



RECEIVED

DEC 9 1969

thereby certify that this Structure was  
built according to plans except as

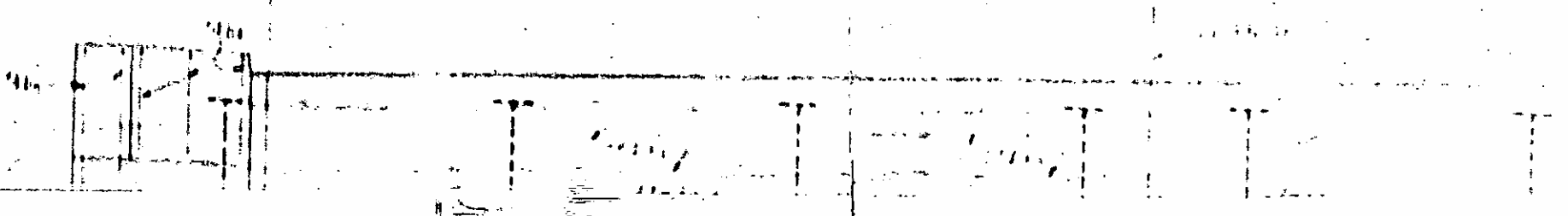
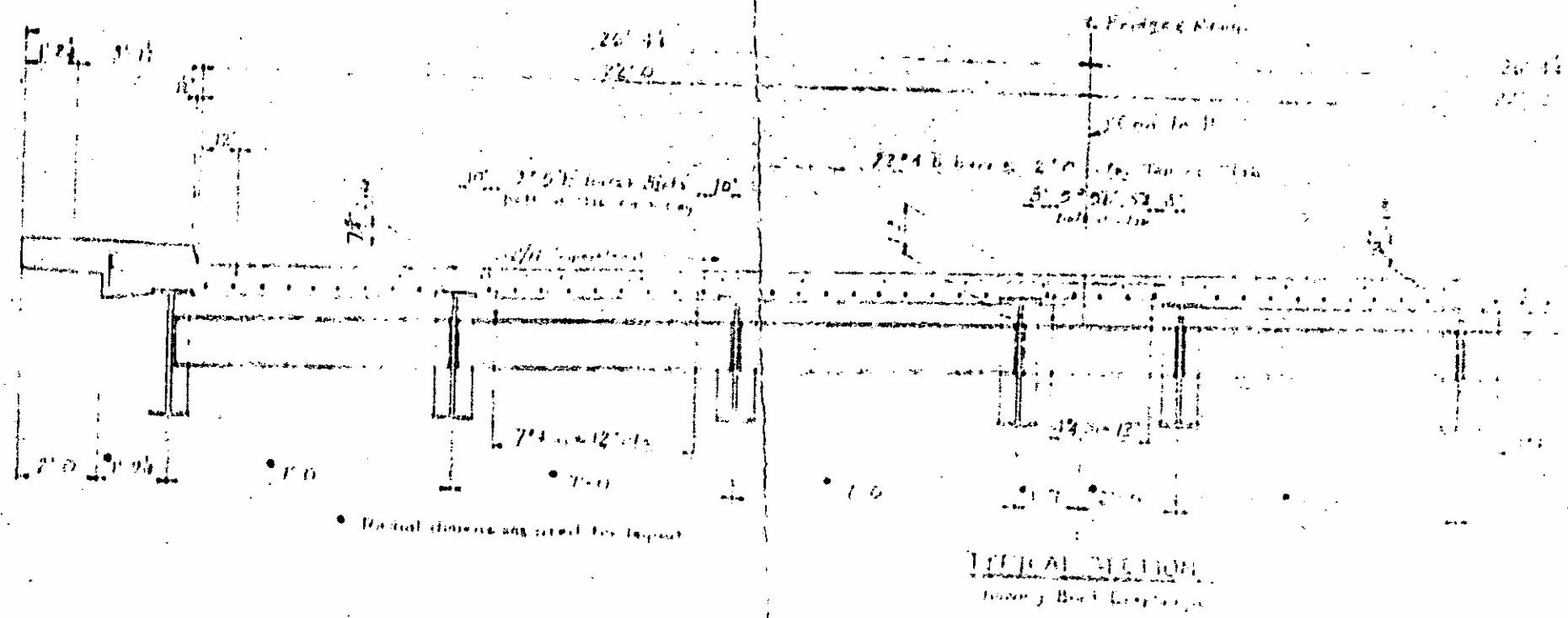
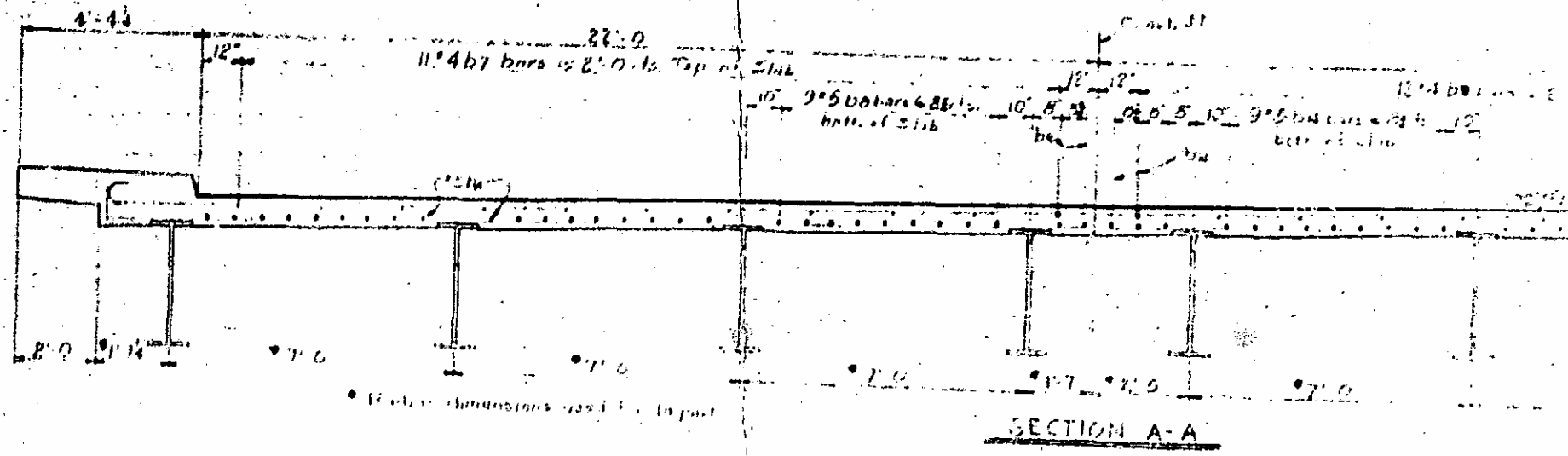
noted herein G.W. Clayton Jr. 9-9-69  
Resident Engineer

PROJECT NO. 10111

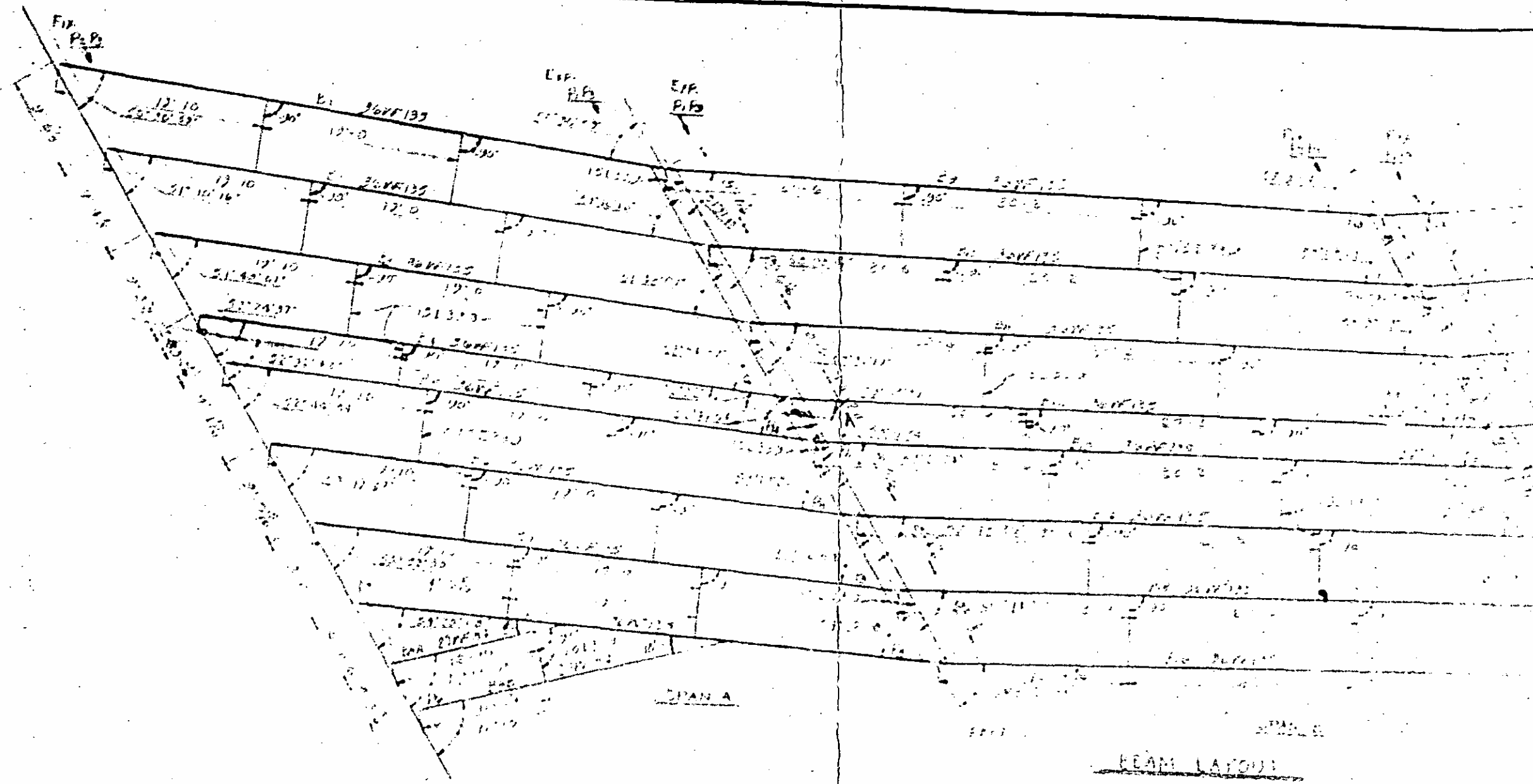
COUNTY

STATION

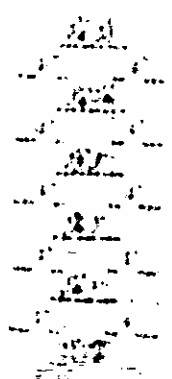
STATE HIGHWAY COMMISSION







1	12' 10"	21' 10" 1/2
2	12' 10"	21' 10" 1/2
3	12' 10"	21' 10" 1/2
4	12' 10"	21' 10" 1/2
5	12' 10"	21' 10" 1/2
6	12' 10"	21' 10" 1/2
7	12' 10"	21' 10" 1/2
8	12' 10"	21' 10" 1/2
9	12' 10"	21' 10" 1/2
10	12' 10"	21' 10" 1/2
11	12' 10"	21' 10" 1/2
12	12' 10"	21' 10" 1/2
13	12' 10"	21' 10" 1/2
14	12' 10"	21' 10" 1/2
15	12' 10"	21' 10" 1/2
16	12' 10"	21' 10" 1/2
17	12' 10"	21' 10" 1/2



**BEAM LAYOUT**

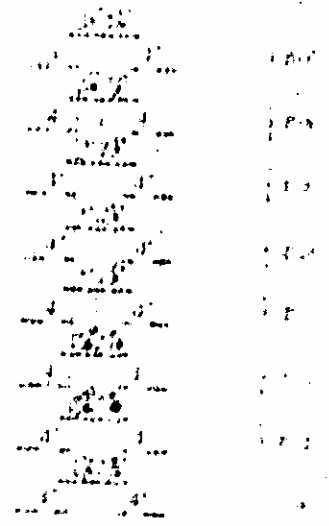






PLAN 1001

PLAN 1001



19' 10"  
 16' 0"  
 14' 0"  
 46' 10"  
 17' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"  
 11' 0"  
 14' 0"

PROJECT NO. \_\_\_\_\_  
 COUNTY \_\_\_\_\_  
 STATION \_\_\_\_\_

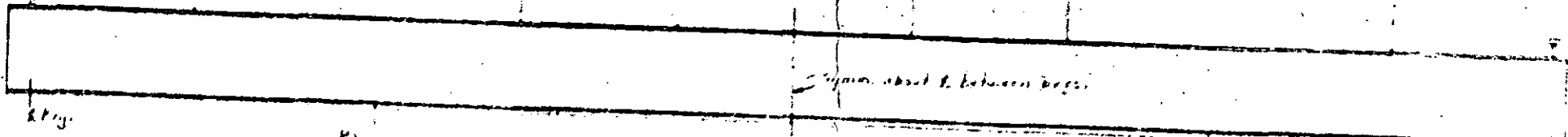
STATE OF NORTH CAROLINA  
**STATE HIGHWAY COMMISSION**  
 Raleigh

DATE OF PLAN \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_

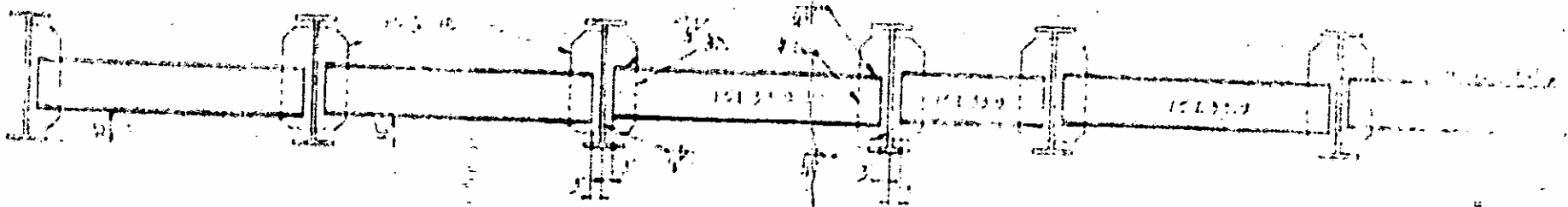
NO.	DATE	BY	DATE

SCALE  
 1" = 20'  
 10' 0" = 10' 0"

B1	19 Spa. 4"	22 Spa. 6"	8 Spa. 9"	9 Spa. 12"	8 Spa. 9"	25 Spa. 6"	17 Spa. 4"
B2	21 Spa. 4"	23 Spa. 6"	8 Spa. 9"	9 Spa. 12"	8 Spa. 9"	23 Spa. 6"	21 Spa. 4"
B3	23 Spa. 4"	22 Spa. 6"	8 Spa. 9"	7 Spa. 12"	8 Spa. 9"	22 Spa. 6"	23 Spa. 6"
B4	24 Spa. 4"	22 Spa. 6"	8 Spa. 9"	7 Spa. 12"	8 Spa. 9"	22 Spa. 6"	24 Spa. 4"
B5	23 Spa. 4"	22 Spa. 6"	8 Spa. 9"	7 Spa. 12"	8 Spa. 9"	22 Spa. 6"	23 Spa. 6"
B6	21 Spa. 4"	22 Spa. 6"	8 Spa. 9"	7 Spa. 12"	8 Spa. 9"	22 Spa. 6"	21 Spa. 4"
B7	21 Spa. 4"	23 Spa. 6"	8 Spa. 9"	7 Spa. 12"	9 Spa. 9"	22 Spa. 6"	21 Spa. 4"
B8	17 Spa. 4"	23 Spa. 6"	8 Spa. 9"	7 Spa. 12"	8 Spa. 9"	23 Spa. 6"	17 Spa. 4"



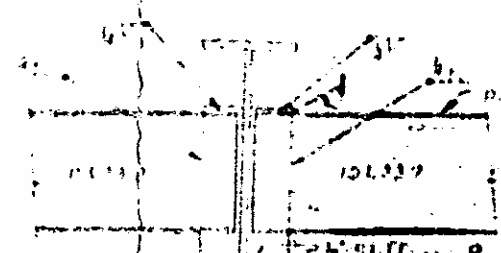
B1	15.2	15.2	6 x 5/16	6 x 1/2	15.2
B2, B3, B4	17.9	17.9	7 x 5/16	8 x 7/16	17.9
B5, B6, B7	16.6	16.6	6 x 5/16	6 x 5/16	16.6
B8	17.6	17.6	7 x 5/16	9 x 1/16	17.6



INTERIOR DIAPHRAGM



14' pipe...  
above floor...  
Exit door only



24' SHILLING P.

21.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
22.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
23.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
24.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
25.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
26.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
27.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
28.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
29.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12
30.00.56	2.00.09	2.00.12	2.00.12	2.00.12	2.00.12

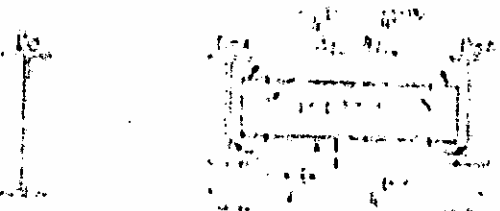
Sheet 13 of 13

NO.	DESCRIPTION	QTY	UNIT
1	10x5" F	39.6'	9X
2	10x5" F	39.6'	9X
3	10x5" F	39.6'	9X

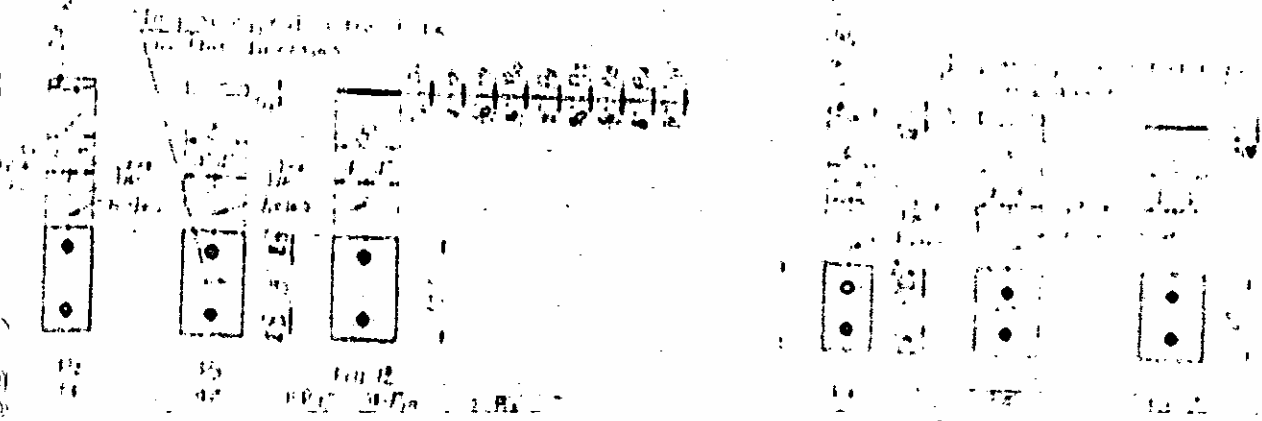
14.00.56	17.0	17.0	17.0
15.00.56	17.0	17.0	17.0
16.00.56	17.0	17.0	17.0

NOTE: The contractor may, at his option but without charge in the contract price, at structural steel, use of the AISC connectors bolted to the beam web and welded to the channel depth in case of the welded plate connections as shown.

In lieu of the weld connections for shop and field welds indicated for the interior diaphragm connections, the contractor may, at his option, use steel connectors bolted to the beam web and field weld the connectors to the connector plates. In this case, the beam must be checked for stress in plates as they weld to the beam web.



PART INCLUDING DIAPHRAGM



Bottom	Dist.
0'-6"	
12'-5.00'-0"	