

TYPICAL HALF SECTION AT INTEGRAL END BENT 1 & 2 DIAPHRAGM

TYPICAL HALF SECTION AT INTERMEDIATE DIAPHRAGM

TYPICAL SECTION

- INDICATES CONTINUOUS REINFORCING
- INDICATES ADDITIONAL REINFORCING AT END BENT

NOTES

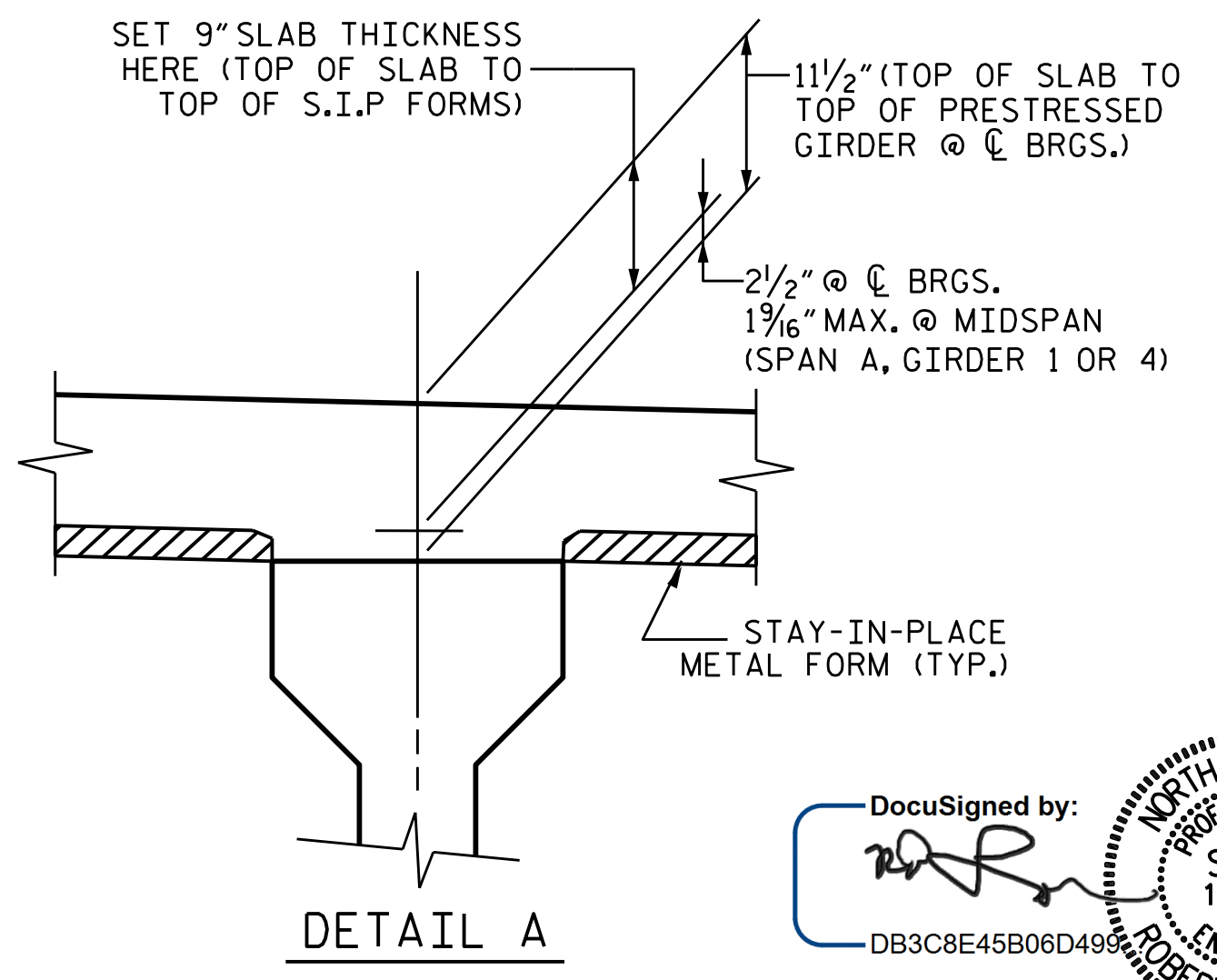
ALL HORIZONTAL DIMENSIONS ARE NORMAL TO LONG CHORD. BRIDGE TO BE CONSTRUCTED ALONG LONG CHORD WITH ADDITIONAL WIDTH AS SHOWN TO ALLOW FOR CURVATURE OF ROADWAY.

PROVIDE 1/4" HIGH BEAM BOLSTERS UPPER AT 4'-0"CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF 'A' BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 4'-0"CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF 'A' BARS A CLEAR DISTANCE OF 2 1/2" ABOVE THE TOP OF THE REMOVABLE FORM.

LONGITUDINAL STEEL MAY BE SHIFTED SLIGHTLY, AS NECESSARY, TO AVOID INTERFERENCE WITH STIRRUPS IN PRESTRESSED CONCRETE GIRDERS.

PREVIOUSLY CAST CONCRETE SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI BEFORE ADDITIONAL CONCRETE IS CAST IN THE SPAN.

SEE "CONCRETE BARRIER RAIL" FOR ADDITIONAL REINFORCING STEEL EMBEDDED IN SLAB.



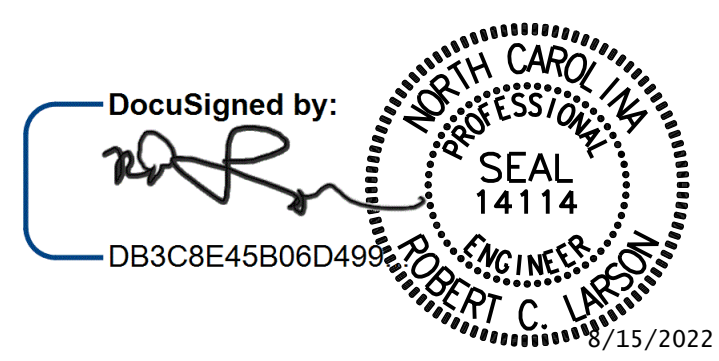
DETAIL A

PROJECT NO. R-2561CA
COLUMBUS COUNTY
 STATION: 16+93.00 -Y-

SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**SUPERSTRUCTURE
 TYPICAL SECTION**



DESIGN ENGINEER OF RECORD:	DATE: 8/15/2022
DRAWN BY: A. K. ALLANKI	DATE: 07/17/19
CHECKED BY: R. C. LARSON	DATE: 04/03/20

**DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED**

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 of North Carolina, P.A.
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REVISIONS		SHEET NO.	
NO.	DATE	NO.	DATE
1		3	
2		4	

TOTAL SHEETS: 24

\$FILEL\$ \$DATES\$ \$TIME\$ \$USER\$ \$PLTDRVS\$ \$PENTBLS\$ \$PROJECT NO. 241704391.04