

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

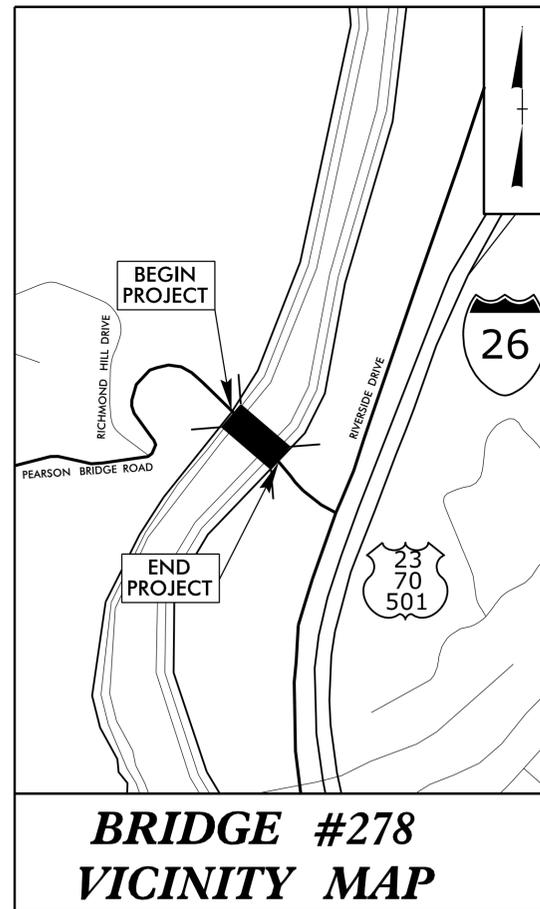
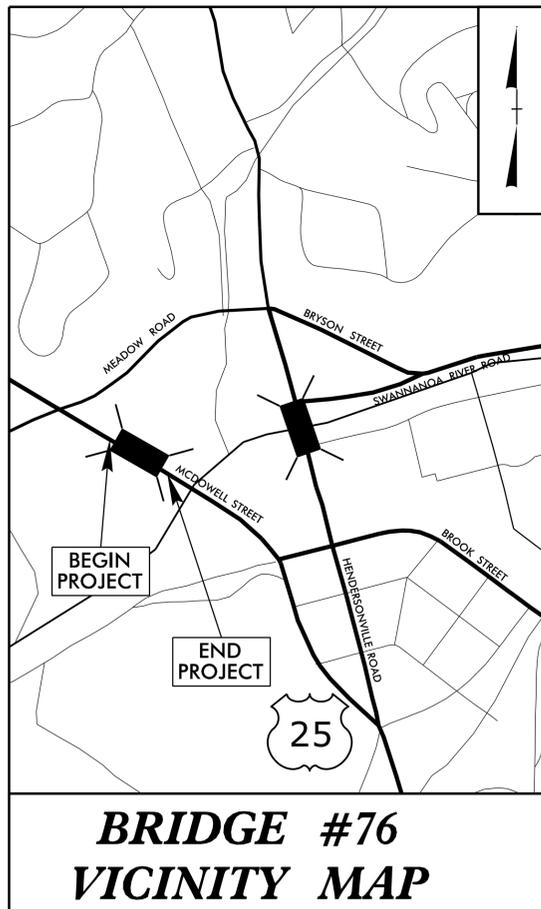
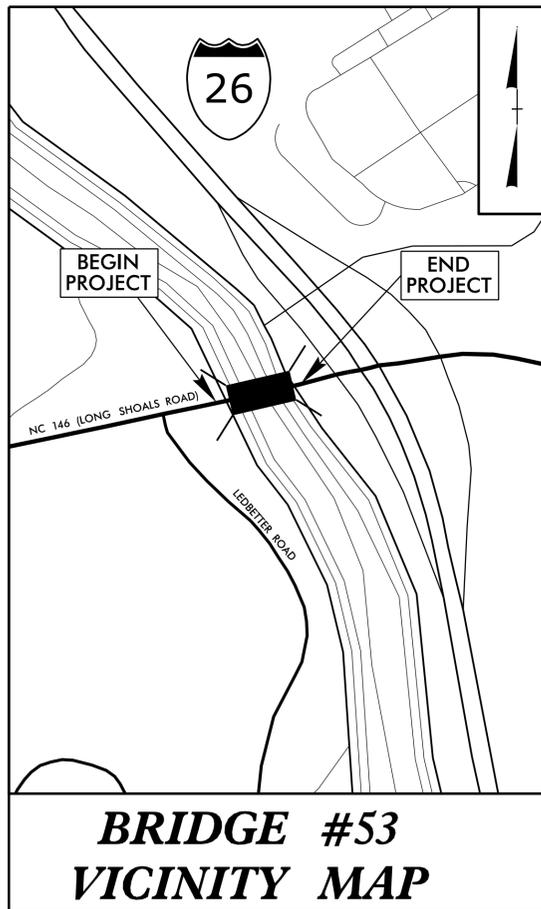
TRANSPORTATION MANAGEMENT PLAN

BUNCOMBE COUNTY

BRIDGE REHABILITATION OF BRIDGE 53 ON NC 146 (LONG SHOALS ROAD) OVER FRENCH BROAD RIVER

BRIDGE REHABILITATION OF BRIDGE 76 ON US 25 (MCDOWELL STREET) OVER SWANNANOVA RIVER

BRIDGE REHABILITATION OF BRIDGE 278 ON PEARSON BRIDGE ROAD OVER FRENCH BROAD RIVER



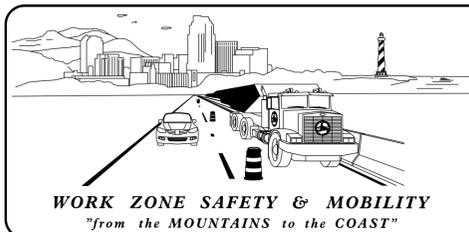
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SHEET NO. TMP-1
PROJECT: 15BPR.128.3

CONTRACT: C204454

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APPROVED: *Zachary M. Esposito*
DATE: 3/25/2025
SEAL

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-2
PLANS PREPARED BY:	
	
DRMP, INC. 8210 UNIVERSITY EXECUTIVE PARK DR., SUITE 220 CHARLOTTE, NC 28262 NC LICENSE NO. F-1524 (704) 549-4260	

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2024 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1145.01	BARRICADES
1150.01	FLAGGERS
1165.01	TRUCK MOUNTED ATTENUATOR
1180.01	SKINNY - DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMP
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.11	PAVEMENT MARKINGS - RAILROAD CROSSINGS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY

LEGEND

GENERAL

-  DIRECTION OF TRAFFIC FLOW
-  DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
-  NORTH ARROW
- PROPOSED PVMT.
- ~~~~~ TEMP. SHORING (LOCATION PURPOSES ONLY)
-  WORK AREA
-  REMOVAL

SIGNALS

-  EXISTING
-  PROPOSED
-  T
-  E
-  M
-  P

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

-  BARRICADE (TYPE III)
-  CONE
-  DRUM
-  SKINNY DRUM
-  TUBULAR MARKER
-  TEMPORARY CRASH CUSHION
-  FLASHING ARROW BOARD
-  FLAGGER
-  LAW ENFORCEMENT
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

-  PORTABLE SIGN
-  STATIONARY SIGN
-  STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

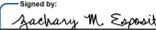
-  CRYSTAL/CRYSTAL
-  CRYSTAL/RED
-  YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

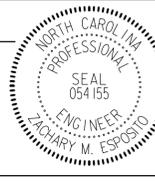
-  PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

P1	WHITE EDGE LINE	PAINT (4")	LF	P11	YELLOW SINGLE CENTER	PAINT (4")	LF
P2	WHITE SOLID LANE LINE	PAINT (4")	LF	P12	10FT. YELLOW SKIP	PAINT (4")	LF
P3	10FT. WHITE SKIP	PAINT (4")	LF	P13	YELLOW DOUBLE CENTER	PAINT (4")	LF
P4	3FT.-9FT./SP WHITE MINISKIP	PAINT (4")	LF	P40	WHITE GORELINE	PAINT (8")	LF
P5	2FT.-6FT./SP WHITE MINISKIP	PAINT (4")	LF	P41	WHITE DIAGONAL	PAINT (8")	LF
				P61	WHITE STOPBAR	PAINT (24")	LF

APPROVED: 
 DATE: 3/25/2025

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ROADWAY STANDARD
DRAWINGS & LEGEND

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
NC 146	MONDAY-SUNDAY 6AM - 8PM
US 25	MONDAY-SUNDAY 6AM - 8PM
I-26 OFF RAMPS	MONDAY-SUNDAY 6AM - 8PM
PEARSON BRIDGE ROAD	NONE

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
NC 146
US 25
I-26 OFF RAMPS

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 8:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:00 P.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 8:00 P.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 8:00 P.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 8:00 P.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 8:00 P.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 10 FT OF AN OPEN TRAVEL LANE ON A DIVIDED FACILITY, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- I) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON NC 146, US 25 AND/OR I-26 OFF RAMPS.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- J) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- K) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA. SEE RSD 1101.11 FOR SIGN SPACING.

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-3A
PLANS PREPARED BY:	
DRMP, INC. 8210 UNIVERSITY EXECUTIVE PARK DR., SUITE 220 CHARLOTTE, NC 28262 NC LICENSE NO. F-1524 (704) 549-4260	

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<h2 style="margin: 0;">GENERAL NOTES</h2>		

GENERAL NOTES CONT.

TRAFFIC PATTERN ALTERATIONS

- L) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- M) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- N) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

- O) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.
- COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- Q) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 350 IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER. SEE RSD 1101.11 FOR SIGN SPACING.

TRAFFIC CONTROL DEVICES

- R) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.
- S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

PAVEMENT MARKINGS AND MARKERS

- T) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- U) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

MISCELLANEOUS

- V) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS AS DIRECTED BY THE ENGINEER.
- W) IN THE EVENT A TIE-IN CANNOT BE MADE IN ONE DAY'S TIME, BRING THE TIE-IN AREA TO AN APPROPRIATE ROADWAY ELEVATION AS DETERMINED BY THE ENGINEER. PLACE BLACK ON ORANGE "LOOSE GRAVEL" SIGNS (W8-7) AND BLACK ON ORANGE "PAVEMENT ENDS" SIGNS (W8-3) 350 FT AND 350 FT RESPECTIVELY IN ADVANCE OF THE UNEVEN AREAS. USE DRUMS TO DELINEATE THE EDGE OF ROADWAY ALONG UNPAVED AREAS.

GENERAL NOTES CONT.

MISCELLANEOUS (CONT.)

- X) ALL CURB RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.
- Y) CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS AT ALL TIMES AS STATED IN THE PHASING. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY SIDEWALKS (CONCRETE, ASPHALT, OR OTHER SUITABLE MATERIAL AS APPROVED BY THE ENGINEER) AT ALL LOCATIONS WHERE THE OPEN PEDESTRIAN TRAVELWAY HAS BEEN REMOVED FOR CONSTRUCTION OPERATIONS (UTILITIES, DRAINAGE, ETC.).

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-3B
PLANS PREPARED BY:	
	
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SIGN NUMBER: SD-1 TYPE: D QUANTITY: 17 SIGN WIDTH: 4'-6" HEIGHT: 2'-6" TOTAL AREA: 11.3 Sq.Ft. BORDER TYPE: RECESS: WIDTH: RADII: 1.5" NO. Z BARS: LENGTH:	BACKG COLOR: Fluorescent Orange COPY COLOR: Black <table border="1"> <tr> <th>SYMBOL</th> <th>X</th> <th>Y</th> <th>WID</th> <th>HT</th> </tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </table> MAT'L: 0.125" (3.2 mm) ALUMINUM	SYMBOL	X	Y	WID	HT																																														DESIGN BY: DJP DRMP PROJECT ID: 22212 CHECKED BY: ZME Jan 7, 2025 DIV: 13
SYMBOL	X	Y	WID	HT																																																

Spacing Factor is 1 unless specified otherwise

LETTER POSITIONS

Letter locations are panel edge to lower left corner

Letter locations are panel edge to lower left corner														Series/Size	
														Text Length	
P	e	a	r	s	o	n									C 2000
14.9	19.2	22.7	26.6	29	32	36									24.1
B	r	i	d	g	e		R	o	a	d					C 2000
6.9	11.2	13.8	15.6	19.5	23.5	26.6	32.6	36.6	40.3	44					40.1

FILENAME: 100278_TC_SD NORTH CAROLINA D.O.T. SIGN DETAIL

APPROVED: DATE: 3/25/2025 SEAL			BRIDGE #278 DETOUR SIGN DESIGN
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PHASING NOTES

BEFORE BEGINNING ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL INSTALL ALL ADVANCE WARNING SIGNS AND TRAFFIC CONTROL DEVICES. FIELD VERIFY LOCATIONS WITH THE ENGINEER PRIOR TO INSTALLATION.

MAINTAIN VEHICULAR ACCESS TO ALL RESIDENCES, SCHOOLS, BUS STOPS, EMERGENCY SERVICES, AND BUSINESSES DURING THE LIFE OF THE CONTRACT, PRIOR TO INCORPORATION, OBTAIN WRITTEN APPROVAL FROM THE ENGINEER ON METHOD TO MAINTAIN ACCESS.

WHEN USING LANE CLOSURES (RSD 1101.02), RETURN TRAFFIC TO EXISTING AND/OR TEMPORARY TRAFFIC PATTERN UPON ACTIVITIES COMPLETION, UNLESS OTHERWISE NOTED IN THE PHASING PLANS.

WHEN PHASING STATES TO USE LANE OR ROAD CLOSURES, REFER TO THE FOLLOWING FOR ALL EXISTING AND PROPOSED ROADS:

- ALL TWO-LANE/TWO-WAY FACILITIES SEE RSD 1101.02 SHEET 1 OF 19
- ALL TWO-LANE/TWO-WAY ROADWAYS WITH 2-WAY LEFT TURN LANE SHEET 2 OF 19
- ALL DIVIDED MULTI-LANE ROADWAYS POSTED < 60 MPH SEE RSD 1101.02 SHEET 3 OF 19
- ALL ENTRANCE AND EXIT RAMP SEE RSD 1101.02 SHEETS 10 AND 11 OF 19
- ALL ROAD CLOSURES BEYOND DETOUR POINT SEE RSD 1101.03 SHEET 1 OF 9

COMPLETE PAVING UP TO, BUT NOT INCLUDING, THE FINAL LAYER OF SURFACE COURSE UNTIL STATED TO PLACE FINAL LAYER IN THE PHASING PLANS.

CONTRACTOR SHALL REPLACE ANY DAMAGED PAVEMENT MARKINGS AND RETURN TRAFFIC TO THE EXISTING TRAFFIC PATTERN AT THE END OF EACH NIGHT TIME WORK PERIOD. FURTHERMORE, ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED AND/OR PULLED TO THE EDGE OF PAVEMENT AT THE END OF EACH NIGHT TIME WORK PERIOD TO ENSURE NO TRAFFIC IMPACTS DURING NON WORKING HOURS.

TRAFFIC CONTROL NOTE 1:
FOR THE DURATION OF CONSTRUCTION, ENSURE ALL DRIVEWAY ACCESS IS MAINTAINED. SPECIAL CARE SHALL BE CONSIDERED WHEN NEAR BILTMORE ESTATE DRIVE AND EXTRA EFFORTS SHALL BE MADE TO ENSURE MINIMUM TRAFFIC IMPACTS TO TOURISTS.

TRAFFIC CONTROL NOTE 2:
CONTRACTOR MUST COORDINATE WITH NCDOT DIVISION 13 AND NORFOLK SOUTHERN RAILWAY FOR ANY WORK ON/NEAR THE RAILROAD UNDERNEATH BRIDGE #76 AND BRIDGE #278 TO ENSURE RAILROAD OPERATIONS IS NOT IMPACTED THROUGHOUT ALL PHASES OF CONSTRUCTION.

TRAFFIC CONTROL NOTE 3:
2 WEEKS PRIOR TO ANY CONSTRUCTION, CONTRACTOR SHALL INSTALL CMS BOARDS TO NOTIFY TRAFFIC OF UPCOMING ROAD CLOSURE.

TRAFFIC CONTROL NOTE 4:
CONTRACTOR MUST COORDINATE WITH NCDOT DIVISION 13 AND ANY CONTRACTOR WORKING WITHIN OR ADJACENT TO THE LIMITS OF BRIDGE #53 AND BRIDGE #278 TO ENSURE THAT THE WORK CAN BE CARRIED OUT TO THE BEST ADVANTAGE OF ALL CONCERNED. SEE CONTRACT PROVISION "COOPERATION BETWEEN CONTRACTORS" FOR MORE DETAILS.

BRIDGE #53 PHASING PLAN

PHASE I

STEP 1: (SEE TMP-5)
INSTALL WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHT TIME PHASE I TRAFFIC PATTERN.

ONCE SHIFTED, BEHIND DRUMS AND AWAY FROM TRAFFIC, BEGIN BRIDGE REHABILITATION ALONG NC 146 OF TWO THE OUTERMOST EASTBOUND LANES. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

NOTE:
CONTRACTOR SHALL INSTALL TRAFFIC CONTROL DEVICES IN SUCH A MANNER THAT TRACTOR TRAILERS CAN MAKE THE RIGHT TURN ONTO I-26 EASTBOUND ON RAMP.

BRIDGE #53 PHASING PLAN (CONT.)

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-4A
PLANS PREPARED BY:	
DRMP, INC. 8210 UNIVERSITY EXECUTIVE PARK DR., SUITE 220 CHARLOTTE, NC 28262 NC LICENSE NO. F-1524 (704) 549-4260	

PHASE II

STEP 1: (SEE TMP-6 TO TMP-8)
MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHT TIME PHASE II TRAFFIC PATTERN.

ONCE SHIFTED, BEHIND DRUMS AND AWAY FROM TRAFFIC, BEGIN BRIDGE REHABILITATION ALONG NC 146 OF THE INNERMOST LANES. EASTBOUND TRAFFIC WILL BE REDUCED DOWN TO THE OUTERMOST THROUGH AND RIGHT LANE. WESTBOUND TRAFFIC WILL BE REDUCED DOWN TO THE OUTERMOST THROUGH LANE. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

NOTE 1:
PRIOR TO TRANSITIONING TO PHASE IIIA TRAFFIC PATTERN, ENSURE 125' OF NC 146 CONCRETE MEDIAN IS REMOVED AND PROPERLY DRUMMED OUT. (SEE TMP-7 FOR MORE DETAILS)

NOTE 2:
LAW ENFORCEMENT SHALL BE UTILIZED FOR THE INTERSECTION OF NC 146 AT I-26 JUNCTION TO ENSURE SAFE TRAFFIC TRANSITIONS. SIGNAL WILL BE PUT INTO FLASH AND LAW ENFORCEMENT WILL GUIDE TRAFFIC THROUGHOUT THE INTERSECTION.

PHASE IIIA

STEP 1: (SEE TMP-9 TO TMP-12)
MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHT TIME PHASE IIIA TRAFFIC PATTERN.

ONCE SHIFTED, BEHIND DRUMS AND AWAY FROM TRAFFIC, FINISH BRIDGE REHABILITATION ALONG NC 146 OF THE OUTERMOST WESTBOUND THROUGH LANE. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

PHASE IIIB

STEP 1: (SEE TMP-13 TO TMP-15)
IMMEDIATELY AFTER COMPLETING PHASE IIIA, USING LANE CLOSURES AS NEEDED, TRANSITION TO THE TEMPORARY NIGHT TIME PHASE IIIB TRAFFIC PATTERN.

ONCE SHIFTED, BEHIND DRUMS AND AWAY FROM TRAFFIC, REINSTALL THE IMPACTED CONCRETE MEDIAN ON NC 146. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

PHASE IV

STEP 1:
MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, FINISH ANY REMAINING REHABILITATION AND PLACE FINAL SURFACE COURSE ALONG WITH FINAL PAVEMENT MARKINGS AND MARKERS.

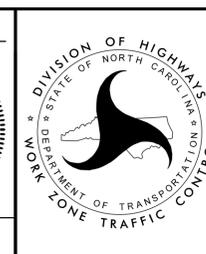
REMOVE ALL REMAINING TRAFFIC CONTROL DEVICES AND ALLOW NORMAL TRAFFIC OPERATION.

APPROVED:
Signed by: Zachary M. Esposito
0847D07ED4884EB...

DATE: 3/25/2025

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PHASING NOTES AND PLAN

BRIDGE #76 PHASING PLAN

PHASE I

STEP 1: (SEE TMP-16 TO TMP-17)
 INSTALL WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHTTIME PHASE I TRAFFIC PATTERN.

AFTER SHIFTING INTO TEMPORARY NIGHT TIME PHASE I TRAFFIC PATTERN, BEGIN REHABILITATION OF BRIDGE #76. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

NOTE:
 EXISTING SIDEWALK ON THE SOUTH SIDE OF MCDOWELL STREET SHALL BE CLOSED AND BARRICADED THROUGHOUT THIS PHASE. PEDESTRIAN TRAFFIC TO BE MAINTAINED VIA DETOUR USING THE EXISTING SIDEWALK ON THE NORTH SIDE OF MCDOWELL STREET

PHASE II

STEP 1: (SEE TMP-18 TO TMP-19)
 MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHTTIME PHASE II TRAFFIC PATTERN.

AFTER SHIFTING INTO TEMPORARY NIGHT TIME PHASE II TRAFFIC PATTERN, BEGIN REHABILITATION OF BRIDGE #76. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

NOTE:
 LAW ENFORCEMENT SHALL BE UTILIZED FOR THE INTERSECTION OF MCDOWELL STREET AT LODGE STREET TO ENSURE SAFE TRAFFIC TRANSITIONS. SIGNAL WILL BE PUT INTO FLASH AND LAW ENFORCEMENT WILL GUIDE TRAFFIC THROUGHOUT THE INTERSECTION.

PHASE III

STEP 1: (SEE TMP-20 TO TMP-21)
 MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE TEMPORARY NIGHT TIME PHASE III TRAFFIC PATTERN.

AFTER SHIFTING INTO TEMPORARY NIGHT TIME PHASE III TRAFFIC PATTERN, FINISH REHABILITATION OF BRIDGE #76. AT THE END OF EACH NIGHT TIME PERIOD, CONTRACTOR TO REMOVE ALL TRAFFIC CONTROL DEVICES TO ALLOW NORMAL TRAVEL CONDITIONS.

PHASE IV

STEP 1:
 MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AS NEEDED, FINISH ANY REMAINING REHABILITATION AND PLACE FINAL SURFACE COURSE ALONG WITH FINAL PAVEMENT MARKINGS AND MARKERS.

REMOVE ALL REMAINING TRAFFIC CONTROL DEVICES AND ALLOW NORMAL TRAFFIC OPERATION.

NOTE:
 CONTRACTOR TO COORDINATE WITH NORFOLK SOUTHERN TO ENSURE RAILROAD OPERATION IS NOT IMPACTED THROUGHOUT CONSTRUCTION OF BRIDGE #76. SEE TRAFFIC CONTROL (PHASING NOTES) NOTE 3 FOR MORE DETAILS.

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-4B
PLANS PREPARED BY:	
DRMP, INC. 8210 UNIVERSITY EXECUTIVE PARK DR., SUITE 220 CHARLOTTE, NC 28262 NC LICENSE NO. F-1524 (704) 549-4260	

BRIDGE #278 PHASING PLAN

PHASE I

STEP 1: (SEE TMP-22)
 INSTALL WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

INSTALL DETOUR SIGNING FOR PEARSON BRIDGE ROAD. (SEE TMP-28)

AWAY FROM TRAFFIC, USING ROAD CLOSURES AS NEEDED, SHIFT TRAFFIC TO THE PHASE I TRAFFIC PATTERN. PHASE I TRAFFIC PATTERN ENTAILS CLOSING PEARSON BRIDGE ROAD FROM RICHMOND HILL DRIVE TO RIVERSIDE DRIVE. TRAFFIC WILL BE DETOURED AWAY FROM PEARSON BRIDGE ROAD FOR THE DURATION OF THIS STEP (SEE TMP-23 FOR DETOUR ROUTE).

AFTER SHIFTING INTO PHASE I - STEP 1 TRAFFIC PATTERN, CONTRACTOR TO REHABILITATE THE DECK OF BRIDGE #278.

AFTER FORTY TWO (42) CONSECUTIVE CALENDAR DAYS, REMOVE ROAD CLOSURE AND ALLOW NORMAL TRAFFIC OPERATION.

NOTE:
 CONTRACTOR TO COORDINATE WITH NORFOLK SOUTHERN TO ENSURE RAILROAD OPERATION IS NOT IMPACTED THROUGHOUT CONSTRUCTION OF BRIDGE #278. SEE TRAFFIC CONTROL (PHASING NOTES) NOTE 3 FOR MORE DETAILS.

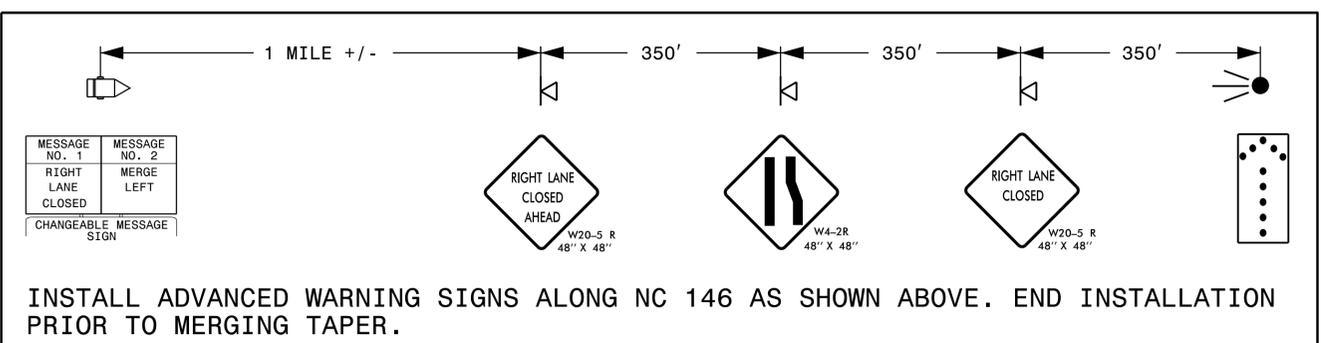
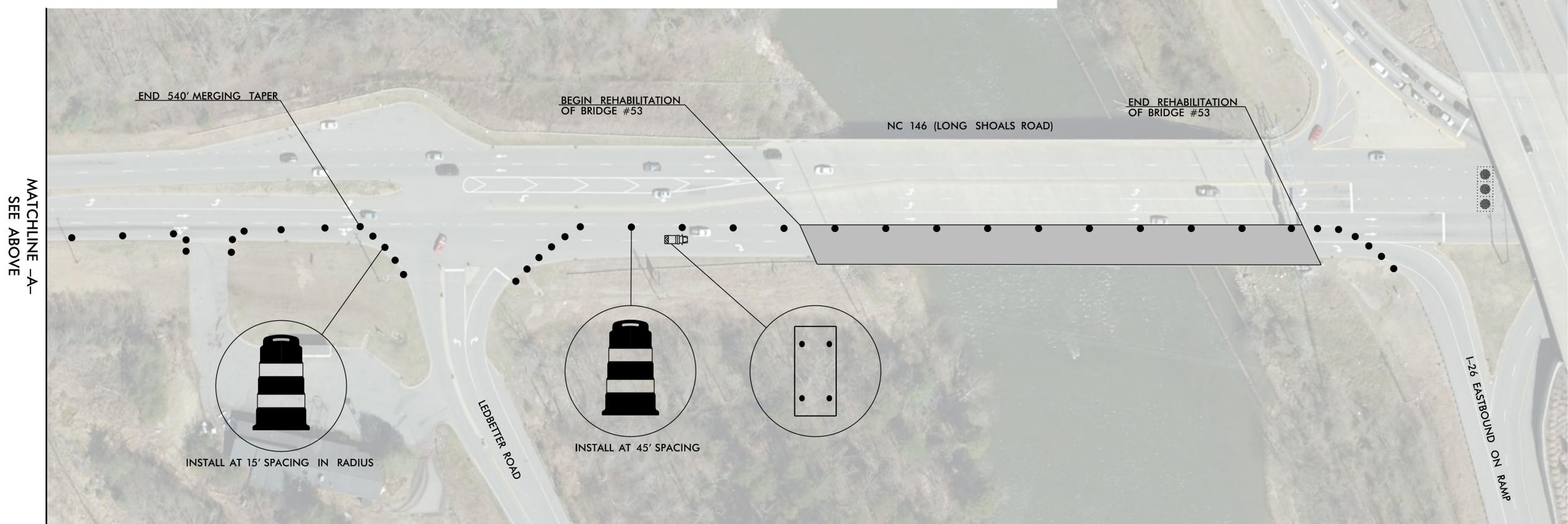
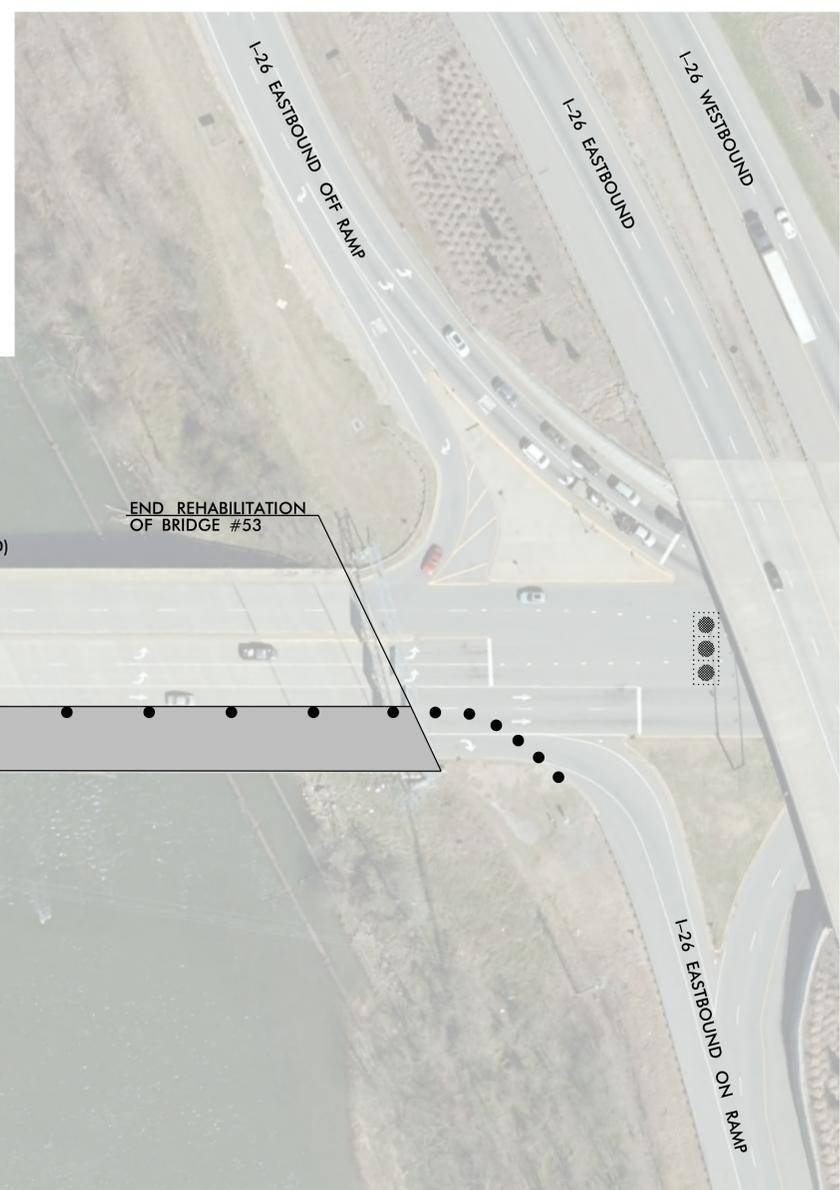
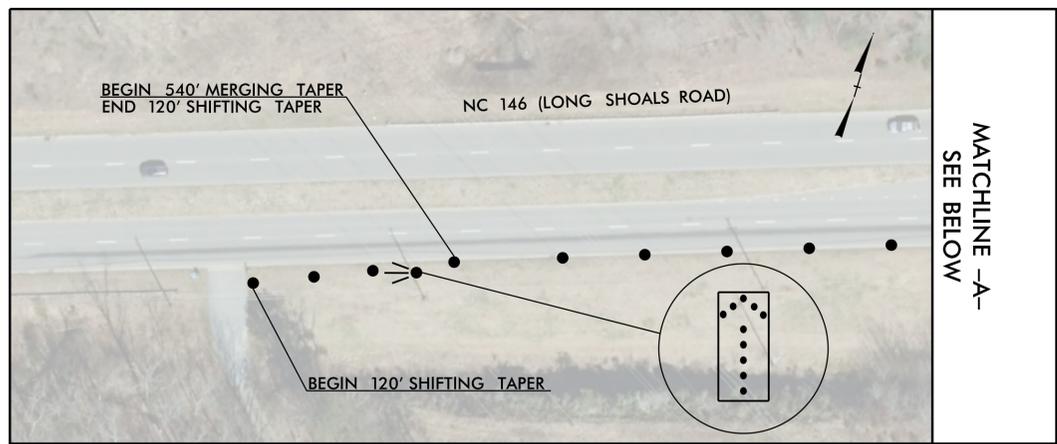
ICT NOTE:
 THE CONTRACTOR SHALL COMPLETE THE WORK REQUIRED IN PHASE 1 - STEP 1 IN FORTY TWO (42) CONSECUTIVE CALENDAR DAYS. SEE ICTS FOR MORE DETAILS.

STEP 2:
 MAINTAIN WORK ZONE ADVANCE WARNING SIGNS. (RSD 1101.01)

AWAY FROM TRAFFIC, USING LANE CLOSURES AND FLAGGERS AS NEEDED, CONTRACTOR TO FINISH ALL NON-DECK RELATED BRIDGE REHABILITATION, PLACE FINAL SURFACE COURSE ALONG WITH FINAL PAVEMENT MARKINGS AND MARKERS.

REMOVE ALL REMAINING TRAFFIC CONTROL DEVICES AND ALLOW NORMAL TRAFFIC OPERATION.

APPROVED: DATE: 3/27/2025 SEAL			<h2 style="margin: 0;">PHASING NOTES AND PLAN</h2>
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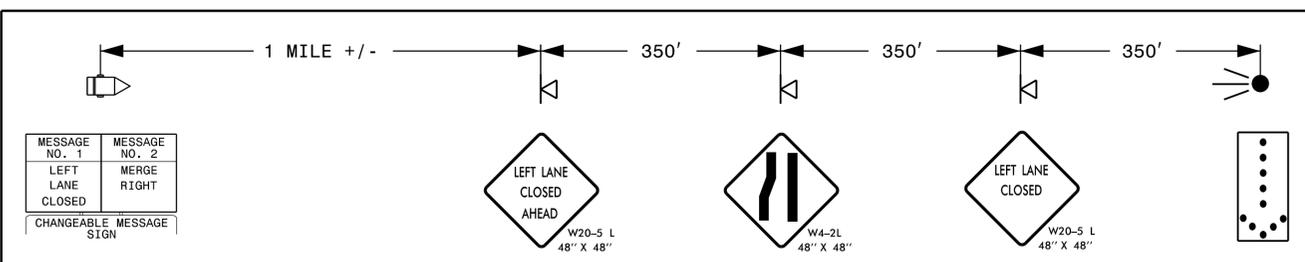


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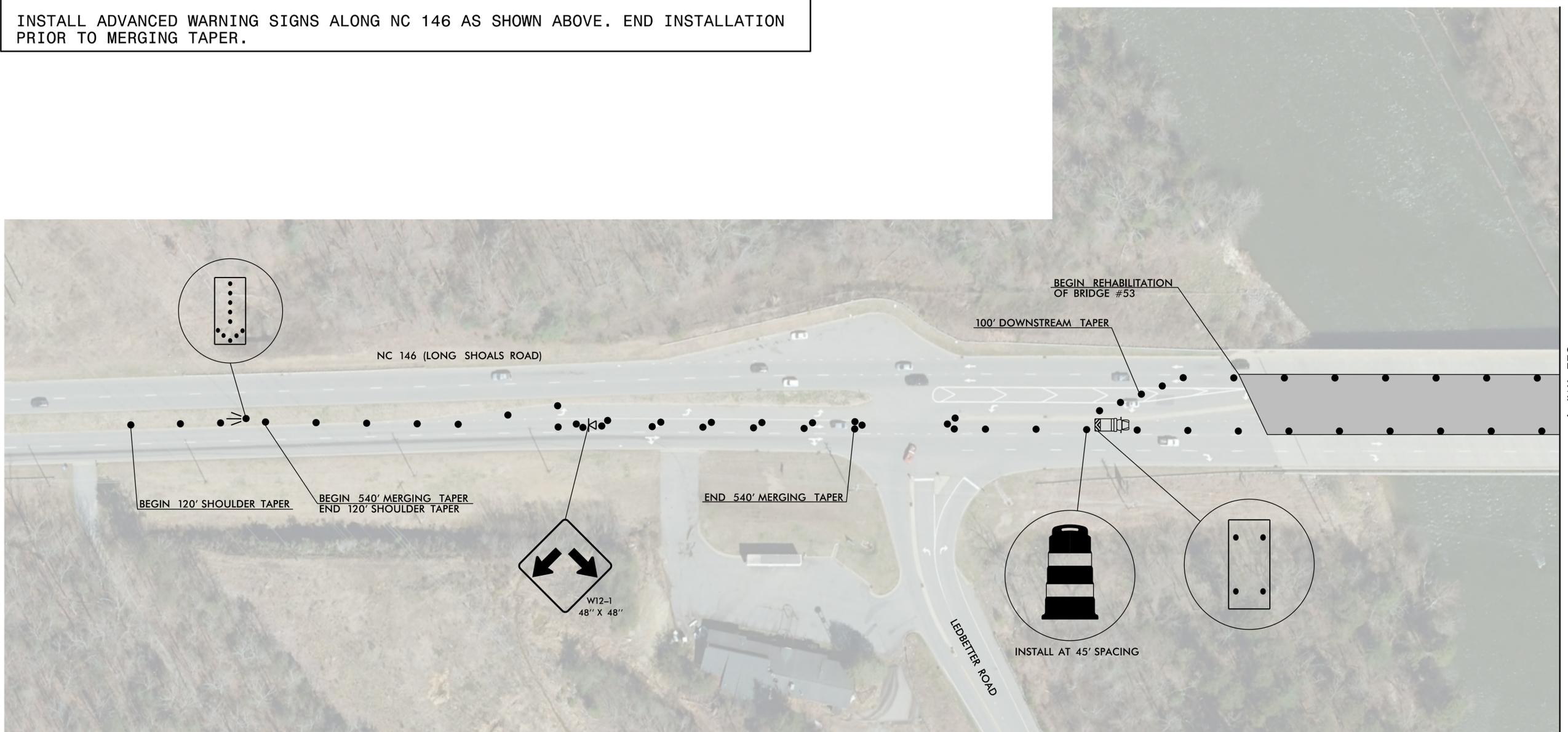


**BRIDGE #53
 TEMPORARY
 TRAFFIC CONTROL
 PHASE I**

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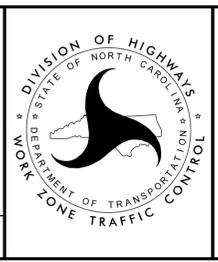
INSTALL ADVANCED WARNING SIGNS ALONG NC 146 AS SHOWN ABOVE. END INSTALLATION PRIOR TO MERGING TAPER.



MATCHLINE -A-
SEE TMP-7

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**BRIDGE #53
TEMPORARY
TRAFFIC CONTROL
PHASE II**



INSTALL AT 15' SPACING IN RADIUS

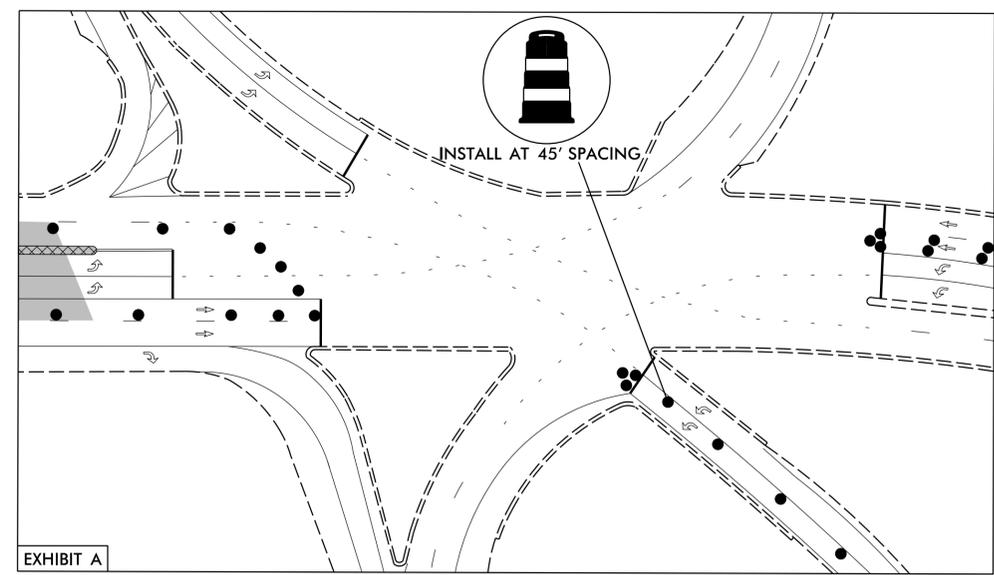
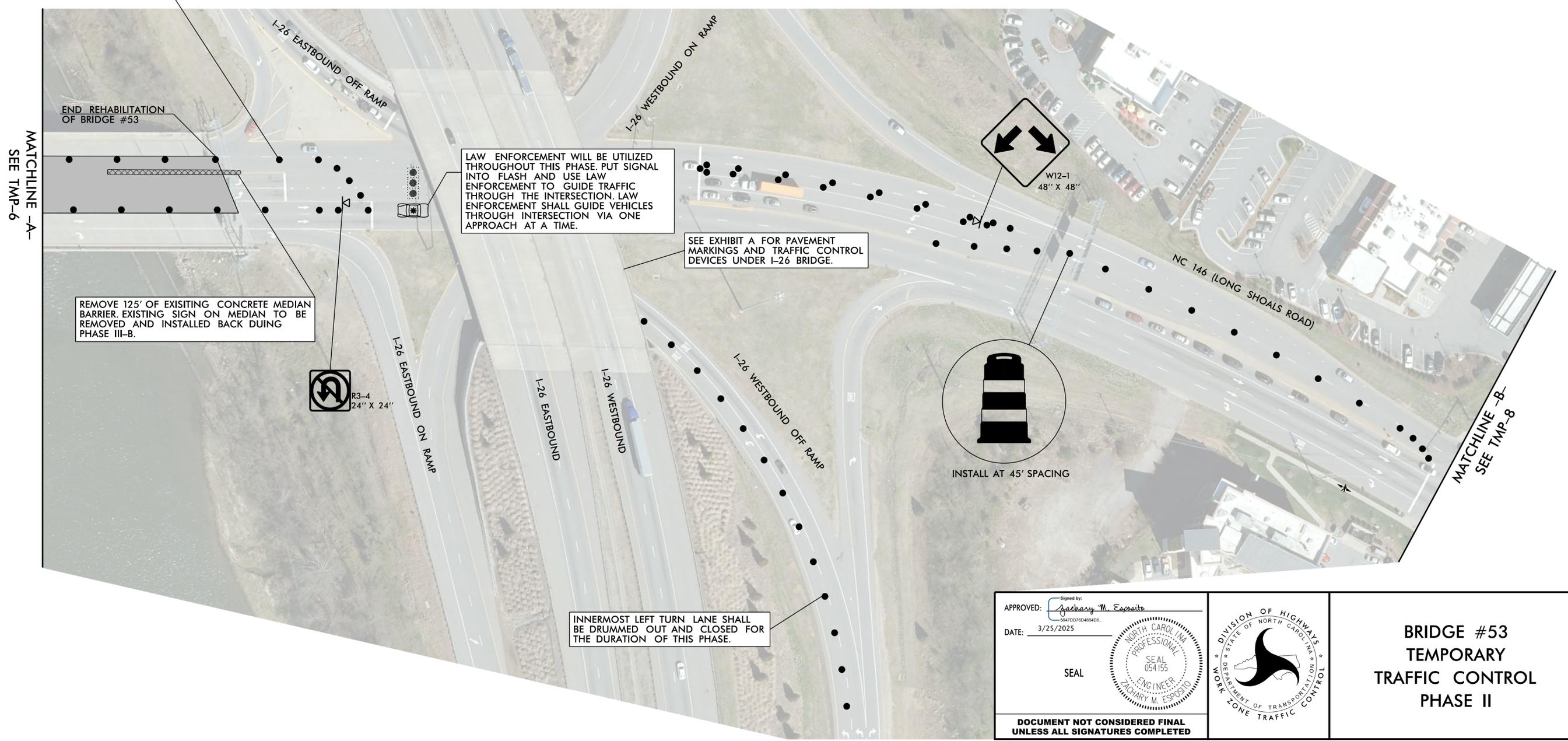


EXHIBIT A



MATCHLINE -A-
SEE TMP-6

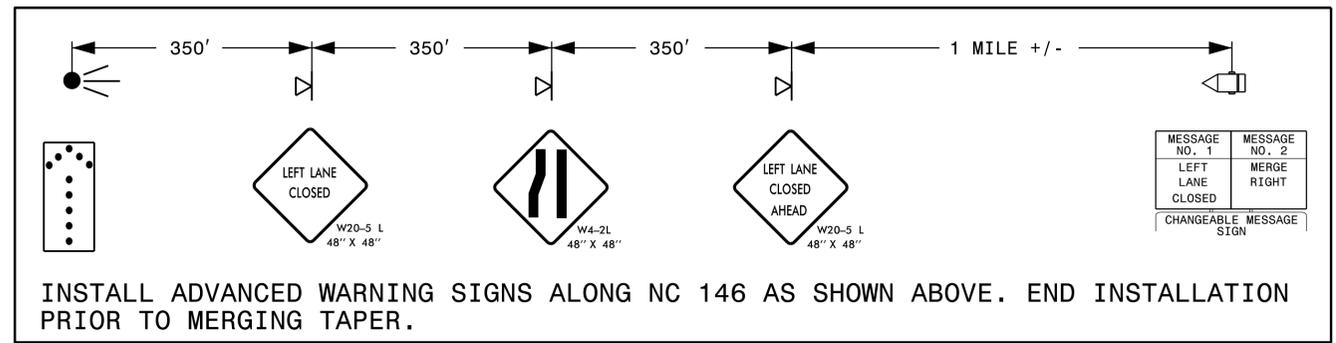
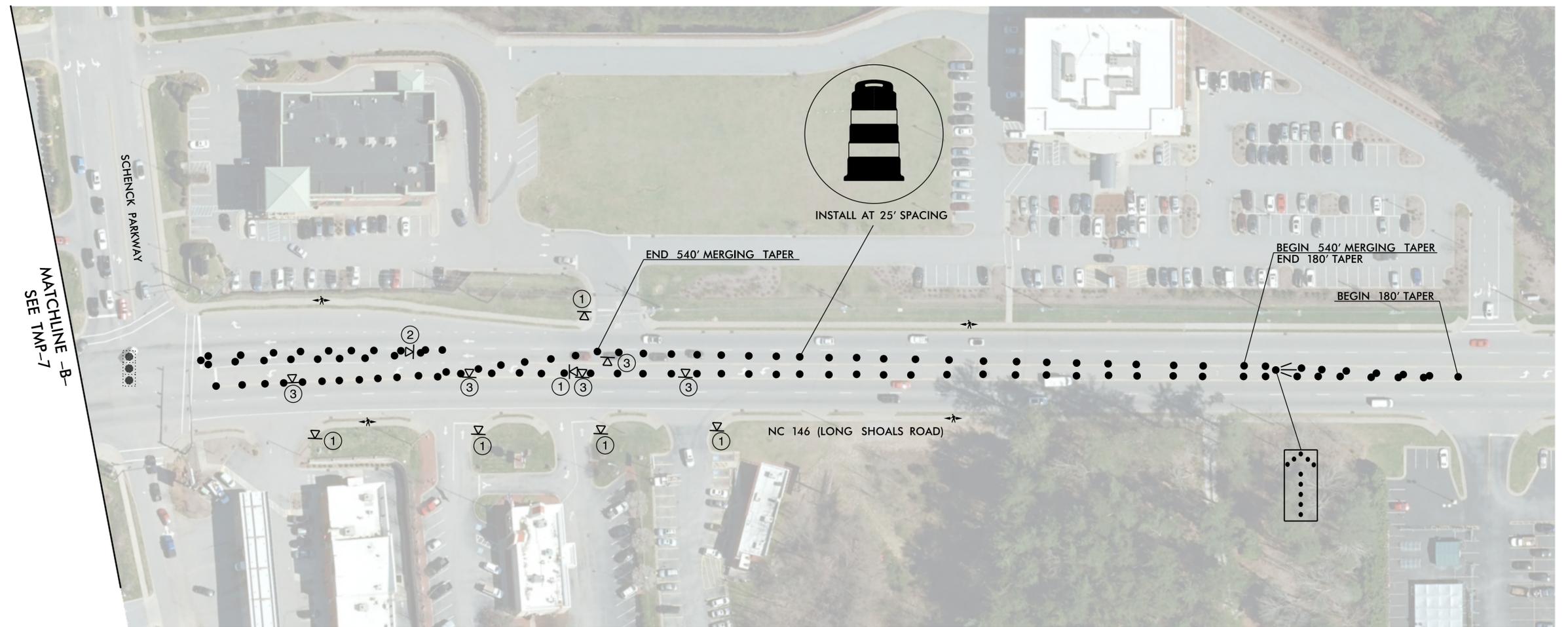
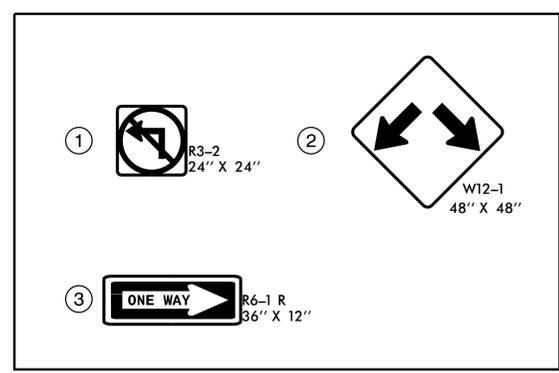
MATCHLINE -B-
SEE TMP-8

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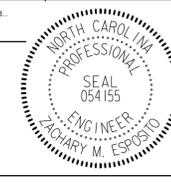
**BRIDGE #53
TEMPORARY
TRAFFIC CONTROL
PHASE II**

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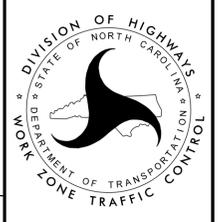


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**BRIDGE #53
 TEMPORARY
 TRAFFIC CONTROL
 PHASE II**

PROJ. REFERENCE NO.	SHEET NO.
15BPR.128.3	TMP-9
PLANS PREPARED BY:	
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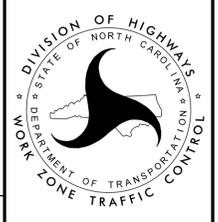


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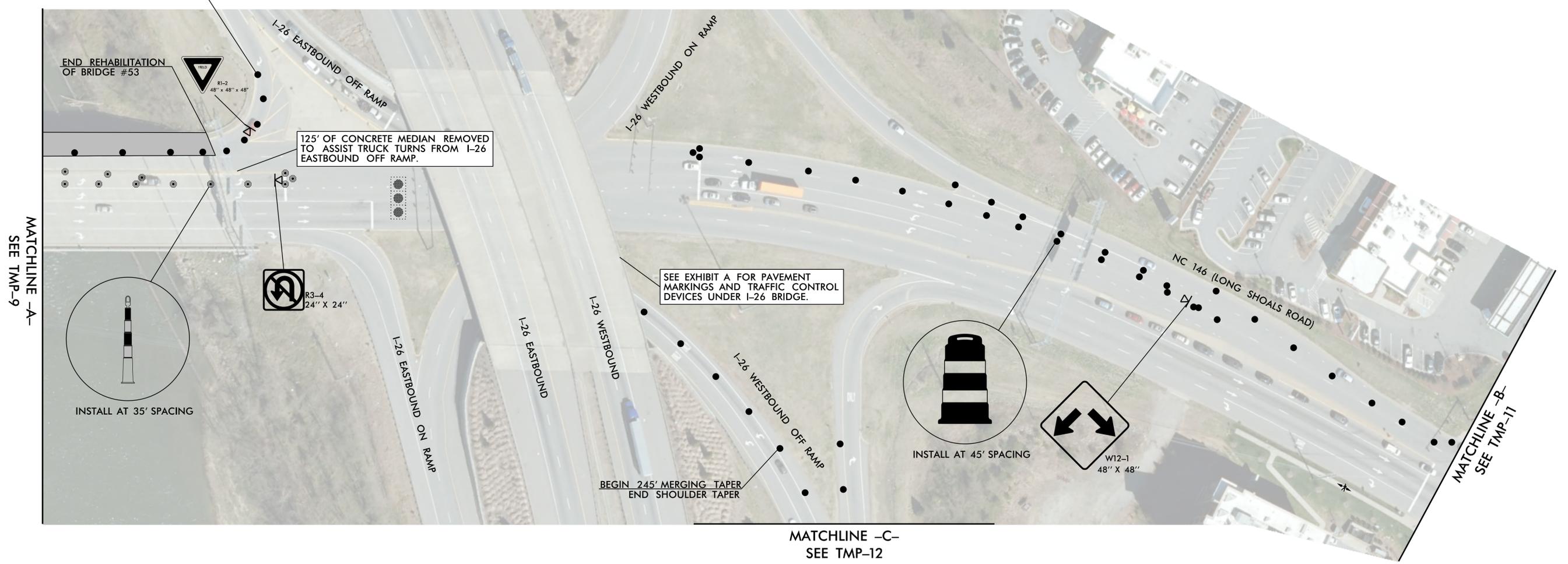
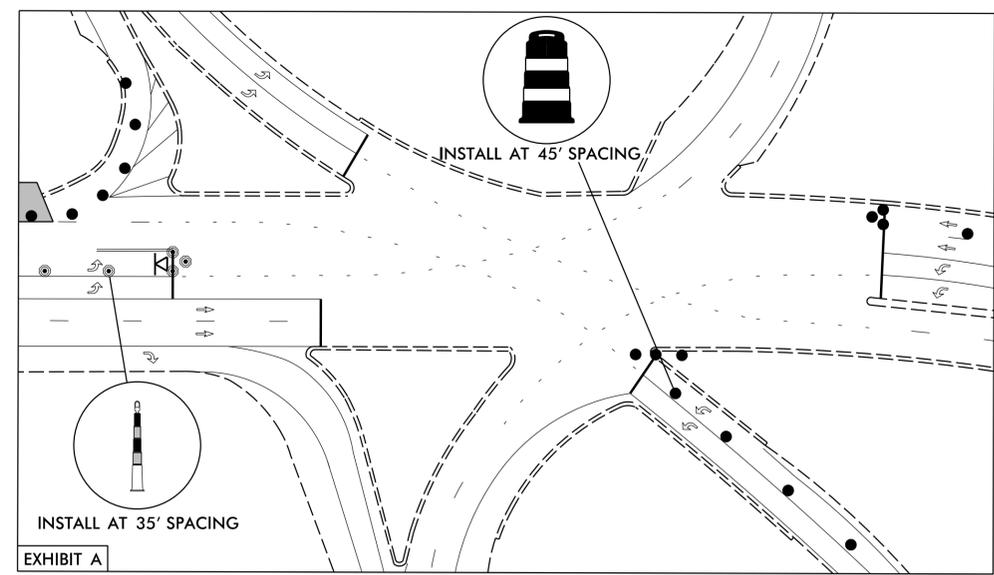
**BRIDGE #53
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 TRAFFIC CONTROL
 PHASE IIIA**

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INSTALL AT 20' SPACING IN RADIUS



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DATE: 3/25/2025

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**BRIDGE #53
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TRAFFIC CONTROL
PHASE IIIA**