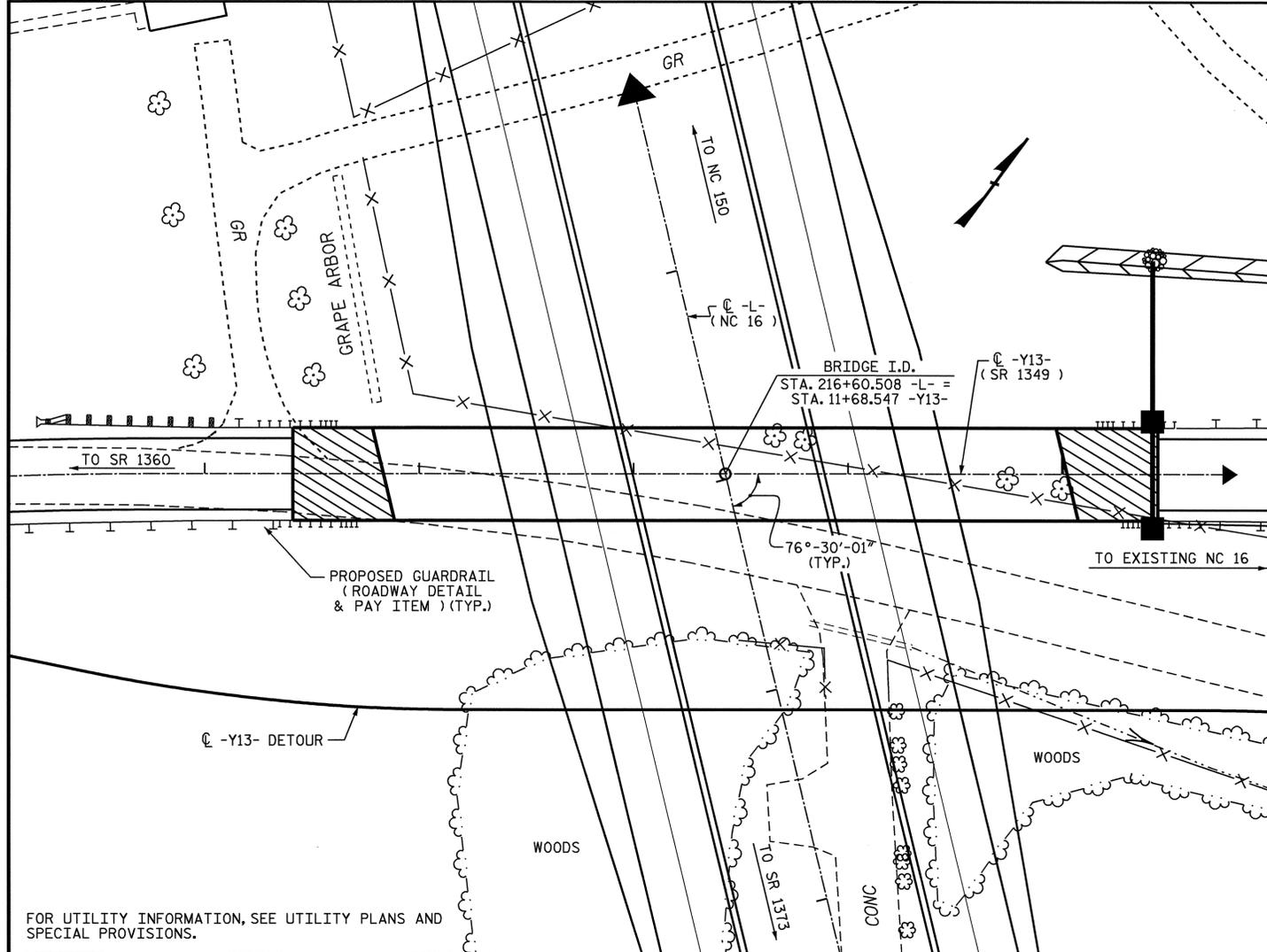


B.M. #3: TOP OF NAIL IN EAST BASE OF A 460mm HICKORY 75.230m LEFT OF -L- STA. 223+09.561 EL. 253.313



LOCATION SKETCH

NOTES

- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS ARE IN METERS.
- ASSUMED LIVE LOAD = MS 18 OR ALTERNATE LOADING.
- FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SNSM.
- THIS BRIDGE HAS BEEN DESIGNED BY THE STRENGTH DESIGN METHOD AS SPECIFIED IN AASHTO STANDARD SPECIFICATIONS.
- REMOVABLE FORMS MAY BE USED IN LIEU OF METAL STAY-IN-PLACE FORMS IN ACCORDANCE WITH ARTICLE 420-3 OF THE STANDARD SPECIFICATIONS.
- THIS BRIDGE HAS BEEN DESIGNED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AASHTO STANDARD SPECIFICATIONS FOR SEISMIC DESIGN OF HIGHWAY BRIDGES FOR SEISMIC PERFORMANCE CATEGORY A.
- THE CONTRACTOR SHALL PROVIDE INDEPENDENT ASSURANCE SAMPLES OF REINFORCING STEEL AS FOLLOWS: FOR PROJECTS REQUIRING UP TO 360,000 kg OF REINFORCING STEEL, ONE 760mm SAMPLE OF EACH SIZE BAR USED, AND FOR PROJECTS REQUIRING OVER 360,000 kg OF REINFORCING STEEL, TWO 760mm SAMPLES OF EACH SIZE BAR USED. THE BARS FROM WHICH THE SAMPLES ARE TAKEN MUST THEN BE SPLICED WITH REPLACEMENT BARS OF THE SIZE AND LENGTH OF THE SAMPLE PLUS A MINIMUM LAP SPLICE OF THIRTY BAR DIAMETERS.
- NEEDLE BEAMS WILL NOT BE ALLOWED UNLESS OTHERWISE CALLED FOR ON THE PLANS OR APPROVED BY THE ENGINEER.
- FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.
- FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.
- FOR MAINTENANCE OF TRAFFIC, SEE TRAFFIC CONTROL PLANS.
- PILES FOR END BENT NOS. 1 & 2 AND BENT NO. 1 SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 450 kN EACH.
- WAITING PERIOD FOR APPROACH SLAB CONSTRUCTION AT END BENT NO. 1 SHALL BE WAIVED.
- THE CONTRACTOR SHALL OBSERVE A ONE MONTH WAITING PERIOD BEFORE BEGINNING ANY WORK FOR END BENT CONSTRUCTION AFTER COMPLETION OF THE EMBANKMENT AT END BENT NO. 2. THE CONTRACTOR MAY BEGIN THE REINFORCED BRIDGE APPROACH FILL CONSTRUCTION AFTER COMPLETION OF END BENT INCLUDING WINGWALLS.
- WHEN DRIVING PILES, THE MAXIMUM BLOW COUNT SHALL NOT BE EXCEEDED.
- THE CONTRACTOR IS ADVISED OF PRESENCE OF SOFT WEATHERED ROCK AT ELEVATION 259.0m± AT END BENT NO. 2 LEFT SIDE.
- THE CLASS AA CONCRETE IN THE BRIDGE DECK SHALL CONTAIN FLY ASH OR GROUND GRANULATED BLAST FURNACE SLAG AT THE SUBSTITUTION RATE SPECIFIED IN ARTICLE 1024-1 AND IN ACCORDANCE WITH ARTICLES 1024-5 AND 1024-6 OF THE STANDARD SPECIFICATIONS. NO PAYMENT WILL BE MADE FOR THIS SUBSTITUTION AS IT IS CONSIDERED INCIDENTAL TO THE COST OF THE REINFORCED CONCRETE DECK SLAB.
- FOR METRIC STRUCTURAL STEEL, SEE SPECIAL PROVISIONS.
- WORK SHALL NOT BE STARTED ON THIS BRIDGE UNTIL ROADWAY SECTION HAS BEEN EXCAVATED.
- FOR FABRICATED METAL STAY-IN-PLACE FORMS, SEE SPECIAL PROVISIONS.

TOTAL BILL OF MATERIAL

	FOUNDATION EXCAVATION	REINFORCED CONCRETE DECK SLAB	GROOVING BRIDGE FLOORS	CLASS A CONCRETE	BRIDGE APPROACH SLABS	REINFORCING STEEL	SPIRAL COLUMN REINFORCING STEEL	1600mm MODIFIED PRESTRESSED CONCRETE GIRDERS	HP 310 X 79 STEEL PILES	HP 360 X 108 STEEL PILES	CONCRETE BARRIER RAIL	100mm SLOPE PROTECTION	ELASTOMERIC BEARINGS	EVAZOTE JOINT SEALS
	LUMP SUM	SQ. METERS	SQ. METERS	CU. METERS	LUMP SUM	kg	kg	NO. METERS	NO. METERS	NO. METERS	METERS	SQ. METERS	LUMP SUM	LUMP SUM
SUPERSTRUCTURE		607.4	615.8		LUMP SUM			8 250.200			126.674		LUMP SUM	LUMP SUM
END BENT #1				23.7		2,124			12 168.0			161.5		
BENT #1	LUMP SUM			36.1		3,506	464			18 396.0				
END BENT #2				22.8		2,091			12 186.0			116.8		
TOTAL	LUMP SUM	607.4	615.8	82.6	LUMP SUM	7,721	464	8 250.200	24 354.0	18 396.0	126.674	278.3	LUMP SUM	LUMP SUM

PROJECT NO. R-2206C  
 LINCOLN-CATAWBA COUNTY  
 STATION: 216+60.508 -L-  
 11+68.547 -Y13-  
 SHEET 3 OF 3

STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH

GENERAL DRAWING  
 FOR BRIDGE ON SR 1349  
 (MUNDY RD.) OVER  
 NC 16 BETWEEN SR 1360  
 AND EXISTING NC 16



REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	TOTAL SHEETS	
1			3			3	104
2			4			2	274

DRAWN BY : T.L. AVERETTE DATE : 04-04  
 CHECKED BY : PEGGY ADKINS DATE : 07-04