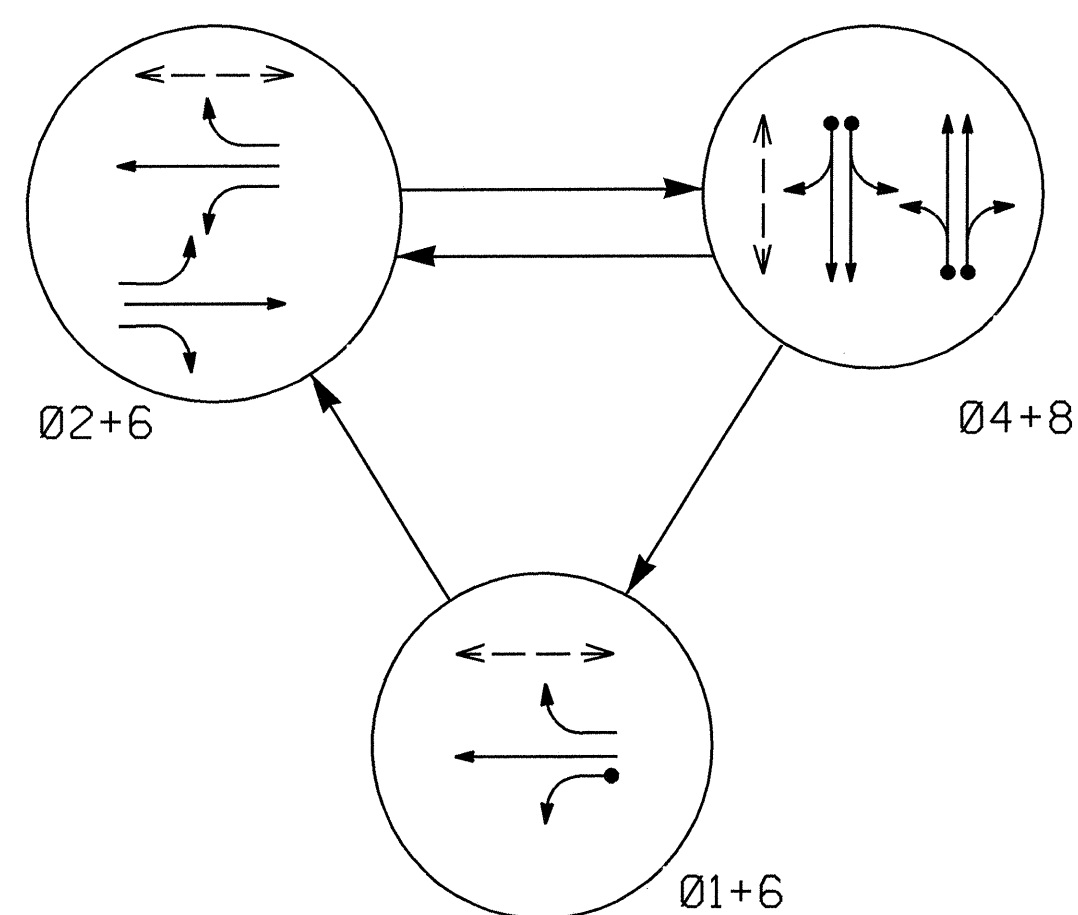
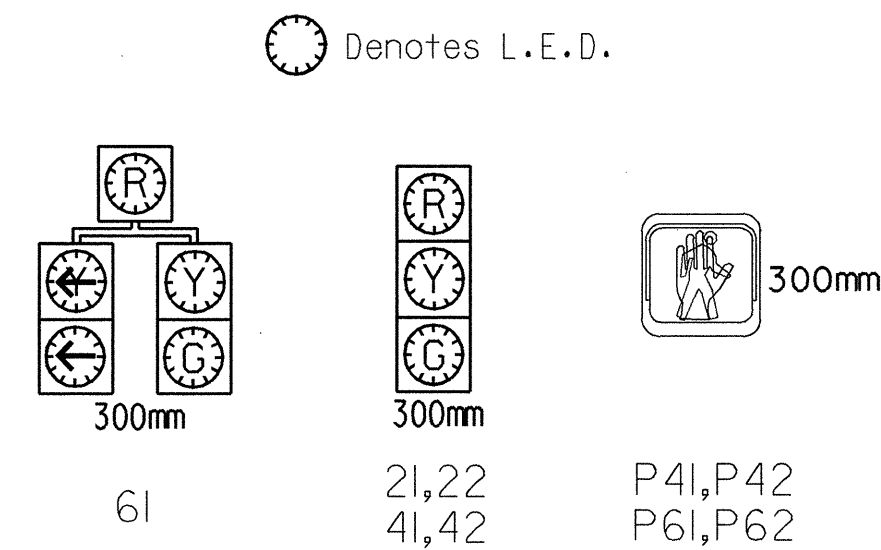


**PHASING DIAGRAM**



SIGNAL FACE	PHASE				
	Ø1+6	Ø2+6	Ø4+8	FLASH	
21,22	R	G	R	Y	
41,42	R	R	G	R	
61		G	R	Y	
62	G	G	R	Y	
81,82	R	R	G	R	
P41,P42	DW	DW	W	DRK	
P61,P62	W	W	DW	DRK	

**SIGNAL FACE I.D.**



LOOP & DETECTOR UNIT INSTALLATION CHART										
PEEK TS-2 CONTROLLER AND CABINET										
LOOP NO.	SIZE (m)	TURNS	DIST. FROM STOPBAR (m)	NEW EXISTING	NEMA PHASE	NEW EXISTING	TIMING		PLACE CALL DURING PHASE	INHIBIT DELAY DURING GREEN?
							FEATURE	TIME		
1A	1.8X18	2-4-2	0	X	Ø1	X	DELAY	15 SEC.	ALL	YES
4A	1.8X18	2-4-2	0	X	Ø4	X	DELAY	3 SEC.	ALL	YES
4B	1.8X18	2-4-2	0	X	Ø4	X	DELAY	10 SEC.	ALL	YES
8A	1.8X18	2-4-2	0	X	Ø8	X	DELAY	3 SEC.	ALL	YES
8B	1.8X18	2-4-2	0	X	Ø8	X	DELAY	10 SEC.	ALL	YES

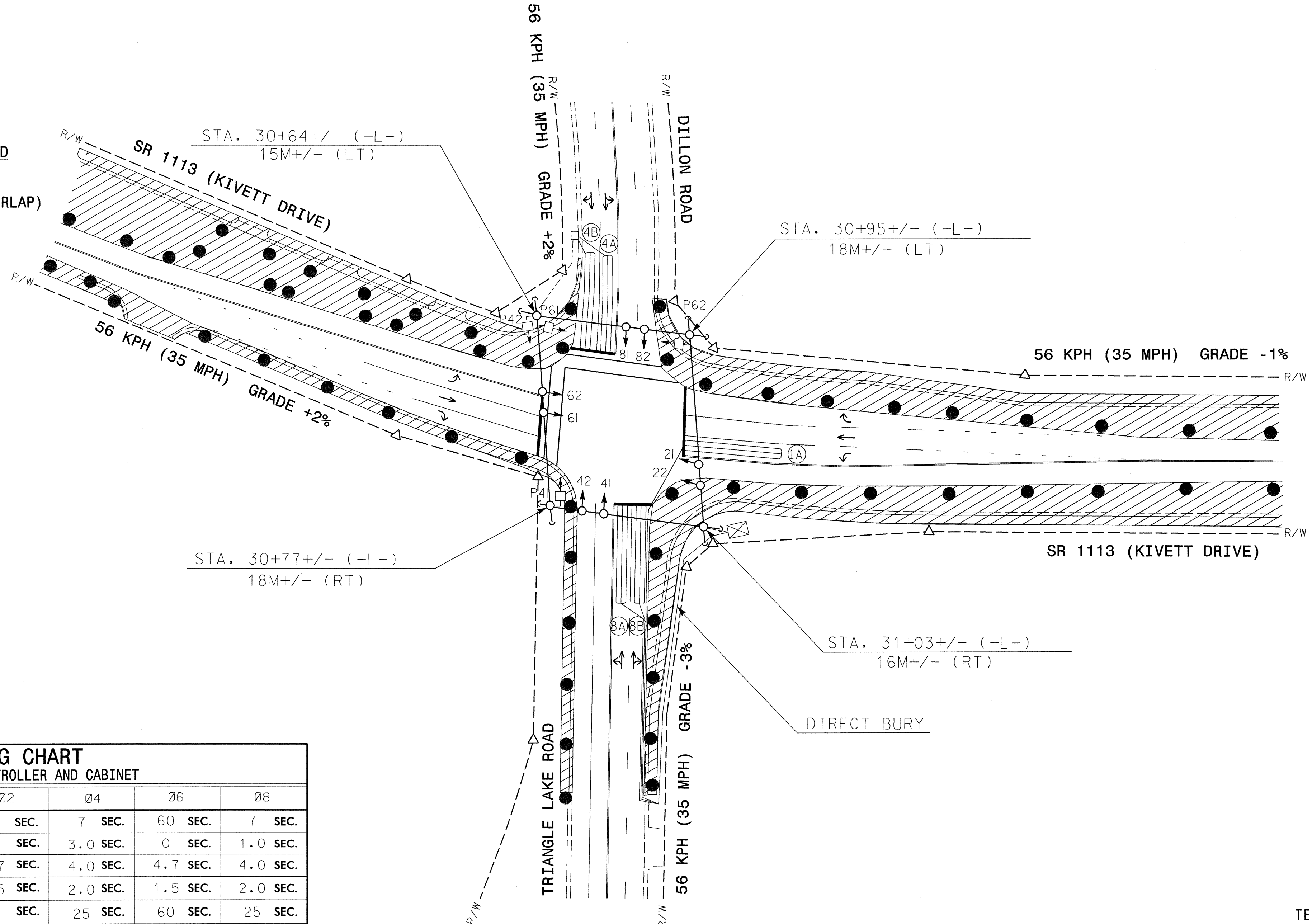
**3 Phase Semi-Actuated (High Point City Signal System)**

**NOTES**

- REFER TO "ROADWAY STANDARD DRAWINGS NCDOT" - RALEIGH, NC, DATED JANUARY 2002 AND "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" DATED JANUARY 2002.
- LOCATE NEW CABINET SO AS NOT TO OBSTRUCT SIGHT DISTANCE OF VEHICLES TURNING RIGHT ON RED.
- OMIT PHASE 1 DURING PHASE 2 ON.
- PROGRAM CONTROLLER TO CLEAR FROM PHASE 2+6 TO PHASE 1+6 BY PROGRESSING THROUGH PHASE 4+8 (SEE ELECTRICAL DETAILS).
- MAXIMUM TIMES SHOWN IN TIMING CHART ARE FOR FREE-RUN OPERATIONS ONLY. COORDINATED SIGNAL SYSTEM TIMING VALUES SHALL SUPERSEDE THESE VALUES.
- OMIT "WALK" AND FLASHING "DONT WALK" WITH NO PEDESTRIAN CALLS.
- DO NOT PROGRAM SIGNAL FOR FOR LATE NIGHT FLASHING OPERATION UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- PROGRAM PHASE 4 AND PHASE 8 FOR DUAL ENTRY.
- SET ALL DETECTOR UNITS TO PRESENCE MODE.
- SIGNAL SYSTEM INTERSECTION ID # 710

**PHASING DIAGRAM DETECTION LEGEND**

- DETECTED MOVEMENT
- UNDETECTED MOVEMENT (OVERLAP)
- UNSIGNALIZED MOVEMENT
- PEDESTRIAN MOVEMENT



PLAN QUANTITIES	
Pay Item	Meters
Signal Cable	205
Messenger Cable	135
Lead-in Cable	170

**LEGEND**

- | PROPOSED | EXISTING |
|----------|----------|
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TIMING CHART					
PEEK TS-2 CONTROLLER AND CABINET					
PHASE	Ø1	Ø2	Ø4	Ø6	Ø8
MINIMUM GREEN	7 SEC.	60 SEC.	7 SEC.	60 SEC.	7 SEC.
PASSAGE GAP	1.0 SEC.	0 SEC.	3.0 SEC.	0 SEC.	1.0 SEC.
YELLOW CHANGE INT.	4.0 SEC.	4.7 SEC.	4.0 SEC.	4.7 SEC.	4.0 SEC.
RED CLEARANCE	1.0 SEC.	1.5 SEC.	2.0 SEC.	1.5 SEC.	2.0 SEC.
MAXIMUM 1	20 SEC.	60 SEC.	25 SEC.	60 SEC.	25 SEC.
RECALL POSITION	NONE	MAX. RECALL	NONE	MAX. RECALL	NONE
VEHICLE CALL MEMORY	NONLOCK	LOCK	NONLOCK	LOCK	NONLOCK
WALK	- SEC.	7 SEC.	7 SEC.	7 SEC.	- SEC.
FLASHING DON'T WALK	- SEC.	14 SEC.	12 SEC.	14 SEC.	- SEC.

**TEMPORARY SIGNAL 1 - TCP PHASE 1-2**

<p><b>PLANS PREPARED BY :</b> RUMMEL KLEPPER &amp; KAHL, LLP consulting engineers 5800 FARINGDON PLACE SUITE 105 RALEIGH, NORTH CAROLINA 27609-3960</p> <p>FOR <b>DIVISION OF HIGHWAYS</b></p>	<p>Prepared for the Offices of:</p> <p>122 N. McDowell St., Raleigh, NC 27603</p>	<p>SR 1113 (KIVETT DRIVE) AT TRIANGLE LAKE ROAD / DILLON ROAD</p> <p>DIVISION 07 GUILFORD COUNTY HIGH POINT</p> <p>PLAN DATE: 03-19-04 REVIEWED BY: D. MORTON</p> <p>PREPARED BY: J. COLE RUMKAX PROJECT NO. 302-079-SIG5</p>	<p>SEAL</p> <p>SEAL 19798 ENGINEER DONALD W. MORTON</p> <p>6-15-04 DATE</p> <p>SIG. INVENTORY NO. 07-2052T1</p>				
	<p>SCALE</p> <p>1 : 500</p>	<p>REVISIONS</p> <table border="1"> <tr> <th>INIT.</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> </tr> </table>	INIT.	DATE			<p>SIGNATURE</p> <p> </p>
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