

+0.5671%  $\Delta$  -2.3702%

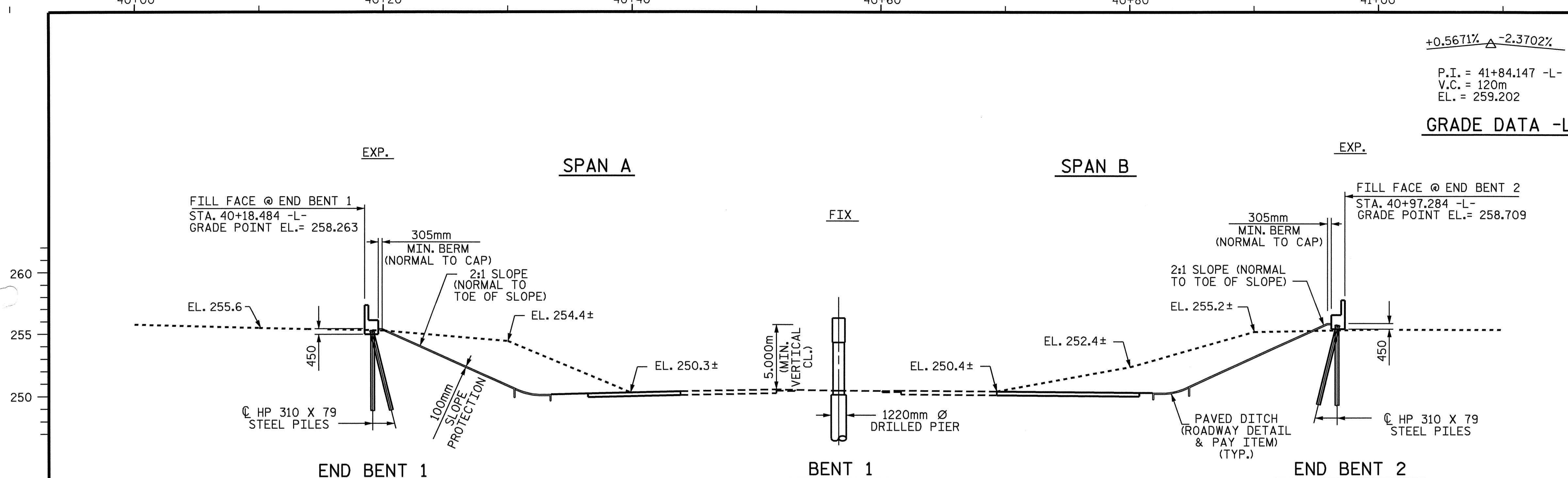
P.I. = 41+84.147 -L-  
V.C. = 120m  
EL. = 259.202

GRADE DATA -L-

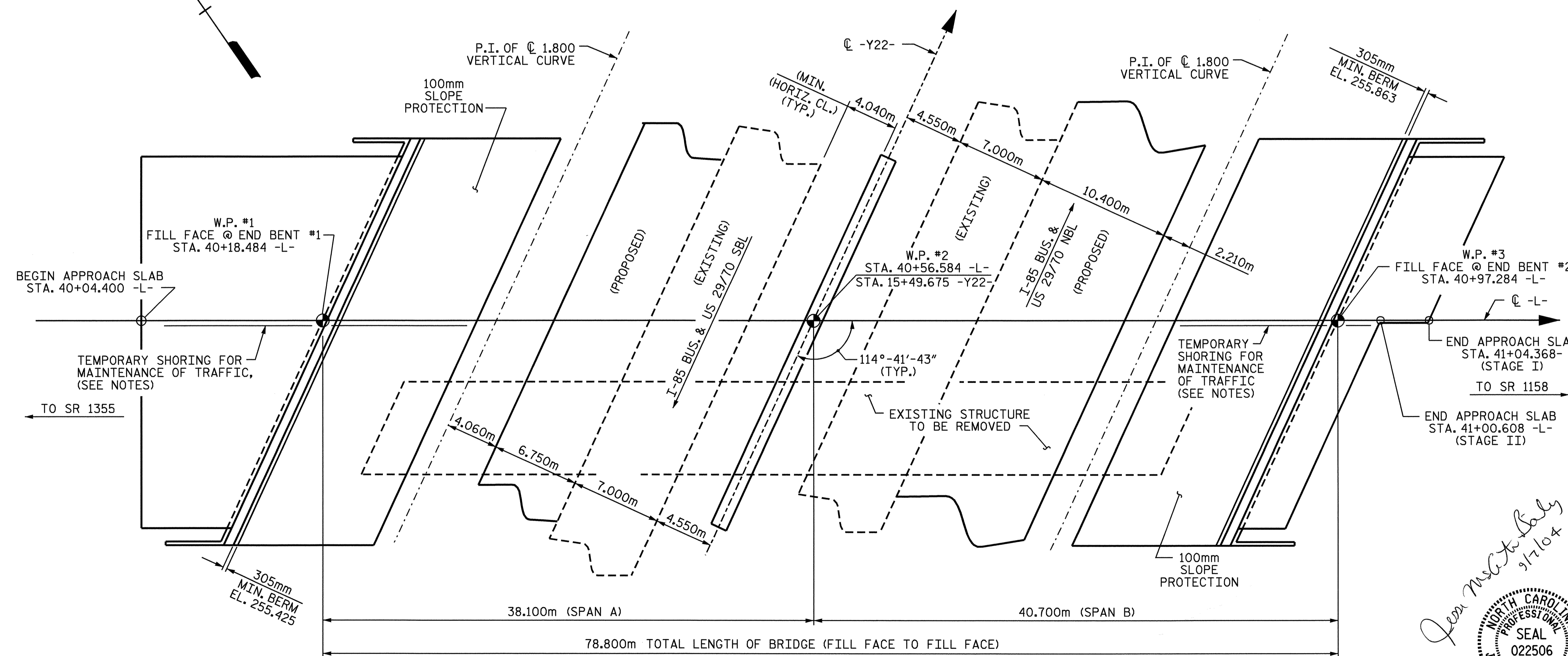
NOTES

- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS ARE IN METERS.
- ASSUMED LIVE LOAD = MS 18 OR ALTERNATE LOADING.
- FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SNSM.
- THIS BRIDGE HAS BEEN DESIGNED BY THE STRENGTH DESIGN METHOD AS SPECIFIED IN AASHTO STANDARD SPECIFICATIONS.
- ALL STRUCTURAL STEEL SHALL BE AASHTO M270 GRADE 345W AND PAINTED IN ACCORDANCE WITH SYSTEM 4 OF ARTICLE 442-7 OF THE STANDARD SPECIFICATIONS UNLESS OTHERWISE NOTED ON THE PLANS.
- REMOVABLE FORMS MAY BE USED IN LIEU OF METAL STAY-IN-PLACE FORMS IN ACCORDANCE WITH ARTICLE 420-3 OF THE STANDARD SPECIFICATIONS.
- PILES AT END BENT NOS. 1 & 2 SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 530 kN EACH.
- WHEN DRIVING PILES, THE MAXIMUM BLOW COUNT SHALL NOT BE EXCEEDED.
- THE DRILLED PIERS AT BENT NO. 1 HAVE BEEN DESIGNED FOR BOTH SKIN FRICTION AND TIP BEARING. THE REQUIRED TIP BEARING CAPACITY IS 1,900 kPG.
- THE REQUIRED TIP BEARING AT BENT NO. 1 SHALL BE VERIFIED.
- DRILLED PIERS FOR BENT NO. 1 HAVE BEEN DESIGNED FOR AN APPLIED LOAD OF 4,326 kN EACH AT THE TOP OF THE COLUMN.
- PERMANENT STEEL CASING IS NOT REQUIRED FOR DRILLED PIERS AT BENT NO. 1.
- DRILLED PIERS AT BENT NO. 1 SHALL EXTEND TO AN ELEVATION NO HIGHER THAN 236.0 m AND SATISFY THE REQUIRED TIP BEARING CAPACITY.
- FOR DRILLED PIERS, SEE SPECIAL PROVISIONS.
- SPT TESTING IS NOT REQUIRED TO DETERMINE THE TIP BEARING CAPACITY OF THE DRILLED PIERS AT BENT NO. 1.
- SLURRY CONSTRUCTION SHALL NOT BE USED FOR THIS PROJECT.
- SID INSPECTIONS ARE NOT REQUIRED TO DETERMINE THE BOTTOM CLEANLINESS OF THE DRILLED PIERS AT BENT NO. 1.
- CSL TUBES ARE REQUIRED AND CSL TESTING MAY BE REQUIRED FOR THE DRILLED PIERS AT BENT NO. 1. SEE SPECIAL PROVISION FOR CROSSHOLE SONIC LOGGING.
- THE BEARING CAPACITY OF DRILLED PIERS NO. 1, 3, AND 6 AT BENT NO. 1 SHALL BE VERIFIED BY DRILLING A 38 mm DIAMETER TEST HOLE 1.83 METERS BELOW THE SHAFT TIP ELEVATION IN ACCORDANCE WITH THE DRILLED PIER SPECIAL PROVISION, SECTION 4.0 (INSPECTION METHODS AND REQUIREMENTS), PART B (BEARING CAPACITY), NUMBER 2.
- THE LOCATION OF THE CONSTRUCTION JOINT IN THE DRILLED PIERS IS BASED ON AN APPROXIMATE GROUND LINE ELEVATION. IF THE CONSTRUCTION JOINT IS ABOVE THE ACTUAL GROUND ELEVATION, THE CONTRACTOR SHALL PLACE THE CONSTRUCTION JOINT 300mm BELOW THE GROUND LINE.

(NOTES CONTINUED ON SHEET 3 OF 3)



SECTION ALONG C -L-  
(BENTS & END BENTS ARE SHOWN AT RIGHT ANGLES)



PLAN  
(PILES AND FOOTINGS ARE NOT SHOWN FOR CLARITY)

PROJECT NO. U-2717  
GUILFORD COUNTY  
STATION: 40+56.584 -L-  
SHEET 1 OF 3 REPLACES BRIDGE NO. 218.

STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION  
RALEIGH  
GENERAL DRAWING  
FOR BRIDGE ON SR 1113  
(KIVETT DR.) OVER I/85 BUS.  
BETWEEN SR 1355  
AND SR 1158

REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	SHEET NO.	
1			3			5-1	
2			4			TOTAL SHEETS 47	

DRAWN BY : J.B. WILSON/GMP DATE : 9/03  
CHECKED BY : D.A. GLADDEN DATE : 10/03

07-SEP-2004 10:42  
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mpatterson

*John McArthur Bailey*  
9/7/04

