

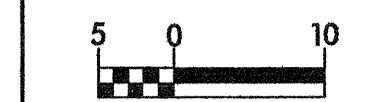
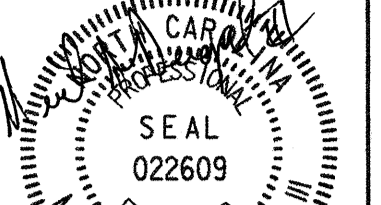
REVISIONS



PROJECT REFERENCE NO. R-2206B SHEET NO. 2-E

R/W SHEET NO.

ROADWAY DESIGN ENGINEER

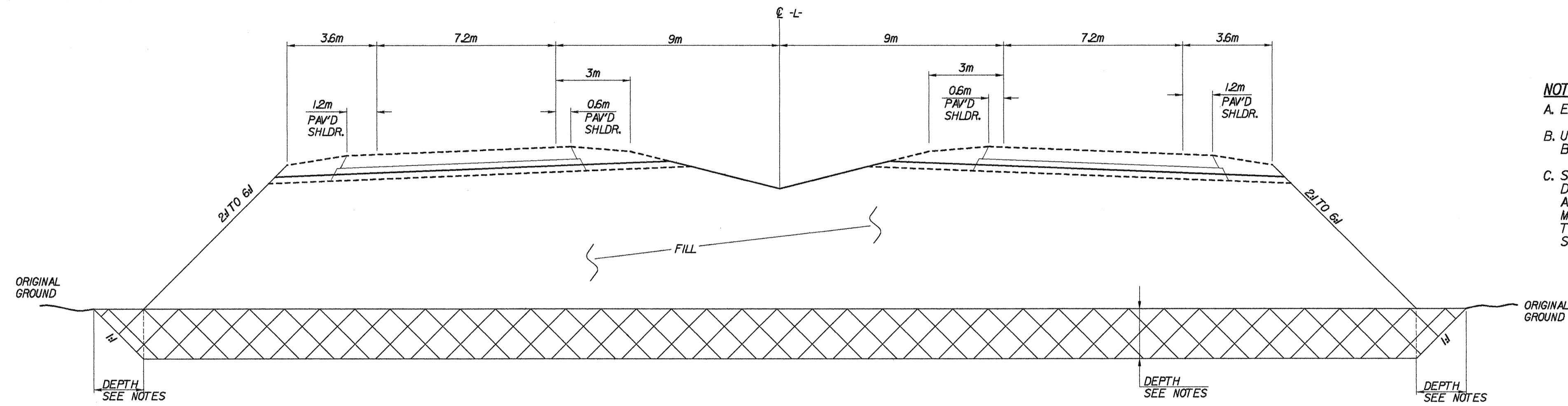


CONST. REV.

R/W REV.

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RALEIGH, N. C. 27602-2478

### TYPICAL UNDERCUT DETAIL



#### NOTES:

- A. EXACT DEPTH TO BE DETERMINED BY THE ENGINEER
- B. UNDERCUT TO BE BACKFILLED WITH MATERIAL AS DIRECTED BY THE ENGINEER.
- C. SOIL STABILIZATION FABRIC SHALL BE USED AT THE DISCRETION OF THE ENGINEER, IN LIEU OF UNDERCUTTING, AS A BRIDGE OVER LOOSE, POTENTIALLY COMPRESSIBLE MATERIAL. FABRIC SHALL BE PLACED FROM TOE OF SLOPE TO TOE OF SLOPE UNLESS IT IS IN THE BASE OF UNDERCUT SECTIONS.