

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. PLACED IN TWO LAYERS.
P1	PRIME COAT AT A RATE OF .35 GAL PER SQ. YARD.

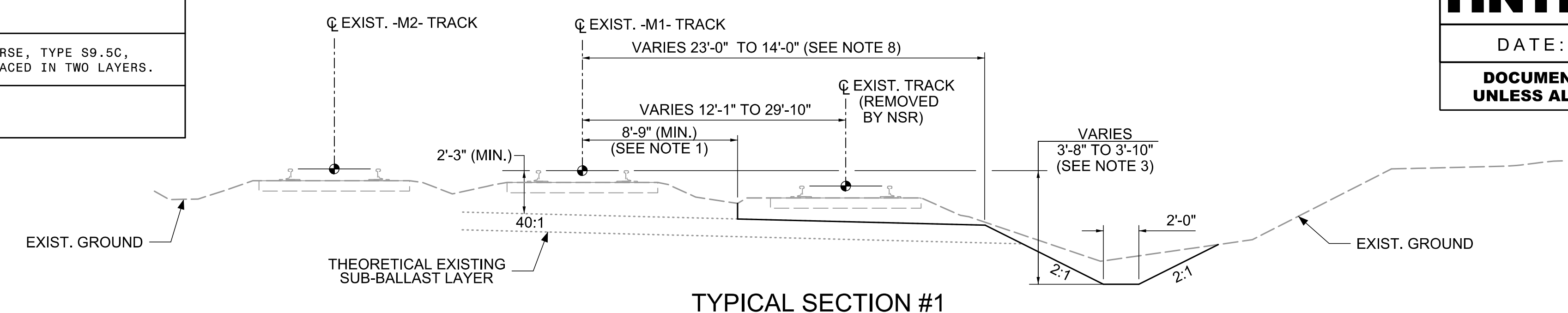
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PROJECT REFERENCE NO. P-5705A SHEET NO. 2A-1

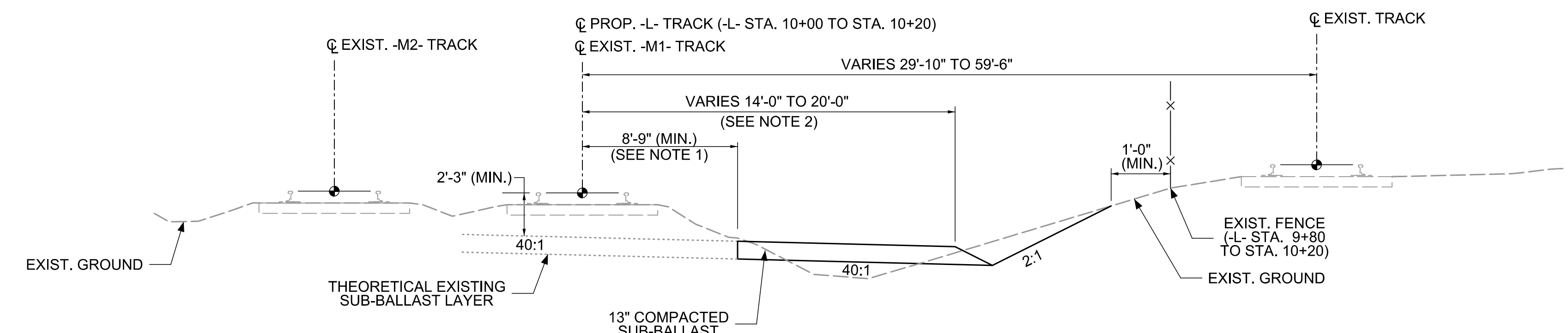
DATE: NOVEMBER 4, 2022

**DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED**

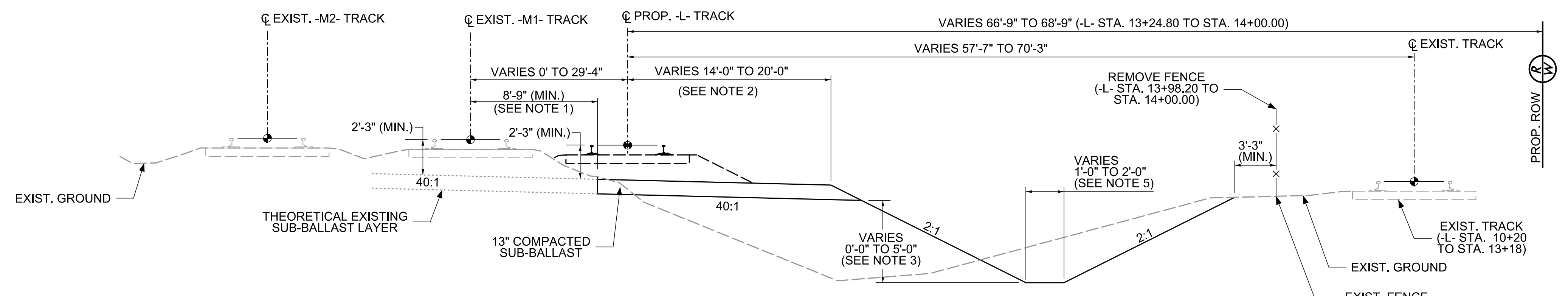
RAILROAD DESIGN ENGINEER



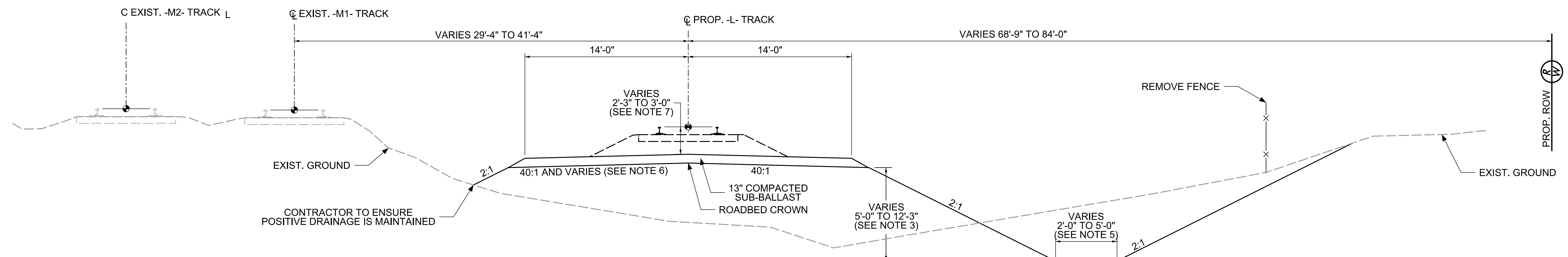
**TYPICAL SECTION #1**  
-L- STA. 8+28.98 TO STA. 9+10



**TYPICAL SECTION #1A**  
-L- STA. 9+10 TO STA. 10+20



**TYPICAL SECTION #2**  
-L- STA. 10+20 TO STA. 14+00



**TYPICAL SECTION #3**  
-L- STA. 14+00 TO STA. 16+00

- NOTES:
1. DIMENSION SHOWN IS FOR BIDDING PURPOSES ONLY AND MUST BE COORDINATED WITH THE NORFOLK SOUTHERN REPRESENTATIVE PRIOR TO CONSTRUCTION. SEE 'EXCAVATION ADJACENT TO ACTIVE TRACK DETAIL' FOR REQUIREMENTS.
  2. THE PROPOSED RIGHT SHOULDER WIDTH VARIES ACCORDING TO THE FOLLOWING:  
STA. 9+10.00 TO STA. 9+33.42: 14'-0" TO 16'-0"  
STA. 9+33.42 TO STA. 9+43.42: 16'-0" TO 20'-0"  
STA. 9+43.42 TO STA. 10+39.98: 20'-0"  
STA. 10+39.98 TO STA. 11+30.55: 20'-0" TO 14'-0"  
STA. 11+30.55 TO STA. 14+00.00: 14'-0"
  3. DITCH ELEVATIONS ARE CONTROLLED BY SPECIAL DITCH GRADES SHOWN ON THE PROFILES.
  4. THE TYPICAL SECTIONS TAKE PRECEDENCE OVER THE CROSS SECTIONS. CROSS SECTIONS ARE PROVIDED FOR REFERENCE ONLY.
  5. RIGHT DITCH BASE WIDTH VARIES AS FOLLOWS:  
STA. 10+20.00 TO STA. 10+97.00: 1'-0"  
STA. 10+97.00 TO STA. 11+07.00: 1'-0" TO 2'-0"  
STA. 11+07.00 TO STA. 14+00.00: 2'-0"  
STA. 14+00.00 TO STA. 14+50.00: 2'-0" TO 5'-0"  
STA. 14+50.00 TO STA. 16+00.00: 5'-0"
  6. THE SUB-BALLAST CROSS SLOPE SHALL TRANSITION FROM 40:1 AWAY FROM THE EXISTING MAINLINE TRACKS AT -L- STA. 14+00 TO 40:1 WITH CROWN CENTERED UNDER THE -L- ALIGNMENT AT -L- STA. 14+50. NORMAL SUB-BALLAST CROWN SHALL BE UTILIZED FROM -L- STA. 14+50 AND STA. 16+00.
  7. THE VERTICAL DISTANCE FROM PROP. TOP OF RAIL ELEV. AND THE TOP OF SUB-BALLAST AT THE -L- ALIGNMENT CENTERLINE SHALL TRANSITION FROM 3'-0" AT -L- STA. 14+00 TO 2'-3" AT -L- STA. 14+50. THE VERTICAL DISTANCE SHALL BE 2'-3" FROM -L- STA. 14+50 TO STA. 16+00.
  8. THE REMAINING BALLAST UNDER THE REMOVED INDUSTRY TRACK SHALL BE STRIPPED DOWN TO THE EXISTING SUB-BALLAST. THE SHOULDER WIDTH SHALL BE 23'-0" AT -L- STA. 8+28.98 AND TRANSITION TO 14'-0" AT STA. 9+10.00.