

PHASING DIAGRAM

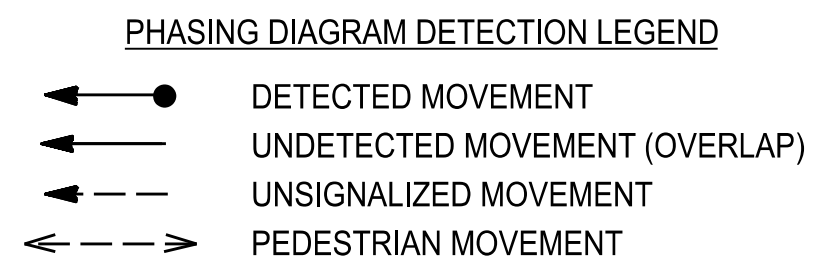
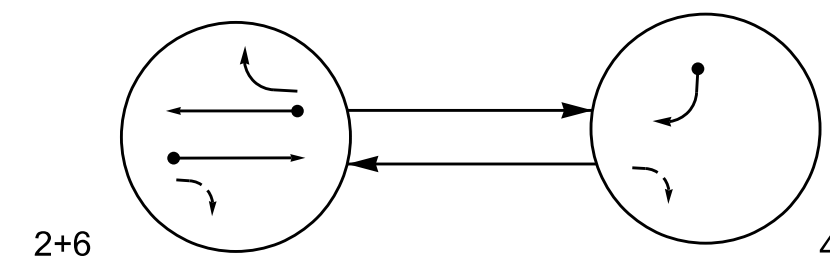
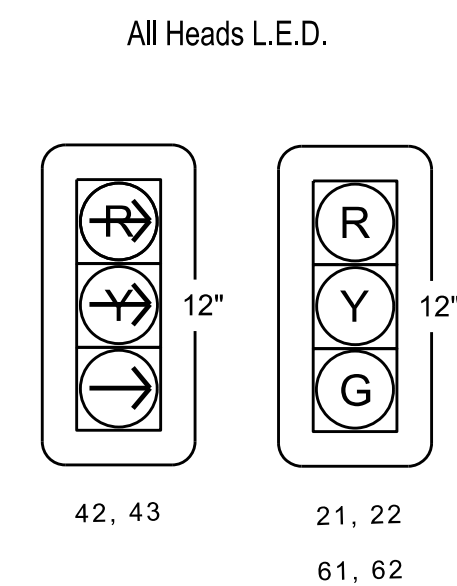


TABLE OF OPERATION

SIGNAL FACE	2+6		4	
	G	R	R	R
21, 22	G	R	R	R
42, 43	R	-	R	R
61, 62	G	R	R	R

SIGNAL FACE I.D.



SE-PAC 2070 LOOP & DETECTOR UNIT INSTALLATION CHART

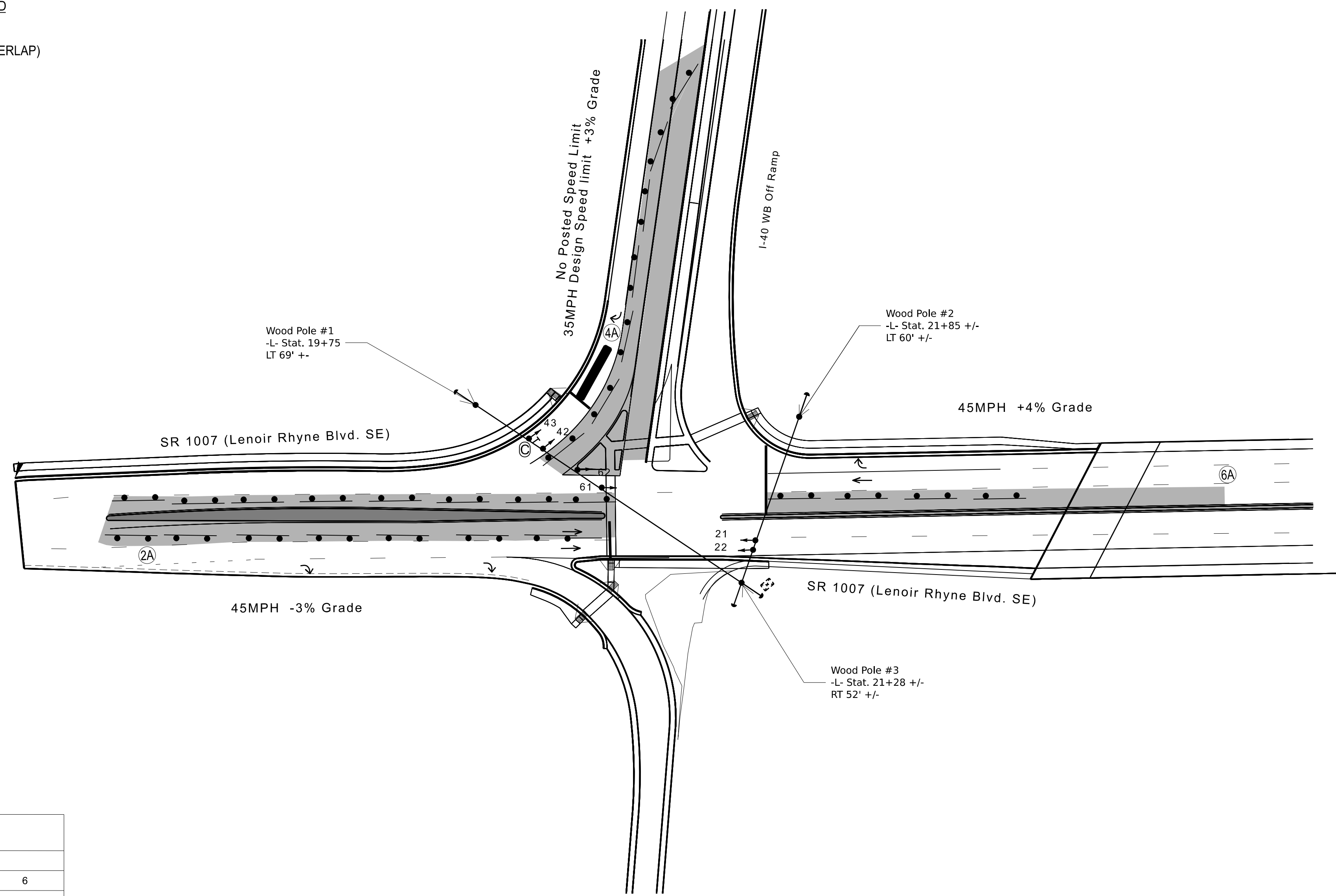
LOOP NO.	SIZE (FT)	TURNS	DIST. FROM STOP LINE (FT)	NEW	EXISTING	ASSIGNED PHASE	DETECTOR PROGRAMMING												
							TIMING		OPERATION MODE							STATUS			
							DELAY	EXTEND (STRETCH)	VEHICLE	PEDESTRIAN	1 CALL	STOP A	STOP B	PROT/PER LEFT	PROT/PER THROUGH	AND	SWITCH	SYSTEM LOOPS	NEW
4A	*	*	0	*	-	4	-	-	X	-	-	-	-	-	-	-	-	-	*

* Multi-Zone Microwave Detection

2-Phase Fully Actuated Hickory City Signal System

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2024 and "Standard Specifications for Roads and Structures" dated January 2024.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Remove signal heads number 41 and 51.
- Remove the existing "No Left Turn" Sign (R3-2).
- Remove the existing Yield Sign (R1-2).
- Reposition all signal heads.
- Refer to Pavement Marking Plans for proposed stop bar and crosswalk locations.
- Set all detector units to presence mode.
- This intersection uses multi-zone microwave detection. Install detectors according to the manufacturer's instructions to achieve the desired detection.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.

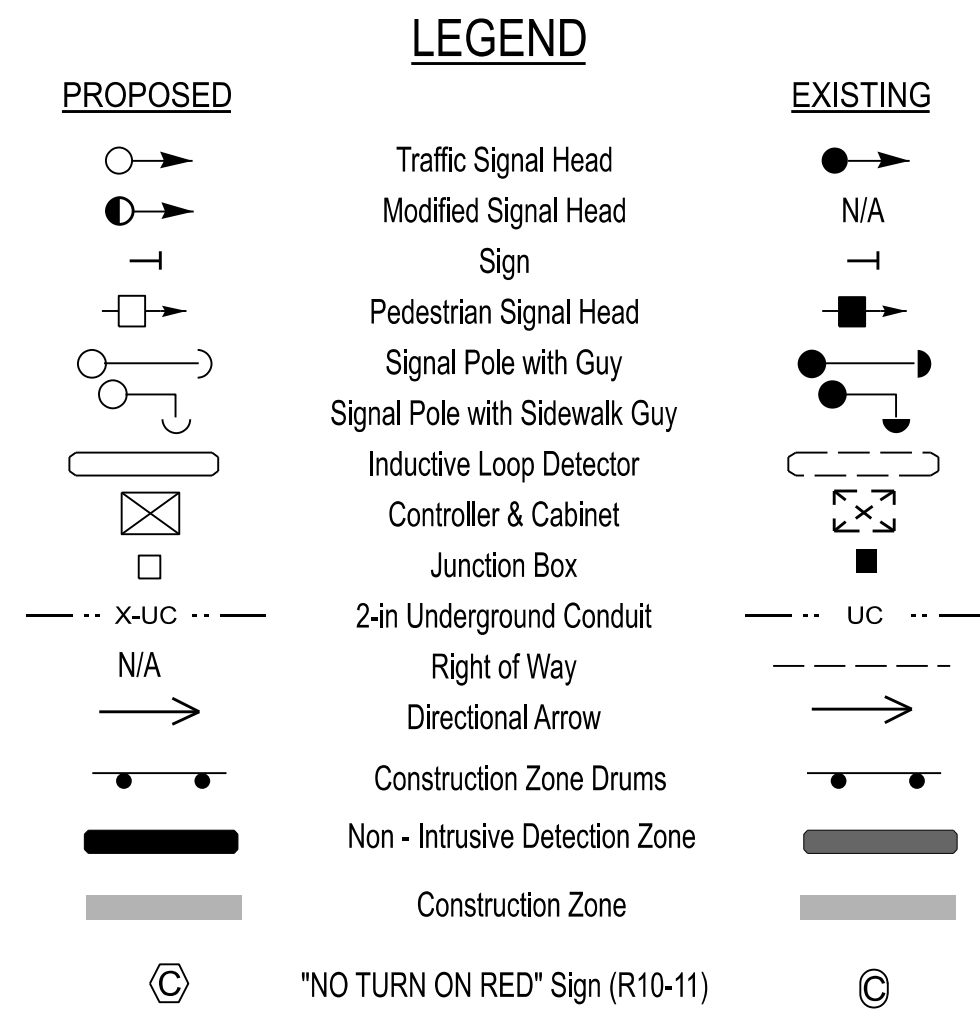


SE-PAC 2070 TIMING CHART

FEATURE	PHASE		
	2	4	6
Min Green*	12	7	12
Passage Gap*	2.0	2.0	2.0
Maximum Green*	90	30	90
Yellow Change	4.8	3.7	4.8
Red Clear	3.2	2.2	3.2
Walk*	-	-	-
Pedestrian Clear	-	-	-
Added Initial*	-	-	-
Maximum Initial*	-	-	-
Time Before Reduction*	-	-	-
Time To Reduce*	-	-	-
Minimum Gap	-	-	-
Recall Mode	MIN RECALL	-	MIN RECALL
Vehicle Call Memory	LOCK	NON-LOCK	LOCK
Dual Entry	-	-	-
Simultaneous Gap	ON	ON	ON

Microwave Detection

FUNCTION	SENSOR 1 (2A)			SENSOR 2 (6A)		
	1	2	Queue	1	2	Queue
Channel	1			1		
Phase	2			6		
Direction of Travel	SB			NB		
Type	Priority			Priority		
Level	1	2	Queue	1	2	Queue
Discovery Zone (ft)	>= 750	<750	-	>= 750	<750	-
Range	900-100	600-100	150-100	900-100	600-100	150-100
Enable Speed	Y	Y	Y	Y	Y	Y
Speed Range(mph)	35-100	35-100	1-35	35-100	35-100	1-35
Enable Estimated Time of Arrival	Y	Y	N	Y	Y	N
Estimated Time of Arrival (sec)	2.5-10.0	2.5-6.5	-	2.5-10.0	2.5-10.0	-



Signal Upgrade -Temporary Design 4 (TMP Phase 7A)

Prepared in the Offices of:

Transportation Mobility and Safety Division
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 Signal Design Section

I-40 EB Ramp at SR 1007 (Lenoir Rhyne Blvd. SE)

Division 12 Catawba County Hickory

PLAN DATE: January 2026 REVIEWED BY: R.N Zinser

PREPARED BY: Adja Fall REVIEWED BY:

REVISIONS: INIT. DATE

750 N. Greenfield Pkwy, Garner, NC 27529

SCALE: 1" = 50'

DocuSigned by: R. Nicholas Zinser 04/13/2026

SIG. INVENTORY NO. 12-0309T4

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL: NORTH CAROLINA PROFESSIONAL ENGINEER RICHARD N. ZINSER 043914

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