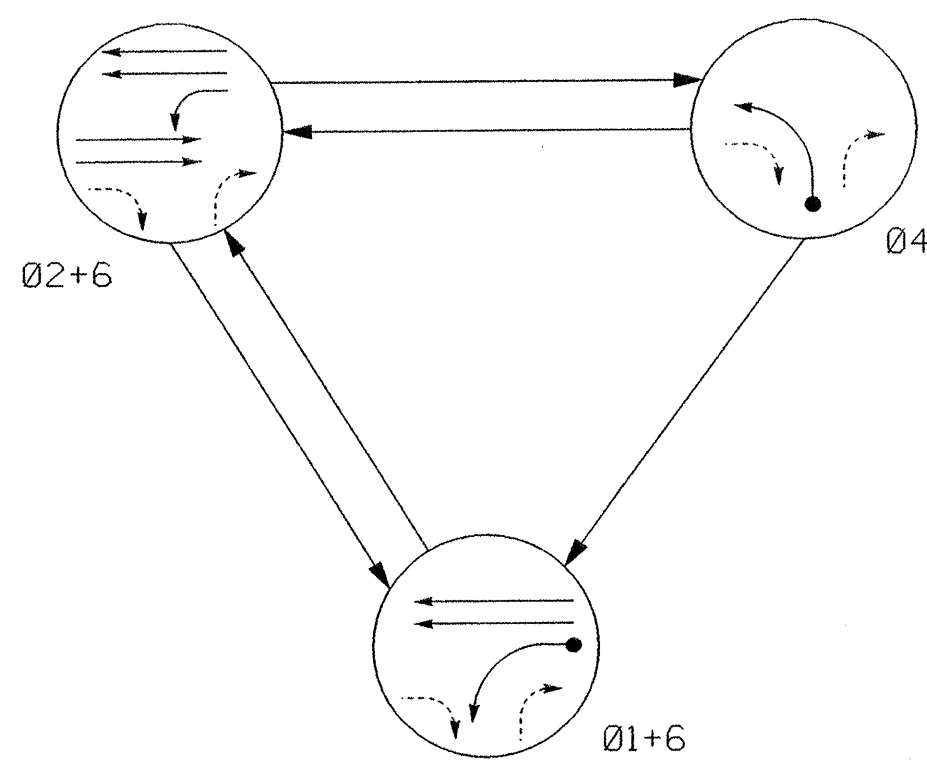


PHASING DIAGRAM

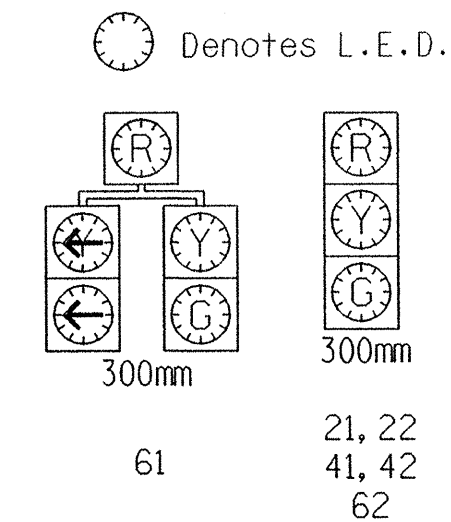


SIGNAL FACE	PHASE			
	Ø 1 + 6	Ø 2 + 6	Ø 4	FLUSH
21,22	R	G	R	Y
41,42	R	R	G	R
61	G	G	R	Y
62	G	G	R	Y

PHASING DIAGRAM DETECTION LEGEND

- ←●→ DETECTED MOVEMENT
- ←○→ UNDETECTED MOVEMENT (OVERLAP)
- UNSIGNALIZED MOVEMENT
- PEDESTRIAN MOVEMENT

SIGNAL FACE I.D.

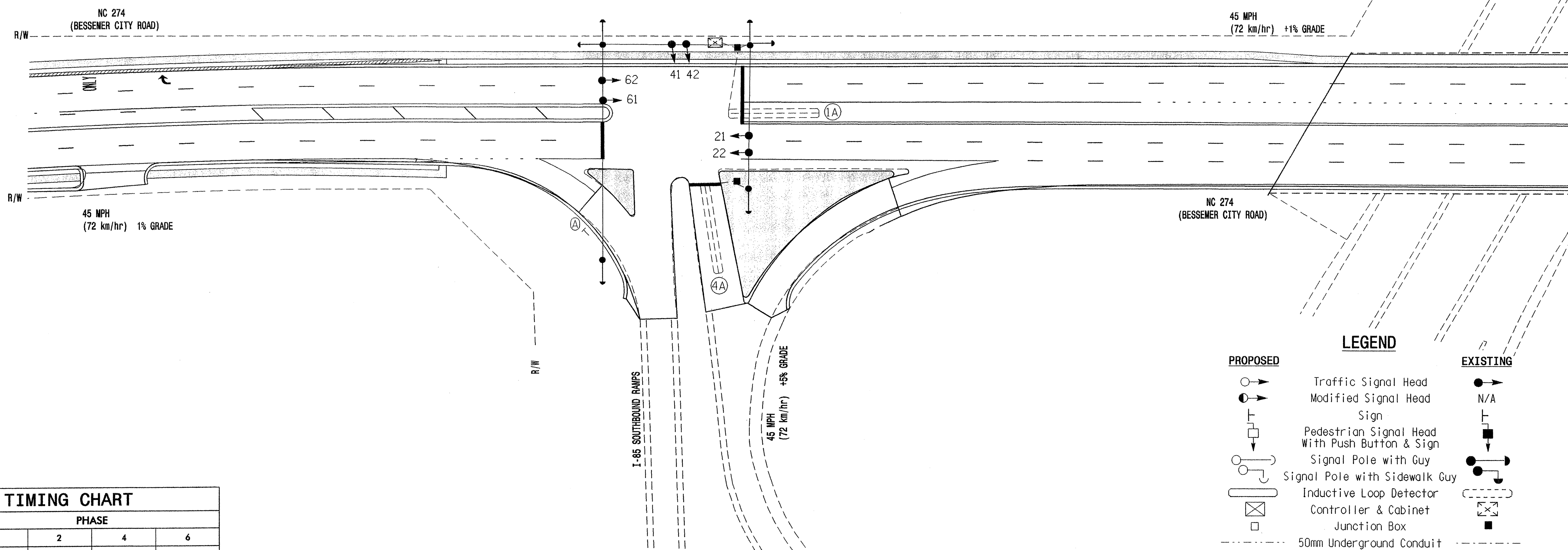


PLAN QUANTITIES	
Pay Item	Meters
Signal Cable	-
Messenger Cable	-
Loop Lead-in Cable	-

3 PHASE SEMI ACTUATED ISOLATED

NOTES

1. Refer to "Roadway Standard Drawings NCDOT" dated January 2002 and "Standard Specifications for Roads and Structures" dated January 2002.
2. Set all detector units to presence mode.
3. Do not program for late night flashing operation unless otherwise directed by the engineer.



LEGEND

- | PROPOSED | EXISTING |
|--|------------------------------|
| ○ → Traffic Signal Head | ● → N/A |
| ○ → Modified Signal Head | ○ → N/A |
| ○ → Sign | ○ → N/A |
| ○ → Pedestrian Signal Head With Push Button & Sign | ○ → N/A |
| ○ → Signal Pole with Guy | ○ → N/A |
| ○ → Signal Pole with Sidewalk Guy | ○ → N/A |
| □ → Inductive Loop Detector | □ → N/A |
| □ → Controller & Cabinet | □ → N/A |
| □ → Junction Box | □ → N/A |
| --- 50mm Underground Conduit | --- 50mm Underground Conduit |
| N/A Right of Way with Marker | △ → Right of Way with Marker |
| → Directional Arrow | → Directional Arrow |
| → Pavement Marking Arrow | → Pavement Marking Arrow |
| ⊙ "YIELD" Sign (R1-2) | ⊙ "YIELD" Sign (R1-2) |

2070L TIMING CHART

FEATURE	PHASE			
	1	2	4	6
Min Green 1 *	7	12	7	12
Extension 1 *	1.0	0.0	1.0	0.0
Max Green 1 *	20	45	15	45
Yellow Clearance	4.0	4.7	4.0	4.7
Red Clearance	2.0	1.5	1.5	1.5
Walk 1 *	-	-	-	-
Don't Walk 1	-	-	-	-
Seconds Per Actuation *	-	-	-	-
Max Variable Initial *	-	-	-	-
Time Before Reduction *	-	-	-	-
Time To Reduce *	-	-	-	-
Minimum Gap	-	-	-	-
Recall Mode	-	MAX RECALL	-	MAX RECALL
Vehicle Call Memory	-	YELLOW	-	YELLOW
Dual Entry	-	-	-	-
Simultaneous Gap	ON	ON	ON	ON

2070L LOOP & DETECTOR INSTALLATION

LOOP	INDUCTIVE LOOPS			DETECTOR PROGRAMMING								
	SIZE (m)	TURNS	DISTANCE FROM STOPBAR (m)	NEW LOOP	PHASE	CALLING	EXTENSION	FULL TIME DELAY	SYSTEM LOOP	STRETCH TIME	DELAY TIME	NEW CARD
1A	1.8 X 18	2-4-2	+2	-	1	Y	Y	-	-	-	15	-
4A	1.8 X 18	2-4-2	0	-	4	Y	Y	-	-	-	-	-

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.

Prepared in the Office of:
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 Raleigh, North Carolina 27609

SIGNAL UPGRADE - TEMPORARY DESIGN 2

	N.C. 274 (BESSEMER CITY ROAD) AT I-85 SOUTHBOUND RAMP & LOOP		SEAL NORTH CAROLINA PROFESSIONAL ENGINEER 027742 ROBERT J. DUBNICKA
	DIVISION 12 GASTON COUNTY GASTONIA PLAN DATE: OCT. 30, 2003 PREPARED BY: RGL	REVIEWED BY: R. DUBNICKA, P.E. REVIEWED BY:	
SCALE 5 0 10 1:500	REVISIONS		