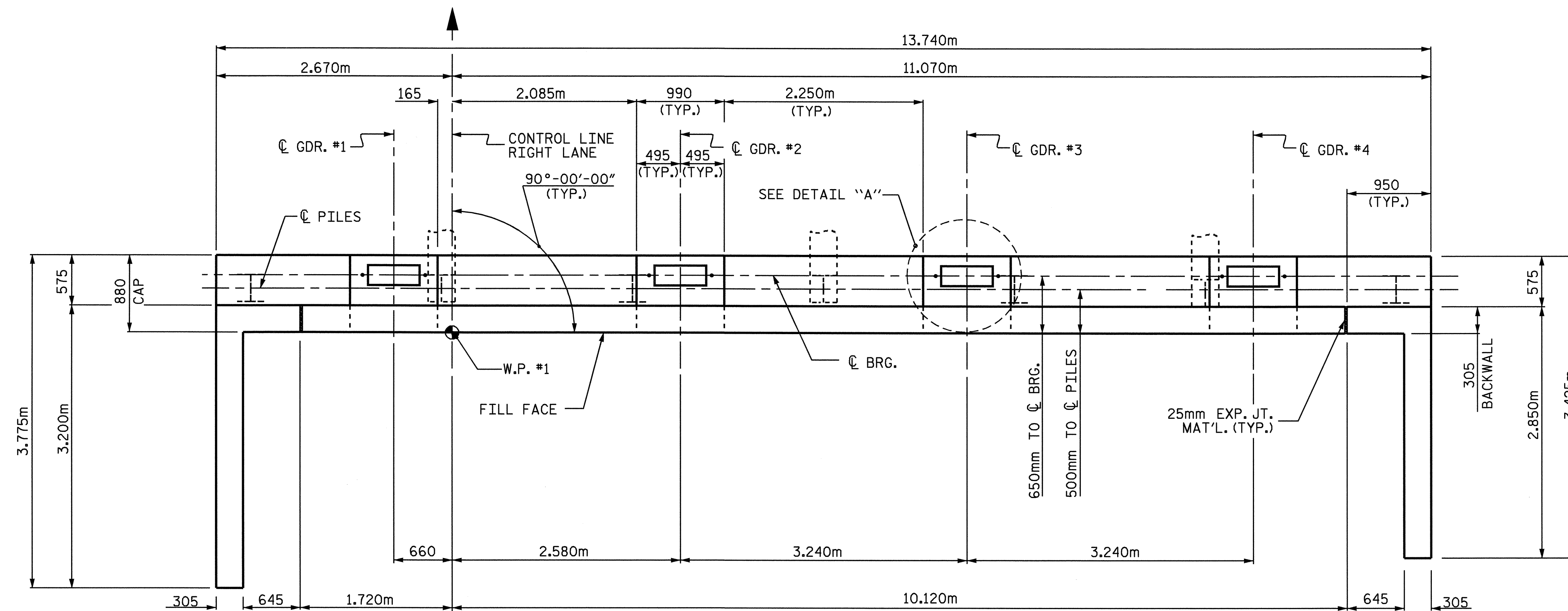
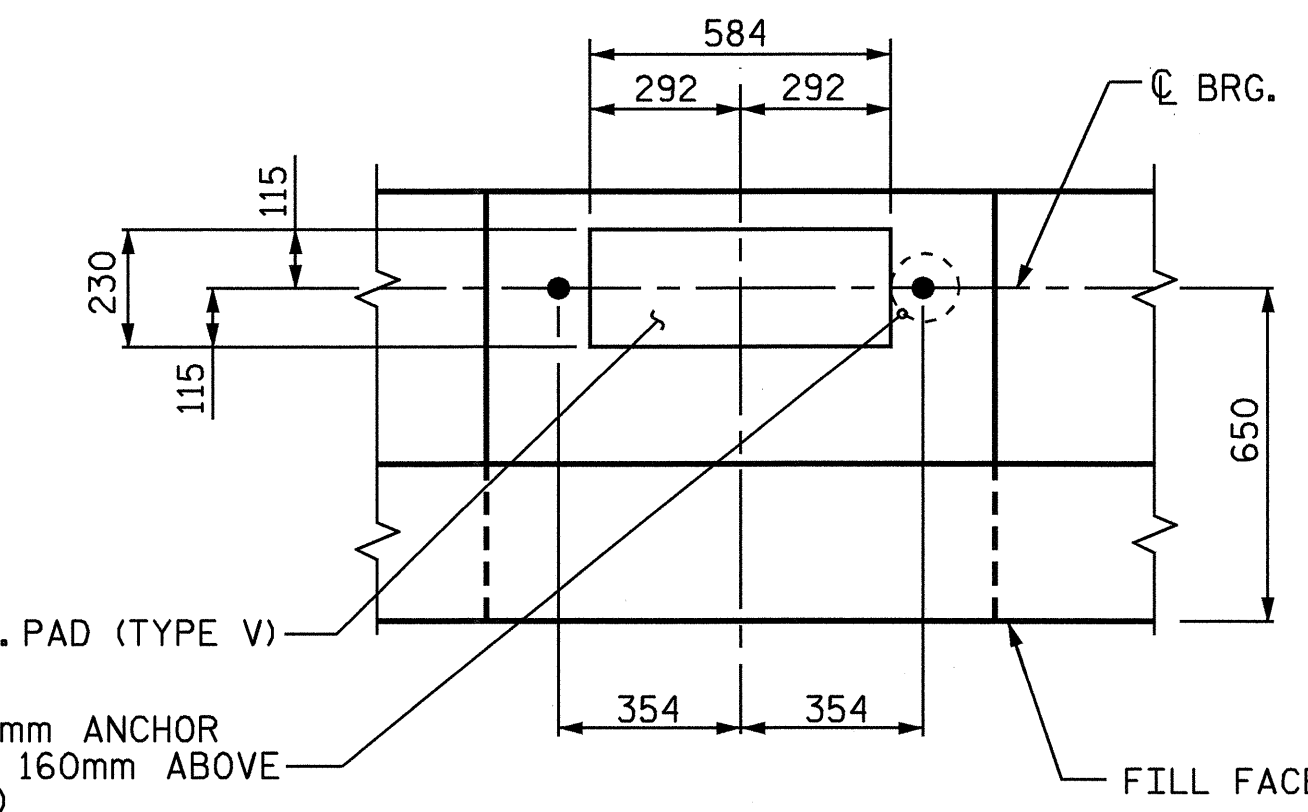


**NOTES**

- STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO ANCHOR BOLTS.
- FOR EPOXY PROTECTIVE COATINGS, SEE SPECIAL PROVISIONS.
- BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.
- THE TOP SURFACE OF THE END BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.
- THE TOP SURFACE OF THE CAP EXCEPT THE BRIDGE SEAT BUILD-UPS SHALL BE SLOPED TRANSVERSLY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.
- THE CONTRACTOR SHALL PROVIDE FOR INSTALLATION OF THE 102mm DIAMETER DRAIN PIPE THROUGH THE WING WALL AS REQUIRED FOR REINFORCED BRIDGE APPROACH FILLS, SEE THE ROADWAY PLANS. REINFORCING STEEL IN THE WING WALL MAY BE SHIFTED AS NECESSARY TO CLEAR THE DRAIN PIPE.

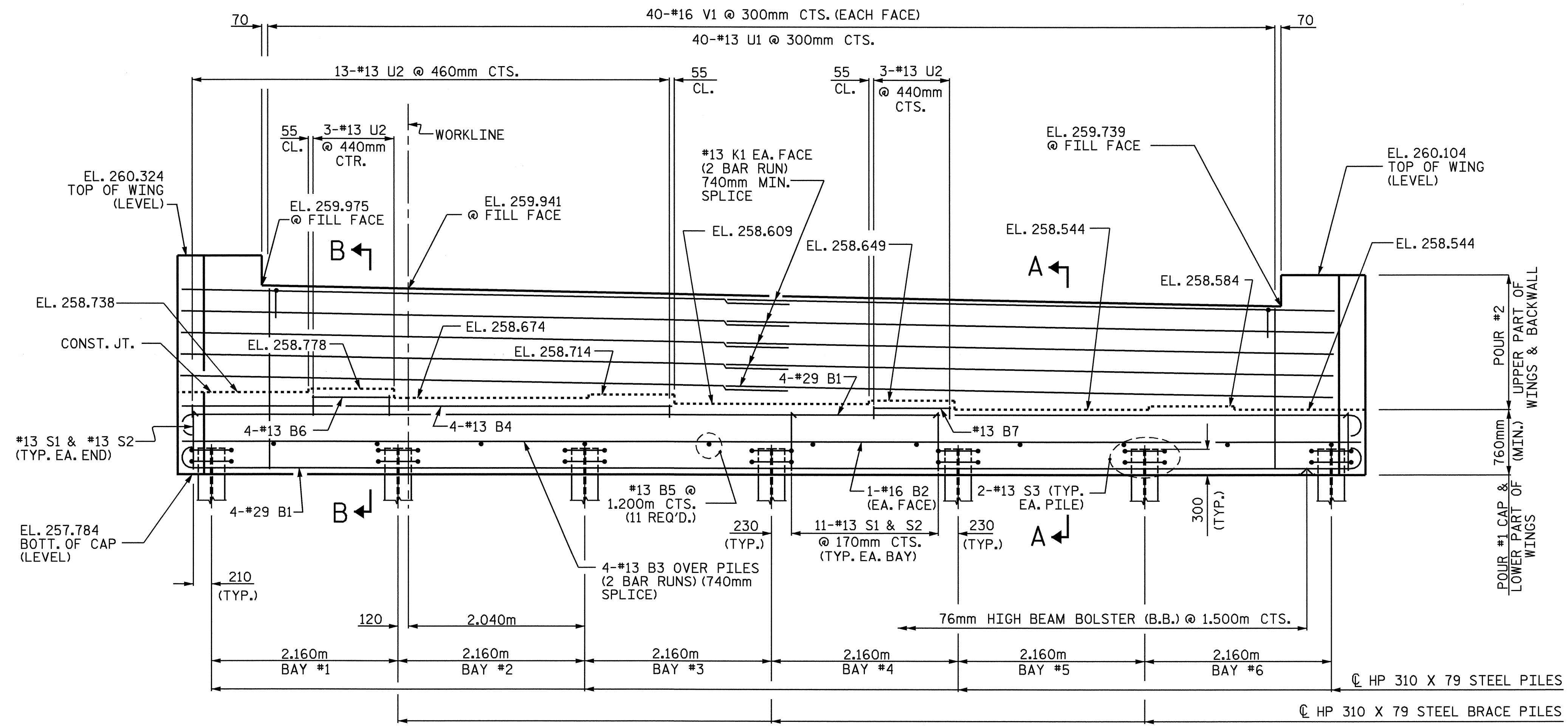


**PLAN**



**DETAIL "A"**  
(TYP. EA. GDR.)

584mm x 230mm ELASTOMERIC BRG. PAD (TYPE V)  
50.80mm Ø X 620mm ANCHOR BOLT TO PROJECT 160mm ABOVE TOP OF CAP (TYP.)

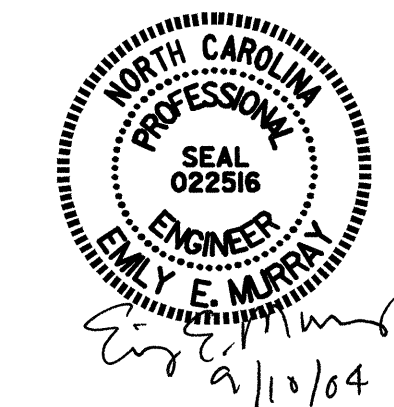


**ELEVATION**

PROJECT NO. R-2206C  
LINCOLN-CATAWBA COUNTY  
STATION: 223+03.850-L-

SHEET 1 OF 3

STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION  
RALEIGH  
SUBSTRUCTURE  
END BENT #1  
RIGHT LANE



REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	S-181	
1			3			TOTAL SHEETS	374
2			4				

DRAWN BY: M.D.PISO DATE: 03/2004  
CHECKED BY: P.ADKINS DATE: 07/2004