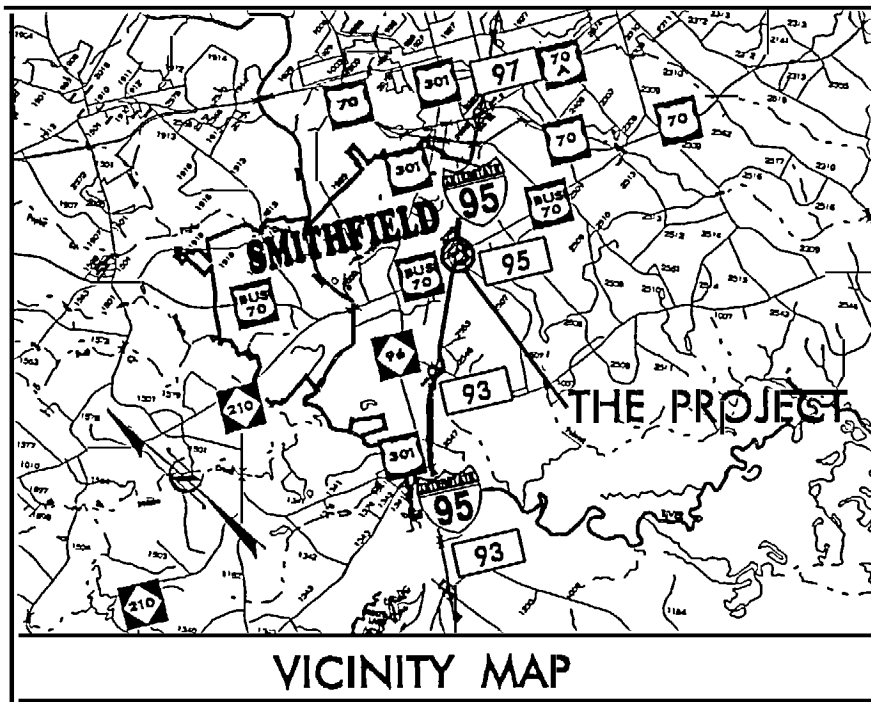


09/08/04

TIP PROJECT: I-4925



VICINITY MAP

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

JOHNSTON COUNTY

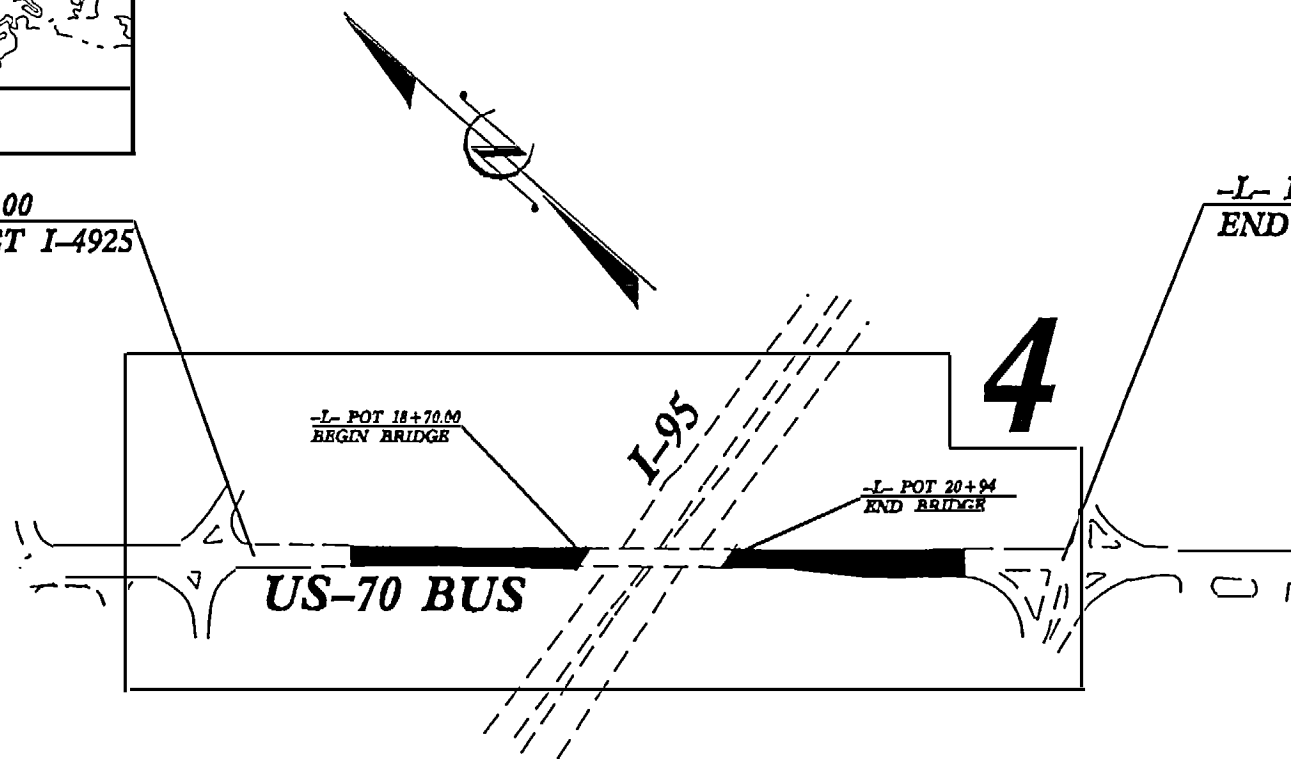
LOCATION: BRIDGE #66 AND APPROACHES ON US 70 BUS.
OVER I-95 IN SMITHFIELD

TYPE OF WORK: BRIDGE JACKING, GRADING, PAVING, MILLING,
AND GUARDRAIL.

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-4925	1	31
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
40891.3.1	IMS-95-2(101)97	CONST	

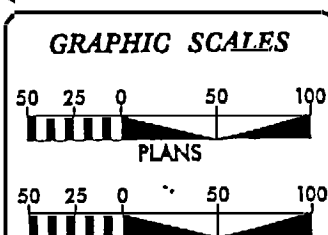
-L- POC 13+50.00
BEGIN PROJECT I-4925

-L- POT 26+20.00
END PROJECT I-4925



#66

TRACT:



DESIGN DATA

ADT 2004 =	14,000
ADT =	
DHV =	%
D =	%
T =	%

PROJECT LENGTH

LENGTH ROADWAY TIP PROJECT I-4925 =	0.198 MILES
LENGTH STRUCTURE TIP PROJECT I-4925 =	0.042 MILES
TOTAL LENGTH OF TIP PROJECT I-4925 =	0.240 MILES

Prepared in the Office of:
DIVISION OF HIGHWAYS
Division 4 DDC 509 Ward Bldg., Wilson NC, 27895

2006 STANDARD SPECIFICATIONS

RIGHT OF WAY DATE: NA

R. E. GREENE, JR., PE
PROJECT ENGINEER

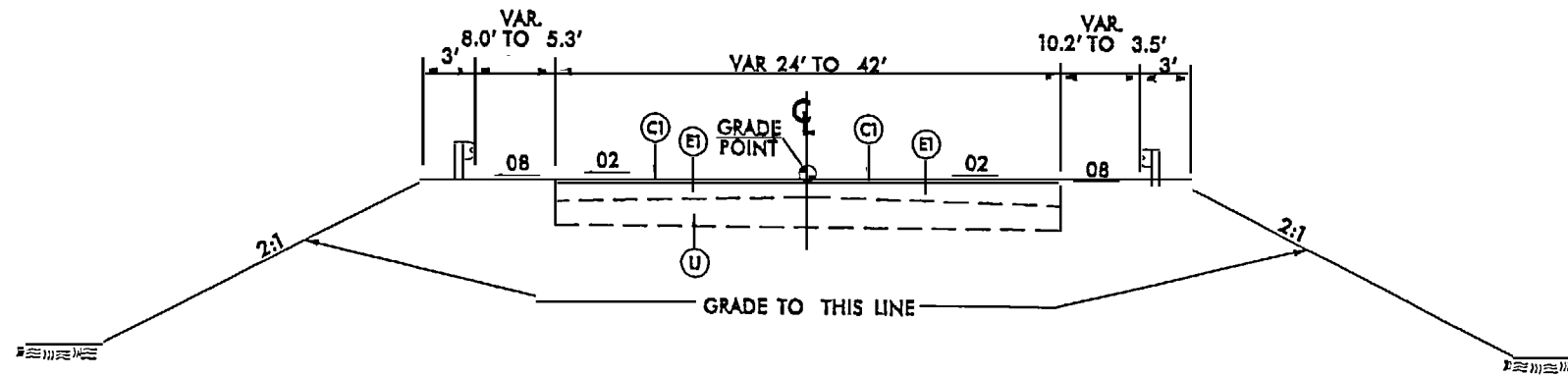
HYDRAULICS ENGINEER

SIGNATURE: _____ P.E.

ROADWAY DESIGN ENGINEER

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

6/22/99

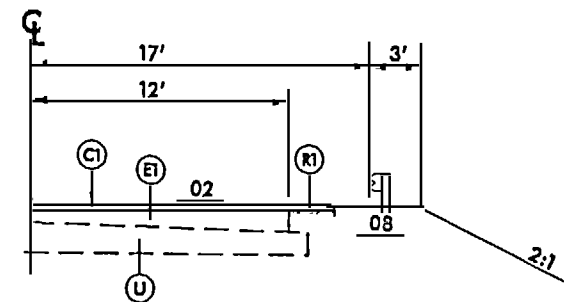


TYPICAL SECTION NO. 1

USE TYPICAL SECTION NO. 1 AS FOLLOWS:

- L- STA 15+00 TO -L- STA 18+70
- L- STA 20+94 TO -L- STA 24+70

PROJECT REFERENCE NO. 1-4925	SHEET NO. 2
ROADWAY DESIGN ENGINEER [Signature]	PAVEMENT DESIGN ENGINEER

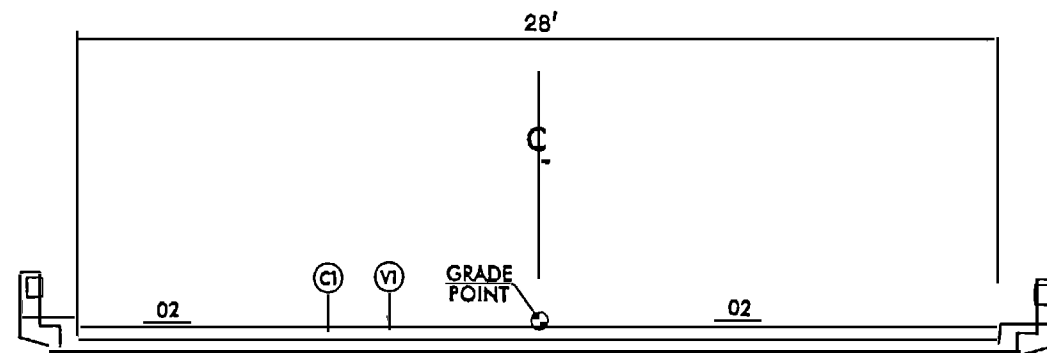


PARTIAL TYPICAL SECTION

USE PARTIAL TYPICAL SECTION WITH TYPICAL SECTION NO. 1 FOR RIGHT AND LEFT SIDES 15' BEFORE AND AFTER BRIDGE

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 69.6B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E1	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B26.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5 1/2" IN DEPTH.
R1	SHOULDER BERM GUTTER
U	EXISTING PAVEMENT.
V1	MILLING BITUMINOUS PAVEMENT. 4" DEPTH.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



TYPICAL SECTION ON STRUCTURE

-L- STA 18+70 TO -L- STA 20+94

08-AUG-2005 14:46
D:\Roadway\Plans\14925-ddc4-typ.dgn

Note: Not to Scale

*S.U.E. = Subsurface Utility Engineering

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

CONVENTIONAL PLAN SHEET SYMBOLS

BOUNDARIES AND PROPERTY:

State Line	-----
County Line	-----
Township Line	-----
City Line	-----
Reservation Line	-----
Property Line	-----
Existing Iron Pin	⊙
Property Corner	⊙
Property Monument	⊙
Parcel/Sequence Number	⊙
Existing Fence Line	-x-x-x-
Proposed Woven Wire Fence	○
Proposed Chain Link Fence	□
Proposed Barbed Wire Fence	◇
Existing Wetland Boundary	-----
Proposed Wetland Boundary	-----
Existing High Quality Wetland Boundary	-----
Existing Endangered Animal Boundary	-----
Existing Endangered Plant Boundary	-----

BUILDINGS AND OTHER CULTURE:

Gas Pump Vent or U/G Tank Cap	○
Sign	⊙
Well	⊙
Small Mine	⊙
Foundation	⊙
Area Outline	⊙
Cemetery	⊙
Building	⊙
School	⊙
Church	⊙
Dam	⊙

HYDROLOGY:

Stream or Body of Water	-----
Hydro, Pool or Reservoir	-----
River Basin Buffer	-----
Flow Arrow	-----
Disappearing Stream	-----
Spring	-----
Swamp Marsh	-----
Proposed Lateral, Tail, Head Ditch	-----
False Sump	-----

RAILROADS:

Standard Gauge	-----
RR Signal Milepost	-----
Switch	-----
RR Abandoned	-----
RR Dismantled	-----

RIGHT OF WAY:

Baseline Control Point	-----
Existing Right of Way Marker	-----
Existing Right of Way Line	-----
Proposed Right of Way Line	-----
Proposed Right of Way Line with Iron Pin and Cap Marker	-----
Proposed Right of Way Line with Concrete or Granite Marker	-----
Existing Control of Access	-----
Proposed Control of Access	-----
Existing Easement Line	-----
Proposed Temporary Construction Easement	-----
Proposed Temporary Drainage Easement	-----
Proposed Permanent Drainage Easement	-----
Proposed Permanent Utility Easement	-----

ROADS AND RELATED FEATURES:

Existing Edge of Pavement	-----
Existing Curb	-----
Proposed Slope Stakes Cut	-----
Proposed Slope Stakes Fill	-----
Proposed Wheel Chair Ramp	-----
Curb Cut for Future Wheel Chair Ramp	-----
Existing Metal Guardrail	-----
Proposed Guardrail	-----
Existing Cable Guiderail	-----
Proposed Cable Guiderail	-----
Equality Symbol	-----
Pavement Removal	-----

VEGETATION:

Single Tree	-----
Single Shrub	-----
Hedge	-----
Woods Line	-----
Orchard	-----
Vineyard	-----

EXISTING STRUCTURES:

MAJOR:	
Bridge, Tunnel or Box Culvert	-----
Bridge Wing Wall, Head Wall and End Wall	-----
MINOR:	
Head and End Wall	-----
Pipe Culvert	-----
Footbridge	-----
Drainage Box: Catch Basin, DI or JB	-----
Paved Ditch Gutter	-----
Storm Sewer Manhole	-----
Storm Sewer	-----

UTILITIES:

POWER:	
Existing Power Pole	-----
Proposed Power Pole	-----
Existing Joint Use Pole	-----
Proposed Joint Use Pole	-----
Power Manhole	-----
Power Line Tower	-----
Power Transformer	-----
U/G Power Cable Hand Hole	-----
H-Frame Pole	-----
Recorded U/G Power Line	-----
Designated U/G Power Line (S.U.E.*)	-----

TELEPHONE:

Existing Telephone Pole	-----
Proposed Telephone Pole	-----
Telephone Manhole	-----
Telephone Booth	-----
Telephone Pedestal	-----
Telephone Cell Tower	-----
U/G Telephone Cable Hand Hole	-----
Recorded U/G Telephone Cable	-----
Designated U/G Telephone Cable (S.U.E.*)	-----
Recorded U/G Telephone Conduit	-----
Designated U/G Telephone Conduit (S.U.E.*)	-----
Recorded U/G Fiber Optics Cable	-----
Designated U/G Fiber Optics Cable (S.U.E.*)	-----

WATER:

Water Manhole	-----
Water Meter	-----
Water Valve	-----
Water Hydrant	-----
Recorded U/G Water Line	-----
Designated U/G Water Line (S.U.E.*)	-----
Above Ground Water Line	-----

TV:

TV Satellite Dish	-----
TV Pedestal	-----
TV Tower	-----
U/G TV Cable Hand Hole	-----
Recorded U/G TV Cable	-----
Designated U/G TV Cable (S.U.E.*)	-----
Recorded U/G Fiber Optic Cable	-----
Designated U/G Fiber Optic Cable (S.U.E.*)	-----

GAS:

Gas Valve	-----
Gas Meter	-----
Recorded U/G Gas Line	-----
Designated U/G Gas Line (S.U.E.*)	-----
Above Ground Gas Line	-----

SANITARY SEWER:

Sanitary Sewer Manhole	-----
Sanitary Sewer Cleanout	-----
U/G Sanitary Sewer Line	-----
Above Ground Sanitary Sewer	-----
Recorded SS Forced Main Line	-----
Designated SS Forced Main Line (S.U.E.*)	-----

MISCELLANEOUS:

Utility Pole	-----
Utility Pole with Base	-----
Utility Located Object	-----
Utility Traffic Signal Box	-----
Utility Unknown U/G Line	-----
U/G Tank; Water, Gas, Oil	-----
AG Tank; Water, Gas, Oil	-----
U/G Test Hole (S.U.E.*)	-----
Abandoned According to Utility Records	-----
End of Information	-----

AATUR
E.O.I.

PROJECT REFERENCE NO.	SHEET NO.
I-4925	4
R/W SHEET NO.	N/A
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

PI Sta 11+85.97
 $\Delta = 9'00''42.0''$ (RT)
 $D = 2'25''40.4''$
 $L = 311.17'$
 $T = 185.97'$
 $R = 2,359.63'$

-L- POC 13+50.00
 BEGIN PROJECT I-4925

-L- PT 13+71.17

-L- PINC 17+59.66
 $\Delta = 00'03''43.0''$ (LT)

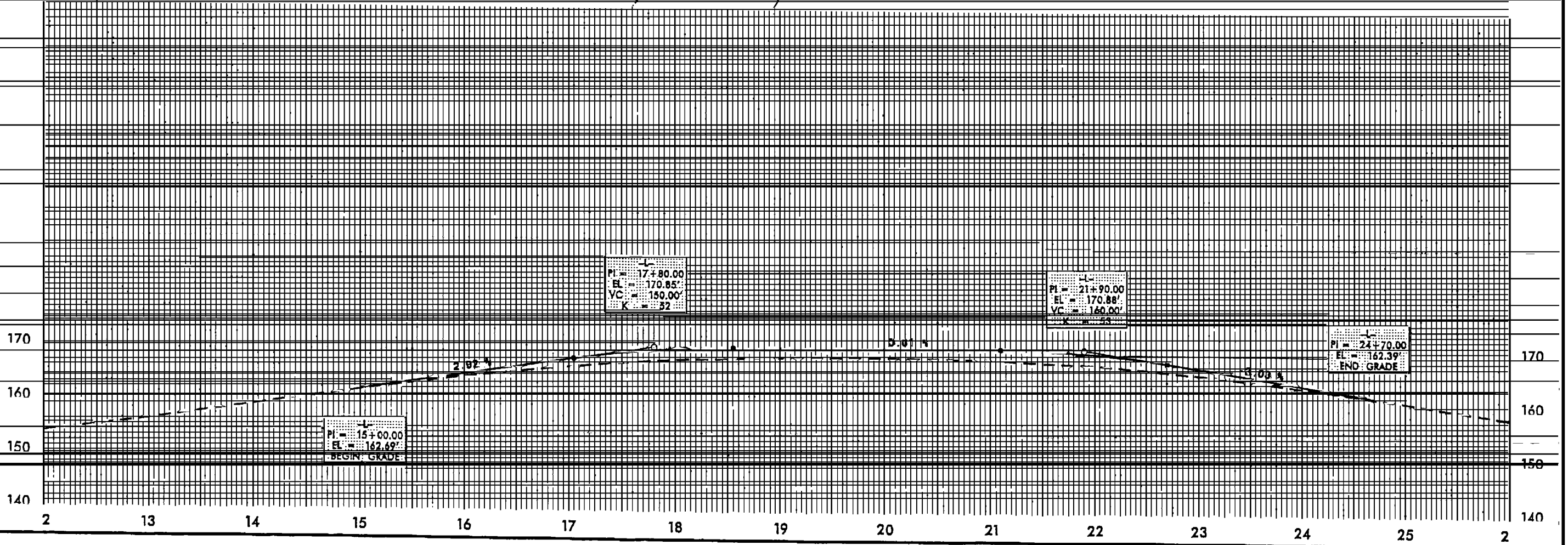
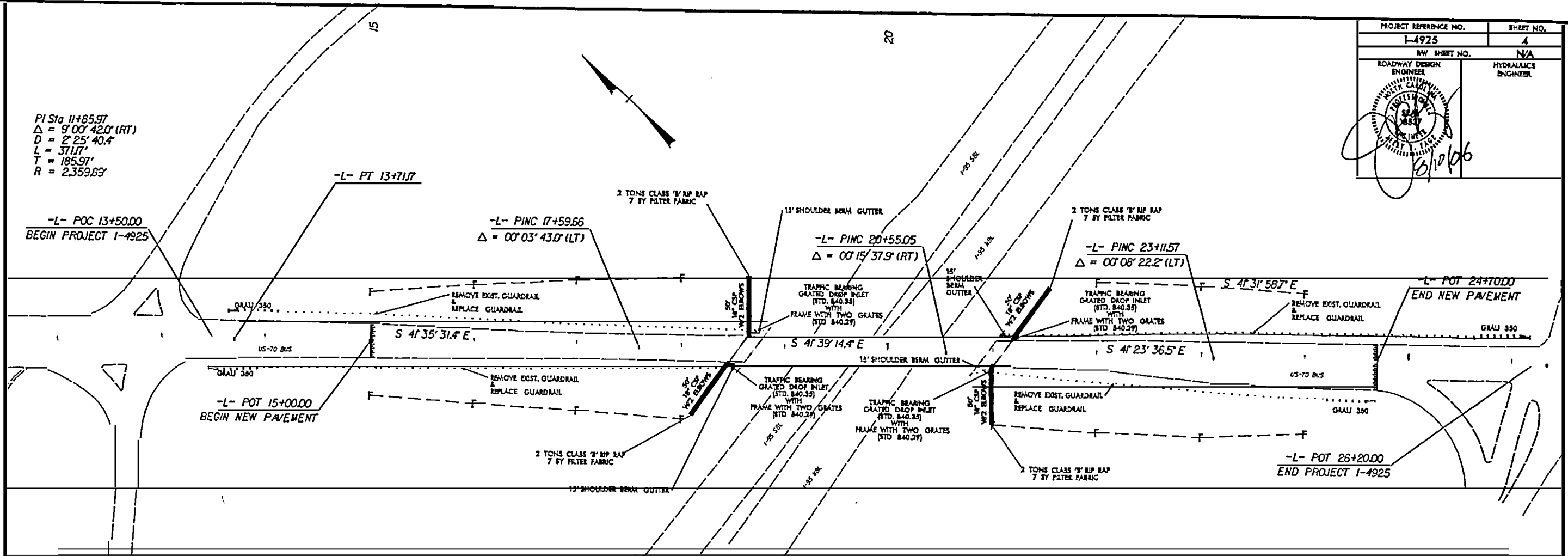
-L- PINC 20+55.05
 $\Delta = 00'15''37.9''$ (RT)

-L- PINC 23+11.57
 $\Delta = 00'08''22.2''$ (LT)

-L- POT 24+70.00
 END NEW PAVEMENT

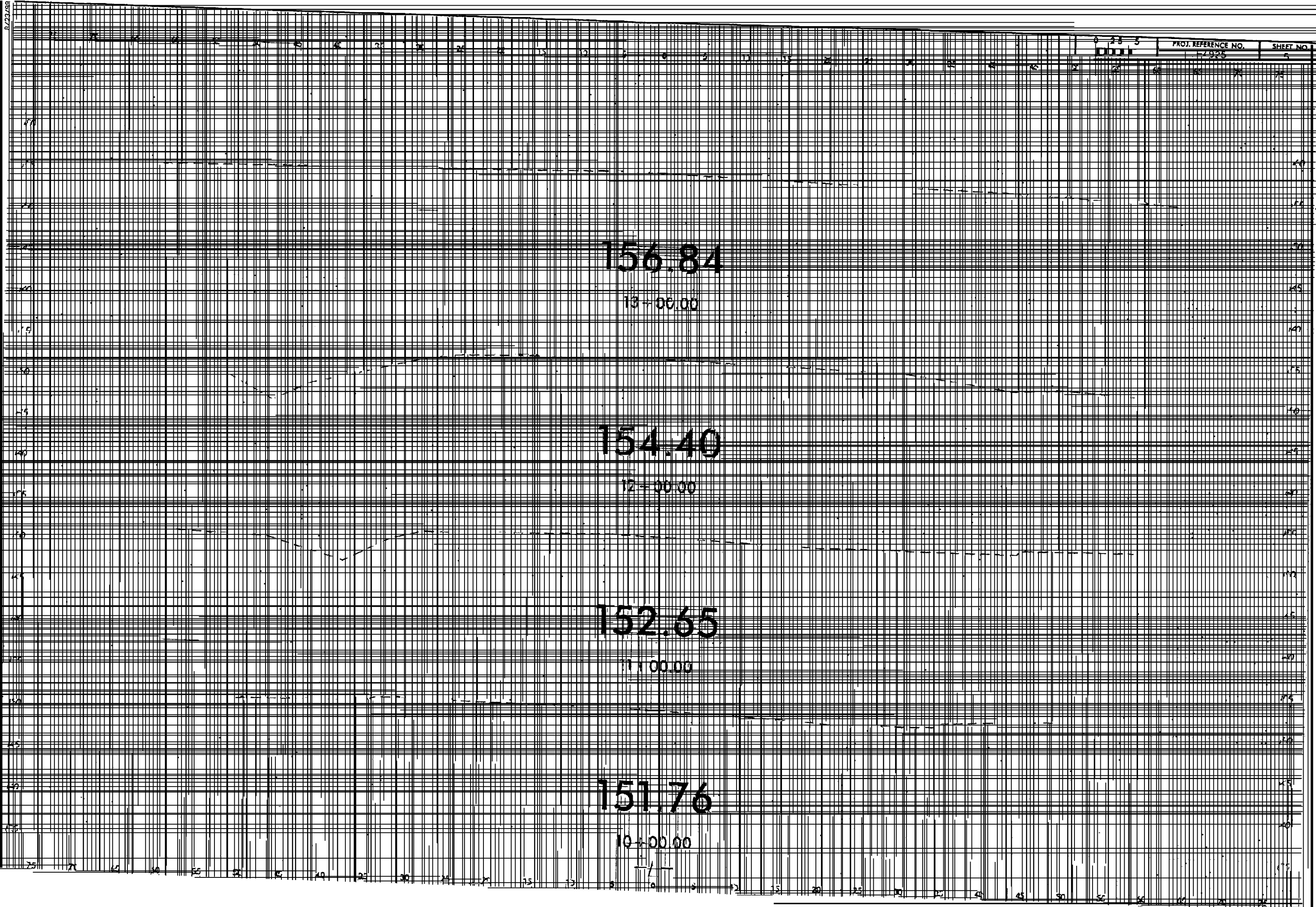
-L- POT 15+00.00
 BEGIN NEW PAVEMENT

-L- POT 26+20.00
 END PROJECT I-4925



08/26/06 11:42 AM
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REVISIONS



PROJ. REFERENCE NO. 17925 SHEET NO. 5

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154.40

12+00.00

152.65

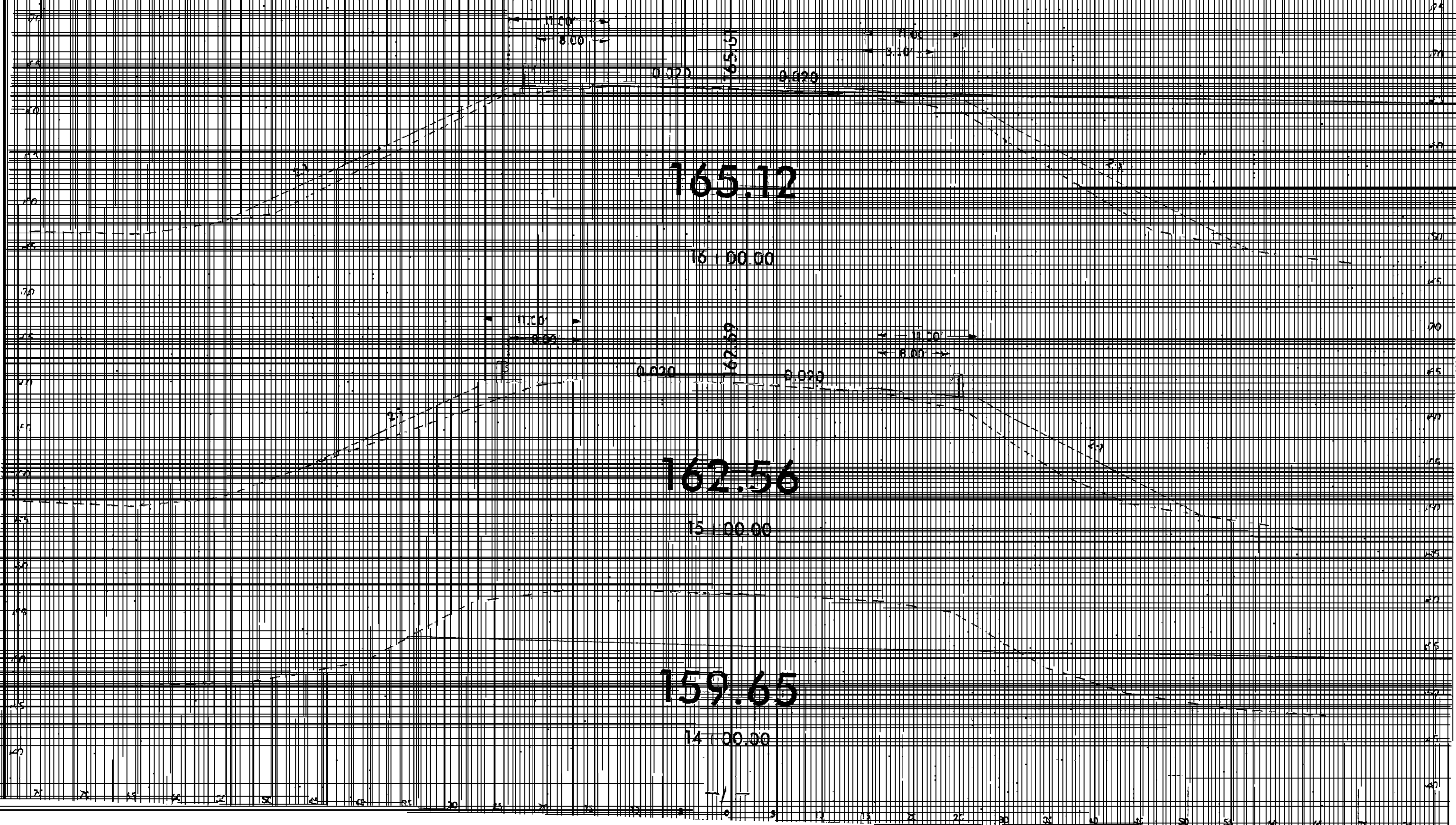
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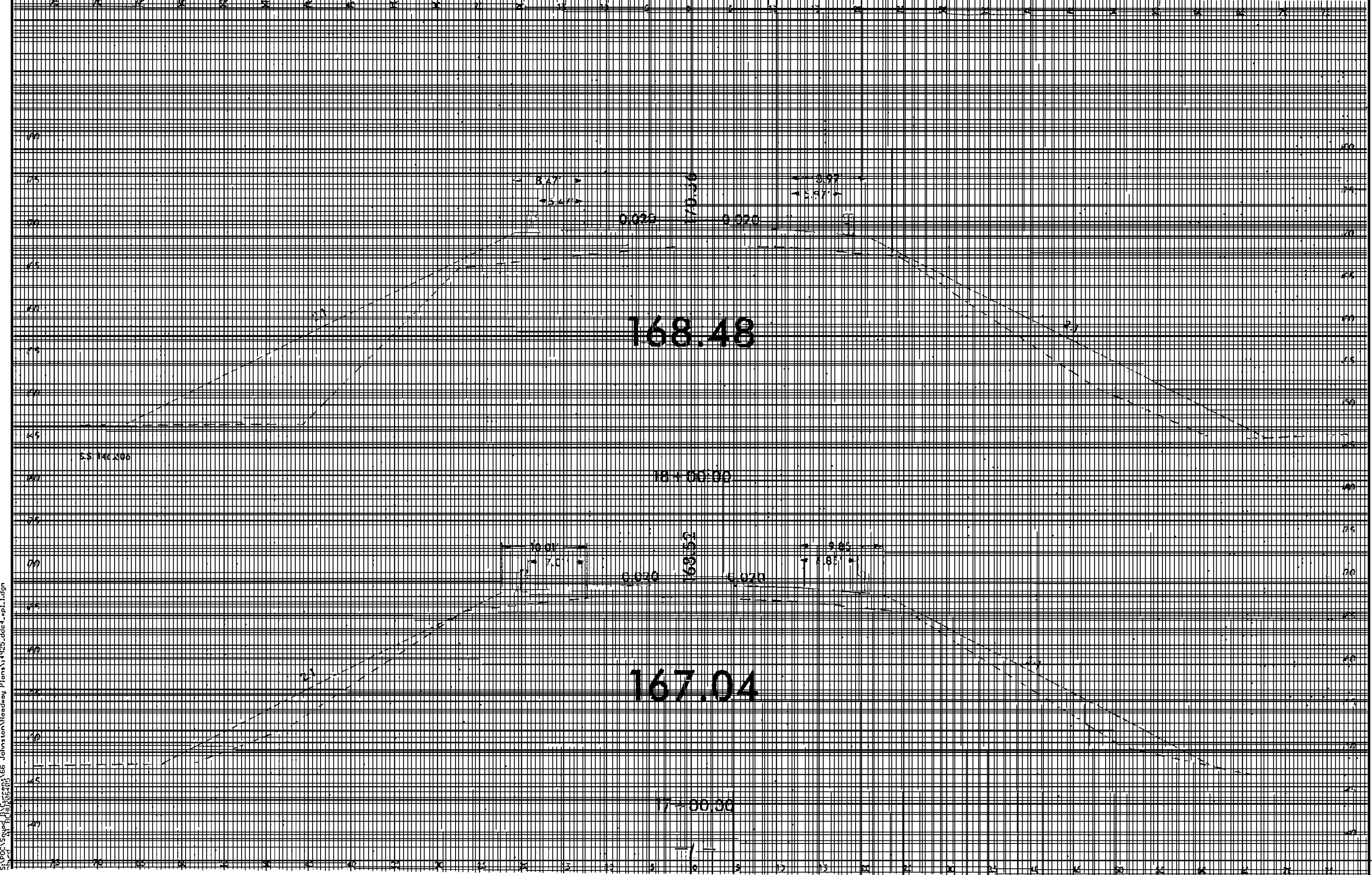
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8/27/99



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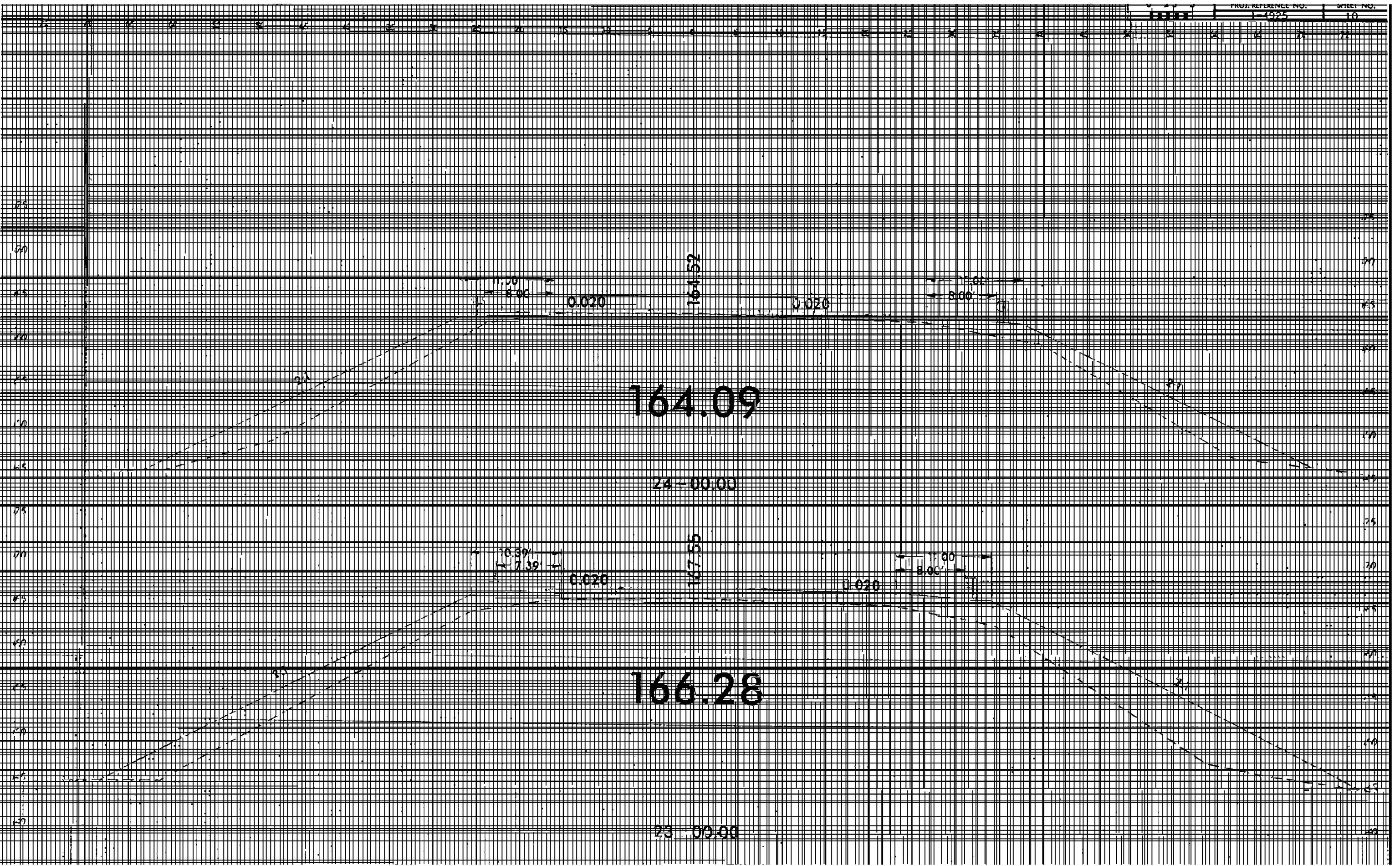
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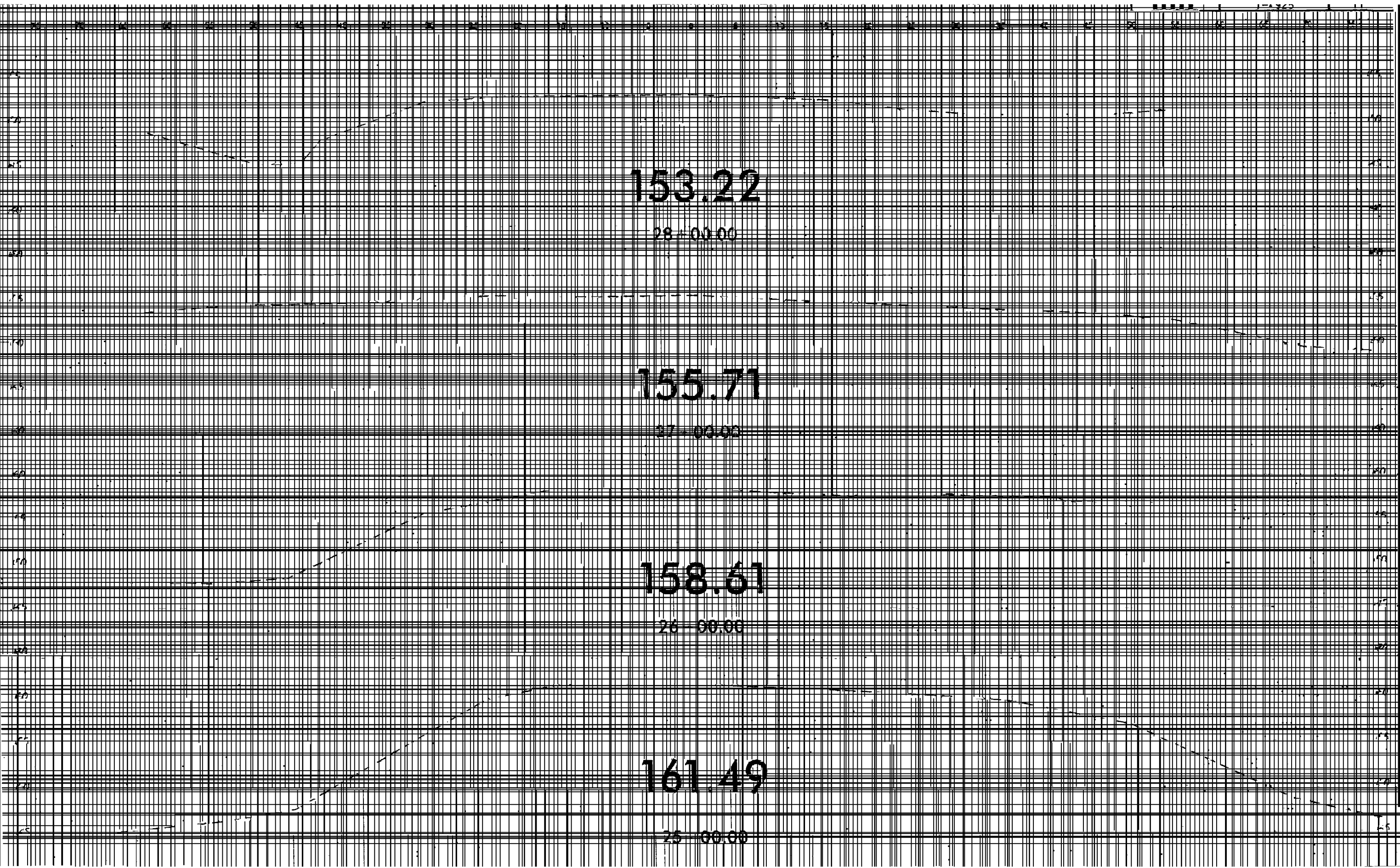
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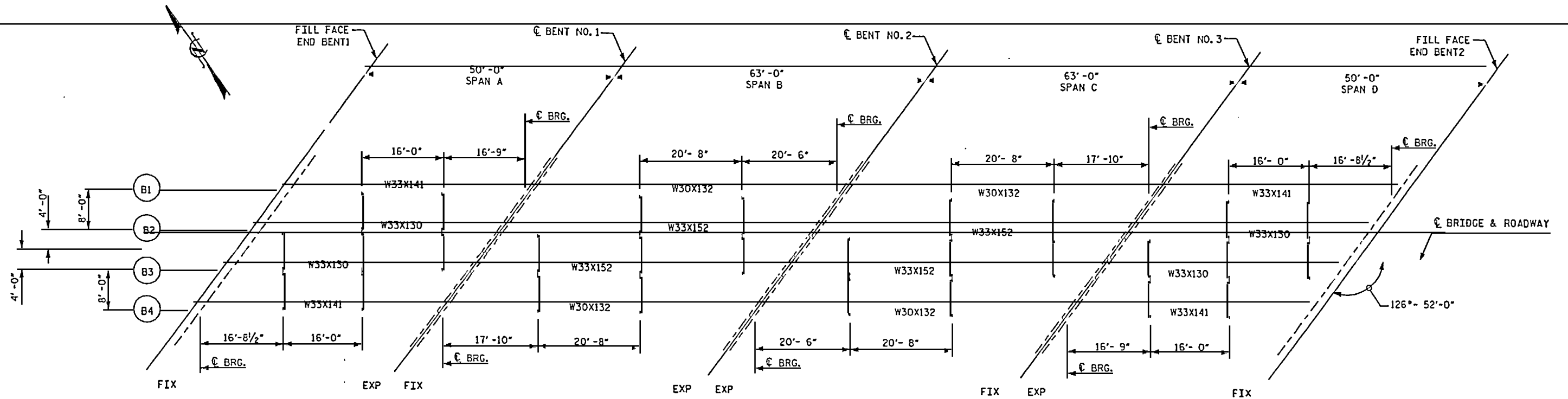
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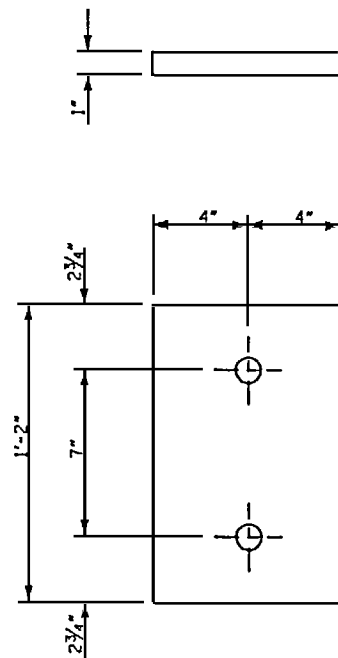


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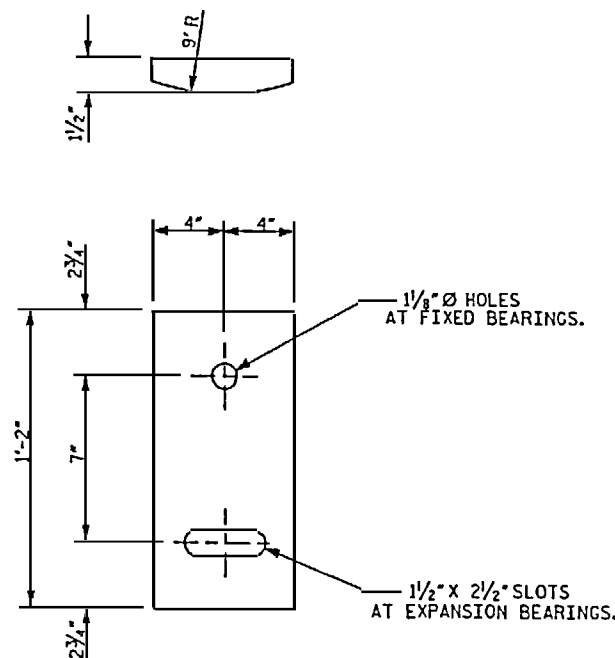
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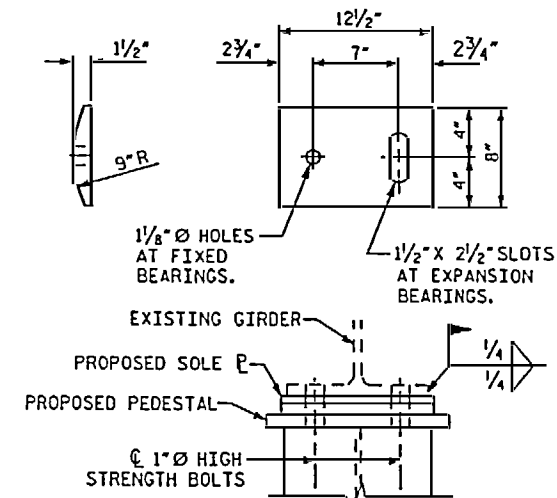
STRUCTURAL STEEL PLAN



EXISTING MASONRY PLATE DETAIL
(8" X 1" X 12 1/2" PL)



EXISTING SOLE PLATE DETAIL
(6" X 1 1/2" X 12 1/2" PL)

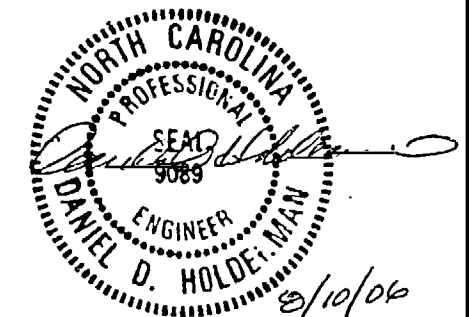


PROPOSED SOLE PLATE

BENTS 1, 2 & 3
(AS REQ'D BY THE ENGINEER)

NOTE

FOR CONNECTION OF PROPOSED SOLE PLATE AND PEDESTAL, SEE BEARING ATTACHMENT DETAIL ON SHEET NO. 20.



PROJECT NO. 40891.3.1
JOHNSTON COUNTY

BRIDGE NO. 66

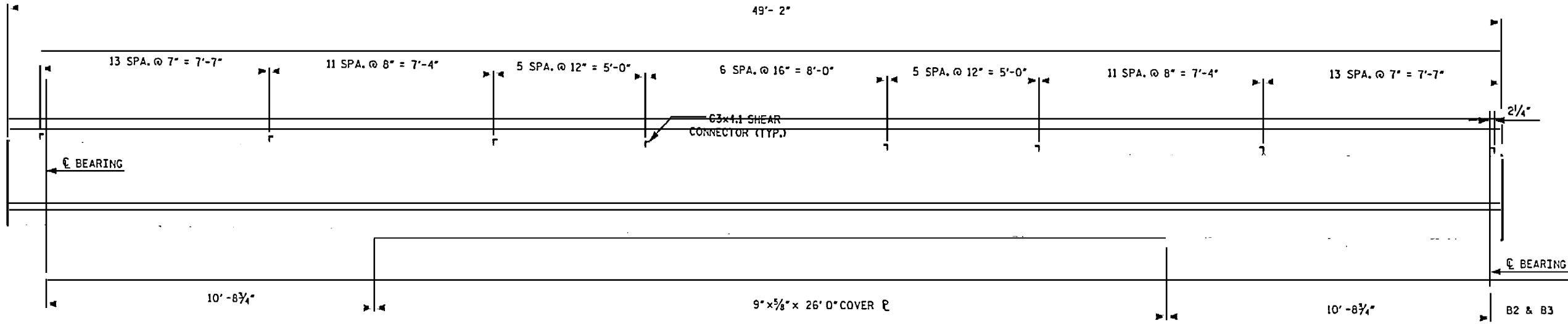
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
**EXISTING SUPERSTRUCTURE
GIRDER LAYOUT**

REVISIONS						SHEET NO.
NO.	BY	DATE	NO.	BY	DATE	13
1			3			31
2			4			

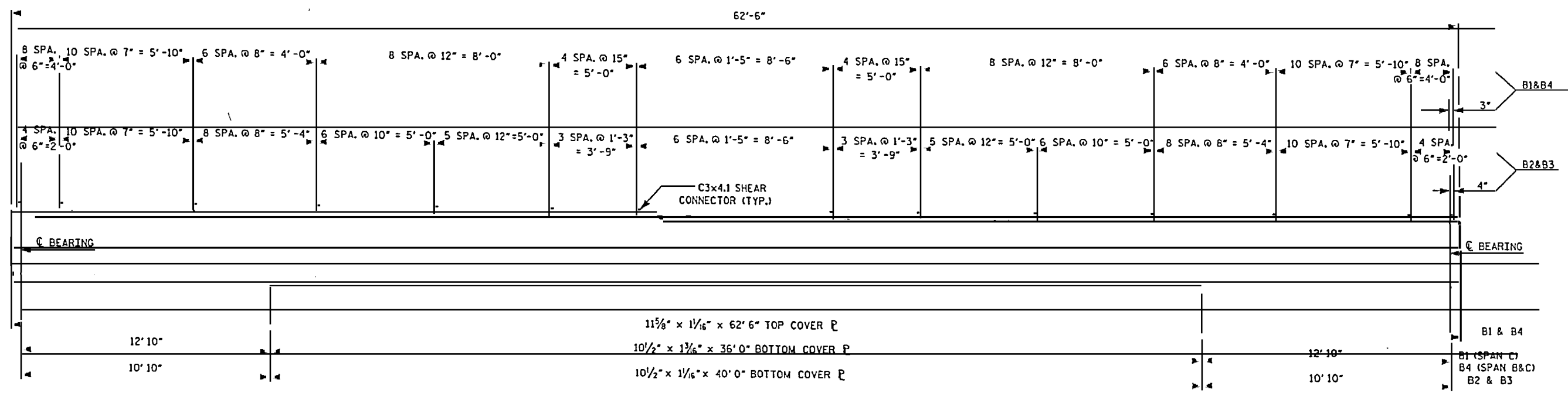
DRAWN BY: P. BYRD / S. I. SANDOR DATE: 7/26/06
CHECKED BY: J. YANNAKONE DATE: 7/27/06

10-AUG-2006 08:35
DGN*****DCN*****
pbyrd AT BCM206105

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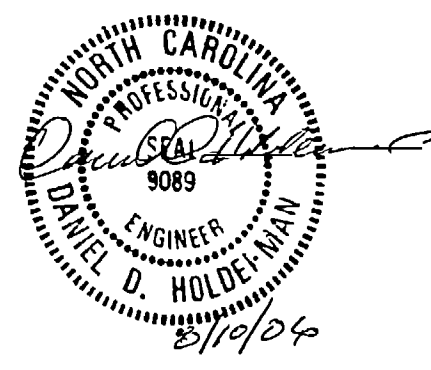


SPAN A&D



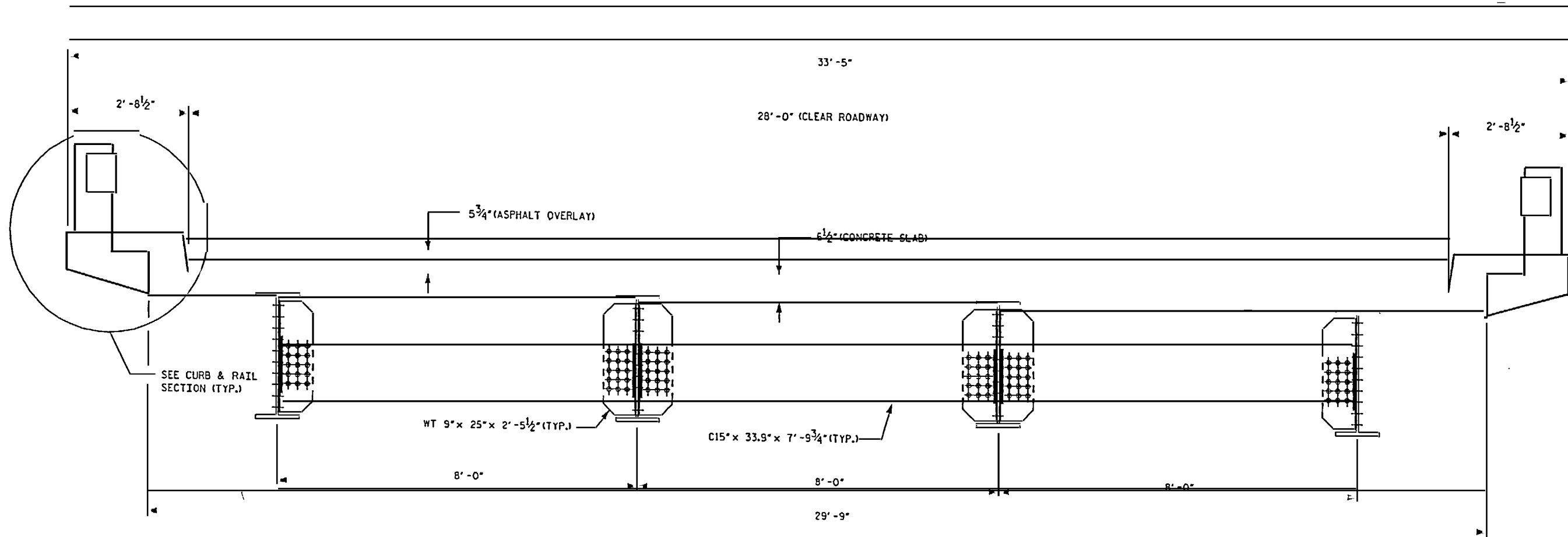
SPAN B&C

BEAM ELEVATION SHOWING COVER & SHEAR CONNECTOR SPACING

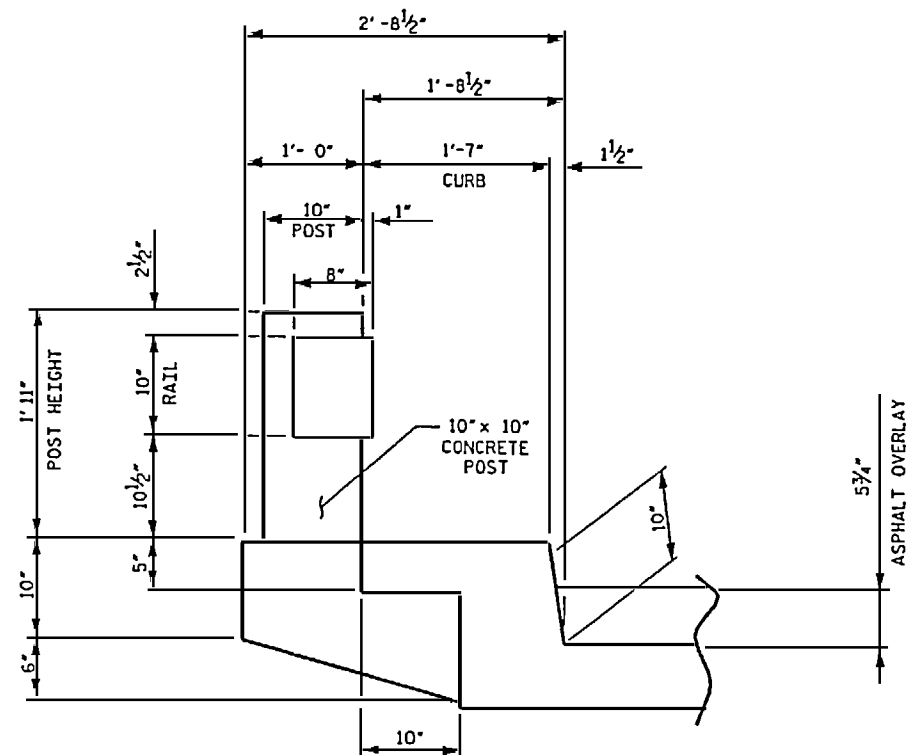


PROJECT NO. 40891.3.1
JOHNSTON COUNTY
BRIDGE NO. 66

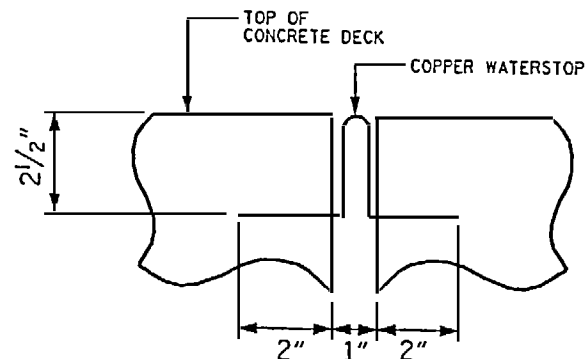
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
EXISTING SUPERSTRUCTURE
GIRDER ELEVATIONS



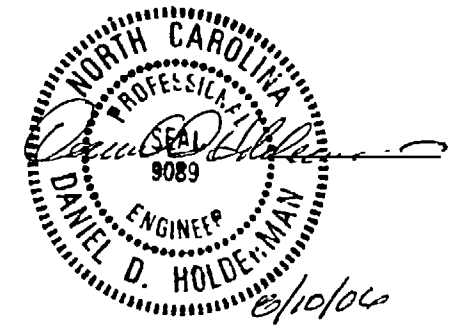
TYPICAL SECTION



CURB AND RAIL SECTION *



EXISTING EXPANSION JOINT DETAIL



PROJECT NO. 40891.3.1
 JOHNSTON COUNTY
 BRIDGE NO. 66

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 EXISTING SUPERSTRUCTURE
 TYPICAL SECTION

DRAWN BY: P. BYRD / S. I. SANDOR DATE: 7/26/06
 CHECKED BY: J. YANNACCONE DATE: 7/27/06

* PER SIDE, THERE ARE 9 POSTS IN SPANS A&D AND 12 POSTS
 IN SPANS B&C WITH 226' OF CONCRETE RAILING.

NOT TO SCALE

10-AUG-2006 08:36

 pbyrd AT BGMU206405

REVISIONS						SHEET NO. 15
NO.	BY	DATE	NO.	BY	DATE	
1			5			TOTAL SHEETS 31
2			4			

NOTES

FIELD VERIFY DIMENSIONS PRIOR TO CONSTRUCTION.

PORTIONS OF EXISTING END BENT SHOWN IN CROSS-HATCHED AREAS SHALL BE REMOVED.

VERTICAL AND HORIZONTAL REINFORCING STEEL EXTENDING FROM THE END BENT CAP INTO THE EXISTING WINGWALLS SHALL BE CLEANED AND STRAIGHTENED. CUT EXISTING REINFORCING STEEL TO MAINTAIN REQUIRED CONCRETE COVER. MINIMUM 14" EXTENSION INTO THE PROPOSED WINGWALL.

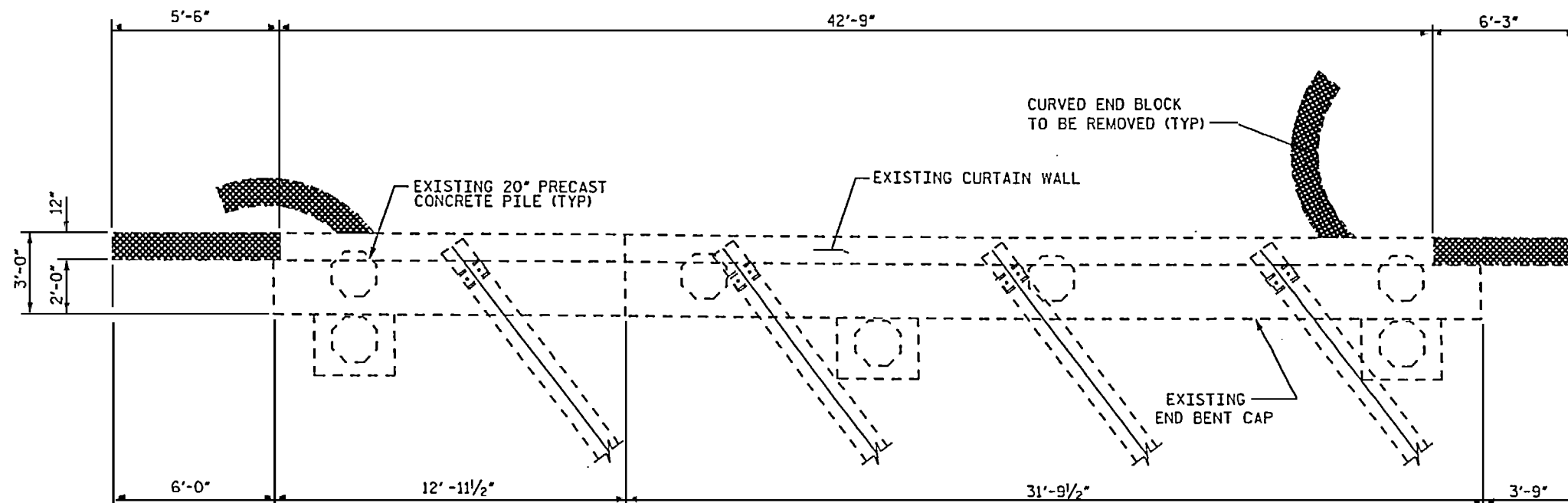
BARs DAMAGED DURING THE CONCRETE REMOVAL SHALL BE REPLACED BY #6 DOWELS SECURED IN THE EXISTING END BENT CAP WITH EPOXY ADHESIVE AT NO ADDITIONAL PAYMENT.

THE #6 DOWEL LENGTH SHALL BE BASED ON A 9" EMBEDMENT INTO EXISTING CONCRETE AND MAY BE ADJUSTED BASED ON THE MINIMUM EMBEDMENT SPECIFIED BY THE MANUFACTURER OF THE EPOXY ADHESIVE BONDING SYSTEM.

EXISTING ANCHOR BOLTS ARE TO BE CUT FLUSH WITH THE EXISTING TOP OF CAP.

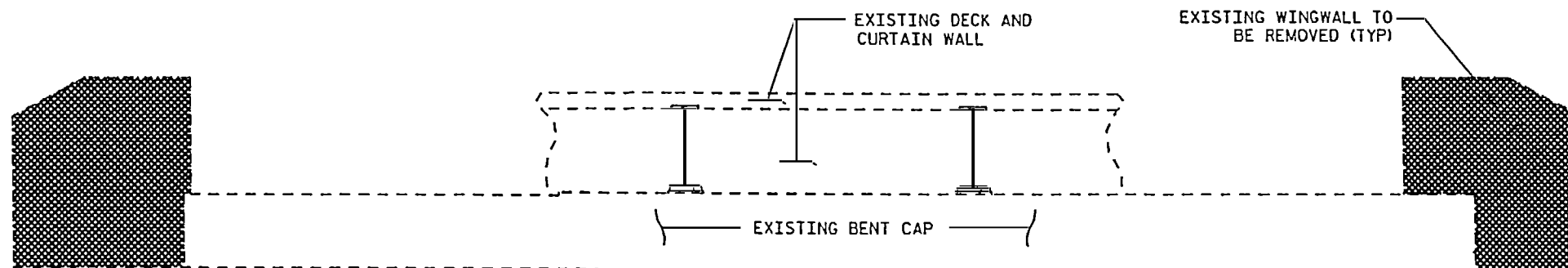
THE EXISTING CONCRETE AROUND ANCHOR BOLTS AND BEARING ASSEMBLIES SHALL BE REMOVED, USING HAND TOOLS, AS NECESSARY TO FREE ANCHOR BOLTS AND BEARING ASSEMBLIES. THE CONTRACTOR SHALL EXERCISE CARE DURING THE REMOVAL OF EXISTING CONCRETE TO INSURE THAT EXISTING GIRDERS, BEARING ASSEMBLIES AND CURTAIN WALL STEEL REMAIN UNDAMAGED.

ALL WORK ON THIS SHEET WILL BE PAID FOR UNDER THE LUMP SUM BID PRICE FOR REMOVAL OF PORTIONS OF EXISTING STRUCTURE.



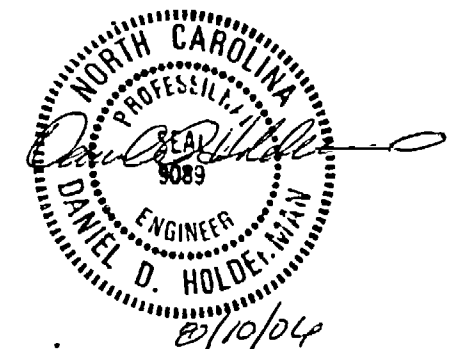
PLAN OF EXISTING CAP

END BENT 1 SHOWN, END BENT 2 SIMILAR



ELEVATION OF EXISTING CAP

END BENT 1 SHOWN, END BENT 2 SIMILAR
CURVED END BLOCK NOT SHOWN



WBS. 40891.3.1
COUNTY JOHNSTON
BRIDGE NO. 66

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

SUBSTRUCTURE
END BENT
CONCRETE REMOVAL

NOT TO SCALE

REVISIONS

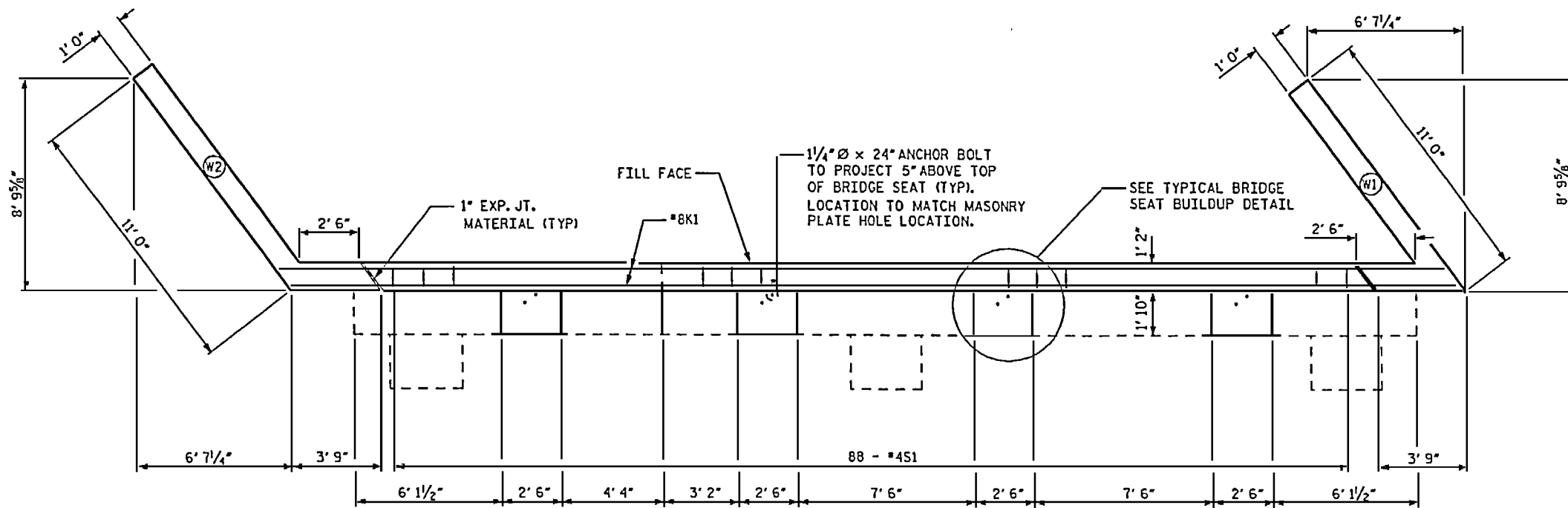
NOTES

THE #6D1, #4K2 & #4K3 BARS SHALL BE SECURED IN EXISTING CONCRETE WITH EPOXY ADHESIVE.

THE VERTICAL LEG LENGTH OF THE #6D1, #4K2 & #4K3 BARS IS BASED ON A 9" EMBEDMENT INTO EXISTING CONCRETE AND MAY BE ADJUSTED BASED ON THE MINIMUM EMBEDMENT SPECIFIED BY THE MANUFACTURER OF THE EPOXY ADHESIVE BONDING SYSTEM.

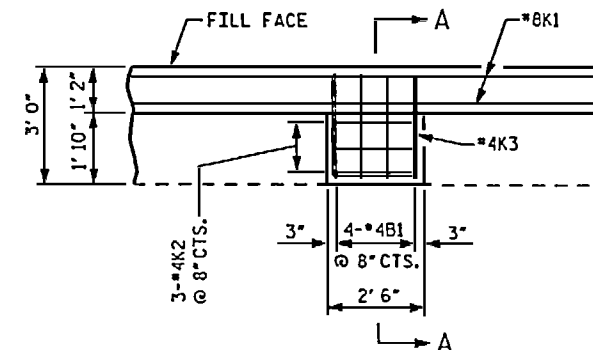
THE AREAS OF THE CURTAIN WALL AROUND ANCHOR BOLTS AND BEARING ASSEMBLIES PREVIOUSLY REMOVED SHALL BE RECAST TO PRODUCE SMOOTH, STRAIGHT FINISHED SURFACES USING CLASS A CONCRETE.

CLEANING AND PAINTING EXISTING BEARING PLATES WILL BE PAID FOR UNDER THE LUMP SUM PRICE FOR MODIFICATION OF SUPERSTRUCTURE. FOR CLEANING AND PAINTING EXISTING BEARING PLATES, SEE SPECIAL PROVISION.



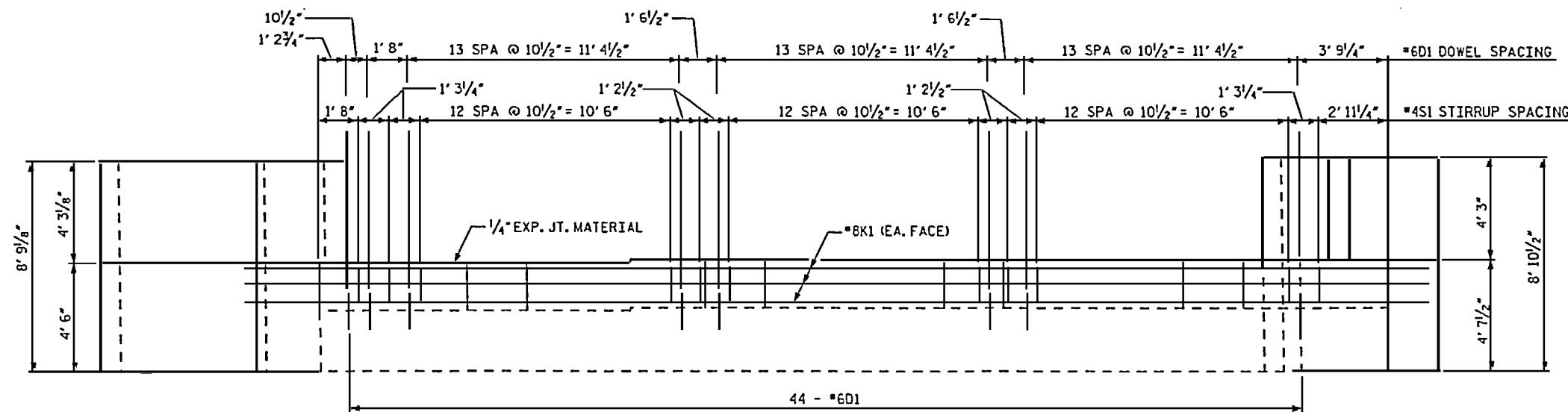
PLAN OF CAP MODIFICATION

END BENT 1 SHOWN, END BENT 2 SIMILAR



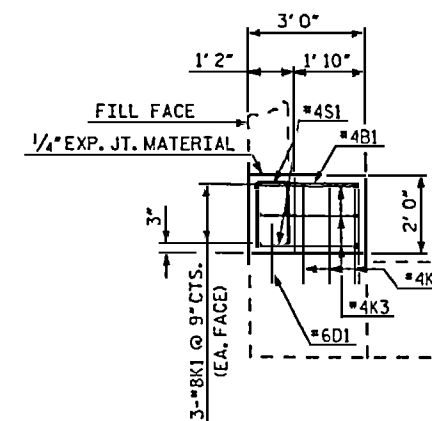
TYPICAL BRIDGE SEAT BUILDUP

#4S1 STIRRUPS & #6D1 DOWELS NOT SHOWN FOR CLARITY

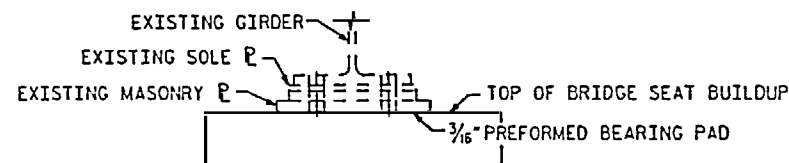


ELEVATION OF CAP MODIFICATION

END BENT 1 SHOWN, END BENT 2 SIMILAR

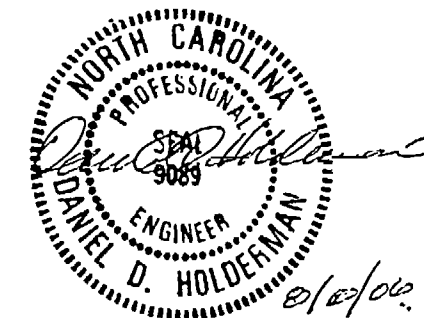


SECTION A-A



TYPICAL BEARING ASSEMBLY

END BENTS 1 & 2



NOT TO SCALE

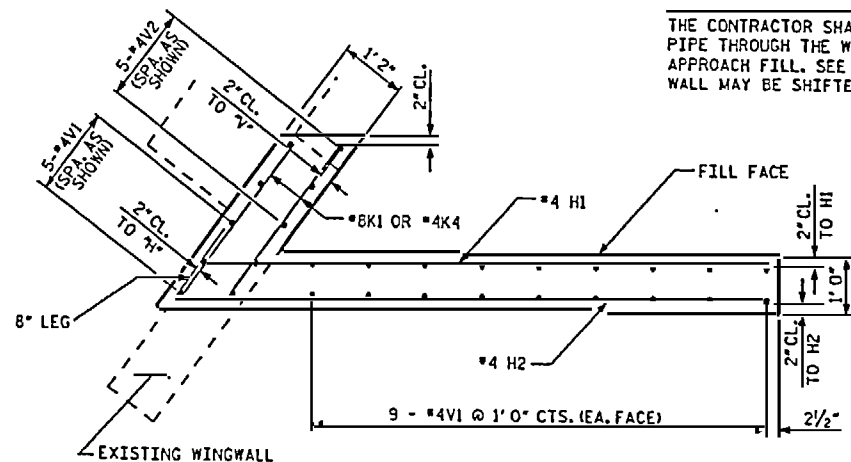
WBS. 40891.3.1
COUNTY JOHNSTON
BRIDGE NO. 66

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

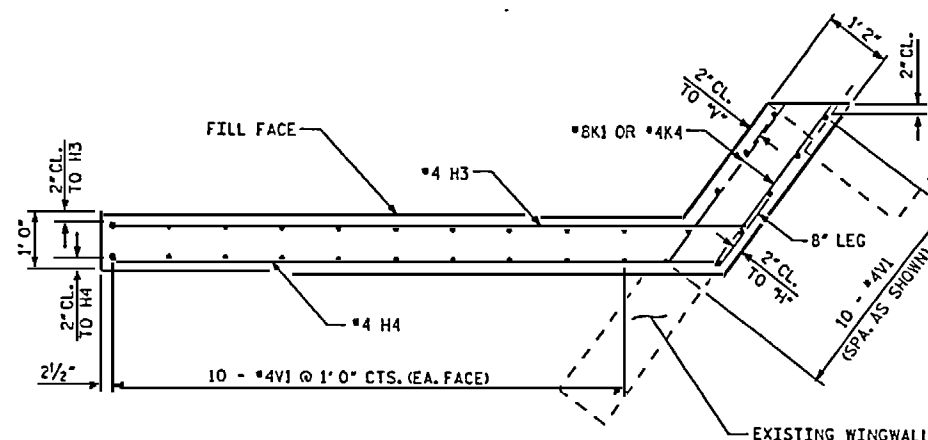
SUBSTRUCTURE
END BENT
CAP MODIFICATIONS

NOTES

THE CONTRACTOR SHALL PROVIDE FOR INSTALLATION OF THE 4" DRAIN PIPE THROUGH THE WING WALL AS REQUIRED FOR REINFORCED BRIDGE APPROACH FILL. SEE STD. DWG. 422.10. REINFORCING STEEL IN THE WING WALL MAY BE SHIFTED SLIGHTLY AS NECESSARY TO CLEAR DRAIN PIPE.



PLAN OF LEFT WING - W1



PLAN OF RIGHT WING - W2

BILL OF MATERIAL FOR ONE END BENT (2 REQ'D)

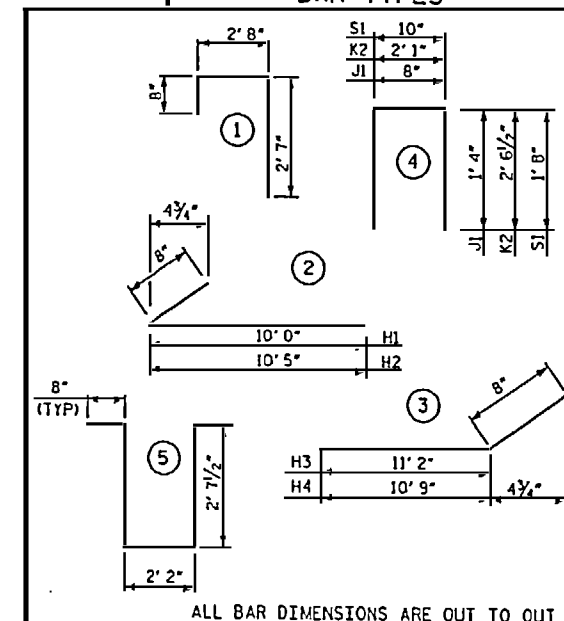
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
B1	16	#4		5'- 11"	63
D1	44	#6	STR	1'- 6"	99
H1	9	#4		10'- 8"	64
H2	9	#4		11'- 1"	67
H3	9	#4		11'- 10"	71
H4	9	#4		11'- 5"	69
J1	39	#5		4'- 4"	176
K1	6	#8	STR	49'- 0"	785
K2	12	#4		7'- 2"	58
K3	12	#4		8'- 9"	70
K4	16	#4	STR	4'- 4"	46
K5	2	#5	STR	37'- 2"	78
S1	88	#5		4'- 2"	382
V1	53	#4	STR	8'- 4"	295
V2	5	#4	STR	6'- 0"	20

REINFORCING STEEL LBS. 2343

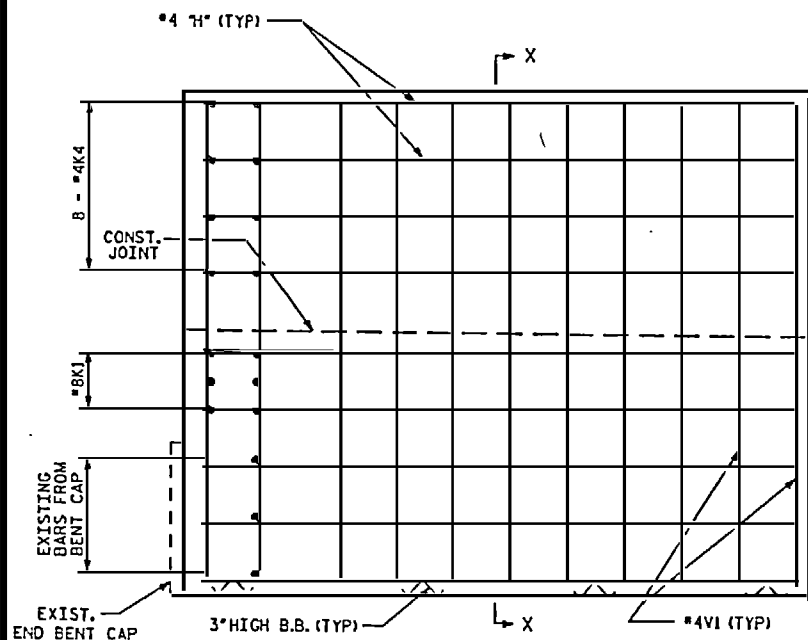
CLASS A CONCRETE BREAKDOWN

POUR	DESCRIPTION	C. Y.	WEIGHT
POUR 1	CAP AND WINGWALLS	8.0	
POUR 2	BRIDGE SEATS AND WINGWALLS	5.7	
POUR 3	APPROACH SLAB BRACKETS	2.1	
	CLASS A CONCRETE	15.8	

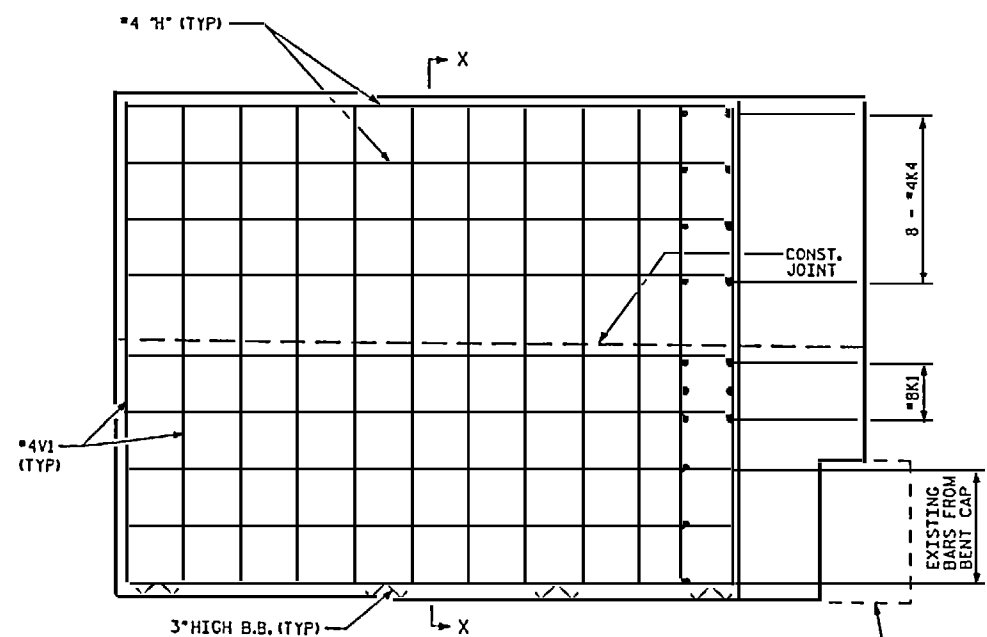
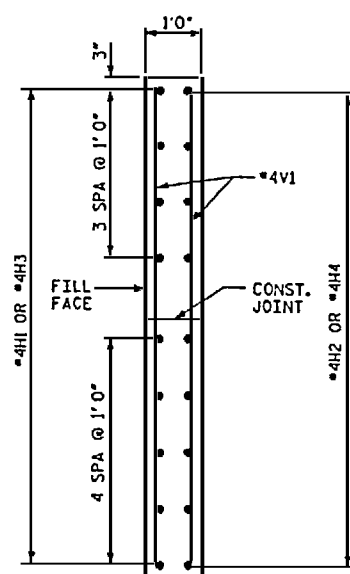
BAR TYPES



ALL BAR DIMENSIONS ARE OUT TO OUT



ELEVATION OF LEFT WING

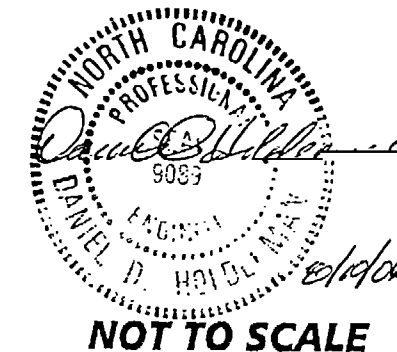


ELEVATION OF RIGHT WING

REINFORCING FOR TURNED BACK WING

END BENT 1 SHOWN, END BENT 2 SIMILAR

DRAWN BY: Phil Byrd DATE: July 2006
 CHECKED BY: J. YANACONE DATE: AUG. 2006



NOT TO SCALE

WBS. 40891.3.1
 COUNTY JOHNSTON
 BRIDGE NO. 66

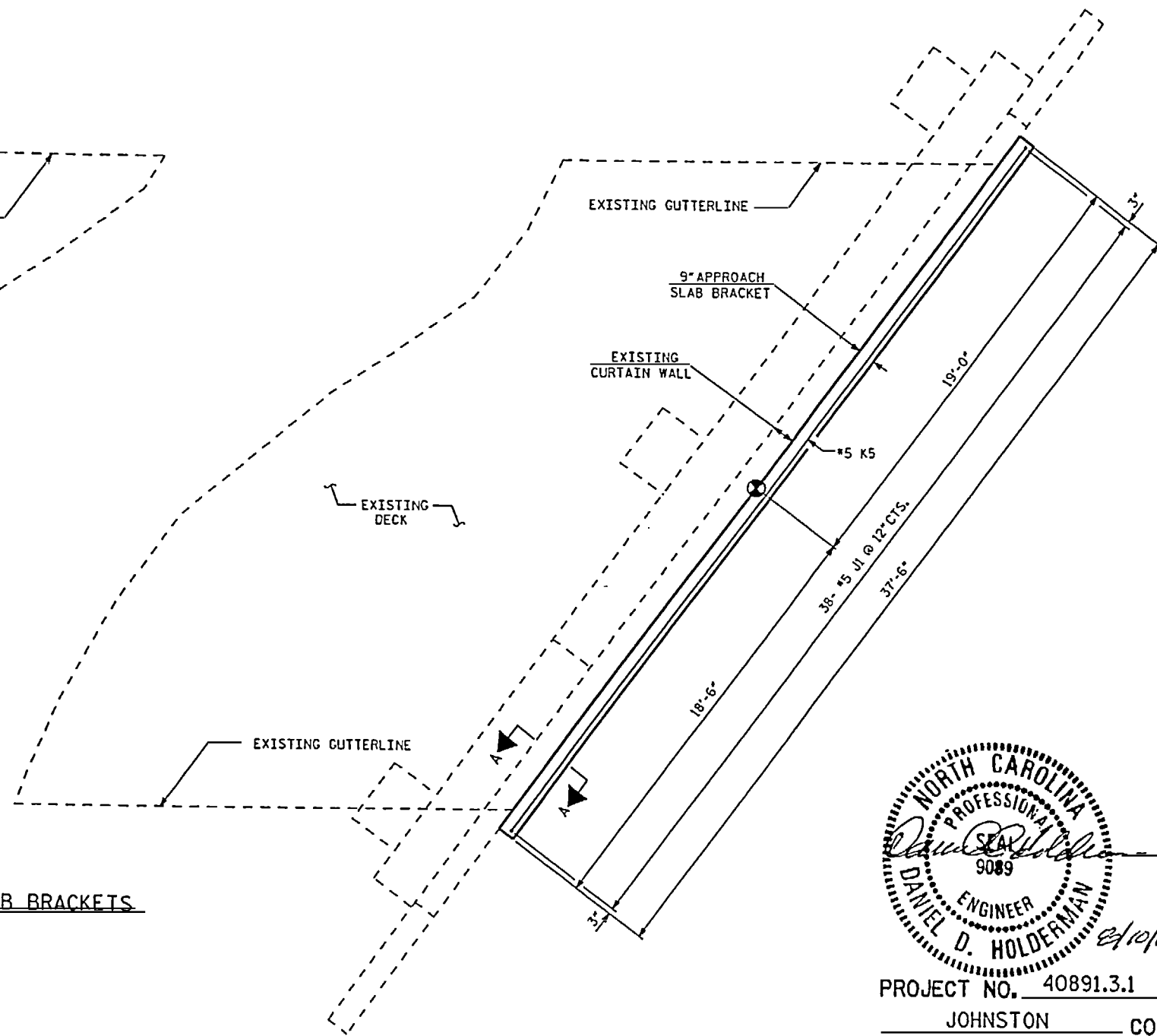
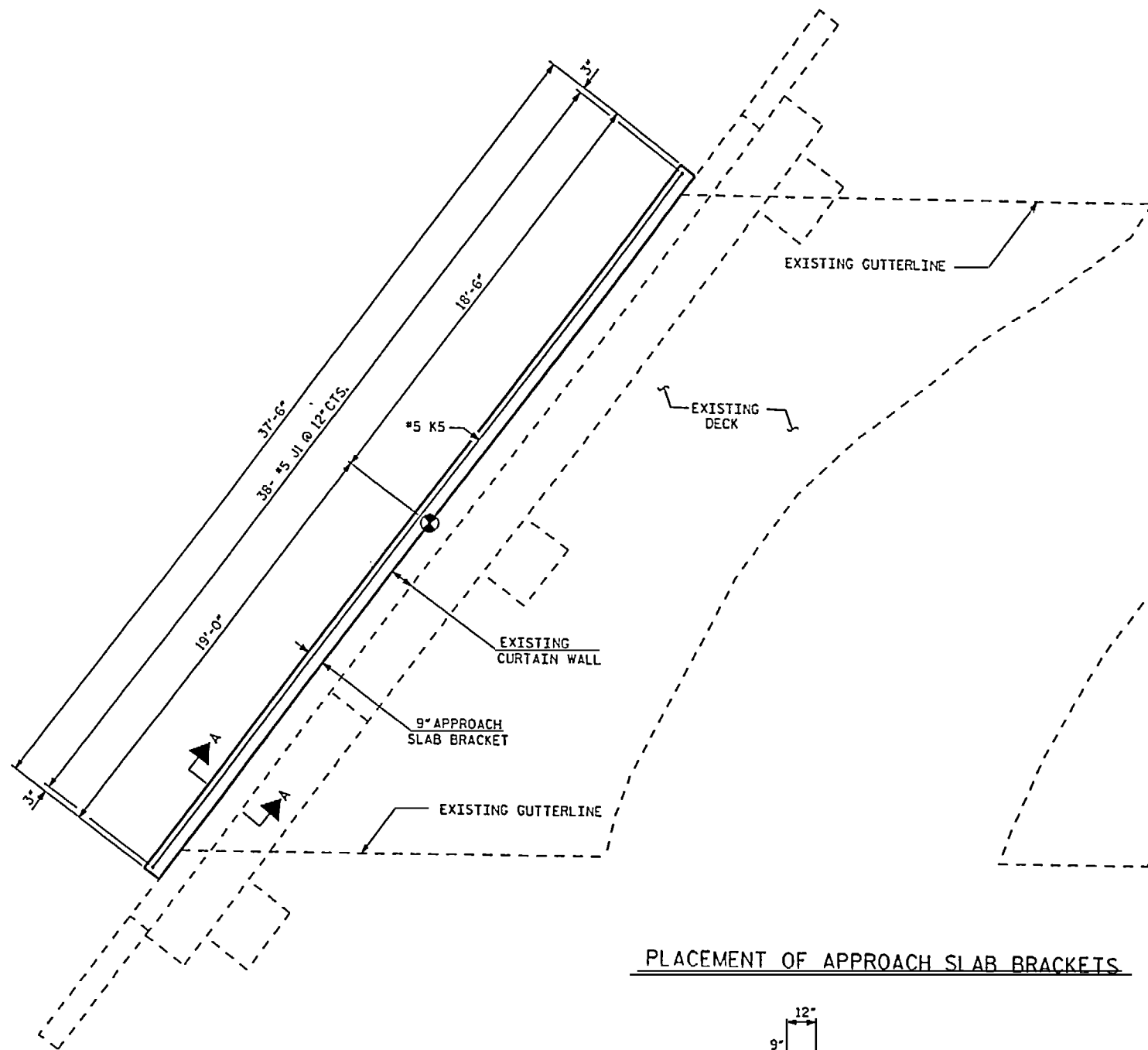
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**SUBSTRUCTURE
 END BENT**

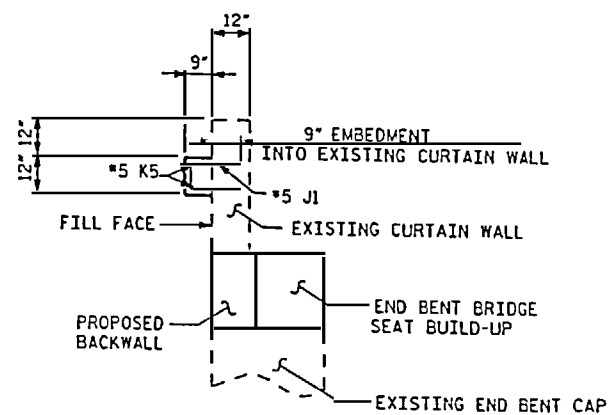
WINGWALL MODIFICATIONS

REVISIONS					NO.
NO.	BY	DATE	NO.	BY	DATE
1			3		
2			4		

TOTAL SHEETS 31



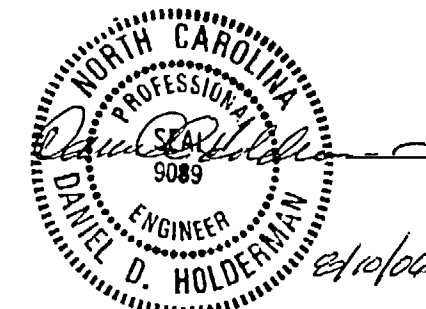
PLACEMENT OF APPROACH SLAB BRACKETS



SECTION A-A

NOTES

THE #5J1 BARS SHALL BE SECURED INTO THE EXISTING CONCRETE WITH EPOXY ADHESIVE.
 THE LEG LENGTH OF THE #5J1 BAR IS BASED ON A 9" EMBEDMENT INTO THE EXISTING CONCRETE AND MAY BE ADJUSTED BASED ON THE MINIMUM EMBEDMENT SPECIFIED BY THE MANUFACTURER OF THE EPOXY ADHESIVE BONDING SYSTEM.



PROJECT NO. 40891.3.1
 JOHNSTON COUNTY

BRIDGE #66

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 Raleigh

SUBSTRUCTURE
 END BENT

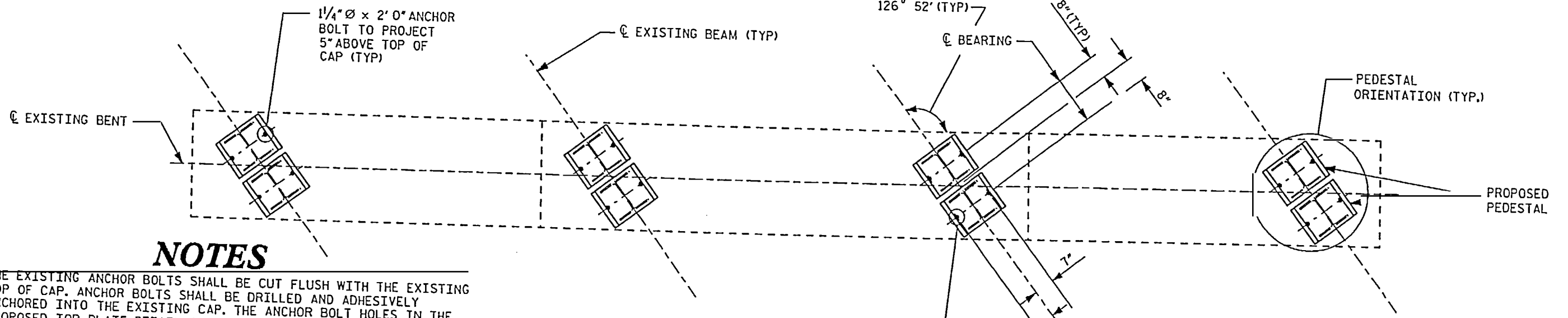
APPROACH SLAB BRACKETS

DRAWN BY: S. T. SANDOR DATE: 7/17/06
 CHECKED BY: J. YANACCONI DATE: 7/21/06

10-AUG-2006 09:37
 *****PCN*****
 ppyrd AT BGMJ206405

NOT TO SCALE

REVISIONS						SHEET NO.
NO.	BY	DATE	NO.	BY	DATE	
1			3			19
2			4			31

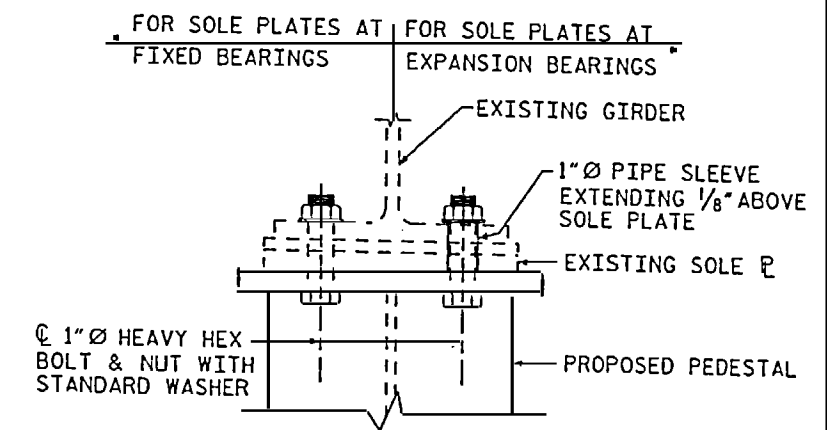


NOTES

THE EXISTING ANCHOR BOLTS SHALL BE CUT FLUSH WITH THE EXISTING TOP OF CAP. ANCHOR BOLTS SHALL BE DRILLED AND ADHESIVELY ANCHORED INTO THE EXISTING CAP. THE ANCHOR BOLT HOLES IN THE PROPOSED TOP PLATE DETAIL SHALL MATCH THE ANCHOR BOLT HOLES IN THE EXISTING BEAMS. THIS MATCH SHALL FACILITATE THE PROPER ALIGNMENT OF THE PEDESTAL AND THE LOCATION OF THE PROPOSED ANCHOR BOLTS INTO THE EXISTING CAP. THE ANCHOR BOLT LENGTH IS BASED ON AN 18\"/>

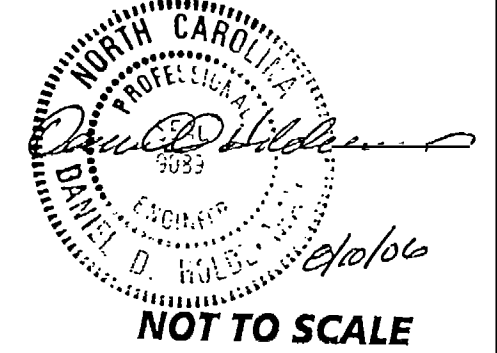
PLAN OF EXISTING BENT

BENT 1 SHOWN, BENT 2&3 SIMILAR



BEARING ATTACHMENT DETAIL

(24 LOCATIONS)



NOT TO SCALE

WBS. 40891.3.1
COUNTY JOHNSTON
BRIDGE NO. 66

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

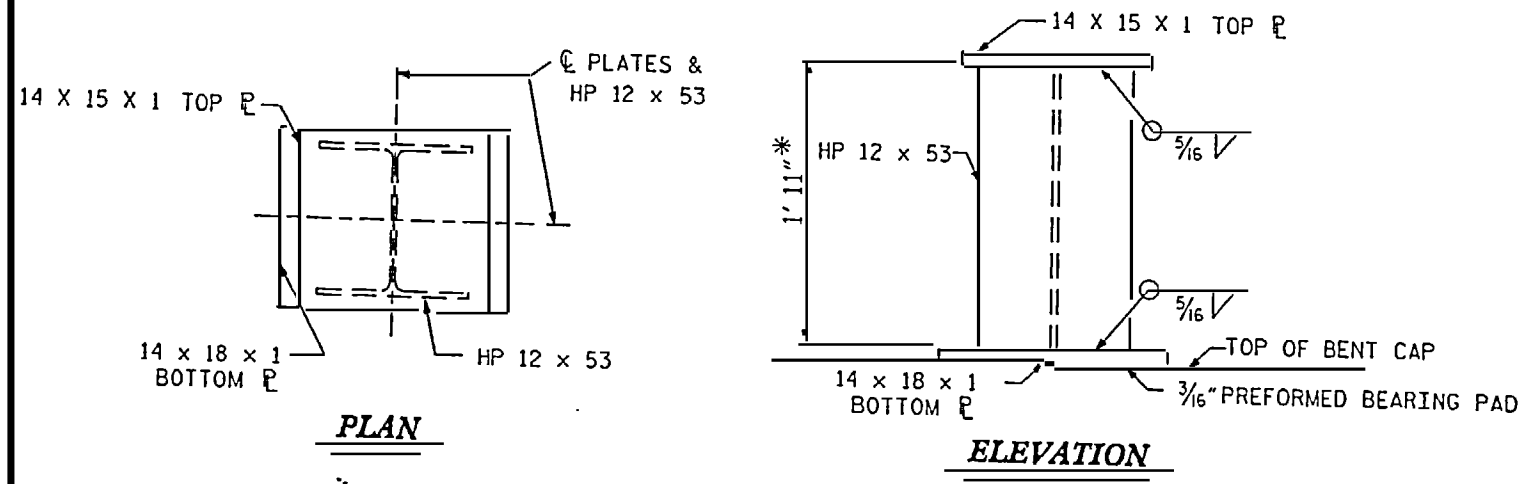
STRUCTURE
INTERIOR BENT
BEARING MODIFICATIONS

REVISIONS				NO.
NO.	BY	DATE	NO.	DATE
1			3	
2			4	

20
31

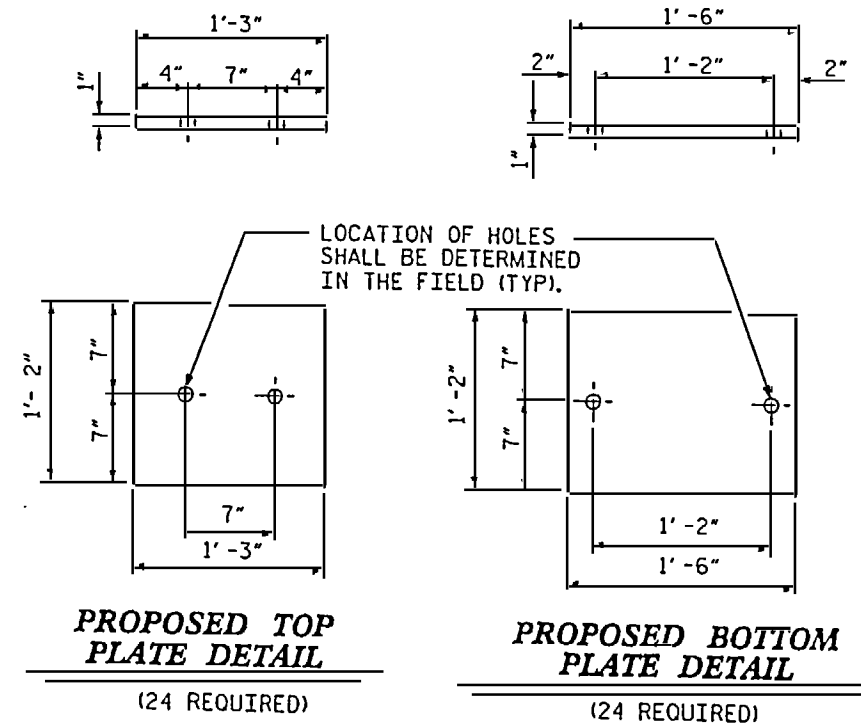
REPLACEMENT OF EXPANSION JOINTS, AND CLEANING AND PAINTING OF EXISTING BEARING PLATES WILL BE PAID FOR UNDER THE LUMP SUM PRICE FOR MODIFICATION OF SUPERSTRUCTURE. FOR CLEANING AND PAINTING EXISTING BEARING PLATES, SEE SPECIAL PROVISION. FOR EXPANSION JOINT DETAILS, SEE SHEET NO. 21 (DETAIL A).

THE ESTIMATED QUANTITY OF STRUCTURAL STEEL PAID FOR UNDER THE LUMP SUM PRICE FOR MODIFICATION OF SUPERSTRUCTURE IS 7470 LBS. THIS QUANTITY IS FOR INFORMATION ONLY AND IS NOT A PART OF THIS CONTRACT. THIS QUANTITY IS FOR INFORMATION ONLY AND NO ACCURACY IS IMPLIED OR GUARANTEED. NO CLAIM WILL BE ALLOWED AS A RESULT OF THE USE OF THIS INFORMATION.



PEDESTAL DETAILS

(24 REQUIRED)



PROPOSED TOP PLATE DETAIL

(24 REQUIRED)

PROPOSED BOTTOM PLATE DETAIL

(24 REQUIRED)

DRAWN BY: P. BYRD / S.T. SANDOR DATE: July 2006
CHECKED BY: J. YANNACCONE DATE: July 2006

NOTES

FOR REINFORCED BRIDGE APPROACH FILL INCLUDING FABRIC, IMPERMEABLE GEOMEMBRANE, 4" Ø DRAINAGE PIPE, #78M STONE, AND SELECT MATERIAL, SEE STD. DWG. 422.10.

AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE AND SHALL BE PAVED. SEE STD. DWG. 422.10.

THE 6" COMP. A.B.C. SHALL EXTEND 10'-0" BEYOND THE END OF THE APPROACH SLAB AND 1'-0" OUTSIDE OF EACH EDGE OF SLAB.

THE CONTRACTOR MAY USE 4" TYPE B-25.08 ASPHALT CONCRETE BASE COURSE IN LIEU OF 6" COMP. A.B.C. IF THIS OPTION IS USED, THE BASE COURSE SHALL EXTEND 1'-0" BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB.

THE CONTRACTOR MAY USE 5" CLASS "A" CONCRETE BASE IN LIEU OF 6" COMP. A.B.C. IF THIS OPTION IS USED, THE CONCRETE BASE SHALL EXTEND 1'-0" BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB. THE CONCRETE SHALL BE FINISHED TO A SMOOTH SURFACE AND A LAYER OF 30 LB ROOFING FELT SHALL BE PLACED BETWEEN THE CONCRETE BASE AND THE APPROACH SLAB TO PREVENT BOND. THE APPROACH SLAB SHALL NOT BE CAST UNTIL THE CONCRETE BASE HAS REACHED AN AGE OF THREE CURING DAYS.

THE 6" BEVEL AT THE END OF THE APPROACH SLAB SHALL EXTEND FROM FRONT FACE OF CURB TO FRONT FACE OF CURB.

SLOPE TOP OF CONCRETE APPROACH SLAB TO MATCH CROSS SLOPE OF EXISTING CONCRETE DECK.

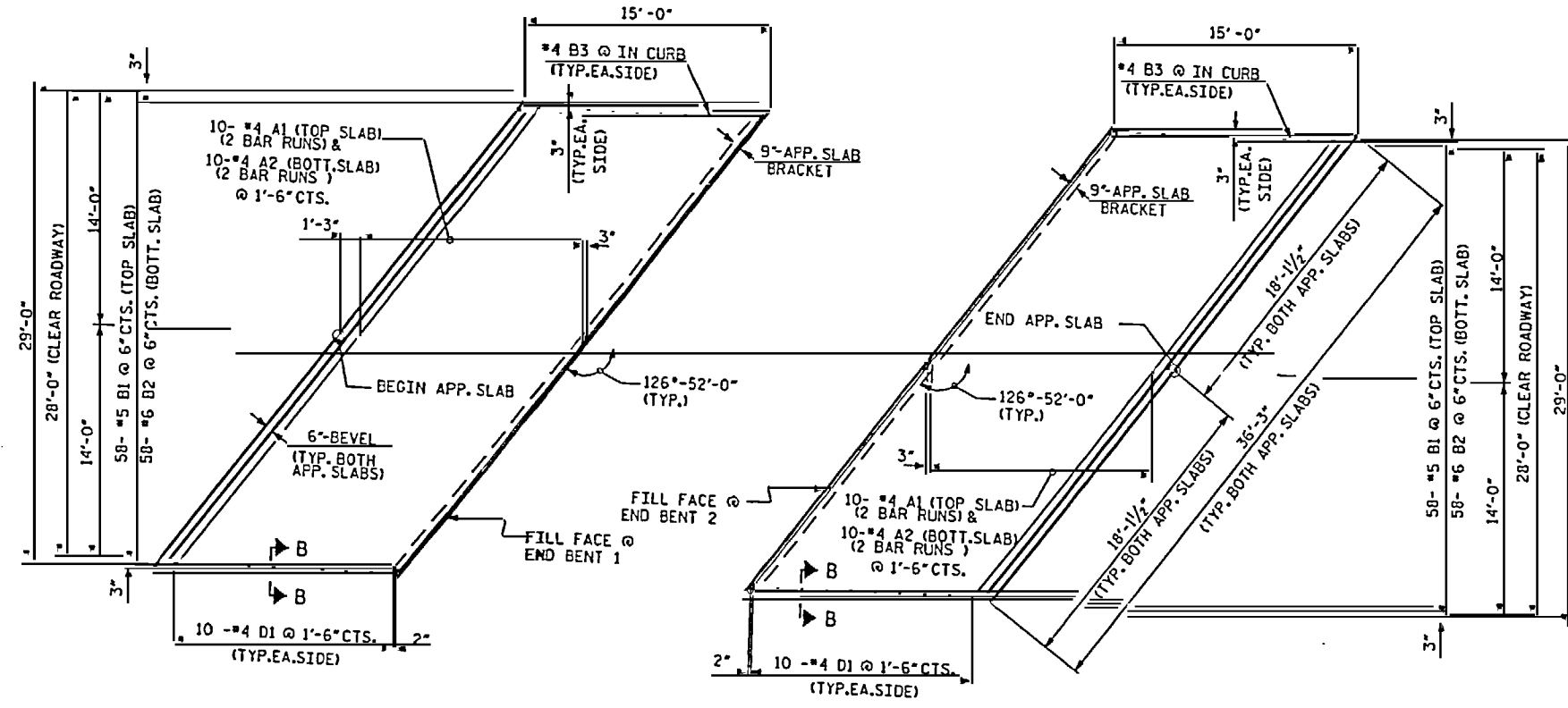
BILL OF MATERIAL
ONE APP. SLAB (2 REQ'D)

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
*A1	20	#4	STR	18'-11"	253
A2	20	#4	STR	18'-10"	252
*B1	58	#5	STR	14'-2"	857
B2	58	#6	STR	14'-8"	1278
*B3	2	#4	STR	14'-8"	20
*D1	20	#4	STR	1'-0"	13

REINFORCING STEEL	lbs.	
*EPOXY COATED REINFORCING STEEL	1530	
REINFORCING STEEL	1143	
CLASS AA CONCRETE		
POUR 1 SLAB	C. Y.	16.0
POUR 2 CURB	C. Y.	0.4
TOTAL CONCRETE	C. Y.	16.4

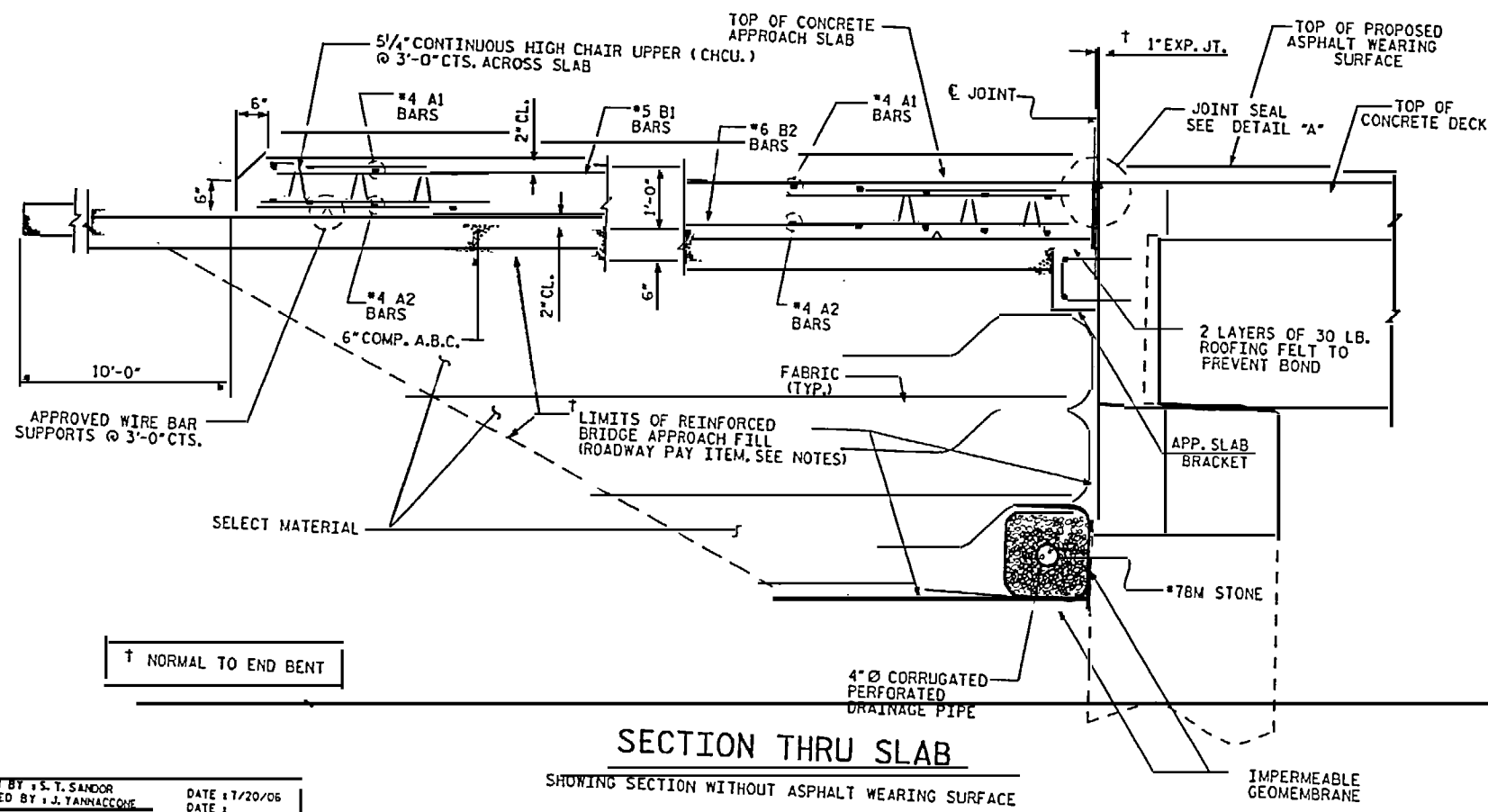
SPLICE CHART

BAR SIZE	EPOXY COATED	UNCOATED
#4	2'-0"	1'-9"
#5	2'-6"	2'-2"
#6	3'-10"	2'-7"



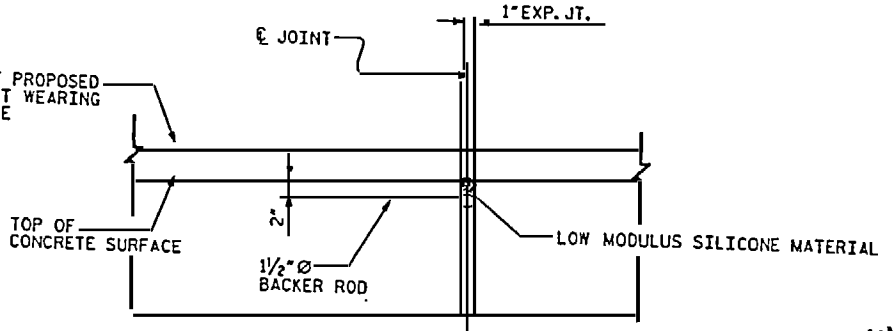
PLAN @ END BENT 1

PLAN @ END BENT 2

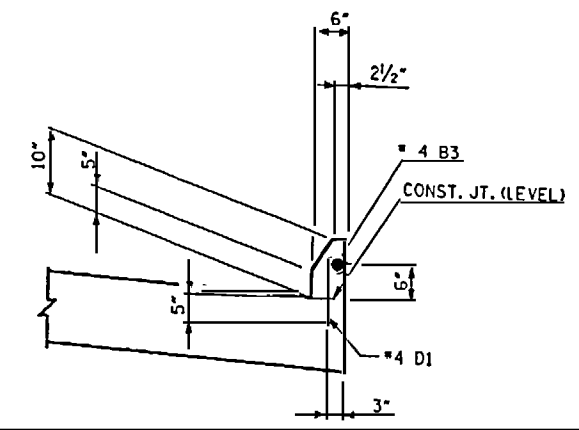


SECTION THRU SLAB

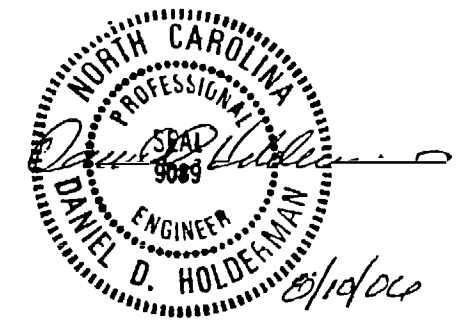
SHOWING SECTION WITHOUT ASPHALT WEARING SURFACE



DETAIL A



SECTION B-B (THRU CURB)



PROJECT NO. 40891.3.1
JOHNSTON COUNTY
BRIDGE NO.66

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH
STANDARD

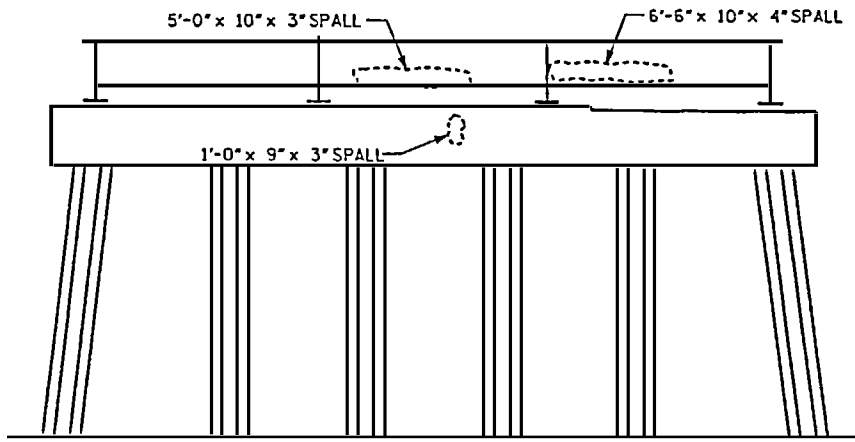
BRIDGE APPROACH SLAB
FOR FLEXIBLE PAVEMENT

REVISIONS						SHEET NO.
NO.	BY	DATE	NO.	BY	DATE	
1			3			21
2			4			31

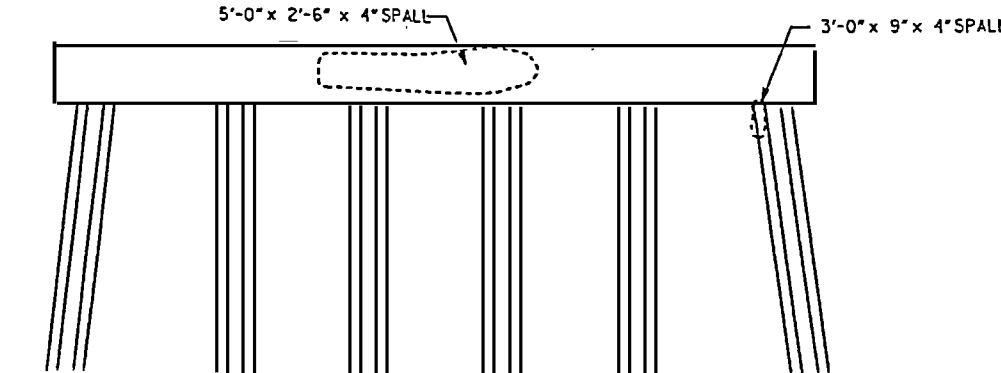
NOT TO SCALE

DRAWN BY S.T. SANDOR
CHECKED BY J. YANACCONO
DATE 1/20/06
DATE 1

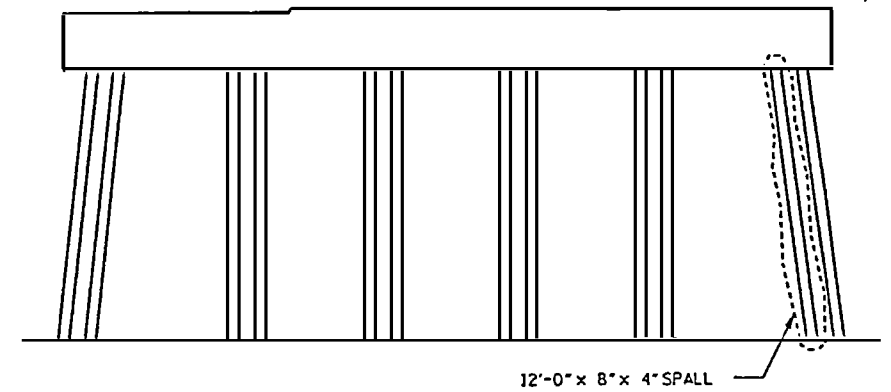
10-AUG-2006 09:38
*****DCN*****
pbyrd AT BGM206405



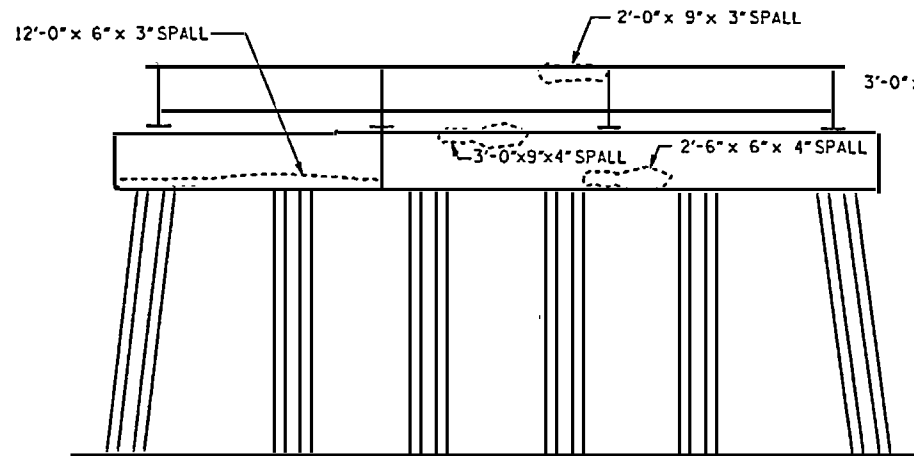
ELEVATION
(LOOKING EAST)



ELEVATION
(LOOKING EAST)

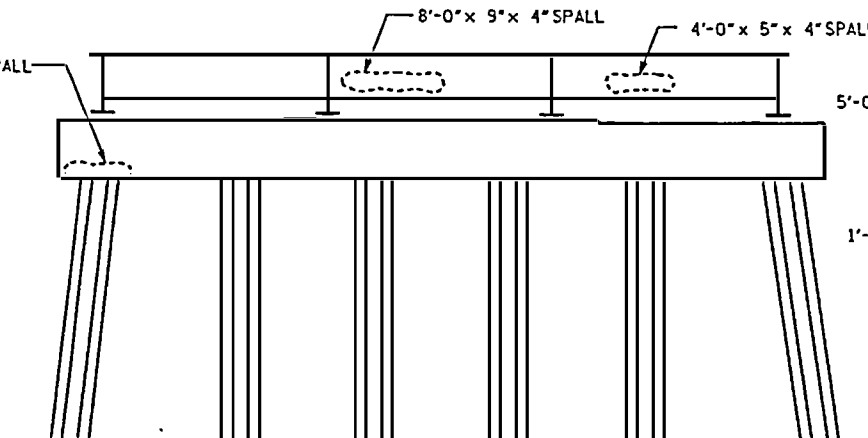


ELEVATION
(LOOKING EAST)



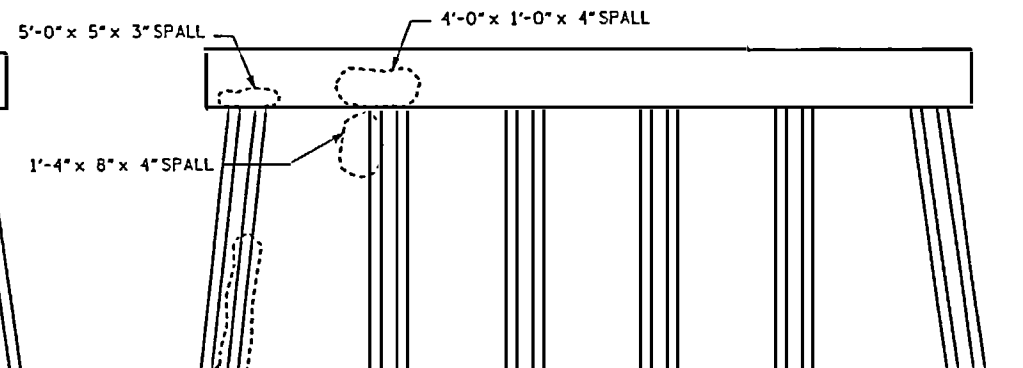
ELEVATION
(LOOKING WEST)

BENT NO. 1



ELEVATION
(LOOKING WEST)

BENT NO. 2



ELEVATION
(LOOKING WEST)

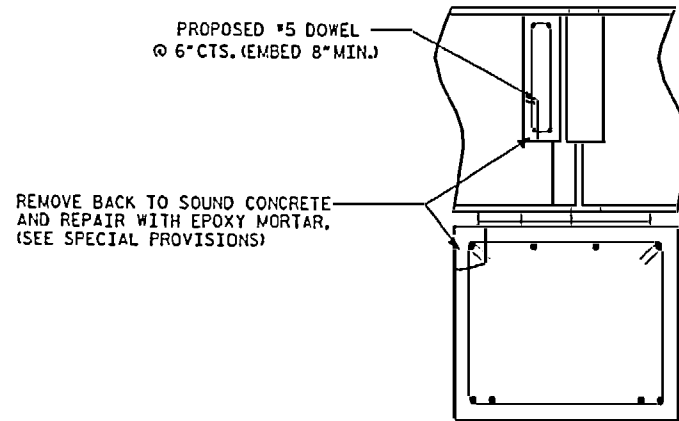
BENT NO. 3

INTERIOR BENTS REPAIR LOCATIONS

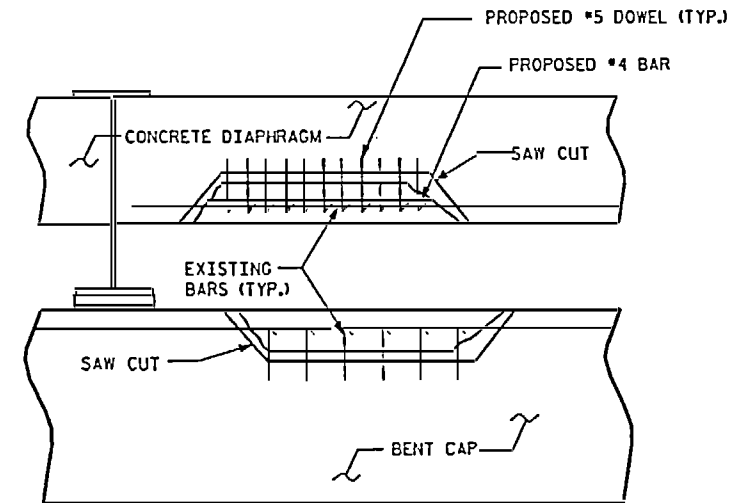
NOTE

REPLACEMENT OF EXISTING HORIZONTAL BAR WITH #4 BAR AND #5 DOWELS AS SHOWN IN TYPICAL BENT & DIAPHRAGM REPAIR DETAIL IS INCLUDED UNDER THE CONTRACT PAY ITEM, EPOXY MORTAR REPAIRS.

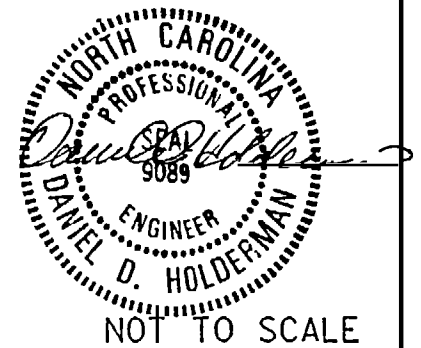
SAWCUT 1/4" - 1/2" DEEP AROUND ALL SPALLS.



SECTION



ELEVATION



PROJECT NO. 40891.3.1
JOHNSTON COUNTY
BRIDGE NO. 66

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

INTERIOR BENT
REPAIRS