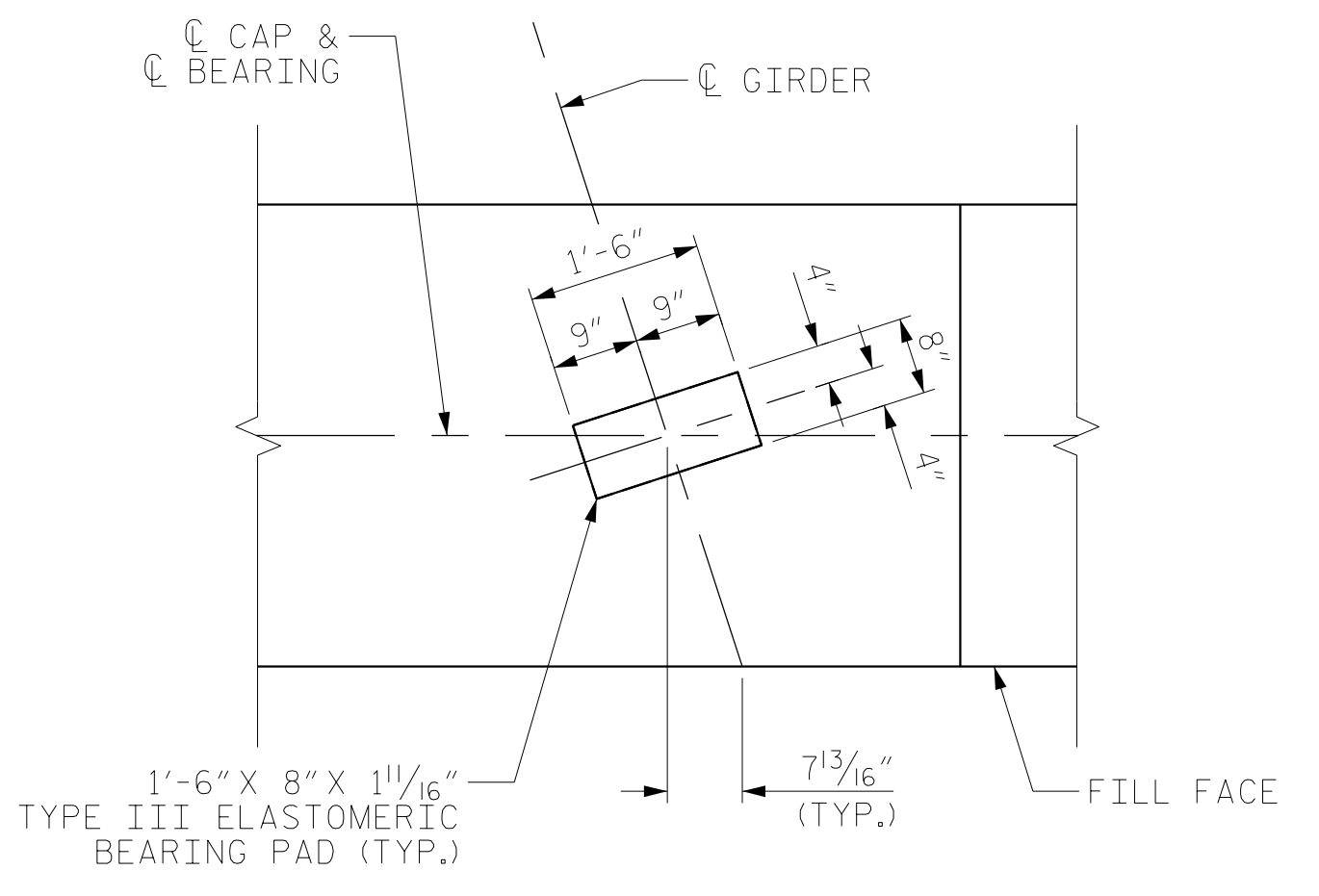
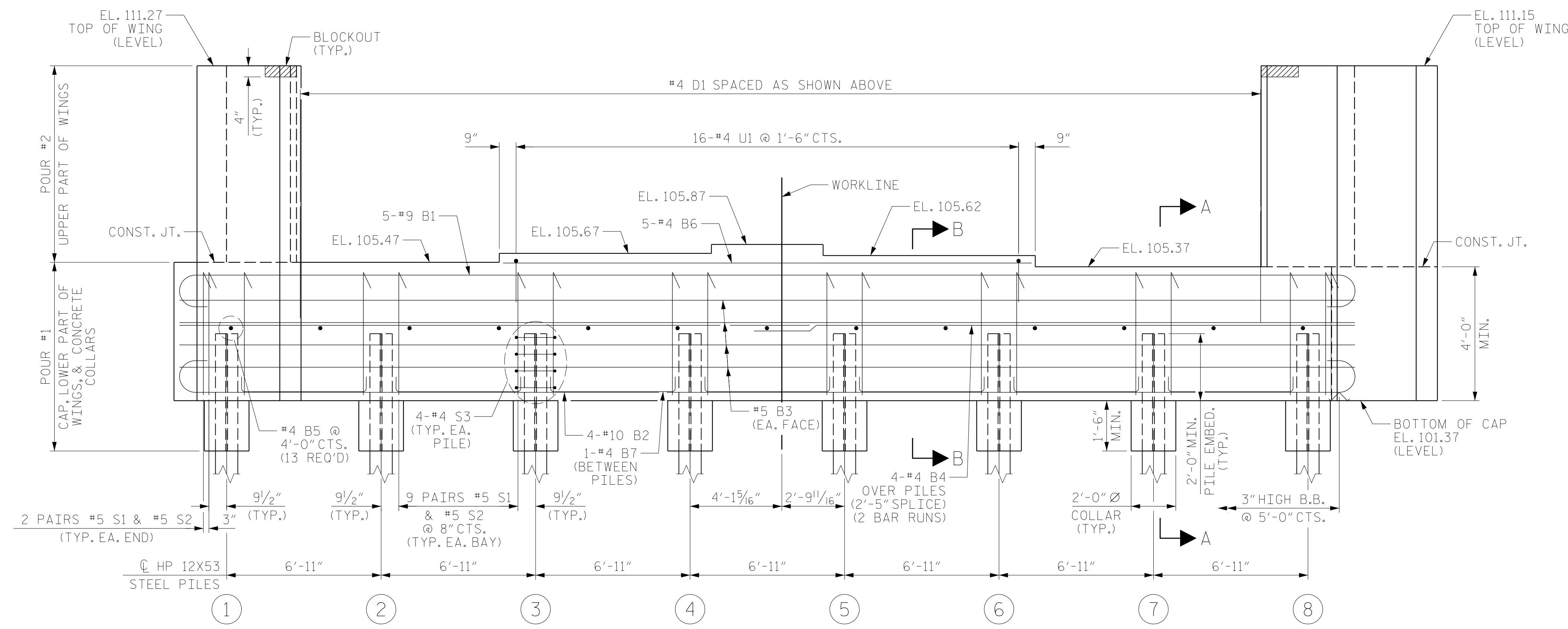


PLAN

NOTES:
 THE CONCRETE IN THE BLOCKOUTS SHALL BE POURED AFTER THE BARRIER RAIL IS CAST IF SLIP FORMING IS USED.
 #4 D1 BARS MAY BE SHIFTED SLIGHTLY TO AVOID STIRRUPS IN THE CAP.
 FOR SECTION A-A AND SECTION B-B, SEE SHEET 3 OF 3.
 THE TOP SURFACE OF THE END BENT CAP AND LOWER WINGS, EXCLUDING THE BEARING AREA, SHALL BE RAKED TO A DEPTH OF 1/4".
 IT SHALL BE BROUGHT TO THE CONTRACTOR'S ATTENTION THAT THE WINGWALLS ARE TO RETAIN NO FILL UNTIL THE INTEGRAL END BENT DIAPHRAGM CONCRETE HAS ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.
 SEE SUPERSTRUCTURE SHEETS FOR UPPER PART OF INTEGRAL END BENT DETAILS.
 STIRRUPS NEAR SKEWED ENDS MAY BE SKEWED TO FIT TO ENSURE CONCRETE CLEARANCES.



DETAIL "A"
 (TYP. EA. GIRDER)
 (PILES AND DOWELS NOT SHOWN FOR CLARITY)



ELEVATION

PROJECT NO. R-5751
 ROBESON COUNTY
 STATION: 37+99.89 -Y1B-

SHEET 1 OF 3

RS&H
 RS&H Architects-Engineers-Planners, Inc.
 8521 Six Forks Road, Suite 400
 Raleigh, NC 27615
 919-926-4100 FAX 919-846-9080
 www.rsandh.com
 North Carolina License No. 50737-5403-C&E

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
SUBSTRUCTURE END BENT 1					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
SHEET NO.					S-18
TOTAL SHEETS					28

DRAWN BY: TWL DATE: 02/2022
 CHECKED BY: MRA DATE: 02/2022
 DESIGN ENGINEER OF RECORD: MKO DATE: 10/2022

DOCUMENT NOT CONSIDERED
 FINAL UNLESS ALL
 SIGNATURES COMPLETED