONTRACTOR SHALL CLOSE THE REST AREA TO PERFORM THE CONCRETE PAVEMENT REMOVAL & REPLACEMENT IN PHASE II, STEP 1 INSTEAD OF USING THE "STOP OR YIELD" SIGNS AS SHOWN ON ROADWAY STANDARD DRAWINGS NO. 1101.02, SHEET 6 OF 7. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE REST AREA FOR ONLY ONE WORK PERIOD (FROM MIDNIGHT SUNDAY TO NOON FRIDAY). ALL REQUIRED CONCRETE PAVEMENT REMOVAL & REPLACEMENT ON I-40 AND THE ACCELERATION/DECELERATION LANES IN THE REST AREA SHALL BE COMPLETED IN THIS WORK PERIOD. THE REST AREA IS DEFINED AS FROM THE BEGINNING OF THE DECELERATION LANE TO THE END OF THE ACCELERATION LANE.

STEP 1: - USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3, 6 & 7 OF 7 AND 1101.04, CONDUCT CONCRETE PAVEMENT REMOVAL AND REPLACEMENT IN THE FOLLOWING SEQUENCE:

- A. PLACE 6" REMOVABLE TAPE PAVEMENT MARKINGS AS SHOWN ON SHEET TCP-6, DETAILS 5 OR 6.
- B. SHIFT TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN, AS SHOWN ON SHEET TCP-6, DETAILS 5 OR 6.
- C. INSTALL PORTABLE CONCRETE BARRIER (PCB) AS SHOWN ON SHEET TCP-6, DETAILS 5 OR 6.
- D. REMOVE AND REPLACE CONCRETE PAVEMENT AS SHOWN IN THE CONSTRUCTION PLANS.
- E. RETURN TRAFFIC TO THE EXISTING FOUR-LANE, TWO-WAY PATTERN BY NOON EACH FRIDAY, UNTIL ALL OF THE CONCRETE PAVEMENT REMOVAL AND REPLACEMENT WORK IS COMPLETED.

NOTE: THE CONTRACTOR MAY WORK PHASE III OR PHASE IIIA (OPTION). HOWEVER, THE CONTRACTOR SHALL NOT BE ALLOWED TO SWITCH BACK AND FORTH BETWEEN THE TWO PHASES.

**PHASE III**

**CONSTRUCTION OF PROPOSED I-40 (-EBL-/-WBL-) UP TO AND INCLUDING THE FIRST LAYER OF SURFACE COURSE.**

WORK IN A CONTINUOUS MANNER TO COMPLETE THE FOLLOWING WORK IN PHASE III, STEPS 1 THRU 5. THE CONTRACTOR SHALL ONLY WORK IN THOSE SECTIONS OF I-40 (-EBL-/-WBL-) THAT CAN BE COMPLETED FROM MIDNIGHT SUNDAY THRU NOON ON FRIDAY.

NOTE: CONTRACTOR SHALL REDUCE SPEED LIMIT ONLY IN ACTIVE WORK AREAS UTILIZING "REDUCE SPEED AHEAD" SIGNS WITH ORANGE PANELS AND SPEED LIMIT FLIP SIGNS OR "SPEED LIMIT 60" (R2-1) SIGNS, AS SHOWN ON SHEETS TCP-8, TCP-12, AND TCP-13.

NOTE: CONTRACTOR SHALL BEGIN INSTALLATION OR INSTALL ATR AS REQUIRED (SEE CONSTRUCTION PLANS).

NOTE: THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE REST AREAS AT ALL TIMES. HOWEVER, THE CONTRACTOR SHALL CLOSE THE REST AREA TO PERFORM THE PAVING OPERATIONS IN PHASE III INSTEAD OF USING THE "STOP OR YIELD" SIGNS AS SHOWN ON ROADWAY STANDARD DRAWINGS NO. 1101.02, SHEET 6 OF 7. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE REST AREA FOR ONLY ONE WORK PERIOD (FROM MIDNIGHT SUNDAY TO NOON FRIDAY). ALL REQUIRED PAVING ON I-40 AND THE ACCELERATION/DECELERATION LANES IN THE REST AREA SHALL BE COMPLETED IN THIS WORK PERIOD. THE REST AREA IS DEFINED AS FROM THE BEGINNING OF THE DECELERATION LANE TO THE END OF THE ACCELERATION LANE.

STEP 1: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 6 & 7 OF 7 AND 1101.04, REMOVE THE EXISTING WHITE EDGELINE AND PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT LINES, 12" PAINT GORE LINES, AND 6" REMOVABLE TAPE LINES ON EXISTING STRUCTURES), AND UTILIZING 280-FOOT TAPERS FOR TRANSITIONS, SHIFT TRAFFIC 8 FEET TO THE OUTSIDE SHOULDERS INTO THE INTERMEDIATE TRAFFIC PATTERNS, AS SHOWN ON SHEET TCP-7, DETAIL 7.


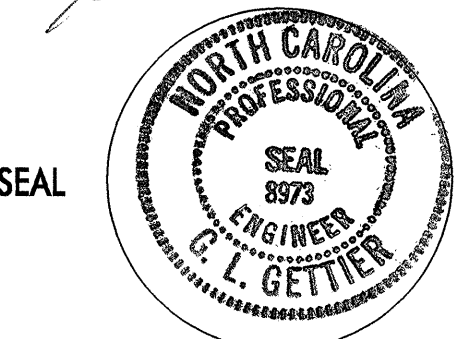
STEP 2: - PAVE THE INSIDE LANES AND SHOULDERS (25 FEET) OF I-40 (-EBL-/-WBL-) UP THROUGH THE BINDER COURSE, [INCLUDING PROPOSED/TEMPORARY TIE-INS TO EXISTING PAVEMENT], AS SHOWN IN THE CONSTRUCTION PLANS AND TCP-7, DETAIL 7.

- WHEN REQUIRED, CONSTRUCT TEMPORARY 2-FOOT LATERAL ASPHALT RAMP-DOWNS FROM THE PROPOSED BINDER COURSE TO THE EXISTING PAVEMENT AT THE FOLLOWING LOCATIONS. (SEE THE CONSTRUCTION PLANS AND SHEET TCP-7, DETAIL 7.) NOTE: RAMP-DOWNS WILL BE UTILIZED TO ALLOW ACCESS TO THE EXIT/ENRANCE RAMPS OF THE REST AREAS IN PHASE III, STEP 3.

I-40/REST  
AREAS: STA. 109+00+/- (-EBL-) TO STA. 118+00+/- (-EBL-)  
STA. 134+00+/- (-EBL-) TO STA. 149+00+/- (-EBL-)  
STA. 105+00+/- (-WBL-) TO STA. 121+00+/- (-WBL-)  
STA. 138+00+/- (-WBL-) TO STA. 146+00+/- (-WBL-)

- PLACE INTERMEDIATE PAVEMENT MARKINGS, (6" PAINT ON BINDER COURSE AND 6" REMOVABLE TAPE ON STRUCTURES), ON THE NEWLY COMPLETED BINDER COURSE FOR THE INTERMEDIATE TRAFFIC PATTERN (ONE 12-FOOT TRAVEL LANE) AND 280-FOOT TAPERS IN EACH DIRECTION OF I-40 (-EBL-/-WBL-). SEE SHEET TCP-7, DETAIL 7.

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Tgermiller - AT - 11/22/23

APPROVED:  DATE: 7/24/04	<b>PROJECT PHASING</b>	
	SCALE: NONE	REVISIONS
	DATE: 8/04	
	DWG. BY: TAG	
	DESIGN BY: TAG	
REVIEWED BY: JWG 