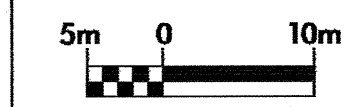


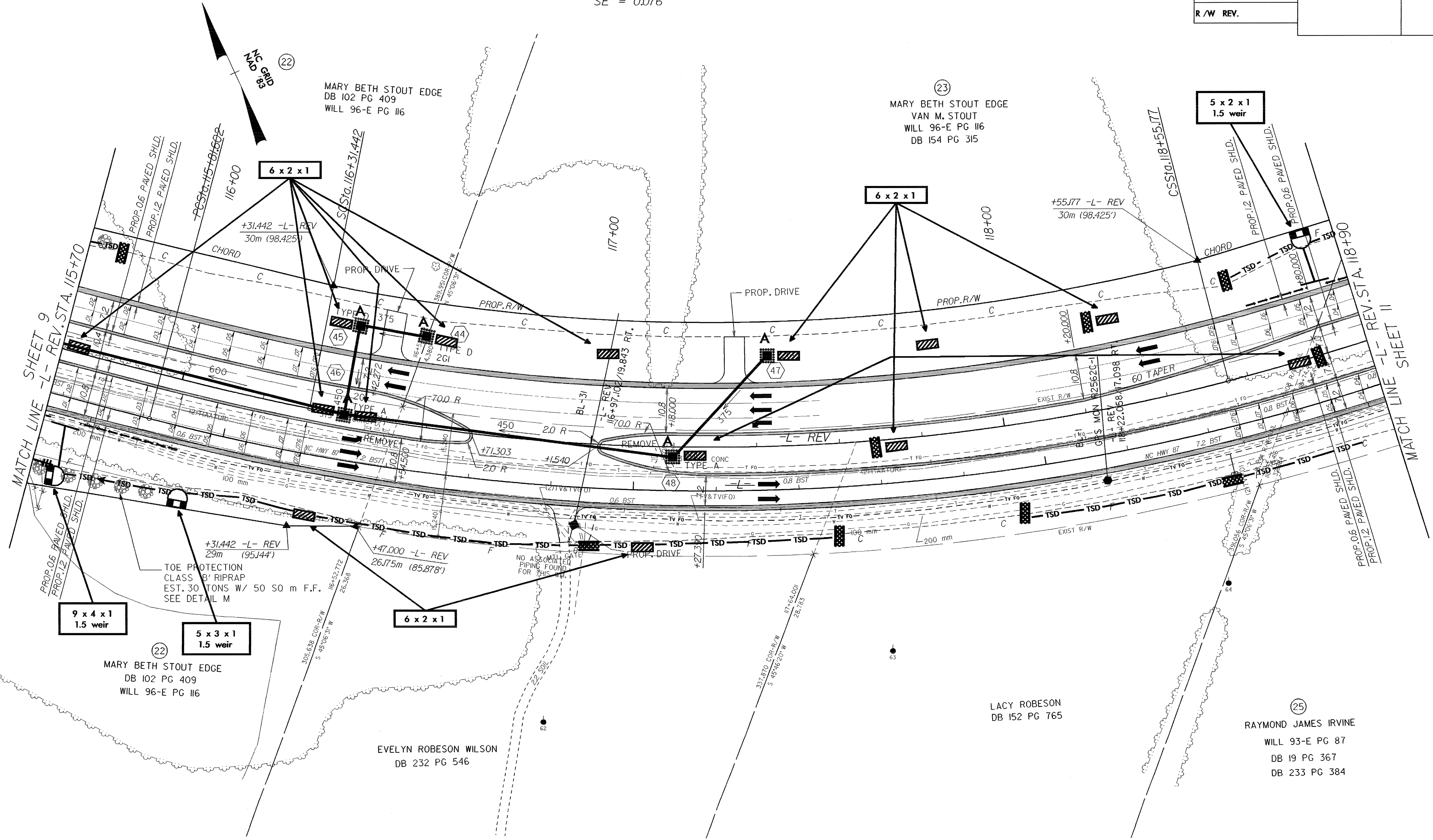


PROJECT REFERENCE NO. R-2562C	SHEET NO. EC-41/CONST.10
R/W SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER
CONST. REV.	
R/W REV.	



-L- REV CURVE DATA

PIs Sta 116+09.785 $\theta_s = 3^\circ 43' 27.2''$ $L_s = 65.000$ $LT = 43.343$ $ST = 21.675$	PI Sta 117+45.214 $\Delta = 25^\circ 38' 17.2''$ (LT) $L = 223.735$ $T = 113.772$ $R = 500.000$ $SE = 0.076$	PIs Sta 118+76.852 $\theta_s = 3^\circ 43' 27.2''$ $L_s = 65.000$ $LT = 43.343$ $ST = 21.675$
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- DRIVEWAY NOTES:**
1. ALL PROP. DRIVEWAY RADII 3.0m UNLESS OTHERWISE SHOWN.
 2. PROP. DRIVEWAY WIDTHS AT END OF RADII SHALL BE 4.9m MIN. UNLESS OTHERWISE SHOWN. TAPER DRIVEWAY TO EXISTING DRIVEWAY WIDTH.

SEE SHEET 35 FOR -L- REV. PROFILE

09/25/23
10.psh
dt-REV