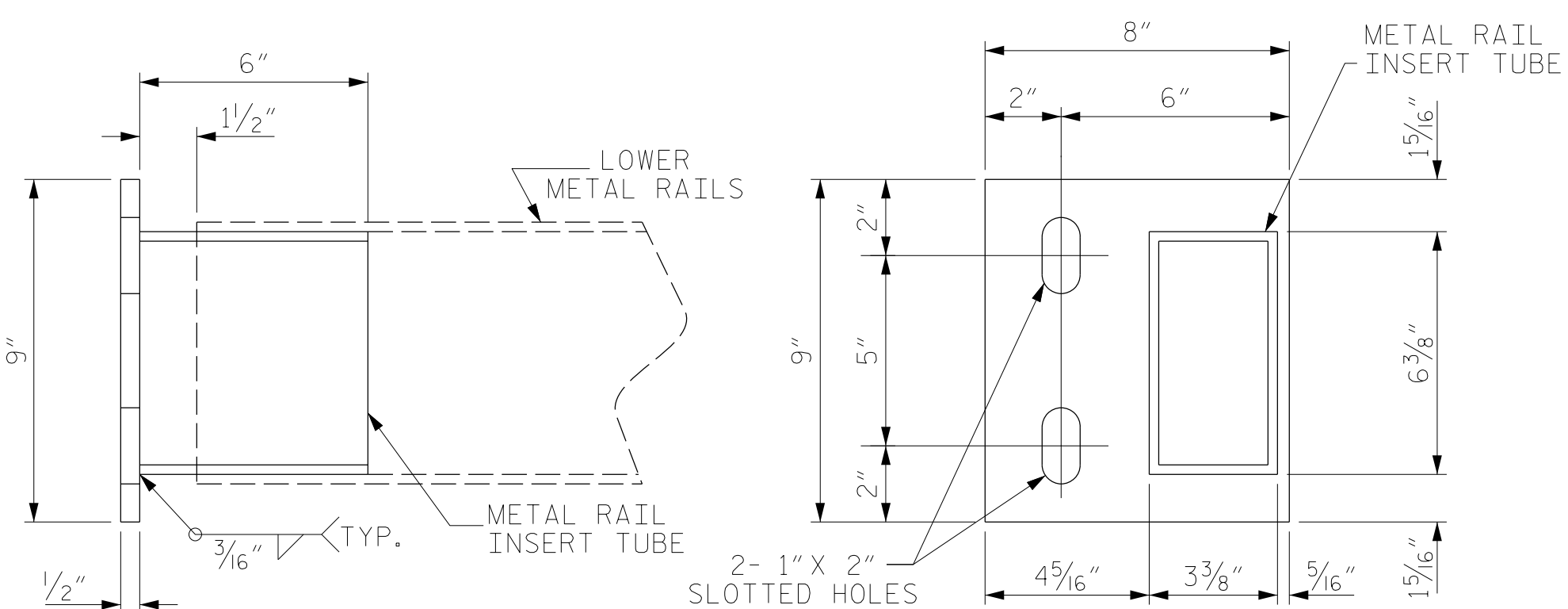


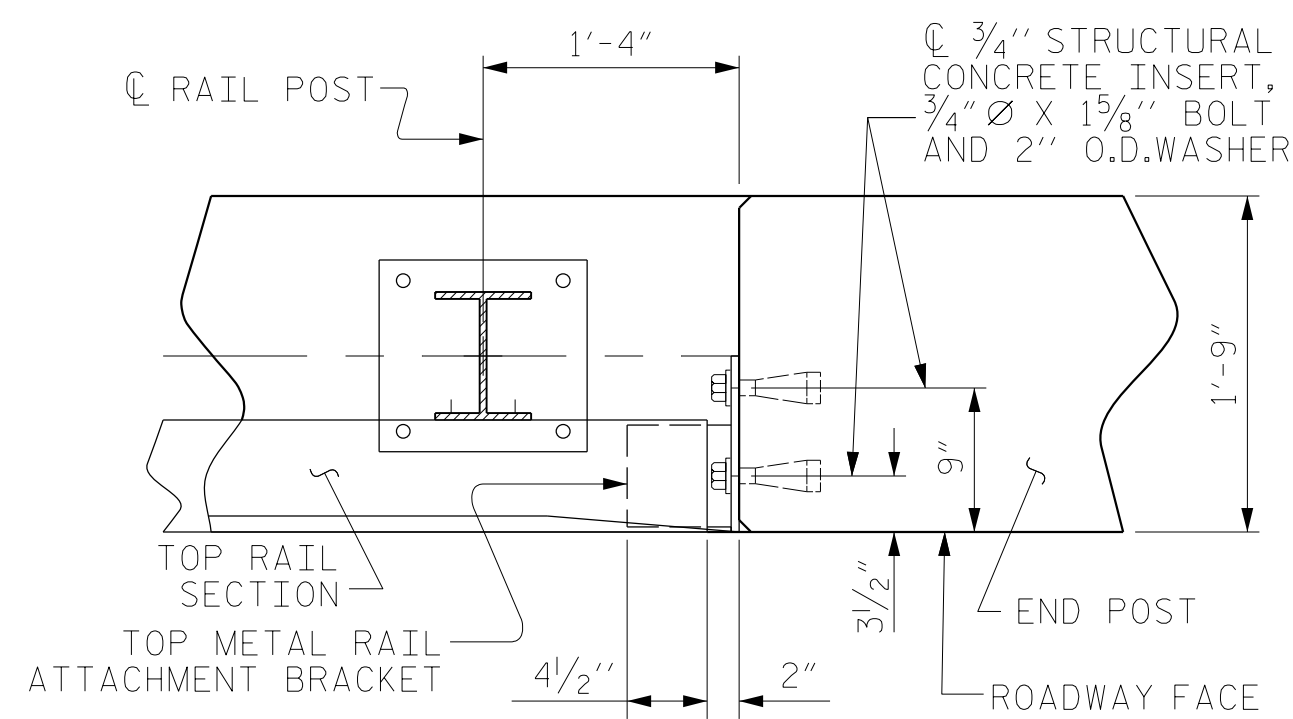
**TOP METAL RAIL ATTACHMENT BRACKET**

THE METAL RAIL INSERT TUBE SHALL BE FABRICATED FROM 1/4" PLATES.

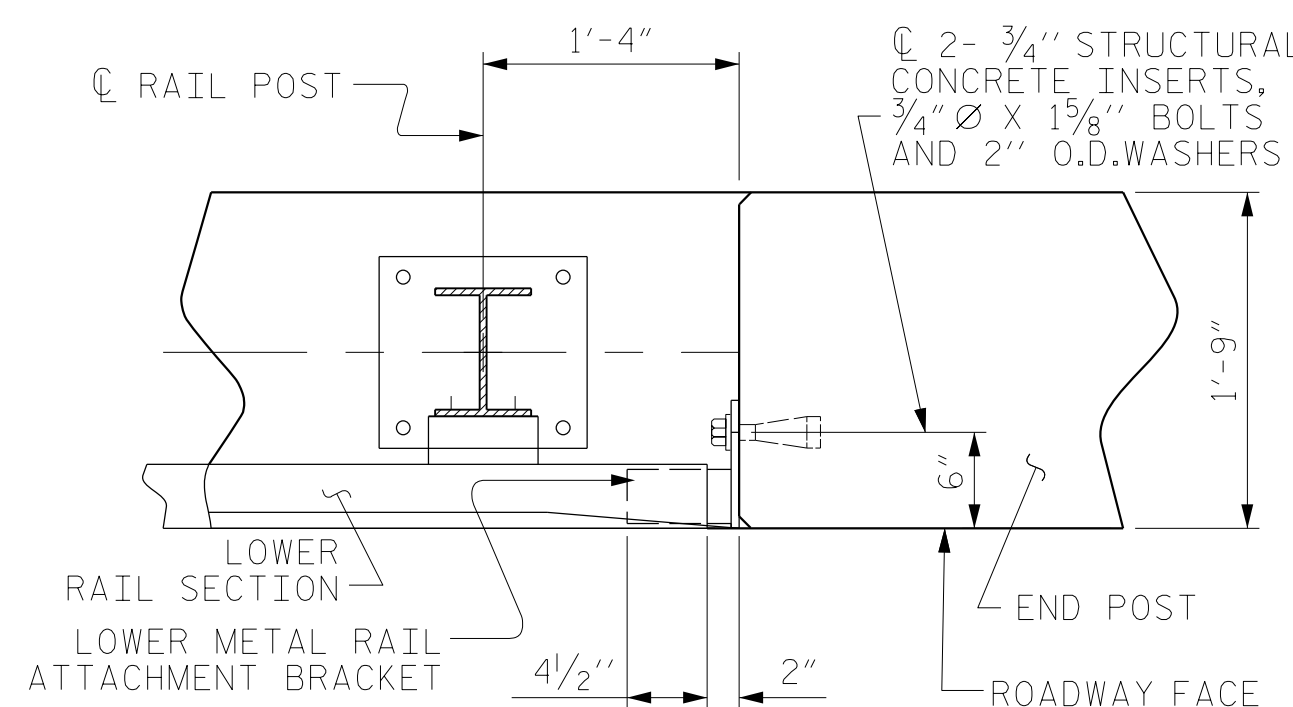


**LOWER METAL RAILS ATTACHMENT BRACKET**

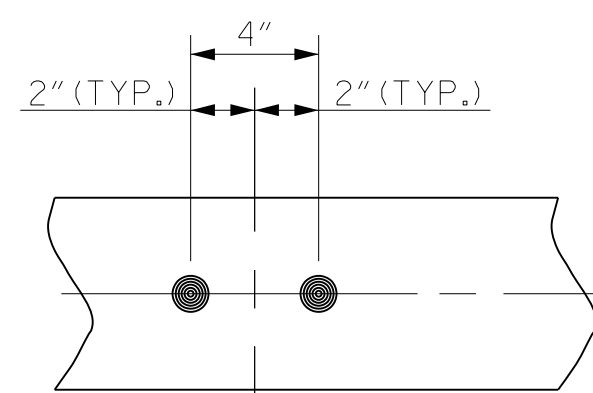
THE METAL RAIL INSERT TUBE SHALL BE FABRICATED FROM 1/4" PLATES.



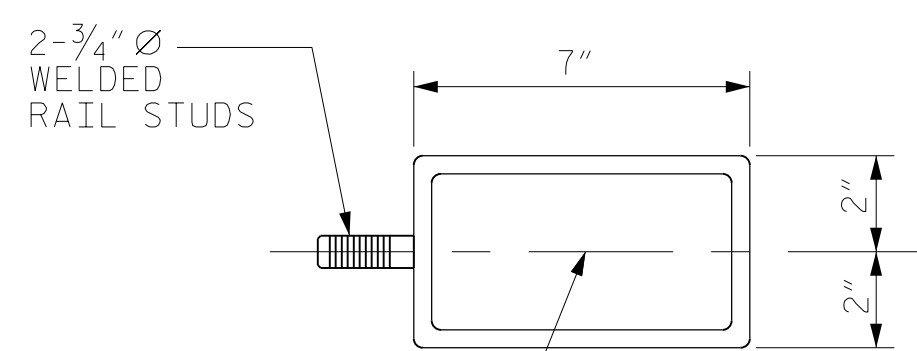
**PLAN - TOP RAIL AND END POST**



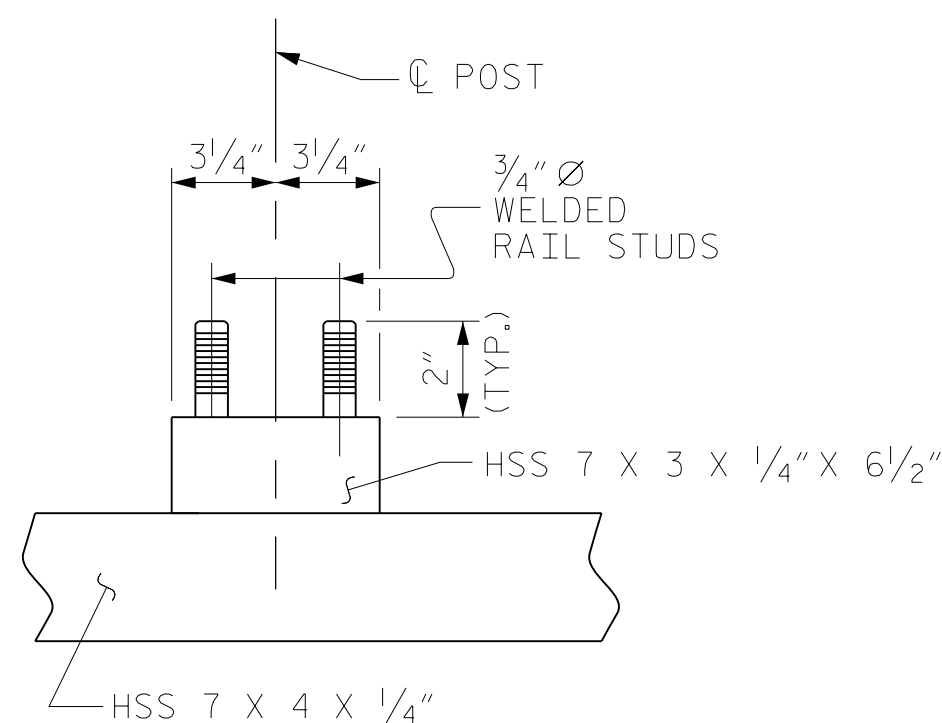
**PLAN - LOWER RAIL AND END POST**



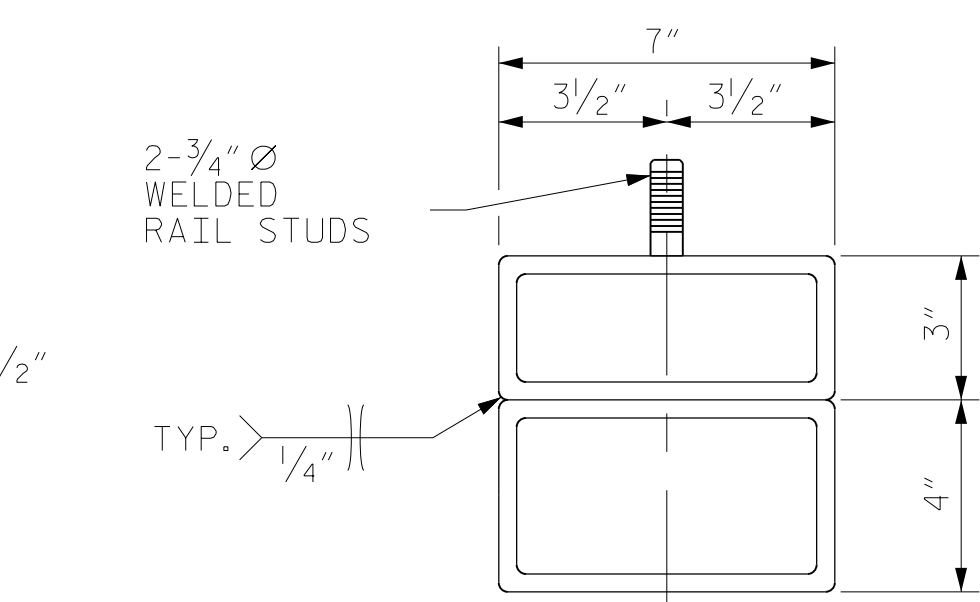
**ELEVATION**



**TOP RAIL SECTION**

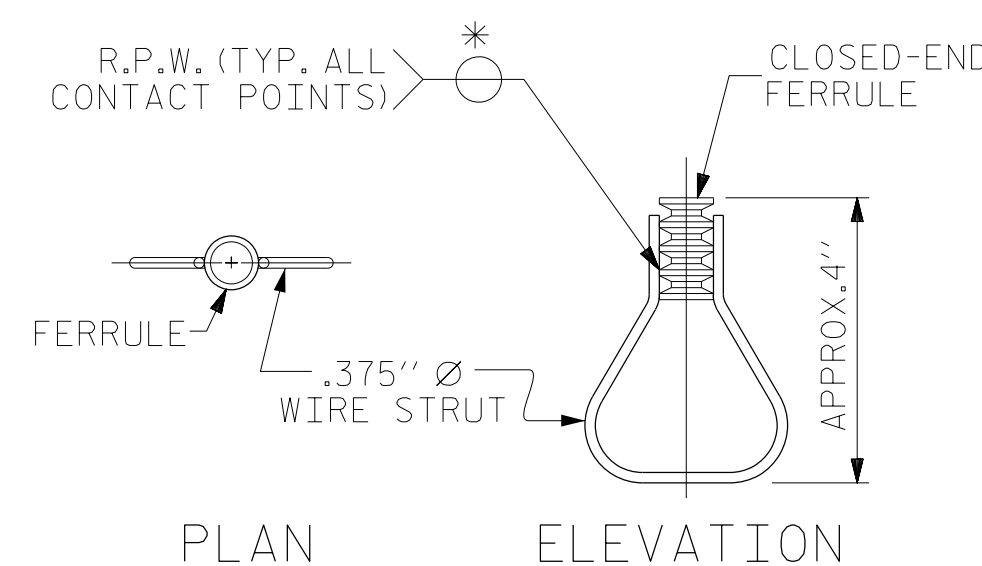


**PLAN**



**SECTION OF LOWER RAILS**

**RAIL STUD DETAILS**



**STRUCTURAL CONCRETE INSERT**

\* EACH WELDED ATTACHMENT OF WIRE TO FERRULE SHALL DEVELOP THE TENSILE STRENGTH OF THE WIRE.

**NOTES**

**STRUCTURAL CONCRETE INSERT**

EACH STRUCTURAL CONCRETE INSERT ASSEMBLY SHALL CONSIST OF THE FOLLOWING COMPONENTS:

- A. FERRULE SHALL BE MADE FROM STEEL MEETING THE REQUIREMENTS OF AASHTO M169, GRADE 12L14 AND SHALL HAVE A MINIMUM LENGTH OF THREADS OF 1/2".
- B. 1 - 3/4" Ø X 1 5/8" BOLT WITH WASHER, BOLT SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307. BOLT AND WASHER SHALL BE GALVANIZED. ( AT THE CONTRACTOR'S OPTION, STAINLESS STEEL BOLT AND WASHER MAY BE USED AS AN ALTERNATE FOR THE 3/4" Ø X 1 5/8" GALVANIZED BOLT AND WASHER, THEY SHALL CONFORM TO OR EXCEED THE MECHANICAL REQUIREMENTS OF ASTM A307. THE USE OF THIS ALTERNATE SHALL BE APPROVED BY THE ENGINEER.)
- C. WIRE STRUT SHOWN IN THE STRUCTURAL CONCRETE INSERT DETAIL IS THE MINIMUM ALLOWABLE SIZE AND SHALL HAVE A MINIMUM TENSILE STRENGTH OF 100,000 PSI. AS AN OPTION, A 1/16" Ø WIRE STRUT WITH A MINIMUM TENSILE STRENGTH OF 90,000 PSI IS ACCEPTABLE.

**NOTES**

**METAL RAIL TO END POST CONNECTION**

EACH METAL RAIL TO END POST CONNECTION SHALL CONSIST OF THE FOLLOWING COMPONENTS:

- A. 1/2" METAL BRACKET PLATE AND 1/4" METAL RAIL INSERT TUBE SHALL CONFORM TO AASHTO M270 GRADE 36 AND SHALL BE GALVANIZED AFTER FABRICATION TO AASHTO M111.
- B. 3/4" STRUCTURAL CONCRETE INSERTS SHALL HAVE A WORKING LOAD SHEAR CAPACITY OF 4800 LBS. THE FERRULES SHALL ENGAGE A 3/4" Ø X 1 5/8" BOLT WITH 2" O.D. WASHER IN PLACE. THE 3/4" Ø X 1 5/8" BOLT SHALL HAVE N. C. THREADS.

THE 3/4" STRUCTURAL CONCRETE INSERTS WITH BOLTS SHALL BE ASSEMBLED IN THE SHOP.

THE COST OF THE 3/4" STRUCTURAL CONCRETE INSERT, THE 1/2" BRACKET PLATES, AND THE RAIL INSERT TUBES COMPLETE IN PLACE SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS.

THE CONTRACTOR, AT HIS OPTION, MAY USE AN ADHESIVE BONDING SYSTEM IN LIEU OF THE STRUCTURAL CONCRETE INSERT EMBEDDED IN THE END POST. IF THE ADHESIVE BONDING SYSTEM IS USED, THE 3/4" Ø X 1 5/8" BOLTS WITH WASHERS SHALL BE REPLACED WITH 3/4" Ø X 6 1/2" BOLTS AND 2" O.D. WASHERS. ALL SPECIFICATIONS THAT APPLY TO THE 3/4" Ø X 1 5/8" BOLTS SHALL APPLY TO THE 3/4" Ø X 6 1/2" BOLTS. FIELD TESTING OF THE ADHESIVE BONDING SYSTEM IS NOT REQUIRED.

PROJECT NO. BR-0160  
BRUNSWICK COUNTY  
 STATION: 21+77.50 -L-

SHEET 2 OF 2

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STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
STANDARD					
END OF RAIL DETAILS					
FOR 42" OREGON RAIL					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
SHEET NO.					S-26
TOTAL SHEETS					42

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

ASSEMBLED BY : NSC	DATE : 11/2021
CHECKED BY : MRA	DATE : 01/2023
DRAWN BY : RWW 7/14	REV. 12/17
CHECKED BY : TMG 7/14	MAA/THC