

NOTES

ALL STRUCTURAL STEEL SHALL BE AASHTO M270 GRADE 345W AND PAINTED IN ACCORDANCE WITH SYSTEM 4 OF ARTICLE 442-7 OF THE STANDARD SPECIFICATIONS UNLESS OTHERWISE NOTED ON THE PLANS.

ALL DIMENSIONS SHOWN ARE HORIZONTAL OR VERTICAL, UNLESS OTHERWISE NOTED.

ALL FIELD CONNECTIONS TO BE 22.23mm DIA. HIGH STRENGTH BOLTS UNLESS OTHERWISE NOTED.

BEARING STIFFENERS ARE TO BE PLACED NORMAL TO THE WEB OF THE GIRDER AND SHALL BE PLUMB.

SHOP SPLICES ARE PERMITTED TO LIMIT THE MAXIMUM REQUIRED FLANGE PIECE LENGTHS TO 18 METERS AND WEB PIECE LENGTHS TO 14 METERS. PERMITTED FLANGE AND WEB SHOP SPLICES SHALL NOT BE LOCATED WITHIN 4.5 METERS OF MAXIMUM DEAD LOAD DEFLECTION (NOR WITHIN 4.5 METERS OF INTERMEDIATE BEARINGS OF CONTINUOUS UNITS). KEEP 600mm MINIMUM BETWEEN WEB AND FLANGE SHOP SPLICES. KEEP 150mm MINIMUM BETWEEN CONNECTOR PLATE OR TRANSVERSE STIFFENER WELDS AND WEB OR FLANGE SHOP SPLICES.

STUDS ON GIRDERS MAY BE SHIFTED UP TO 25mm IF NECESSARY TO CLEAR FLANGE SPLICE WELD.

ENDS OF BEAMS AND GIRDERS SHALL BE PLUMB.

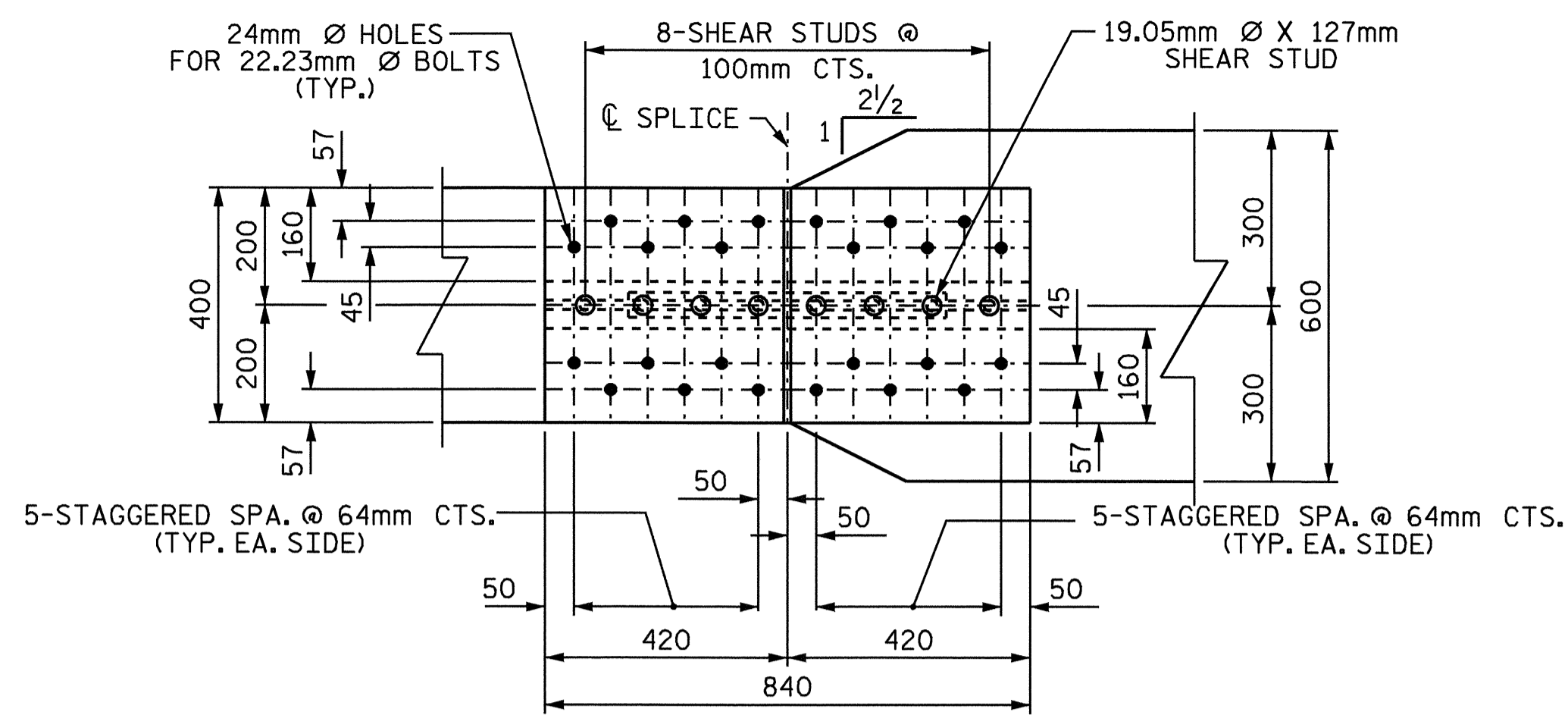
CHARPY V-NOTCH TEST IS REQUIRED ON ALL GIRDER SECTIONS, COVER PLATES AND SPLICE PLATES AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 1072-9 OF THE STANDARD SPECIFICATIONS.

CAMBERED GIRDER LENGTHS SHALL BE ADJUSTED AND BEARINGS ARE TO BE PLACED ON THE CAMBERED GIRDER SO AS TO BE ALIGNED WITH THE ANCHORS AFTER THE DEAD LOAD DEFLECTION HAS OCCURRED. SHOP DRAWINGS SHALL BE PREPARED ACCORDINGLY.

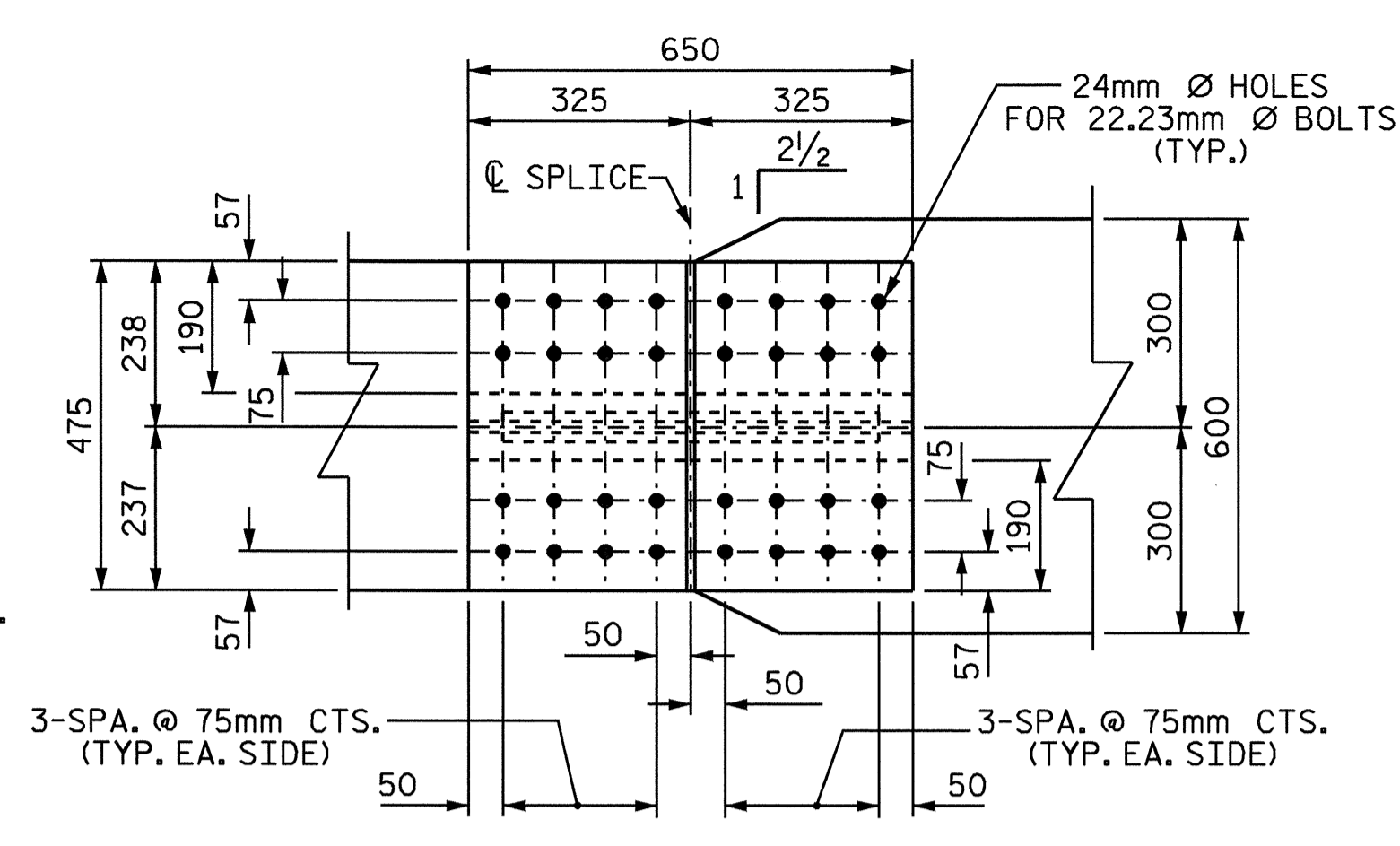
TENSION ON THE AASHTO M164 BOLTS SHALL BE CALIBRATED USING DIRECT TENSION INDICATOR WASHERS IN ACCORDANCE WITH ARTICLE 440-10 OF THE STANDARD SPECIFICATIONS.

BEARING STIFFENER MAY REQUIRE COPING IF WIDER THAN BOTTOM FLANGE TO AVOID INTERFERENCE WITH THE ANCHOR BOLT.

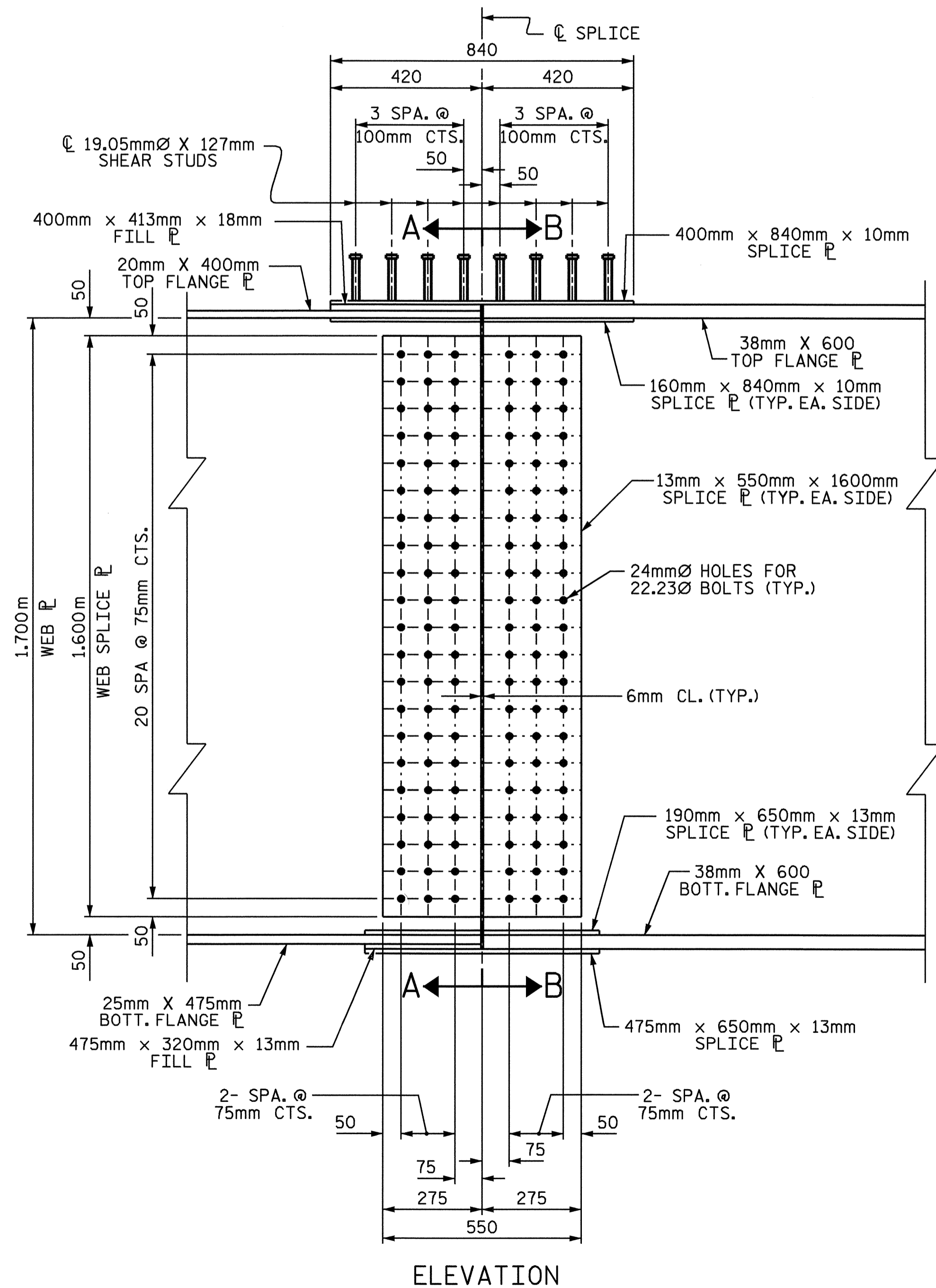
NUTS ON BOLTS FOR CONNECTING DIAPHRAGM TO CONNECTOR PLATE SHALL BE LEFT LOOSE FOR PURPOSE OF ADJUSTMENT UNTIL BOTH SIDES OF SLAB HAVE BEEN POURED.



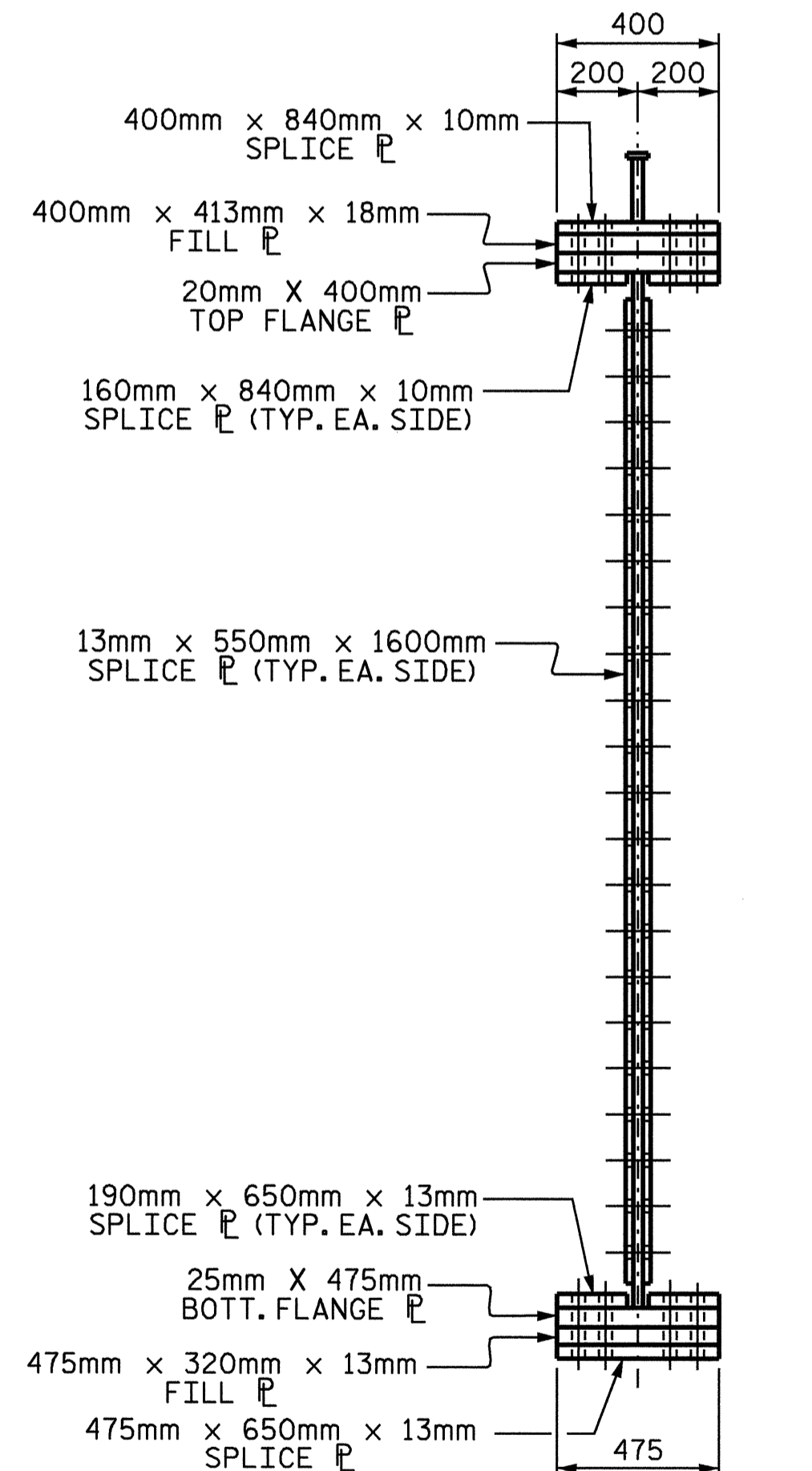
PLAN - TOP FLANGE



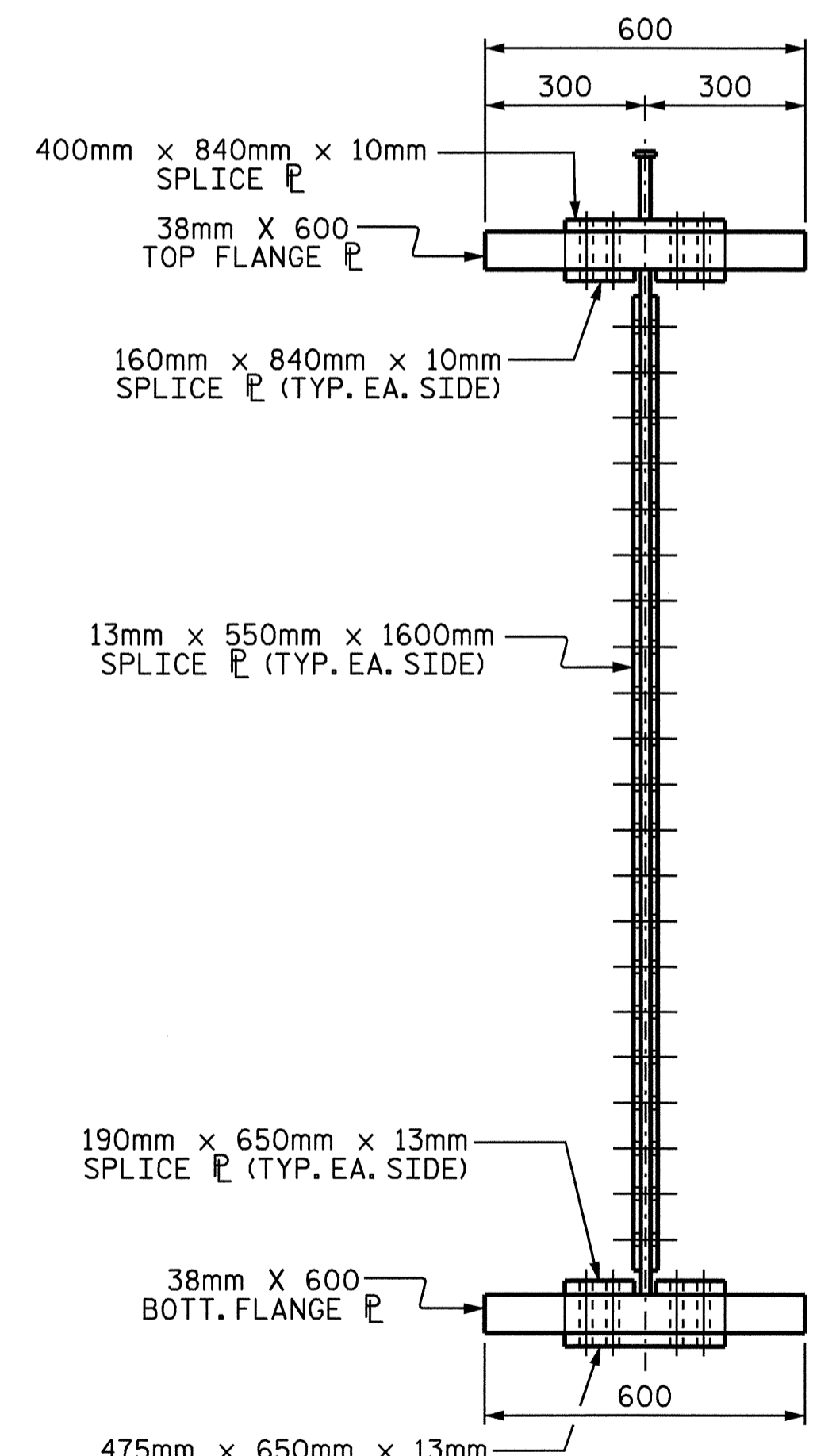
PLAN - BOTTOM FLANGE



ELEVATION



SECTION "A-A"



SECTION "B-B"

DRAWN BY : M. G. SHAIKH DATE : 7-13-04
 CHECKED BY : D. A. GLADDEN DATE : 08-04

08-SEP-2004 11:56
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 mshalkh

PROJECT NO. U-2717
 GUILFORD COUNTY
 STATION: 40+56.584 -L-

SHEET 4 OF 4

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

SUPERSTRUCTURE
 STRUCTURAL STEEL
 BOLTED FIELD
 SPLICE DETAILS



REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	5-17	
1			3			TOTAL SHEETS	
2			4			47	

STR #1