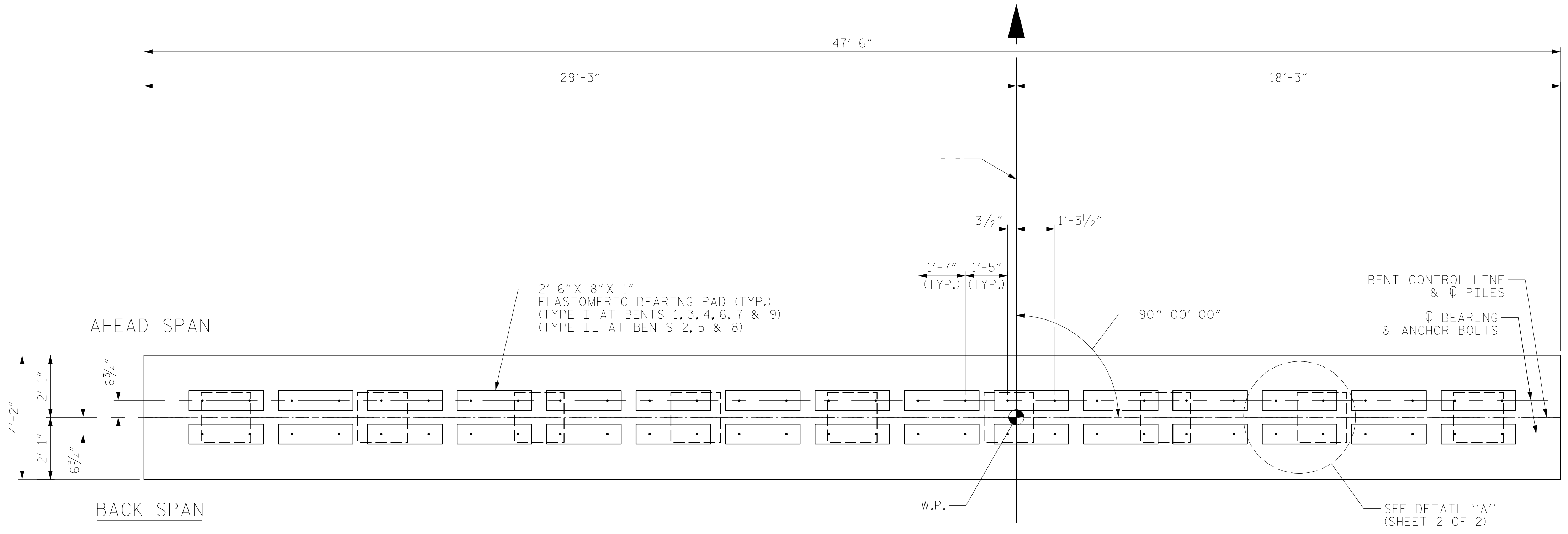


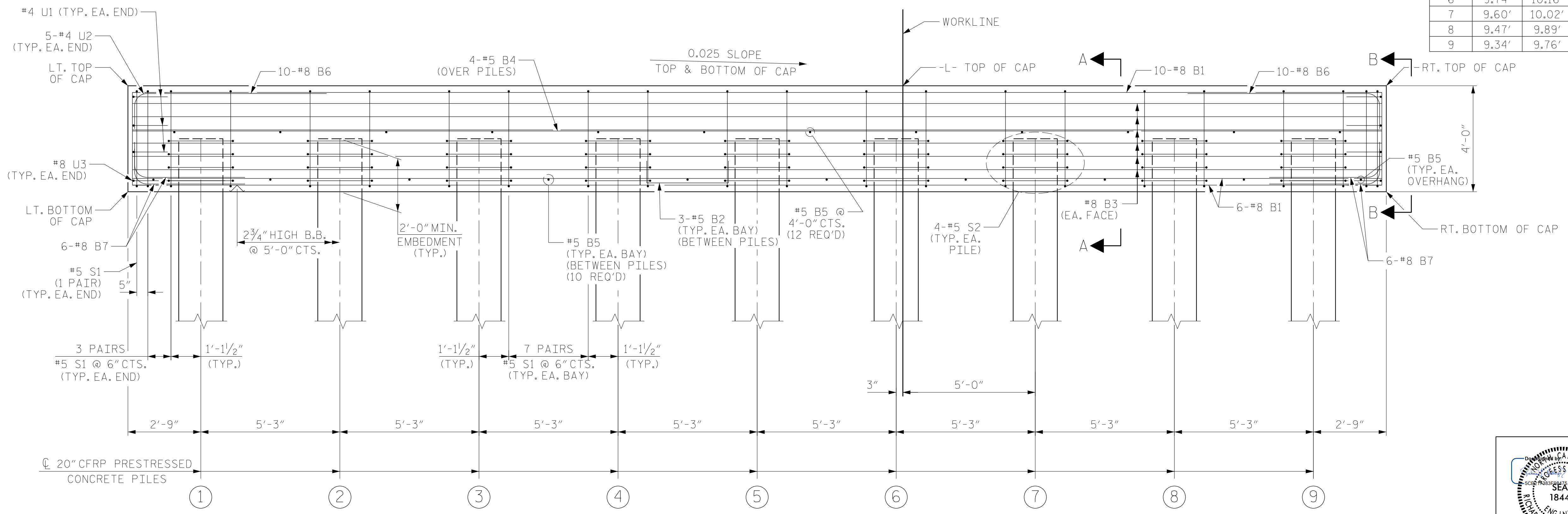
### NOTES

- STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.
- CONCRETE DISPLACED BY THE CONCRETE PILES HAS BEEN DEDUCTED FROM THE CAP CONCRETE QUANTITY.
- ALL REINFORCING IN THE CAP IS TO BE GLASS FIBER REINFORCED POLYMER (GFRP) BARS. FOR GLASS FIBER REINFORCED POLYMER BARS, SEE SPECIAL PROVISIONS.
- FOR SECTION A-A AND VIEW B-B, SEE SHEET 2 OF 2.
- ANCHOR BOLTS SHALL MEET THE REQUIREMENTS OF ASTM F1554 GR.105. ANCHOR PLATES, WASHERS, AND NUTS SHALL MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS. ANCHOR BOLTS, ANCHOR PLATES, WASHERS, AND NUTS SHALL BE GALVANIZED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- NO SEPARATE PAYMENT SHALL BE MADE FOR THE ANCHOR BOLTS, ANCHOR PLATES, WASHERS, AND NUTS. THE COST OF THE MATERIAL AND INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO VARIOUS PAY ITEMS.
- THE TOP SURFACE OF THE BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.
- EPOXY COAT THE TOP SURFACE OF THE BENT CAP, SECTION 420-18(b) LINES 13 AND 14 OF THE STANDARD SPECIFICATIONS SHALL BE DISREGARDED. NO SEPARATE PAYMENT SHALL BE MADE FOR THE EPOXY PROTECTIVE COATING AS THIS IS CONSIDERED INCIDENTAL TO THE VARIOUS PAY ITEMS.
- MATERIAL QUANTITIES FOR GFRP BARS INCLUDE THE ANTICIPATED SPLICES DETAILED ON THE PLANS. ADDITIONAL SPLICES REQUIRED DUE TO MANUFACTURING LIMITATIONS WILL BE AT NO ADDITIONAL COST TO THE DEPARTMENT.



PLAN

PILE NO.	BENT 1	BENT 2	BENT 3	BENT 4	BENT 5	BENT 6	BENT 7	BENT 8	BENT 9
1	10.39'	10.81'	11.16'	11.33'	11.34'	11.19'	10.91'	10.53'	10.14'
2	10.26'	10.68'	11.03'	11.20'	11.21'	11.06'	10.78'	10.39'	10.01'
3	10.13'	10.55'	10.89'	11.07'	11.08'	10.93'	10.65'	10.26'	9.88'
4	10.00'	10.42'	10.76'	10.94'	10.95'	10.80'	10.51'	10.13'	9.75'
5	9.87'	10.29'	10.63'	10.81'	10.81'	10.67'	10.38'	10.00'	9.61'
6	9.74'	10.16'	10.50'	10.68'	10.68'	10.54'	10.25'	9.87'	9.48'
7	9.60'	10.02'	10.37'	10.55'	10.55'	10.41'	10.12'	9.74'	9.35'
8	9.47'	9.89'	10.24'	10.41'	10.42'	10.28'	9.99'	9.61'	9.22'
9	9.34'	9.76'	10.11'	10.28'	10.29'	10.14'	9.86'	9.48'	9.09'



ELEVATION

BENT NO.	LT. BOT. OF CAP	LT. TOP OF CAP	-L- TOP OF CAP	RT. TOP OF CAP	RT. BOT. OF CAP
1	8.46'	12.46'	11.73'	11.27'	7.27'
2	8.88'	12.88'	12.15'	11.69'	7.69'
3	9.23'	13.23'	12.49'	12.04'	8.04'
4	9.40'	13.40'	12.67'	12.21'	8.21'
5	9.41'	13.41'	12.68'	12.22'	8.22'
6	9.26'	13.26'	12.53'	12.08'	8.08'
7	8.98'	12.98'	12.25'	11.79'	7.79'
8	8.59'	12.59'	11.86'	11.41'	7.41'
9	8.21'	12.21'	11.48'	11.02'	7.02'

PROJECT NO. BR-0160  
BRUNSWICK COUNTY  
 STATION: 21+77.50 -L-

SHEET 1 OF 2



**RS&H**  
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STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH SUBSTRUCTURE					
BENT NO. 1 THRU 9					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		

SHEET NO.	S-33
TOTAL SHEETS	42

DRAWN BY: MRA DATE: 01/2023  
 CHECKED BY: MKO DATE: 01/2023  
 DESIGN ENGINEER OF RECORD: RLB DATE: 03/2023

DOCUMENT NOT CONSIDERED  
 FINAL UNLESS ALL  
 SIGNATURES COMPLETED