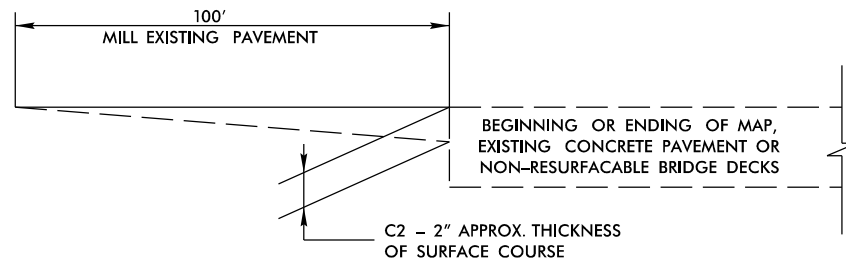


**DETAIL A
FULL DEPTH PAVED SHOULDER WIDENING
(INSIDE/MEDIAN)
STEP 1**

TO BE USED IN CONJUNCTION WITH T.S. 1 & 2
 I-74 EB MEDIAN STA. 72+00.00 TO 142+00.00 +/-
 I-74 EB MEDIAN STA. 142+00.00 TO 195+67.00 +/-
 I-74 EB MEDIAN STA. 212+28.00 TO 352+50.00 +/-

FOR FINAL MILLING AND RESURFACING FILL
 SEE TYPICAL SECTIONS 1 & 2



INCIDENTAL MILLING AT PAVEMENT TIE-INS

NOTES:

*PAVEMENT REHABILITATION FOR RAMP ENTRANCE AND EXIT LANES WILL INCLUDE ADDITIONAL WIDTH FOR AREAS OF GORES. (SEE PLANS FOR LIMITS)

**PAVEMENT REHABILITATION FOR MEDIAN CROSSOVERS AND LEFT TURN LANES WILL INCLUDE ADDITIONAL WIDTH. (SEE PLANS FOR LIMITS) LIMITS OF TYPICAL SECTIONS INDICATED ON THE PLANS ARE APPROXIMATE. TYPICAL SECTION LIMITS AND TRANSITIONS TO BE VERIFIED BY THE ENGINEER IN FIELD.

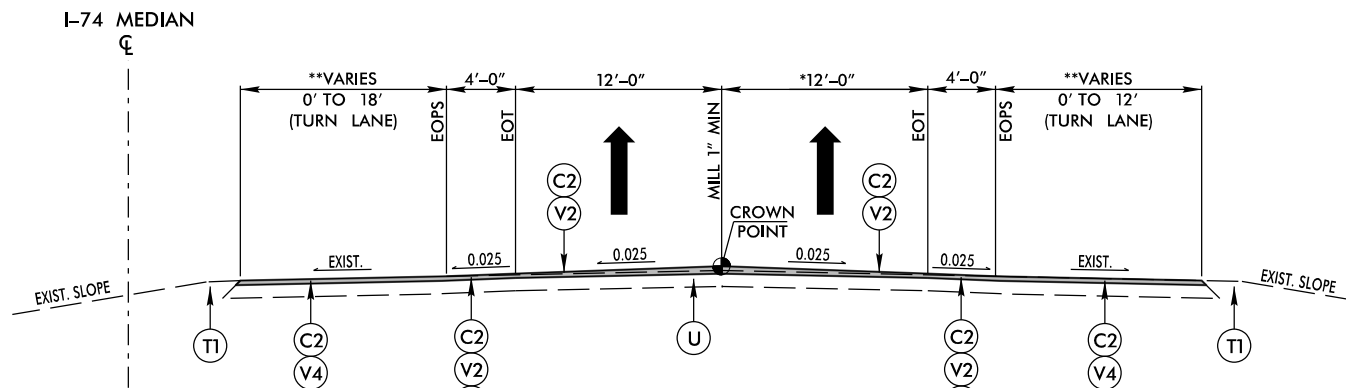
PAVE SHOULDERS FOR VEHICLE PULL-OFF AREA AS DIRECTED BY ENGINEER.

SURVEY EXISTING CURVE FROM APPROXIMATE STATION 88+35 TO 131+70 PROVIDE SURVEY TO NCDOT ENGINEER FOR DEPARTMENT/PEF REPRESENTATIVE TO COMPUTE RESURFACING PROFILE TO BE PROVIDED FOR YOUR USE PRIOR TO PAVING.

WITH RECOMMENDED RESURFACING GRADE PROVIDED, CONTRACTOR SHALL STAKE EVERY 50 FEET THROUGHOUT EACH CURVE FOR EACH LIFT. ITEM SHALL BE PAID FOR UNDER CONSTRUCTION SURVEYING.

EXISTING CROSS-SECTIONS WERE DEVELOPED FOR WEDGING QUANTITIES FROM LIDAR ONLY AND ARE PROVIDED FOR INFORMATIONAL BIDDING PURPOSES ONLY. FINAL QUANTITIES MAY VARY.

MILLED SECTIONS SHALL BE RESURFACED IN THE SAME NIGHT.

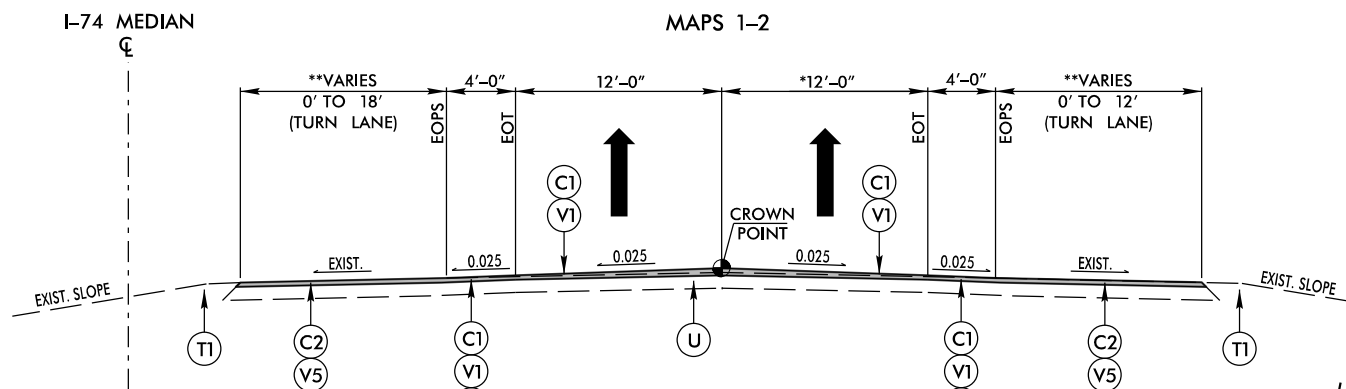


USE V2 (1" TO 2" MILLING) TO MILL A 0.025 SE GRADE

TYPICAL SECTION NO. 1

I-74 EB STA. 147+50.00 TO 352+40.00
 I-74 EB STA. 404+25.00 TO 447+55.00
 I-74 EB STA. 468+70.00 TO 529+25.00
 I-74 EB STA. 589+05.00 (END BRIDGE) TO 620+05.00 (BEGIN BRIDGE)
 I-74 EB STA. 622+85.00 (END BRIDGE) TO 627+80.00 (BEGIN BRIDGE)
 I-74 EB STA. 629+20.00 (END BRIDGE) TO 635+00.00

I-74 WB STA. 147+90.00 TO 345+85.00
 I-74 WB STA. 395+05.00 TO 447+90.00
 I-74 WB STA. 469+05.00 TO 529+60.00
 I-74 WB STA. 589+25.00 (END BRIDGE) TO 620+15.00 (BEGIN BRIDGE)
 I-74 WB STA. 623+00.00 (END BRIDGE) TO 627+95.00 (BEGIN BRIDGE)
 I-74 WB STA. 629+30.00 (END BRIDGE) TO 635+10.00

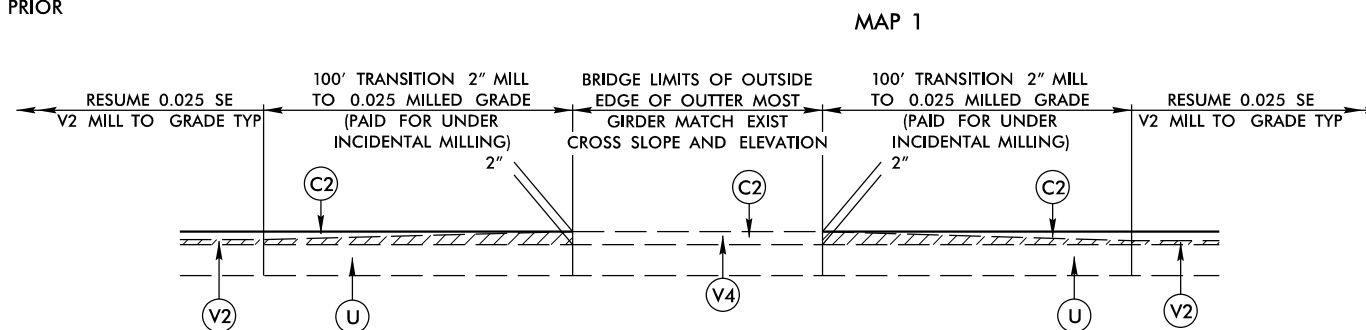


USE V1 (0.75" TO 1.5" MILLING) TO MILL A 0.025 SE GRADE

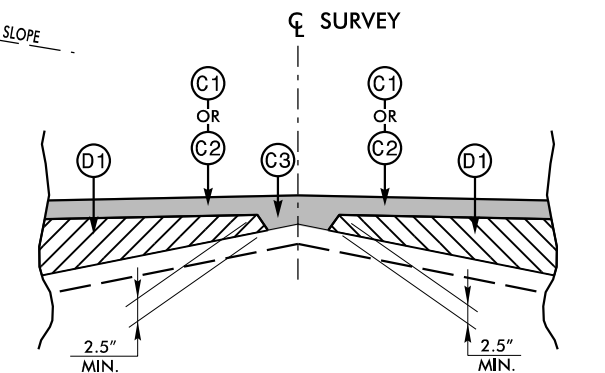
TYPICAL SECTION NO. 2

I-74 EB STA. 72+00.00 TO 147+50.00
 I-74 EB STA. 352+40.00 TO 404+25.00
 I-74 EB STA. 447+55.00 TO 468+70.00

I-74 WB STA. 72+00.00 TO 147+90.00
 I-74 WB STA. 345+85.00 TO 395+05.00
 I-74 WB STA. 447+90.00 TO 469+05.00



MILLING UNDER BRIDGES



PAVEMENT REHABILITATION LIMITS LEGEND

SEE PLAN SHEETS

NOT TO SCALE

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
C2	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YARD
C3	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH TO BE PLACED IN LAYERS NOT TO EXCEED 2.0" IN DEPTH.
D1	PROP. VAR. DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH TO BE PLACED IN LAYERS NOT LESS THAN 2.5" OR GREATER THAN 4" IN DEPTH.
E1	PROP. APPROX. 7.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 855 LBS. PER SQ. YARD
T1	SHOULDER RECONSTRUCTION
U	EXISTING ASPHALT PAVEMENT
V1	3/4" TO 1 1/2" MILLED ASPHALT PAVEMENT
V2	1" TO 2" MILLED ASPHALT PAVEMENT
V3	MILLED RUMBLE STRIPS
V4	2" MILLING
V5	1.5" MILLING

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE. SEE SHOULDER WEDGE DETAIL.