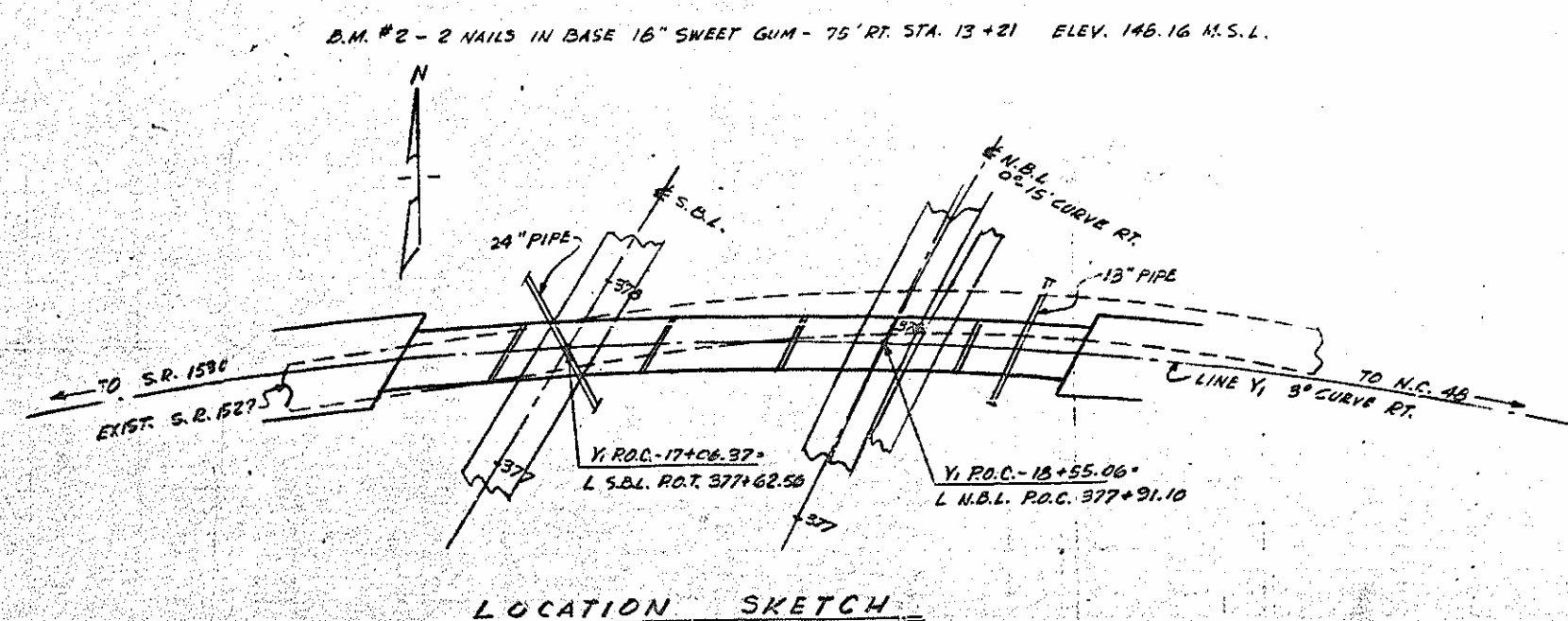


NOTE: E.B.T. #1 AND 375 #1 & #2 ARE PARALLEL TO S.B. LANE - LINE "L"
 B.T. #3 IS PARALLEL TO TANGENT THRU INTERSECTION OF N.B. LANE - LINE "L" & "Y"

BRIDGE LAYOUT SKETCH



LOCATION SKETCH

NOTES
 ASSUMED LIVE LOAD - H15-44
 FOR OTHER DESIGN DATA AND GENERAL NOTE, SEE SHEET 5-N
 PILES SHALL BE DRIVEN TO THE FOLLOWING MINIMUM BEARING CAPACITY:
 E.B.T. 1 - 26 TONS/PILE
 B.T. 1, B.T. 3, & E.B.T. 3 - 30 TONS/PILE
 B.T. 2 - 29 TONS/PILE
 B.T. 4 - 28 TONS/PILE
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING LENGTHS OF PILES REQUIRED. SEE SPECIAL PROVISIONS. THE CONTRACTOR WILL BE REQUIRED TO EXCAVATE COMPLETELY THROUGH THE FILL AT E.B.T. 1 & E.B.T. 3 BEFORE DRIVING PILES. SEE SHEET 5-N
 THE ROADWAY CONTRACTOR WILL BE REQUIRED TO REMOVE THE EXISTING PAVEMENT AND SCARIFY THE ROADBED TO A MINIMUM DEPTH OF 2'-0" WITHIN THE AREA OF THE E. BENT PILES.
 UNCLASSIFIED STRUCTURE EXCAVATION FOR INT. B.TS. SHALL BE MEASURED FROM SURFACE OF EXISTING GROUND LINE.
 TRAFFIC ON Y1 WILL BE ROUTED OVER A PROPOSED TEMPORARY DETOUR DURING CONSTRUCTION. SEE ROADWAY PLANS.
 THE LENGTH OF BRIDGE WAS COMPUTED ON THE BASIS OF NO DITCH SECTION WITH THE TOP OF SLOPE 2'-6" OUTSIDE OF SHOULDER.

PROJECT No. 8-11616
 NASH COUNTY
 STATION: 377+91.10

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION

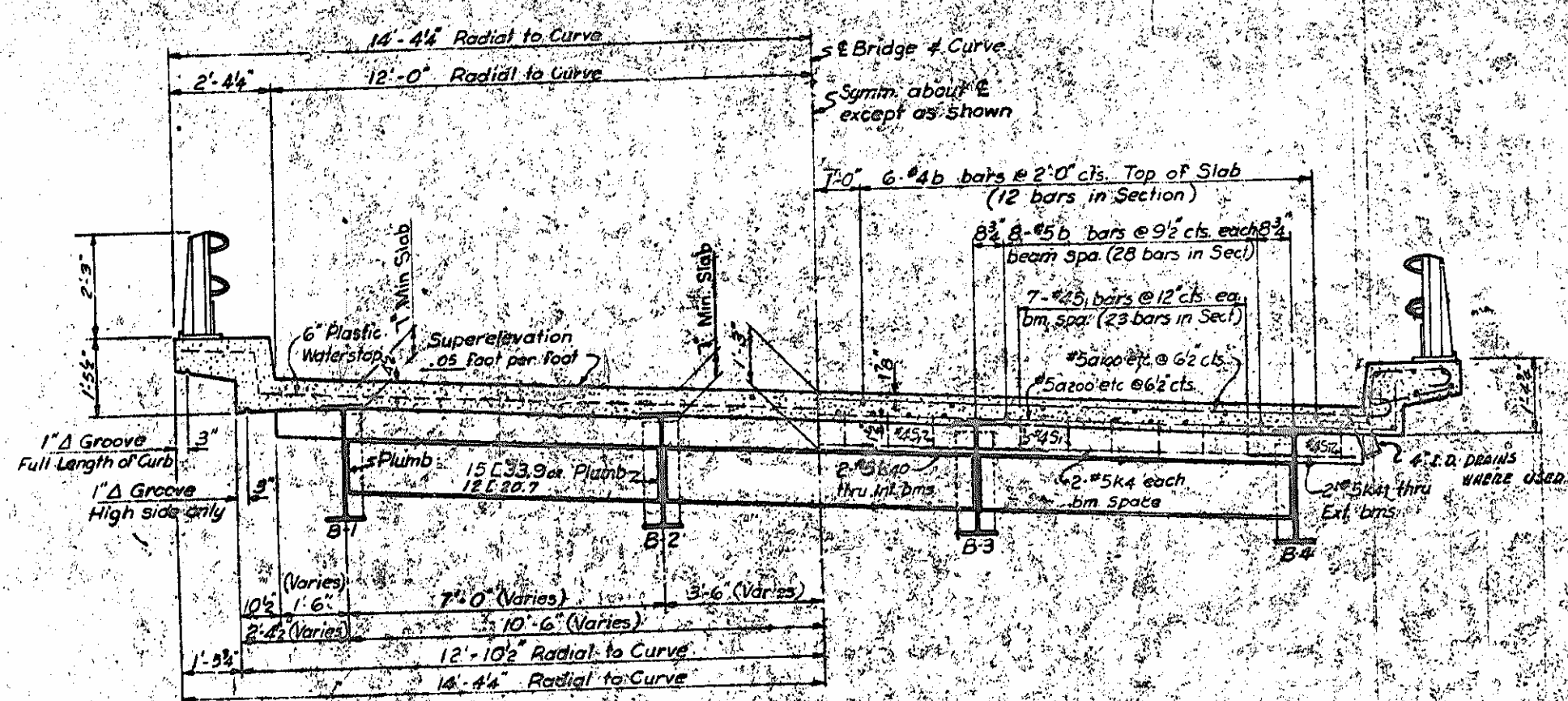
GENERAL DRAWING FOR
 OVER PROJECT ON Y (E.P.)
 BETWEEN S.R. 1536 AND

OCTOBER 1956

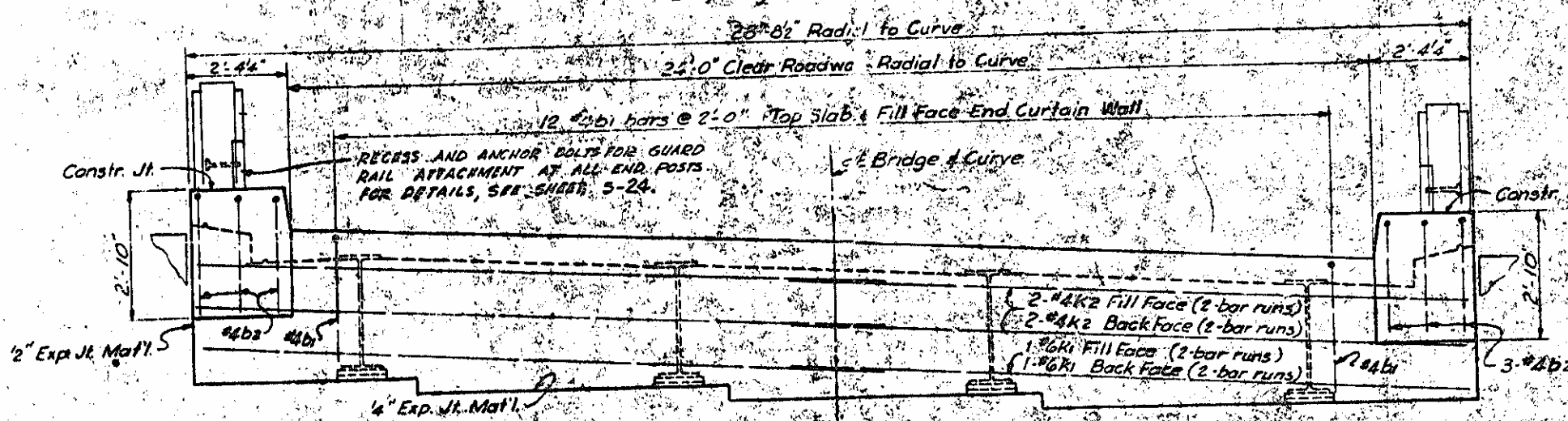
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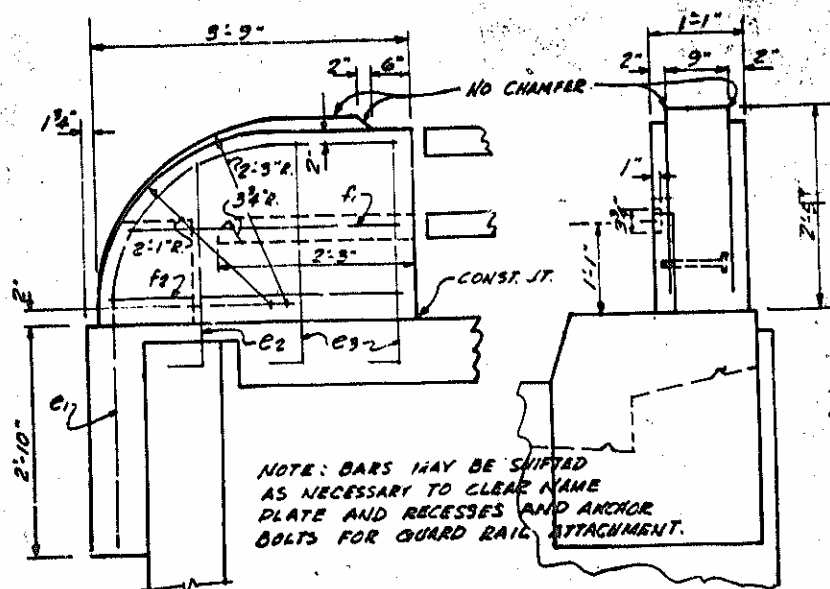
DESIGNED BY
 R.G. ALFORD, JR.
 CHECKED BY
 H.A. WILSON



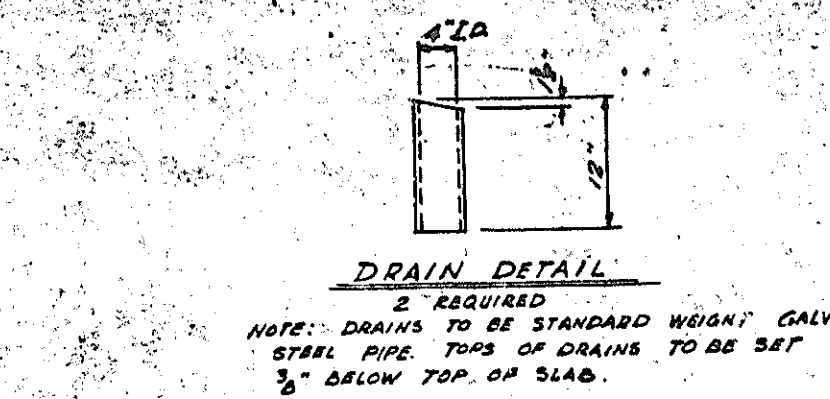
TYPICAL SECTION
Diaphragm at Best Shown.



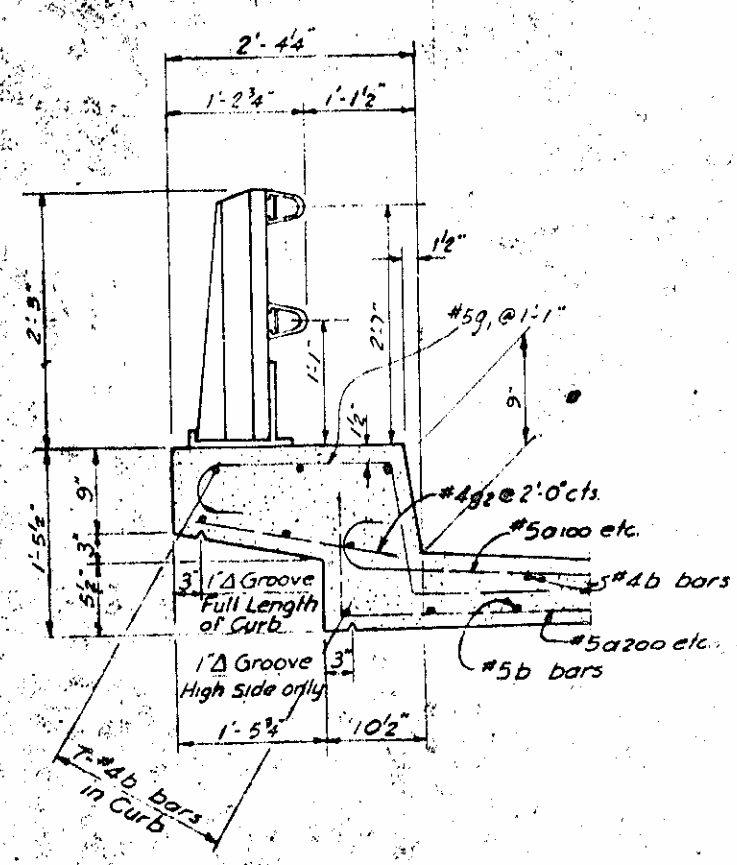
END ELEVATION



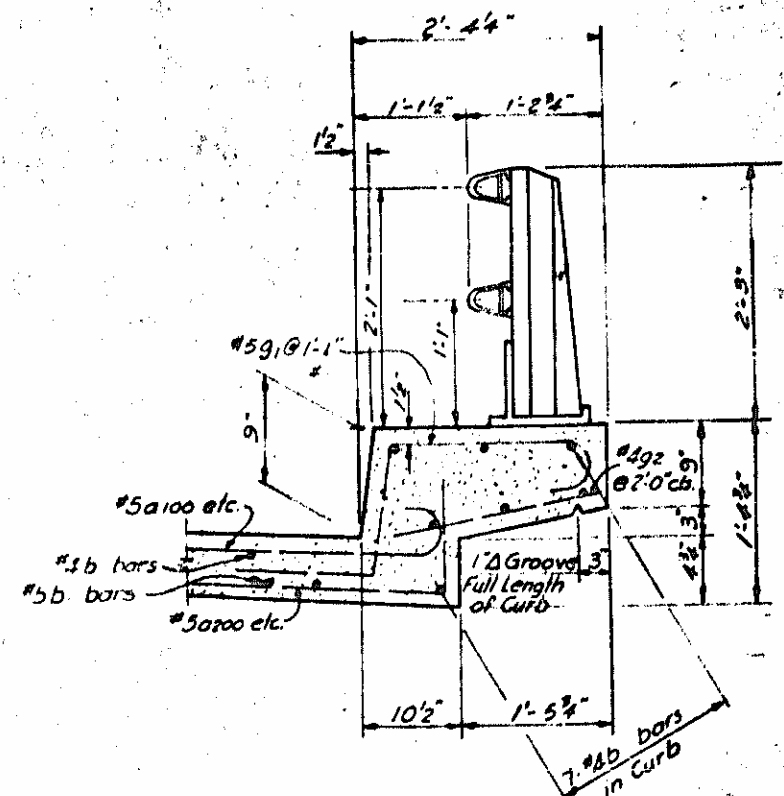
END POST DETAILS



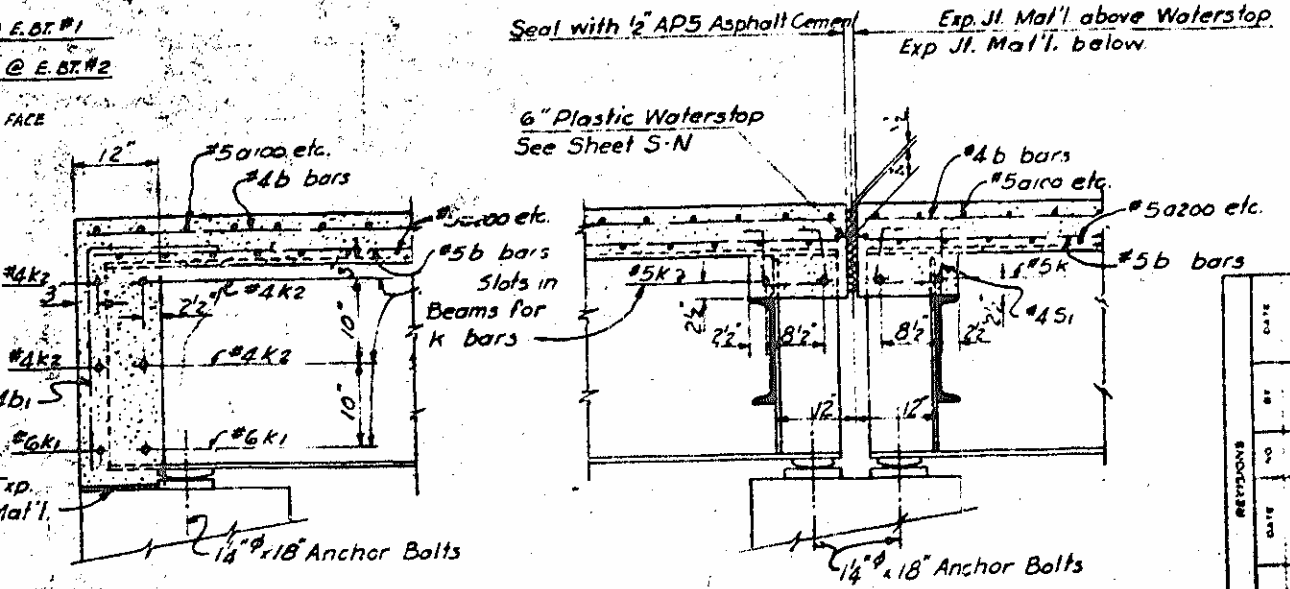
DRAIN DETAIL



SECTION THRU LT. CURB



SECTION THRU RT. CURB



SECTION A-A

SECTION B-B

~NOTES~
Assumed Live Load: H15 (44)
Concrete in compression 4,000 lbs./sq. in.
Reinforcing Steel in tension 20,000 lbs./sq. in.
Stress in extreme fiber of Str Steel 20,000 lbs./sq. in.
For other Design Data and General Notes, see S-5.
For bars indicated and no bar mark shown, see Concrete Plan for the different spans.
Expansion joints to be kept free of concrete and sealed with AP5 Asphalt Cement.

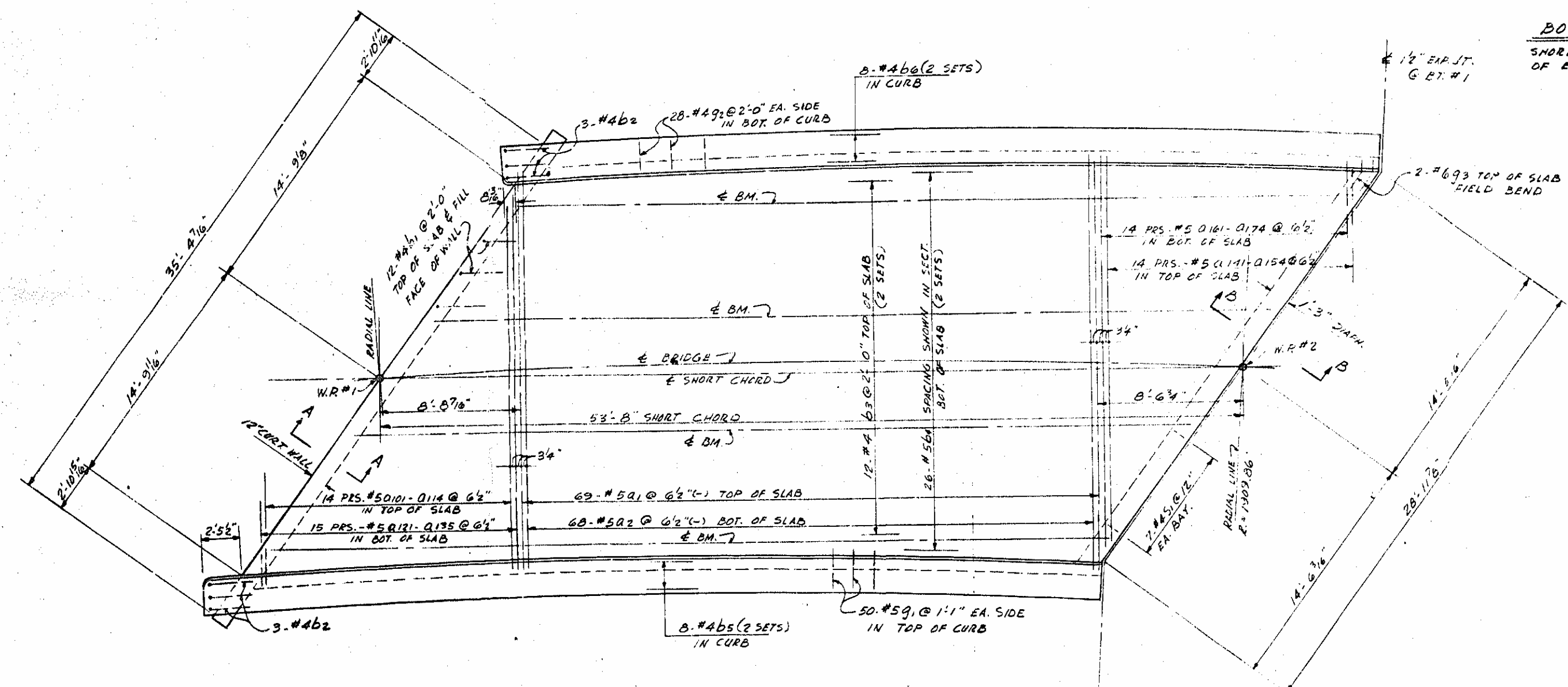
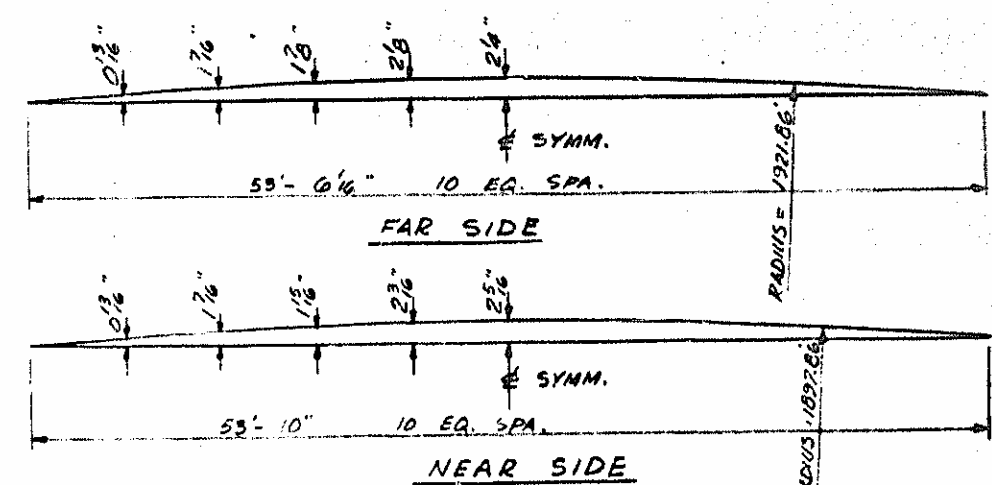
PROJECT NO. 8.11618
NASH COUNTY
STATION: 377+91.10 N.D.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
STANDARD TYPICAL SECTIONS
24' Roadway ~ 12" Curbs ~ R.H. Curb
4 Steel Beams ~ H15 LL
Metal Post & Rail
September 1962

SPECIAL	APPROVED BY: E. G. ALFORD, JR.	DATE: Dec. 1962
STANDARD	DESIGNED BY: GEORGE B. WOODS	DATE: Oct. 62
	DRAWN BY: J. H. LEE	DATE: Sept. 10, 1962
	CHECKED BY: H. C. LEE	DATE: Sept. 12, 1962

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NOTES:
 FIELD BEND OR FLARE 91 BARS TO SUIT CORNER CONDITIONS.
 "2" BARS SHALL BE PLACED PERPENDICULAR TO THE SHORT CHORD & SPACED ALONG THE SHORT CHORD.

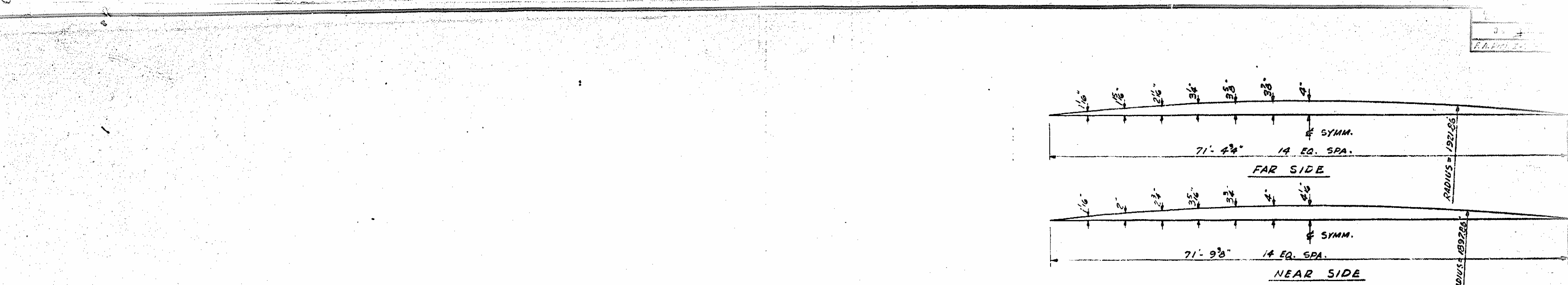
PROJECT No. 8.11618
 NASH COUNTY
 STATION: 377 + 91.10

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 SPAN A

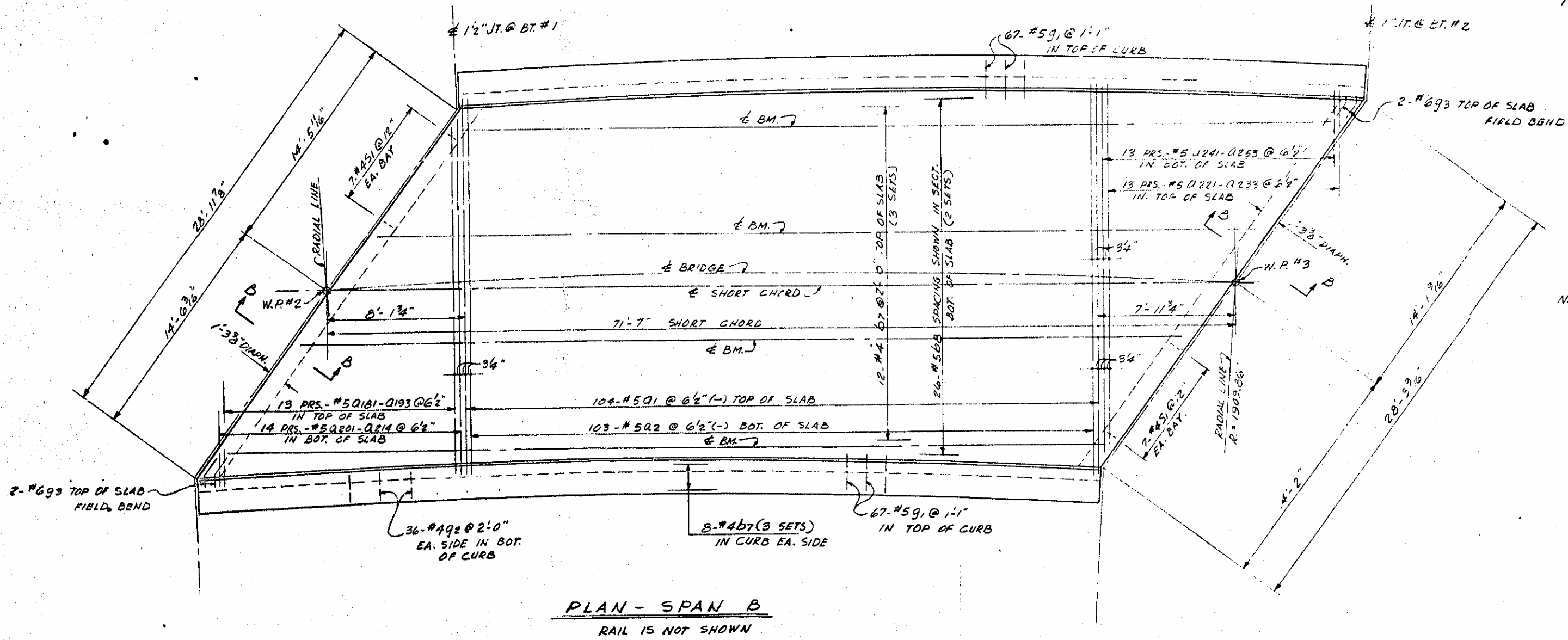
OCTOBER 1964

ED.	BY	DATE	NO.	BY	DATE
1			3		
2			6		

DESIGNED BY: R.G. ALFORD, JR. DATE: OCT. 64
 CHECKED BY: GEORGE R. KENNEDY DATE: OCT. 64



BOTTOM OF CURB OFFSETS
 SHORT CHORD IS MEASURED FROM C.B.M. #1 TO C.B.M. #2.



NOTES:
 FIELD BEND OR FLARE 91 BARS TO SUIT CORNER CONDITIONS
 "2" BARS SHALL BE PLACED PERPENDICULAR TO THE SHORT CHORD & SPACED ALONG THE SHORT CHORD.

PLAN - SPAN B
 RAIL IS NOT SHOWN

PROJECT NO. 8.11618
 NASH COUNTY
 STATION: 377+91.10 N.B.L.

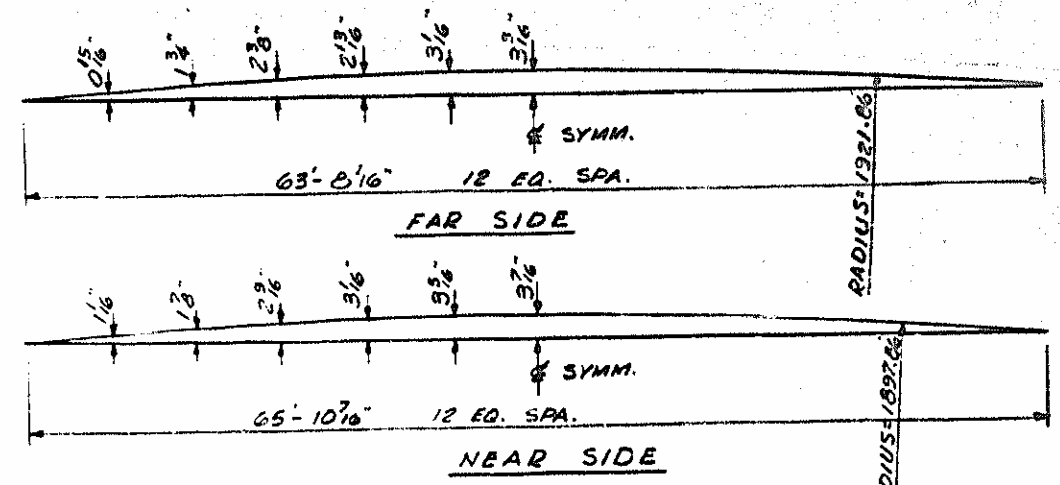
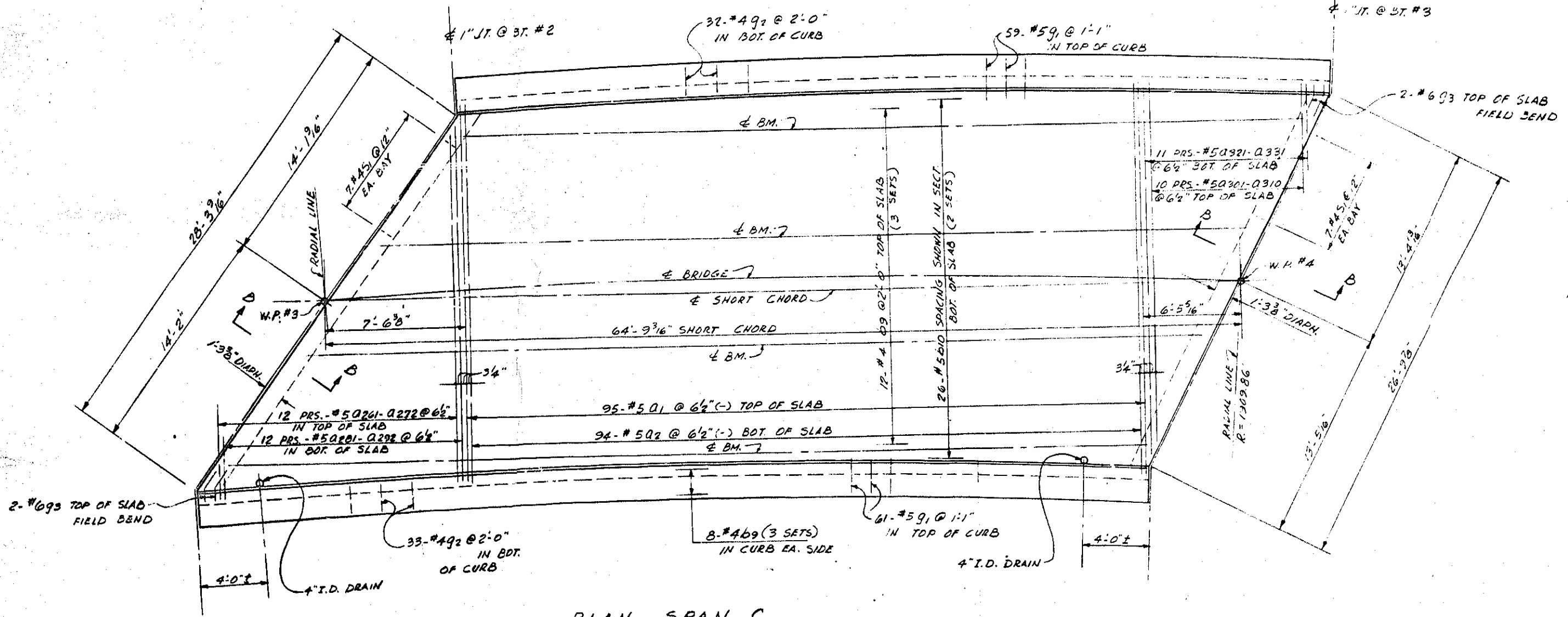
STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 SPAN B

REVISIONS				
NO.	BY	DATE	DESCRIPTION	APPROVED
1				
2				

DESIGNED BY E.G. ALFORD, JR. DATE OCT. '67
 CHECKED BY GEORGE B. WILSON DATE OCT. '67

PROJECT NO. 11618

DATE: 10/1/69



BOTTOM OF CURB OFFSETS
 SHORT CHORD IS MEASURED FROM # BT #2 TO # BT #3.

NOTES:
 FIELD BEND OR FLARE 91 BARS TO SUIT CORNER CONDITIONS
 "A" BARS SHALL BE PLACED PERPENDICULAR TO THE SHORT CHORD & SPACED ALONG THE SHORT CHORD.

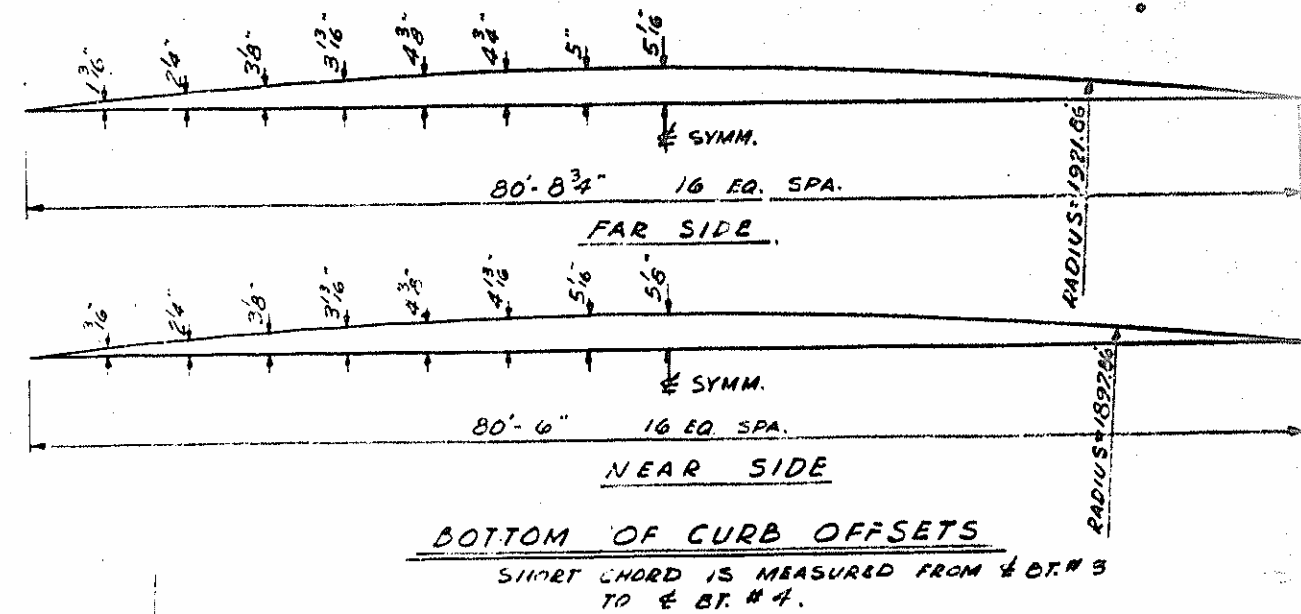
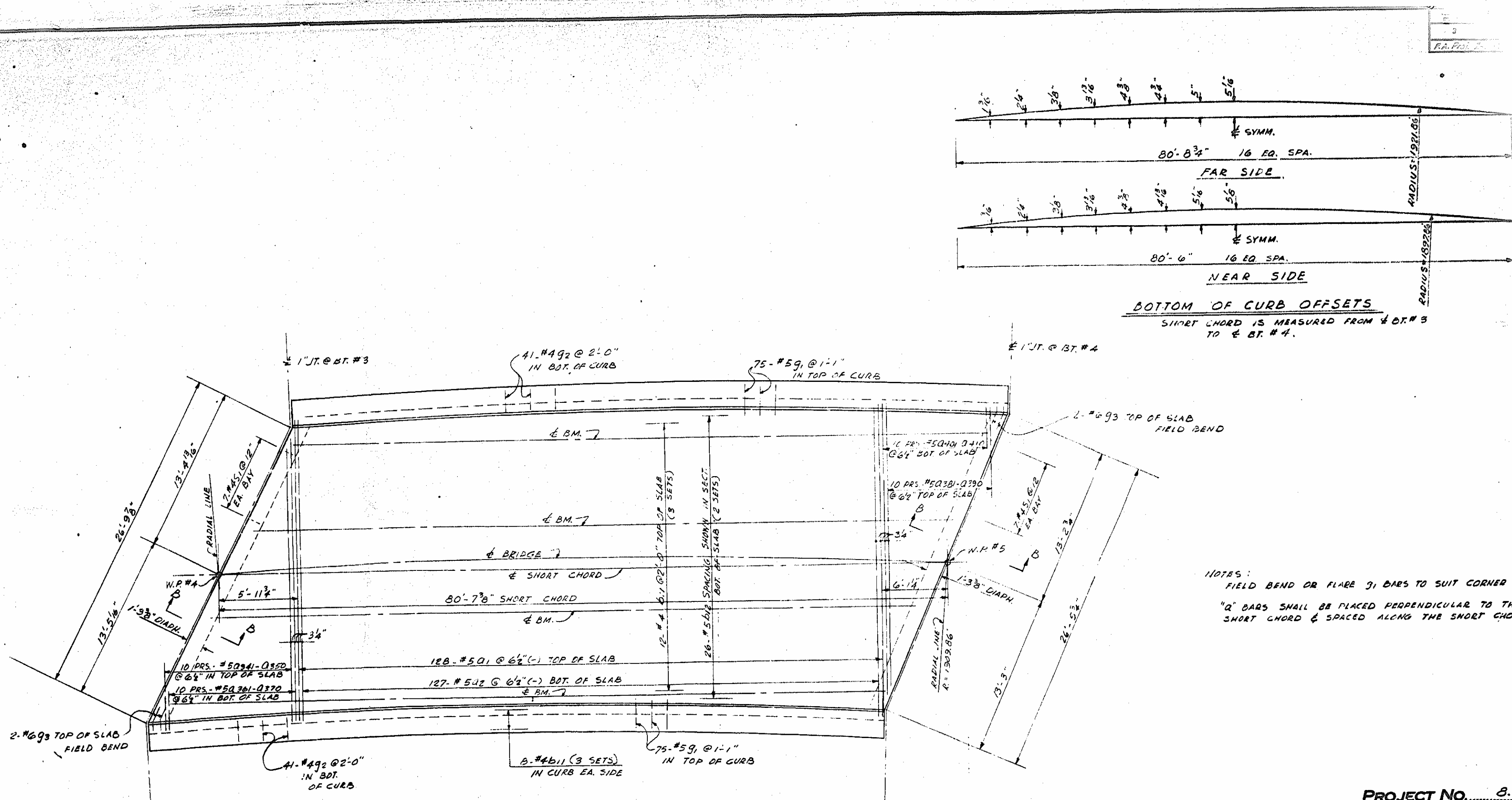
PLAN - SPAN C
 RAIL IS NOT SHOWN

PROJECT NO. 11618
 NASH COUNTY
 STATION: 377 + 91.10 ALD.

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 SPAN C

REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		
3			4		

DESIGNED BY: F.G. ALFORD, JR. DATE: Oct. 69
 CHECKED BY: DATE: Oct. 69
 APPROVED BY: DATE: Oct. 69



NOTES:
 FIELD BEND OR FLARE 91 BARS TO SUIT CORNER CONDITION.
 "Q" BARS SHALL BE PLACED PERPENDICULAR TO THE SHORT CHORD & SPACED ALONG THE SHORT CHORD.

PLAN - SPAN D
 RAIL IS NOT SHOWN

PROJECT NO. 8.11618
 NASH COUNTY
 STATION: 377 + 91.10 H.L.

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 SPAN D

REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

DESIGNED BY: E. G. ALPORD, JR. DATE: OCT. 1968
 CHECKED BY: GEORGE B. WISHINE DATE: OCT. 1968

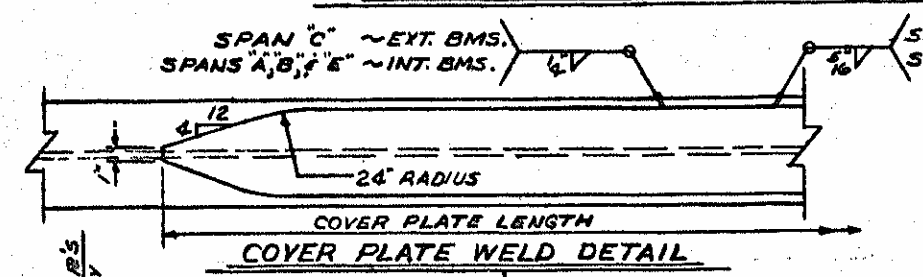
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Q101	2	#5	23'-0"	49	Q101	2	#5	23'-7"	49	Q201	2	#5	23'-5"	49	Q201	2	#5	23'-0"	48	Q201	2	#5	22'-10"	48	Q1	470	#5	1	25'-7"	13,031
Q102	1	#5	22'-2"	46	Q102	1	#5	21'-11"	46	Q202	1	#5	21'-5"	45	Q202	1	#5	21'-0"	45	Q202	1	#5	20'-6"	43	Q2	465	#5	2	27'-5"	13,297
Q103	1	#5	20'-7"	43	Q103	1	#5	20'-4"	42	Q203	1	#5	19'-11"	42	Q203	1	#5	18'-5"	38	Q203	1	#5	18'-1"	38	B1	24	#4	7	4'-0"	64
Q104	1	#5	19'-1"	40	Q104	1	#5	18'-8"	39	Q204	1	#5	18'-1"	38	Q204	1	#5	16'-2"	34	Q204	1	#5	15'-8"	33	B1	24	#4	7	4'-0"	64
Q105	1	#5	17'-7"	37	Q105	1	#5	17'-0"	35	Q205	1	#5	16'-4"	34	Q205	1	#5	15'-11"	33	Q205	1	#5	13'-4"	28	B1	24	#4	7	4'-0"	64
Q106	1	#5	16'-0"	33	Q106	1	#5	15'-6"	32	Q206	1	#5	14'-6"	30	Q206	1	#5	13'-0"	27	Q206	1	#5	11'-0"	24	B1	24	#4	7	4'-0"	64
Q107	1	#5	14'-6"	30	Q107	1	#5	13'-9"	29	Q207	1	#5	12'-9"	27	Q207	1	#5	11'-0"	24	Q207	1	#5	9'-6"	18	B1	24	#4	7	4'-0"	64
Q108	1	#5	12'-11"	27	Q108	1	#5	12'-1"	25	Q208	1	#5	11'-0"	24	Q208	1	#5	9'-6"	18	Q208	1	#5	8'-6"	15	B1	24	#4	7	4'-0"	64
Q109	1	#5	11'-5"	24	Q109	1	#5	10'-5"	22	Q209	1	#5	9'-2"	19	Q209	1	#5	8'-10"	10	Q209	1	#5	7'-1"	15	B1	24	#4	7	4'-0"	64
Q110	1	#5	9'-10"	21	Q110	1	#5	8'-9"	18	Q210	1	#5	7'-5"	16	Q210	1	#5	6'-10"	10	Q210	1	#5	5'-7"	12	B1	24	#4	7	4'-0"	64
Q111	1	#5	8'-4"	17	Q111	1	#5	7'-2"	15	Q211	1	#5	5'-7"	12	Q211	1	#5	4'-10"	10	Q211	1	#5	3'-9"	8	B1	24	#4	7	4'-0"	64
Q112	1	#5	6'-10"	14	Q112	1	#5	5'-6"	11	Q212	1	#5	3'-10"	8	Q212	1	#5	2'-7"	5	Q212	1	#5	2'-7"	5	B1	24	#4	7	4'-0"	64
Q113	1	#5	5'-3"	11	Q113	1	#5	4'-0"	8	Q213	1	#5	2'-7"	5	Q213	1	#5	2'-7"	5	Q213	1	#5	2'-7"	5	B1	24	#4	7	4'-0"	64
Q114	2	#5	3'-9"	8	Q114	2	#5	3'-9"	8	Q214	2	#5	3'-9"	8	Q214	2	#5	3'-9"	8	Q214	2	#5	3'-9"	8	B1	24	#4	7	4'-0"	64
Q121	2	#5	24'-6"	51	Q121	2	#5	24'-5"	51	Q201	2	#5	24'-4"	51	Q201	2	#5	23'-11"	50	Q201	2	#5	23'-10"	50	B1	24	#4	7	4'-0"	64
Q122	1	#5	22'-11"	48	Q122	1	#5	22'-9"	47	Q202	1	#5	22'-6"	47	Q202	1	#5	21'-8"	45	Q202	1	#5	21'-6"	45	B1	24	#4	7	4'-0"	64
Q123	1	#5	21'-4"	45	Q123	1	#5	21'-1"	44	Q203	1	#5	20'-9"	43	Q203	1	#5	19'-5"	41	Q203	1	#5	19'-1"	40	B1	24	#4	7	4'-0"	64
Q124	1	#5	19'-11"	42	Q124	1	#5	19'-6"	41	Q204	1	#5	18'-11"	39	Q204	1	#5	17'-2"	36	Q204	1	#5	16'-8"	35	B1	24	#4	7	4'-0"	64
Q125	1	#5	18'-4"	39	Q125	1	#5	17'-10"	37	Q205	1	#5	17'-2"	36	Q205	1	#5	15'-11"	31	Q205	1	#5	14'-8"	30	B1	24	#4	7	4'-0"	64
Q126	1	#5	16'-10"	35	Q126	1	#5	16'-2"	34	Q206	1	#5	15'-5"	32	Q206	1	#5	13'-7"	28	Q206	1	#5	12'-11"	27	B1	24	#4	7	4'-0"	64
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Q128	1	#5	13'-9"	29	Q128	1	#5	12'-11"	27	Q208	1	#5	11'-10"	25	Q208	1	#5	10'-4"	22	Q208	1	#5	9'-6"	20	B1	24	#4	7	4'-0"	64
Q129	1	#5	12'-2"	25	Q129	1	#5	10'-10"	23	Q209	1	#5	10'-0"	21	Q209	1	#5	8'-10"	17	Q209	1	#5	8'-10"	17	B1	24	#4	7	4'-0"	64
Q130	1	#5	10'-8"	22	Q130	1	#5	9'-7"	20	Q210	1	#5	8'-3"	17	Q210	1	#5	7'-6"	14	Q210	1	#5	6'-6"	14	B1	24	#4	7	4'-0"	64
Q131	1	#5	9'-2"	19	Q131	1	#5	8'-0"	17	Q211	1	#5	6'-0"	14	Q211	1	#5	5'-10"	12	Q211	1	#5	4'-10"	10	B1	24	#4	7	4'-0"	64
Q132	1	#5	7'-7"	16	Q132	1	#5	6'-4"	13	Q212	1	#5	4'-0"	10	Q212	1	#5	3'-10"	8	Q212	1	#5	3'-10"	8	B1	24	#4	7	4'-0"	64
Q133	1	#5	6'-1"	13	Q133	1	#5	4'-0"	8	Q213	1	#5	3'-0"	6	Q213	1	#5	2'-7"	5	Q213	1	#5	2'-7"	5	B1	24	#4	7	4'-0"	64
Q134	1	#5	4'-6"	9	Q134	1	#5	3'-0"	6	Q214	1	#5	2'-0"	4	Q214	1	#5	1'-11"	3	Q214	1	#5	1'-11"	3	B1	24	#4	7	4'-0"	64
Q135	1	#5	3'-0"	6	Q135	1	#5	2'-0"	4	Q215	1	#5	1'-0"	2	Q215	1	#5	0'-0"	0	Q215	1	#5	0'-0"	0	B1	24	#4	7	4'-0"	64
Q141	2	#5	23'-0"	49	Q201	2	#5	23'-7"	49	Q201	2	#5	23'-5"	49	Q201	2	#5	23'-0"	48	Q201	2	#5	22'-10"	48	K1	6	#6	57R	19'-2"	280
Q142	1	#5	22'-7"	48	Q202	1	#5	22'-4"	47	Q202	1	#5	22'-0"	46	Q202	1	#5	21'-7"	45	Q202	1	#5	21'-2"	43	K2	10	#6	57R	18'-10"	281
Q143	1	#5	20'-6"	43	Q203	1	#5	20'-2"	42	Q203	1	#5	19'-11"	42	Q203	1	#5	18'-5"	39	Q203	1	#5	18'-1"	39	K4	48	#5	57R	7'-7"	280
Q144	1	#5	19'-0"	40	Q204	1	#5	18'-6"	39	Q204	1	#5	18'-10"	38	Q204	1	#5	16'-4"	34	Q204	1	#5	16'-0"	34	K6	32	#5	57R	4'-2"	189
Q145	1	#5	17'-5"	36	Q205	1	#5	17'-10"	35	Q205	1	#5	17'-2"	34	Q205	1	#5	14'-2"	30	Q205	1	#5	14'-0"	29	K6	5	#5	57R	4'-9"	159
Q146	1	#5	15'-10"	33	Q206	1	#5	15'-2"	32	Q206	1	#5	14'-9"	31	Q206	1	#5	12'-0"	26	Q206	1	#5	11'-8"	24	K6	5	#5	57R	4'-9"	159
Q147	1	#5	14'-5"	30	Q207	1	#5	13'-9"	29	Q207	1	#5	13'-7"	28	Q207	1	#5	11'-11"	25	Q207	1	#5	11'-7"	24	S1	16	#4	6	3'-4"	410
Q148	1	#5	12'-9"	27	Q208	1	#5	12'-1"	25	Q208	1	#5	11'-9"	24	Q208	1	#5	10'-7"	22	Q208	1	#5	10'-0"	20	S1	16	#4	6	3'-4"	410
Q149	1	#5	11'-2"	23	Q209	1	#5	10'-10"	23	Q209	1	#5	10'-7"	22	Q209	1	#5	8'-5"	18	Q209	1	#5	7'-6"	16	S1	16	#4	6	3'-4"	410
Q150	1	#5	9'-7"	20	Q210	1	#5	8'-5"	18	Q210	1	#5	8'-3"	17	Q210	1	#5	6'-4"	15	Q210	1	#5	5'-3"	11	S1	16	#4	6	3'-4"	410
Q151	1	#5	8'-0"	17	Q211	1	#5	6'-8"	14	Q211	1	#5	6'-0"	14	Q211	1	#5	5'-3"	11	Q211	1	#5	4'-11"	10	S1	16	#4	6	3'-4"	410
Q152	1	#5	6'-5"	13	Q212	1	#5	5'-0"	10	Q212	1	#5	4'-0"	10	Q212	1	#5	3'-10"	8	Q212	1	#5	3'-10"	8	S1	16	#4	6	3'-4"	410
Q153	1	#5	4'-10"	10	Q213	1	#5	3'-4"	7	Q213	1	#5	2'-0"	4	Q213	1	#5	1'-11"	3	Q213	1	#5	1'-11"	3	S1	16	#4	6	3'-4"	410
Q154	2	#5	3'-0"	6	Q214	2	#5	2'-0"	4	Q214	2	#5	1'-0"	2	Q214	2	#5	0'-0"	0	Q214	2	#5	0'-0"	0	S1	16	#4	6	3'-4"	410
Q161	2	#5	24'-6"	51	Q201	2	#5	24'-5"	51	Q201	2	#5	24'-1"	50	Q201	2	#5	24'-0"	50	Q201	2	#5	24'-0"	50	Q201	2	#5	4	24'-0"	50
Q162	1	#5	22'-11"	48	Q202	1	#5	22'-9"	47	Q202	1	#5	22'-0"	46	Q202	1	#5	21'-9"	45	Q202	1	#5	21'-9"	45	Q202	1	#5	4	21'-9"	45
Q163	1	#5	21'-4"	45	Q203	1	#5	21'-1"	44	Q203	1	#5	20'-9"	43	Q203	1	#5	19'-5"	41	Q203	1	#5	19'-1"	40	Q203	1	#5	4	19'-1"	40
Q164	1	#5	19'-11"	42	Q204	1	#5	19'-6"	41																					

SPAN "A"	EXT.	50' 10" = 5'-0"	4 @ 1'-3" = 5'-0"	3 @ 1'-6" = 4'-6"	2 @ 1'-9" = 3'-0"	4 EQ. SPA. = 7'-8 1/2" ~ B1	4 EQ. SPA. = 7'-10 1/2" ~ B2
	INT.	9 @ 6" = 4'-6"	8 @ 7 1/2" = 5'-0"	6 @ 9" = 5'-0"	4 @ 10 1/2" = 4'-2"	4 EQ. SPA. = 4'-3 1/2" ~ B2	4 EQ. SPA. = 4'-3 1/2" ~ B3
SPAN "B"	EXT.	11 @ 1'-0" = 11'-0"	9 @ 1'-3" = 9'-0"	4 @ 1'-6" = 6'-0"	2 @ 1'-9" = 3'-0"	6 EQ. SPA. = 10'-10 1/2" ~ B1	6 EQ. SPA. = 11'-0 1/2" ~ B2
	INT.	13 @ 9" = 9'-9"	6 @ 1'-0" = 6'-0"	5 @ 1'-3" = 5'-0"	3 @ 1'-6" = 4'-6"	5 EQ. SPA. = 8'-0 1/2" ~ B2	5 EQ. SPA. = 8'-9 1/2" ~ B3
SPAN "C"	EXT.	9 @ 1'-0" = 9'-0"	5 @ 1'-3" = 5'-0"	2 @ 1'-6" = 3'-0"	1 @ 1'-9" = 1'-9"	5 EQ. SPA. = 9'-7 1/2" ~ B1	5 EQ. SPA. = 9'-7 1/2" ~ B2
	INT.	10 @ 1'-0" = 10'-0"	8 @ 1'-3" = 8'-0"	4 @ 1'-6" = 6'-0"	2 @ 1'-9" = 3'-0"	4 EQ. SPA. = 7'-10 1/2" ~ B1	2 EQ. SPA. = 7'-10 1/2" ~ B2
SPAN "D"	EXT.	12 @ 1'-0" = 12'-0"	10 @ 1'-3" = 10'-0"	4 @ 1'-6" = 6'-0"	2 @ 1'-9" = 3'-0"	7 EQ. SPA. = 13'-4 1/2" ~ B1	7 EQ. SPA. = 13'-3" ~ B2
	INT.	6 @ 9" = 5'-0"	7 @ 10 1/2" = 7'-3"	6 @ 1'-0" = 6'-0"	3 @ 1'-3" = 3'-0"	3 EQ. SPA. = 5'-11 1/2" ~ B2	3 EQ. SPA. = 5'-10 1/2" ~ B3
SPAN "E"	EXT.	4 @ 1'-0" = 4'-0"	3 @ 1'-3" = 3'-0"	2 @ 1'-6" = 3'-0"	1 @ 1'-9" = 1'-9"	4 EQ. SPA. = 7'-10 1/2" ~ B1	4 EQ. SPA. = 7'-7 1/2" ~ B2
	INT.	7 @ 6" = 4'-0"	9 @ 7 1/2" = 5'-7 1/2"	4 @ 9" = 3'-0"	4 @ 10 1/2" = 4'-2"	5 @ 1'-0" = 5'-0"	5 EQ. SPA. = 11'-0 1/2" ~ B2

SPAN "A" - INT.	16'-0"	(8" x 1/2" x 32'-0" COVER PLATE)
SPAN "B" - EXT.	22'-9"	(10" x 1/2" x 44'-6" COVER PLATE)
SPAN "B" - INT.	21'-9"	(10" x 1/2" x 43'-6" COVER PLATE)
SPAN "C" - EXT. B1	17'-9"	(10" x 1/2" x 36'-6" COVER PLATE)
SPAN "C" - EXT. B2	19'-9"	(10" x 1/2" x 38'-6" COVER PLATE)
SPAN "C" - INT.	21'-9"	(9" x 1/2" x 43'-6" COVER PLATE)
SPAN "D" - EXT.	27'-9"	(10" x 1/2" x 54'-6" COVER PLATE)
SPAN "D" - INT.	25'-6"	(10" x 1/2" x 51'-0" COVER PLATE)
SPAN "E" - INT.	14'-9"	(6" x 1/2" x 28'-6" COVER PLATE)

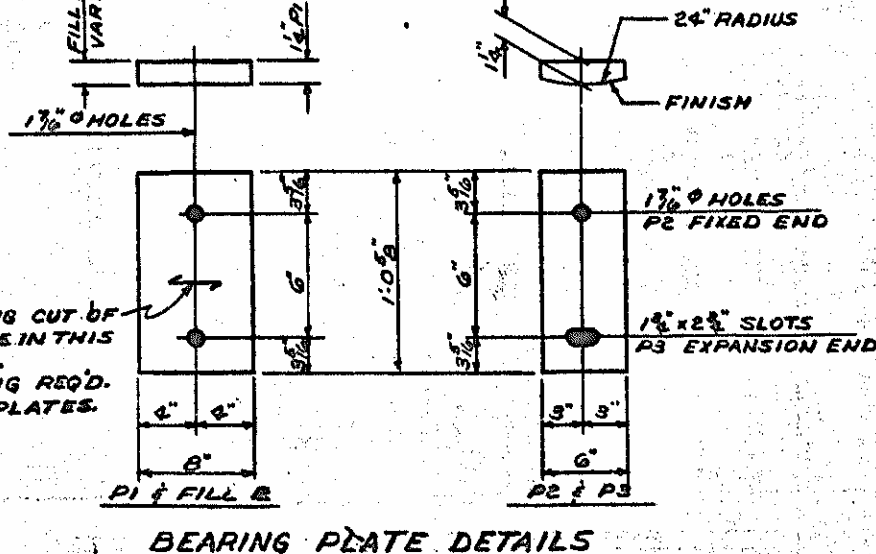
NOTE: NO COVER PLATES ARE REQUIRED ON EXTERIOR BEAMS FOR SPAN "A" & SPAN "E".

STUD SPACING & COVER PLATES



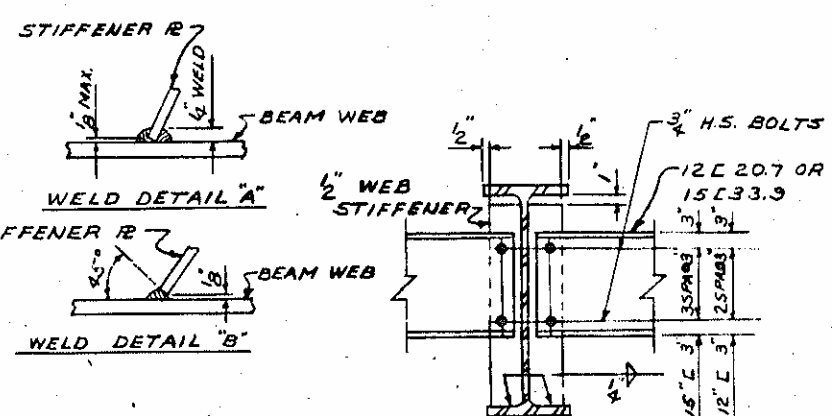
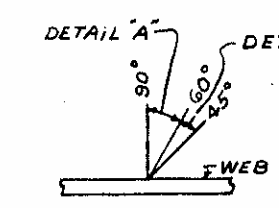
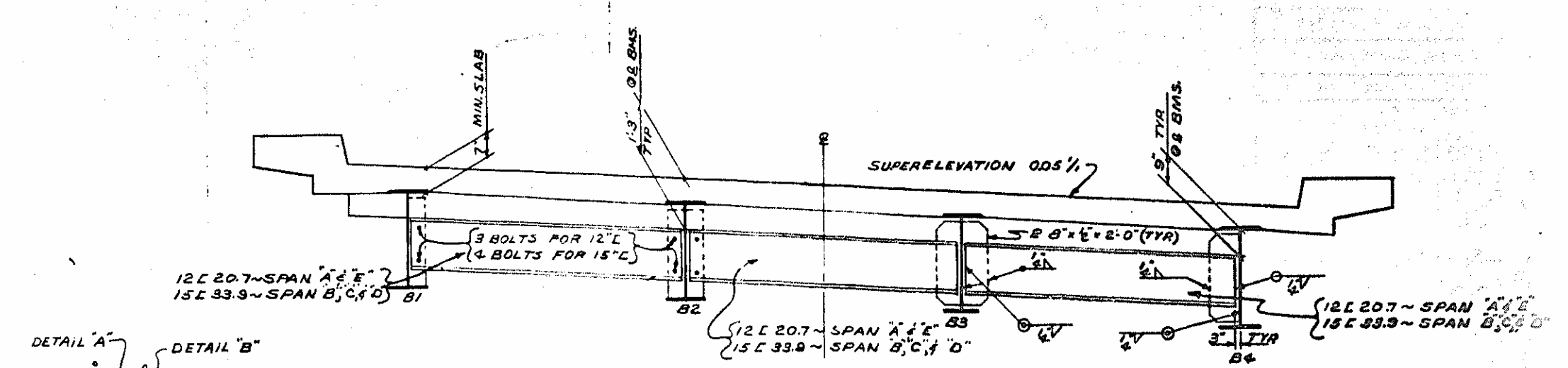
NOTE: COVER PLATE LENGTHS ARE SYMM. @ 2" BETWEEN CENTER TO CENTER OF BEARING AND FOR BOTTOM FLANGE ONLY.

NOTE: AT THE CONTRACTOR'S OPTION HE MAY SUBSTITUTE FOR THE COVER PLATES DESIGNATED ON THE PLANS COVER PLATES OF EQUIVALENT AREA PROVIDED THESE PLATES ARE AT LEAST 3/8" IN THICKNESS AND DO NOT EXCEED A WIDTH EQUAL TO THE FLANGE WIDTH LESS 2" OR A THICKNESS EQUAL TO THE FLANGE THICKNESS PLUS 1/8". THE SIZE OF THE WELD FOR ATTACHING THESE COVER PLATES SHALL BE IN ACCORDANCE WITH THE A.W.S. SPECIFICATIONS.

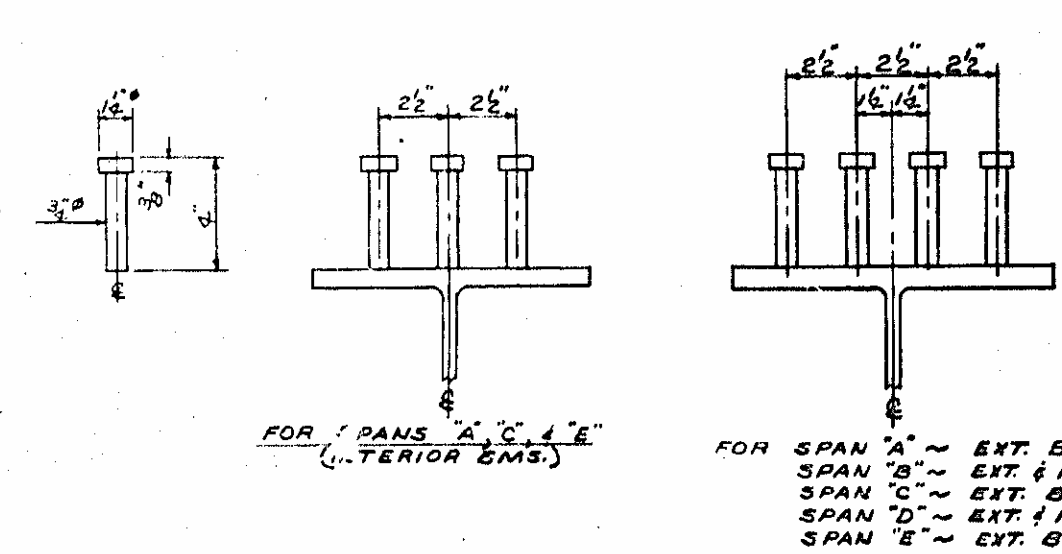


IN PLACING CUT OF TOOL TO BE IN THIS DIRECTION. NO PLANING REQ'D. FOR FILL PLATES.

DESIGNED BY	G.T. PHILLIPS	DATE	SEPT. 62
CHECKED BY	GEORGE B. WYNN	DATE	OCT. 62



STUD DETAILS



BEARING PLATES REQ'D.

40	P1	6" x 1/2" x 1'-0"	PLANE FINISH TO 1/4"
20	P2	6" x 1/2" x 1'-0"	AS DETAILED
20	P3	6" x 1/2" x 1'-0"	AS DETAILED

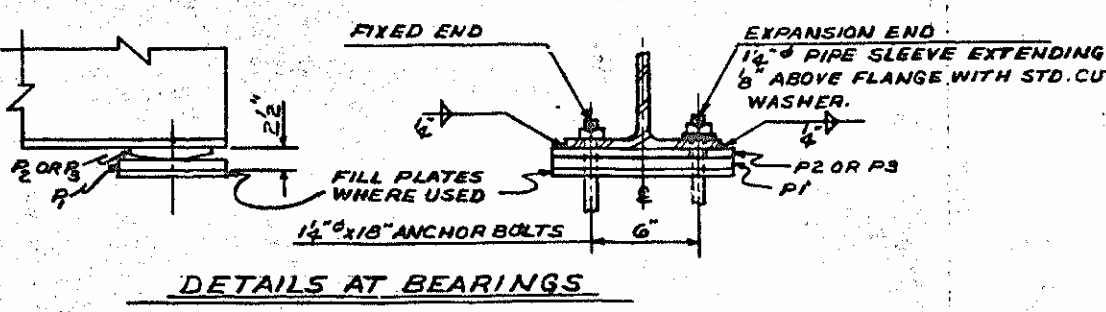
FILL PLATES REQ'D.

4	8" x 3/8" x 1'-0"
2	8" x 3/8" x 1'-0"
1	8" x 1/2" x 1'-0"

APPROX. STRUCTURAL STEEL WEIGHT = 189,900 LBS.

DEAD LOAD DEFLECTION TABLE

SPAN	BEAM	BEAM DEF'L.	SUPERIM. D.L. DEF'L.	TOTAL D.L. DEF'L.	V.C. ORDINATE ELEVATION	SUPER. ELEVATION	CAMBER
"A"	EXT.	1/8"	1/8"	1/4"	2"	-1/8"	1 1/8"
	INT.	3/16"	1/8"	1/2"	2"	-3/16"	1 1/2"
"B"	EXT.	3/16"	2"	2 1/16"	2"	-3/16"	3 1/2"
	INT.	3/16"	1 1/8"	2 1/16"	2"	-3/16"	2 3/4"
"C"	EXT. B1	1/8"	1 3/8"	1 5/8"	1 1/2"	-3/16"	2 3/4"
	INT. B2	3/16"	1 1/8"	1 5/8"	2"	-3/16"	2 3/4"
	EXT. B3	3/16"	1 1/8"	1 5/8"	2"	-3/16"	2 3/4"
"D"	EXT.	3/16"	2 3/8"	3 1/8"	1 1/2"	-1/4"	4"
	INT.	3/16"	2 3/8"	3 1/8"	1 1/2"	-1/4"	3 3/4"
"E"	EXT.	1/8"	1 1/8"	1 3/8"	7"	-1/8"	1 7/8"
	INT.	1/8"	1 1/8"	1 3/8"	7"	-1/8"	1 7/8"



PROJECT No. 8.11618
NASH COUNTY
STATION: 377+91.0 N.E.L.

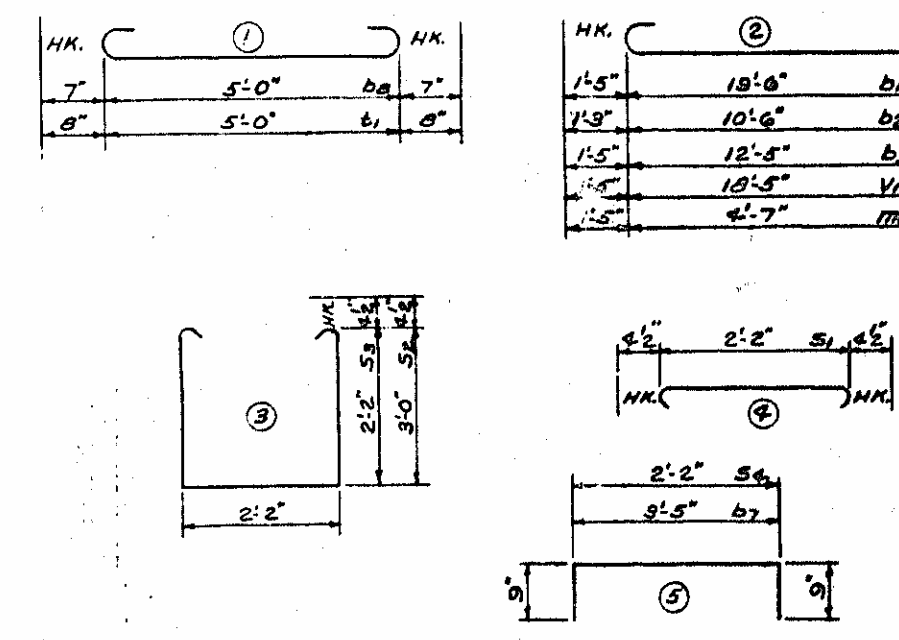
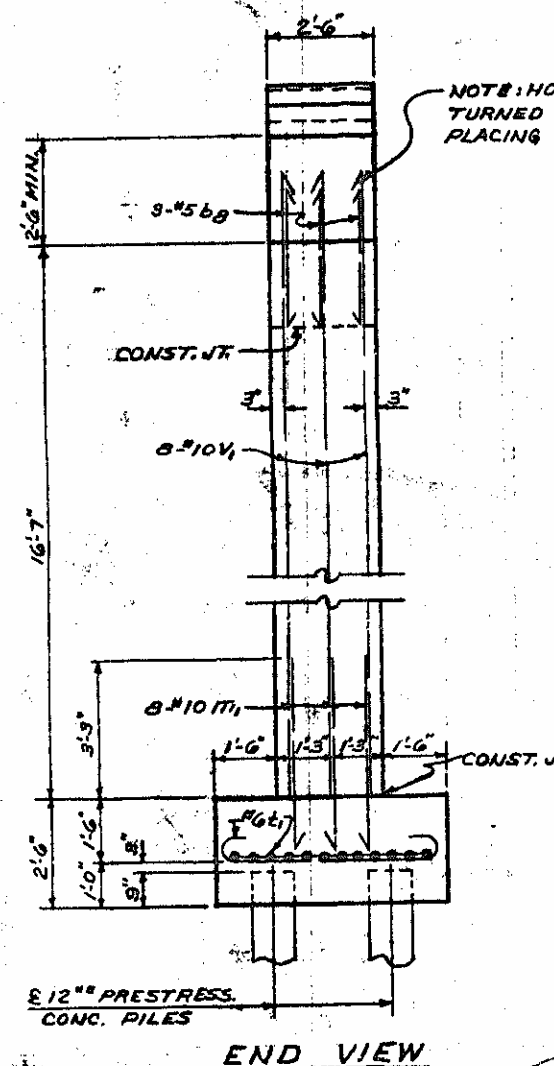
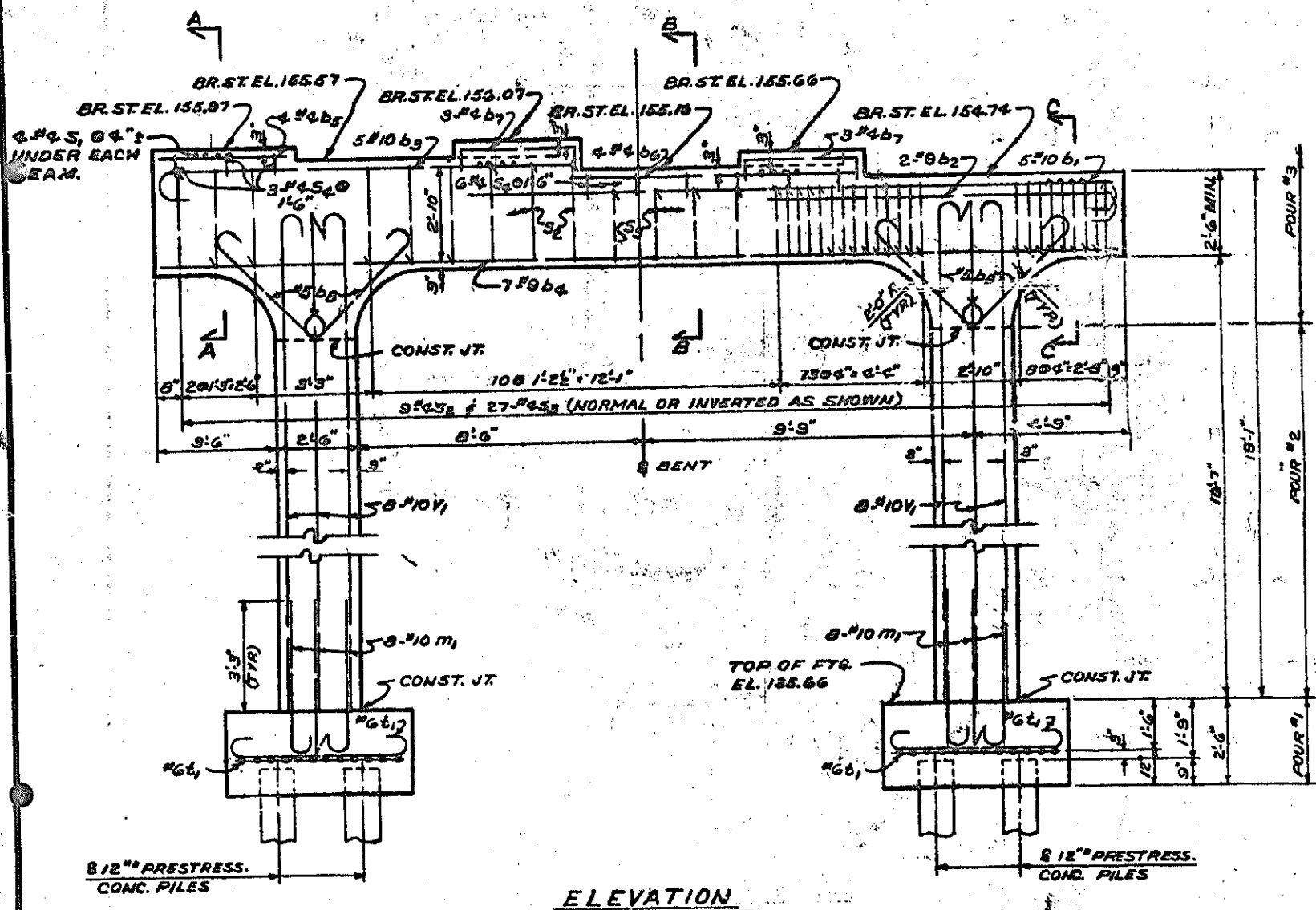
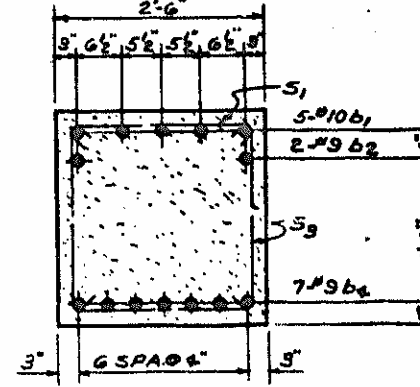
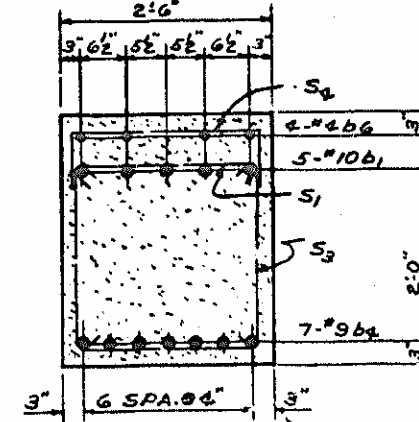
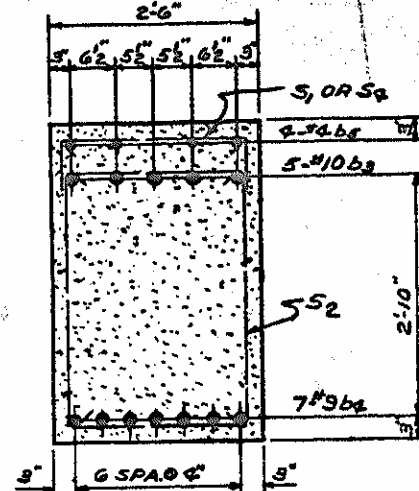
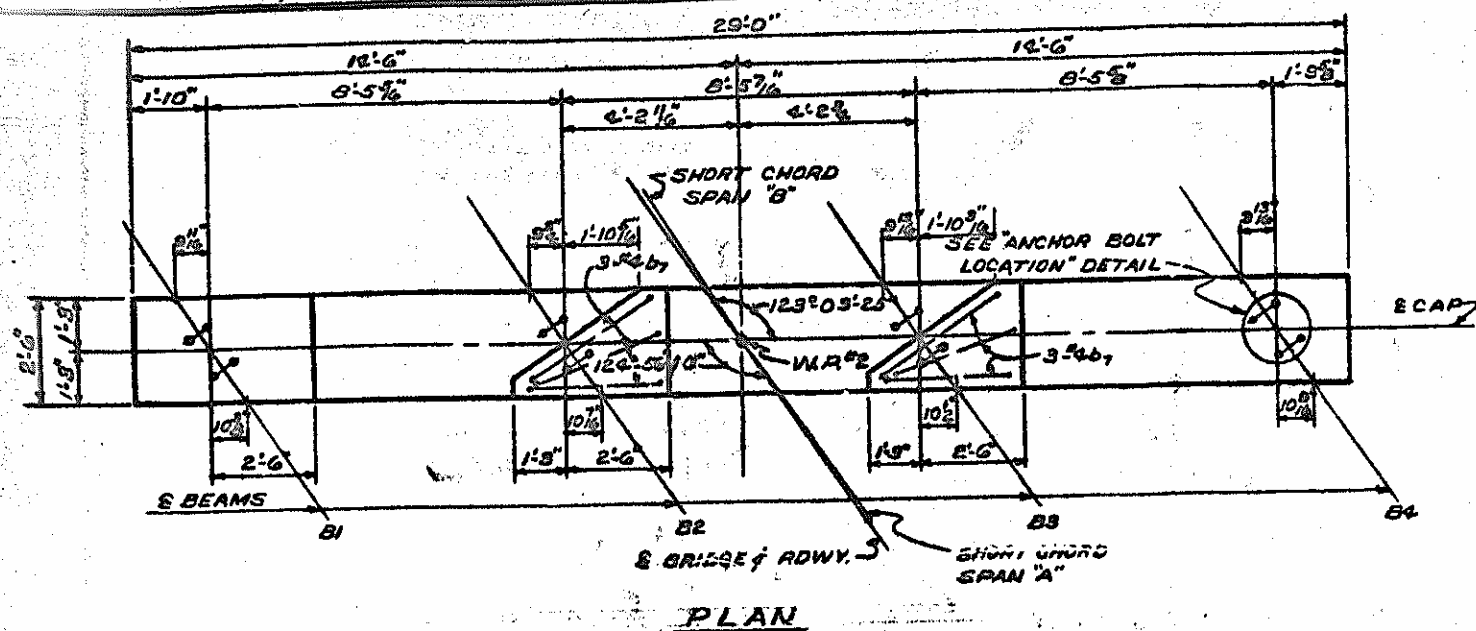
STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION

STRUCTURAL STEEL

SEPT. 1962

REVISIONS				
NO.	BY	DATE	DESCRIPTION	DATE
1				
2				

DESIGN CHECK



ALL BAR DIMENSIONS ARE OUT TO OUT.

BILL OF MATERIALS FOR BENT #1

BAR NO.	SIZE	TYPE	LENGTH	QTY
b1	5	#10	20'-0"	2
b2	2	#10	15'-0"	2
b3	3	#10	15'-0"	2
b4	7	#10	20'-0"	2
b5	4	#10	15'-0"	2
b6	6	#10	15'-0"	2
b7	6	#10	15'-0"	2
b8	12	#10	15'-0"	2
v1	16	#10	15'-0"	2
m1	16	#10	15'-0"	2
b1	16	#10	15'-0"	2
s1	16	#10	15'-0"	2
s2	8	#10	15'-0"	2
s3	27	#10	15'-0"	2
s4	8	#10	15'-0"	2

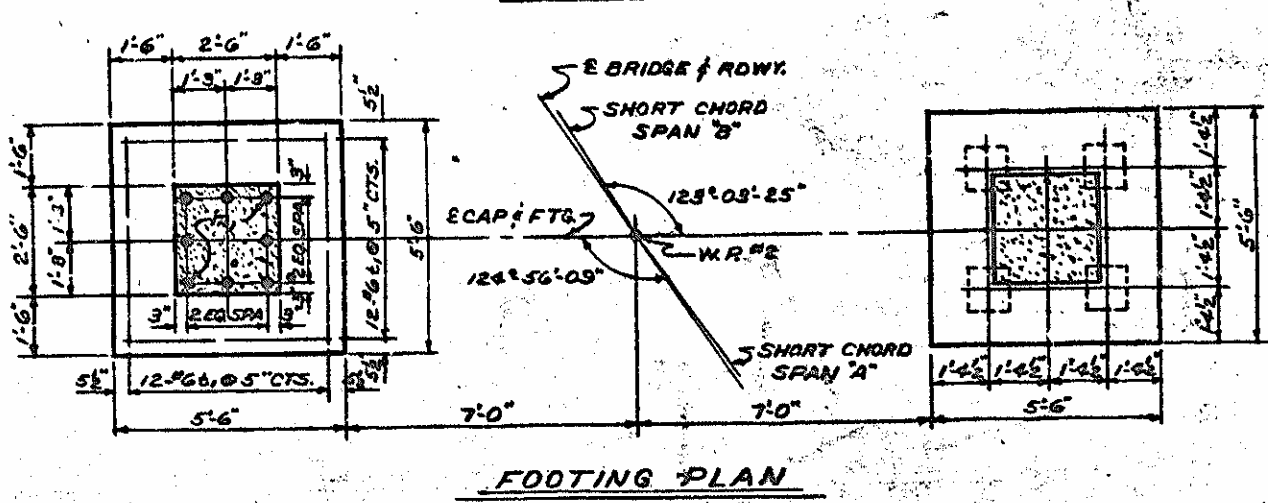
REINFORCING STEEL LBS. @ 110	CLASS 'A' CONCRETE CU YDS.
FOUR #1	5.20
FOUR #2	6.0
FOUR #3	5.6
TOTAL	21.8

NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN DEDUCTED.
PROJECT NO. 8.11618
NASH COUNTY
STATION: 377+91.10' R.T.L.

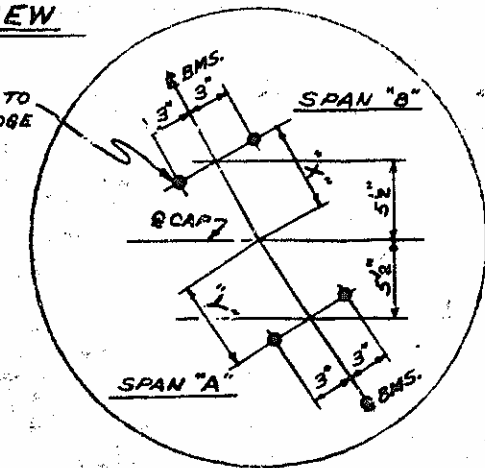
STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION

BENT #1

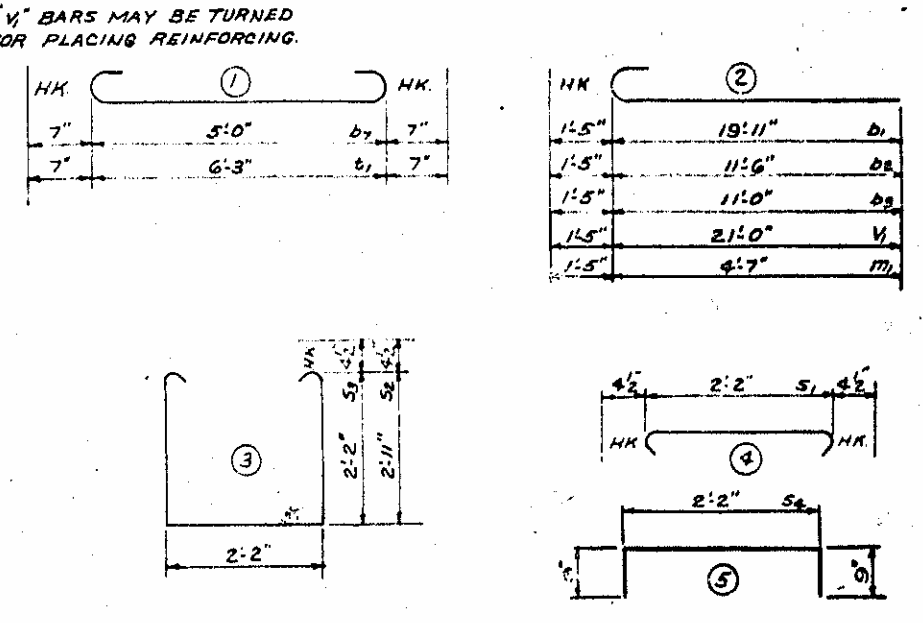
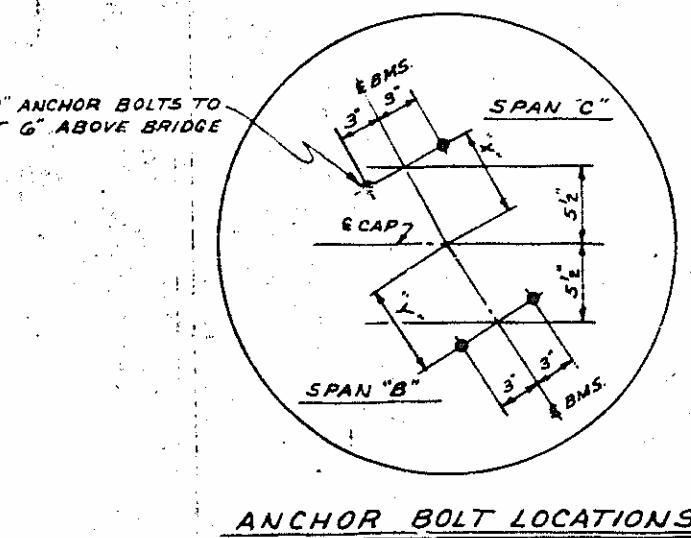
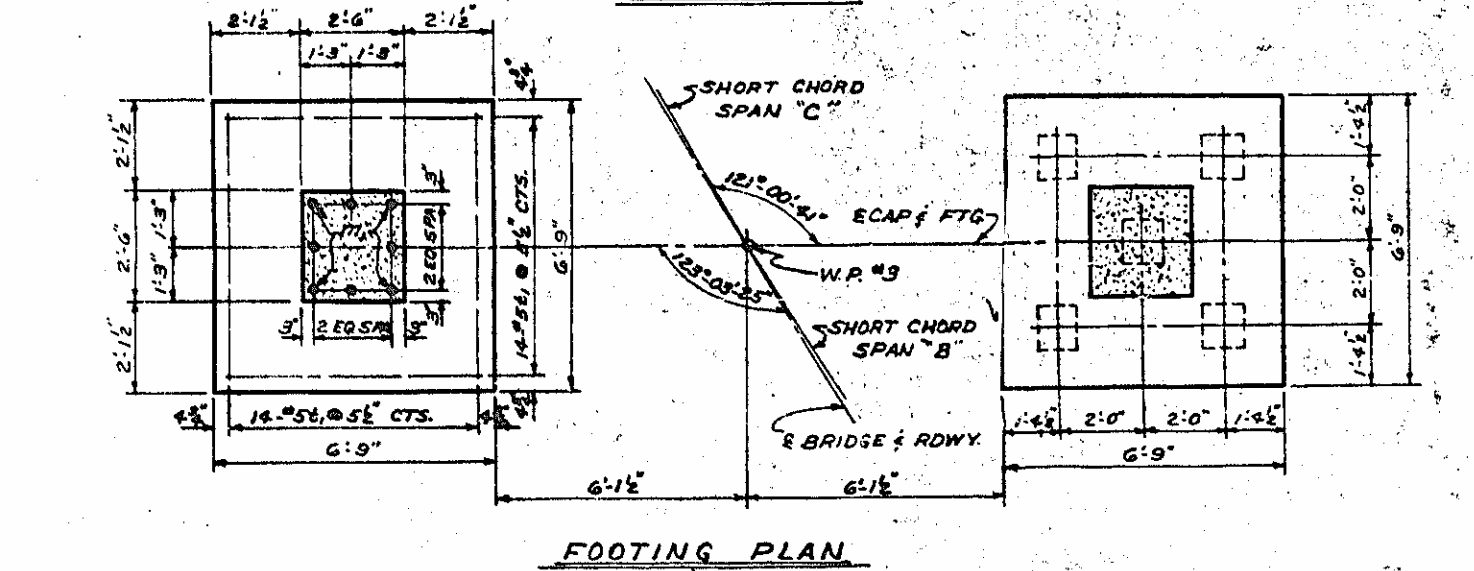
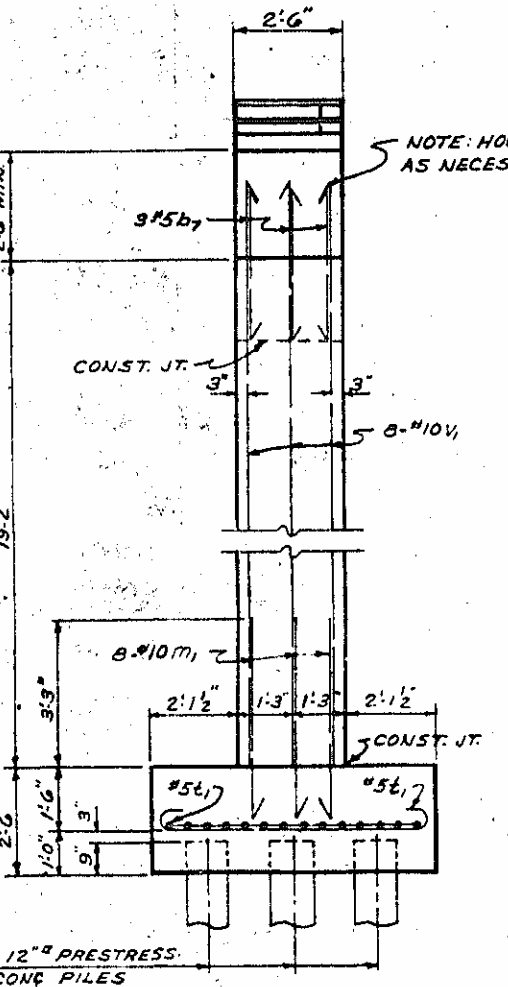
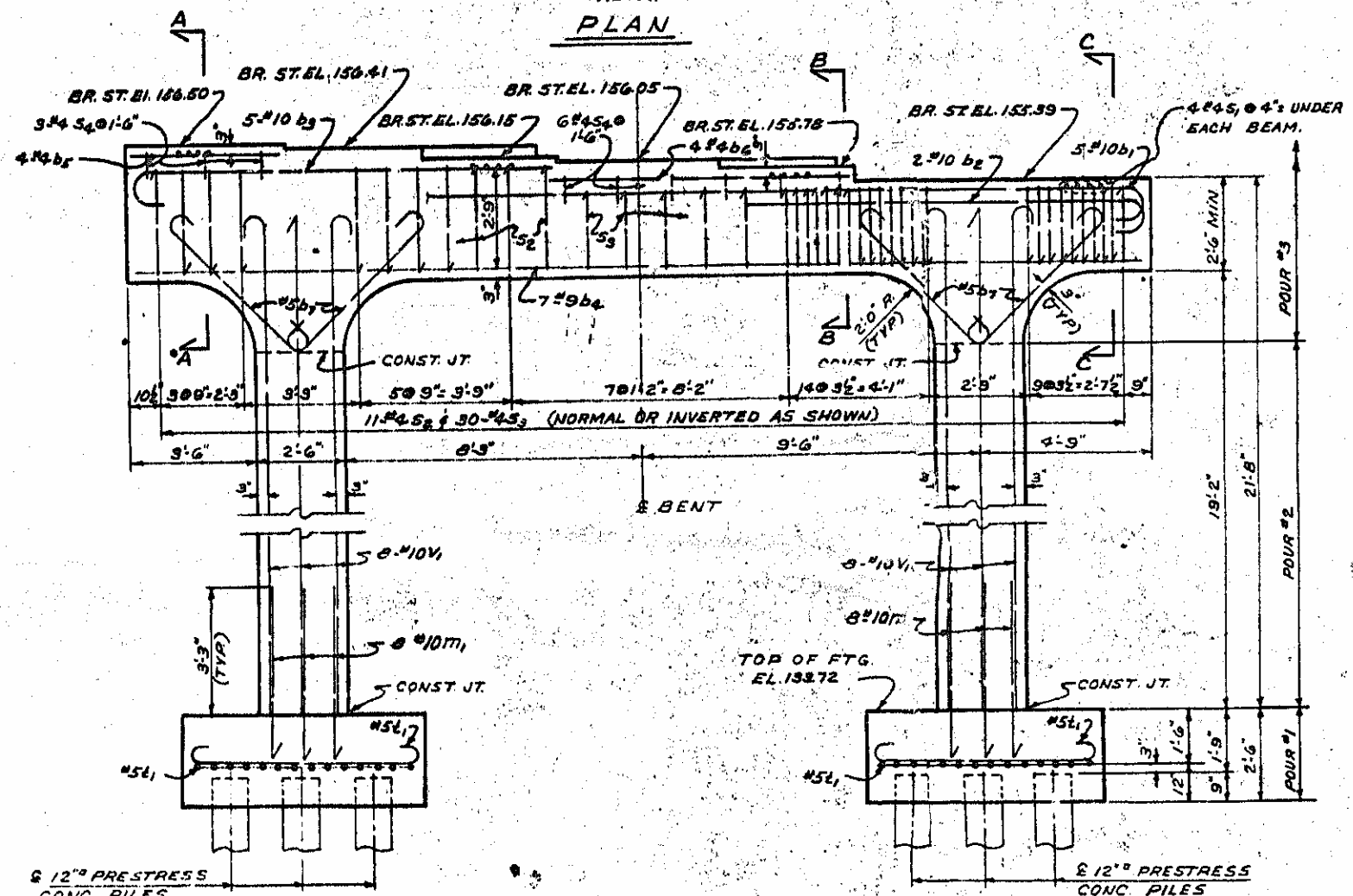
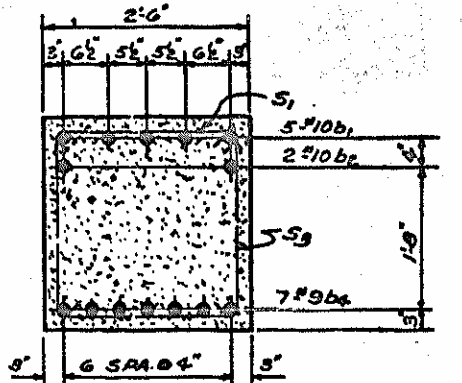
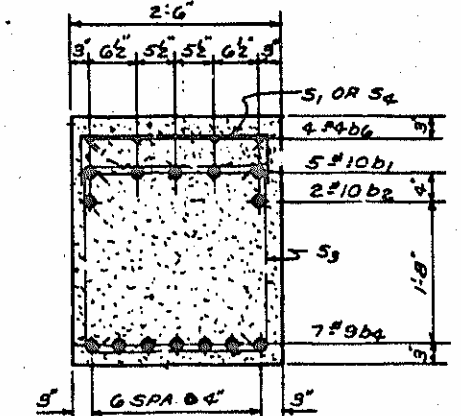
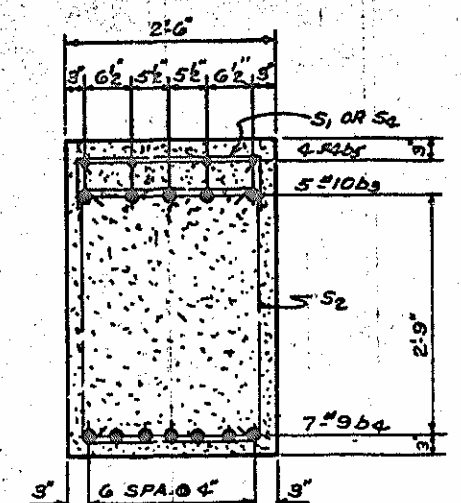
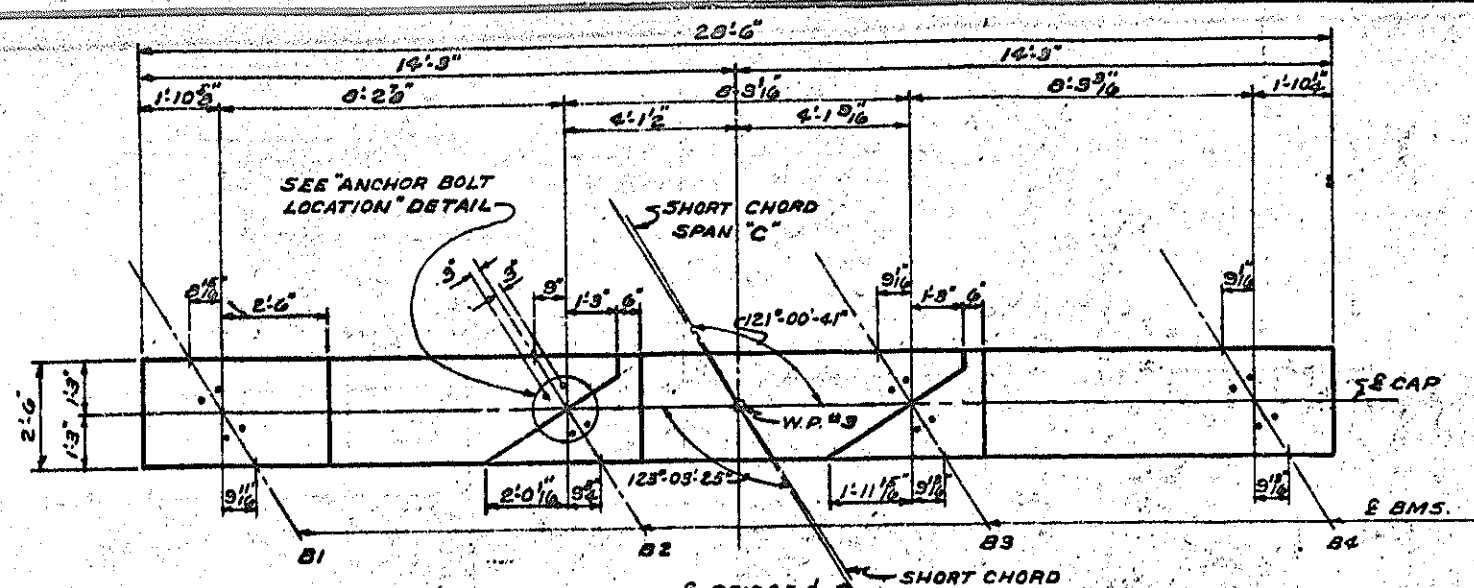
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		



1/4" x 1/8" ANCHOR BOLTS TO PROJECT 6" ABOVE BRIDGE SEATS.



BEAM	X	Y
1	6'-0"	6'-11"
2	6'-0"	6'-11"
3	6'-0"	6'-11"
4	6'-0"	6'-11"



BAR TYPES
ALL BAR DIMENSIONS ARE OUT TO OUT.

BILL OF MATERIAL			
FOR BENT NO. 2			
BAR NO.	NO.	SIZE	LENGTH
B1	5	#10	2 21'-0"
B2	2	#10	2 12'-11"
B3	5	#10	2 12'-0"
B4	7	#8 STR.	25'-2"
B5	4	#4 STR.	6'-0"
B6	4	#4 STR.	0'-9"
B7	12	#5	1 6'-0"
V1	16	#10	2 22'-5"
M1	16	#10	2 6'-0"
B1	56	#5	1 7'-5"
S1	16	#4	4 2'-11"
S2	11	#4	3 0'-9"
S3	30	#4	3 7'-9"
S4	8	#4	5 0'-0"

REINFORCING STEEL LBS. 4,269	
CLASS "A" CONC.	CU. YDS.
POUR #1	0.2
POUR #2	7.9
POUR #3	0.4
TOTAL	8.5

12" PRESTRESS CONC. PILES	
NO. 10	LIN. FT.
	283.7

* NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN DEDUCTED.

PROJECT NO. B.11618
NASH COUNTY
STATION: 377-9110 N.B.L.

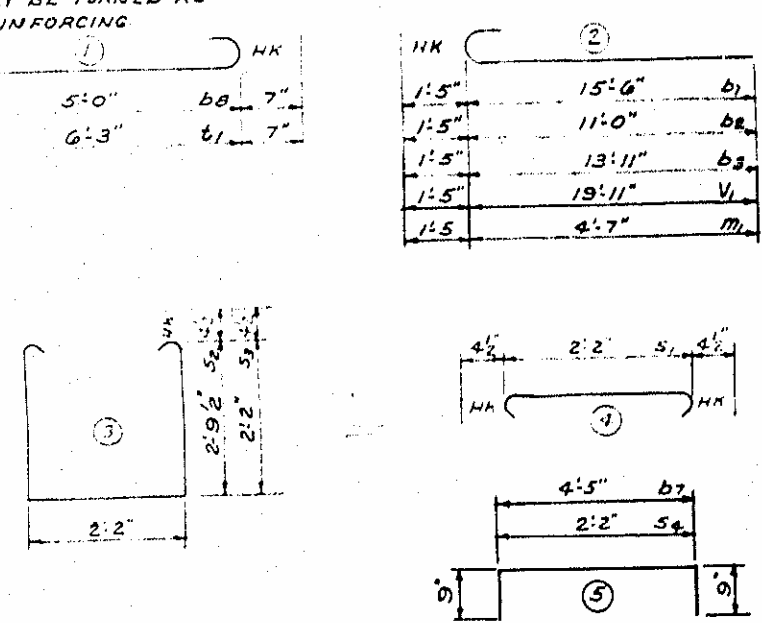
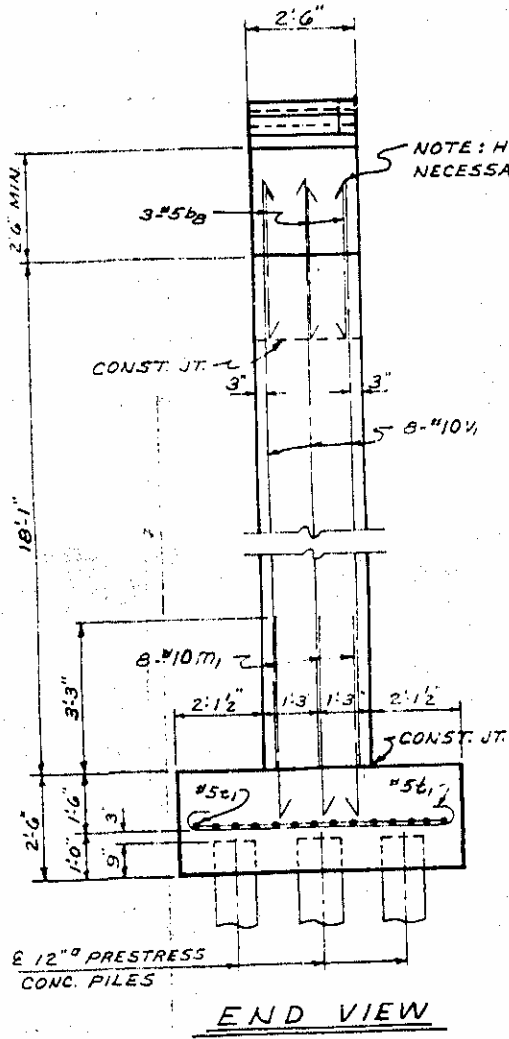
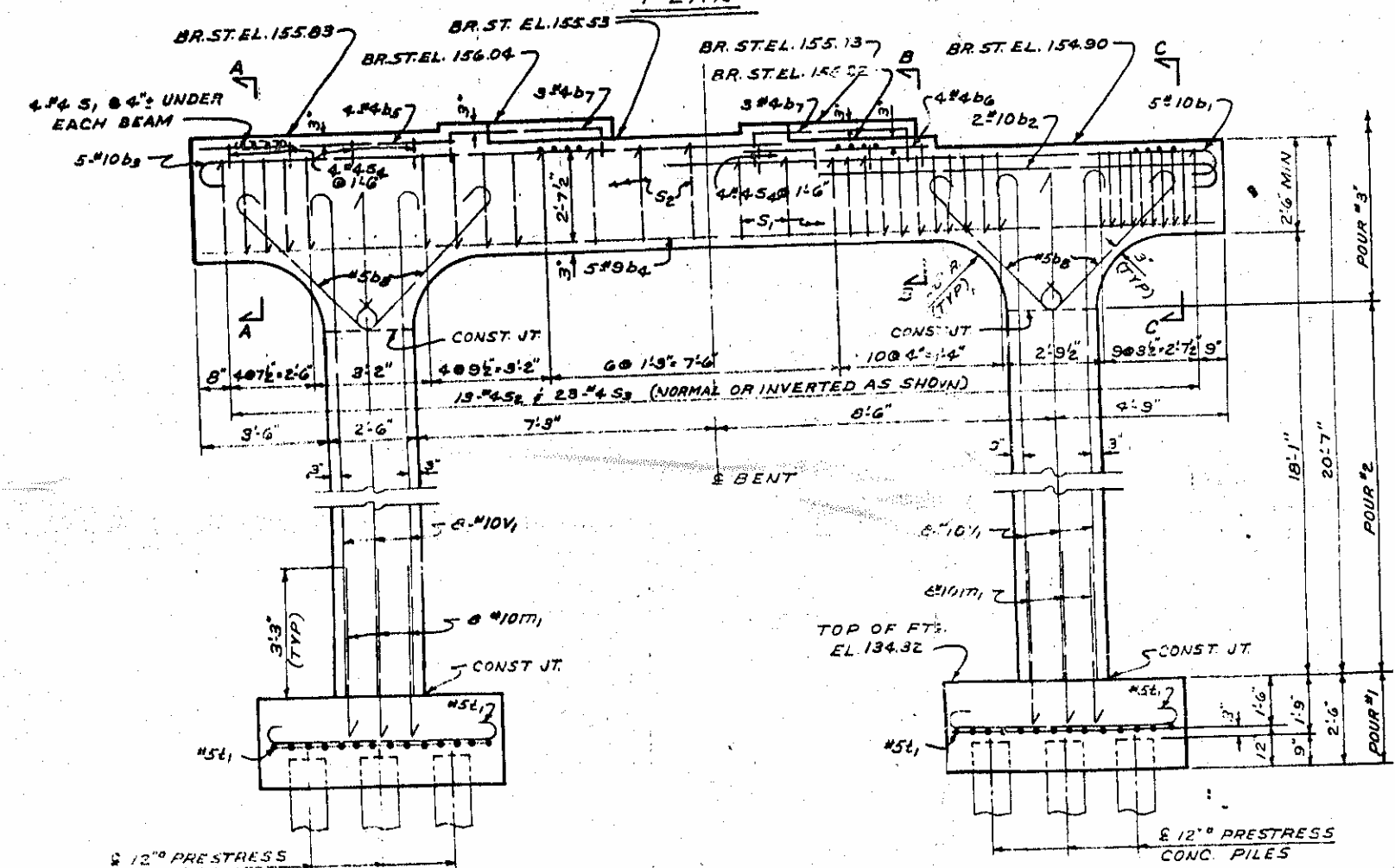
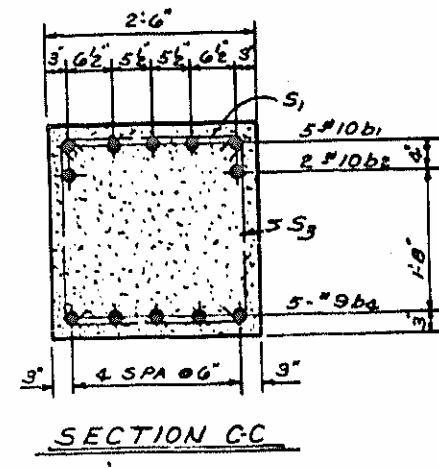
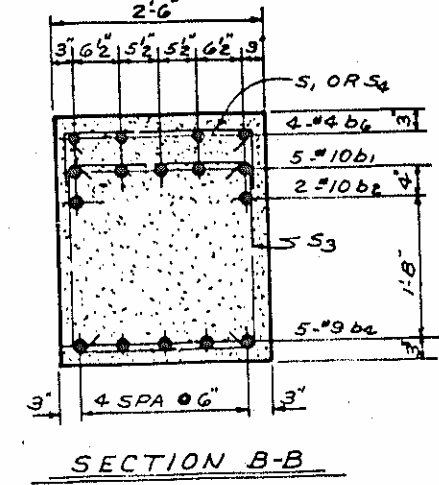
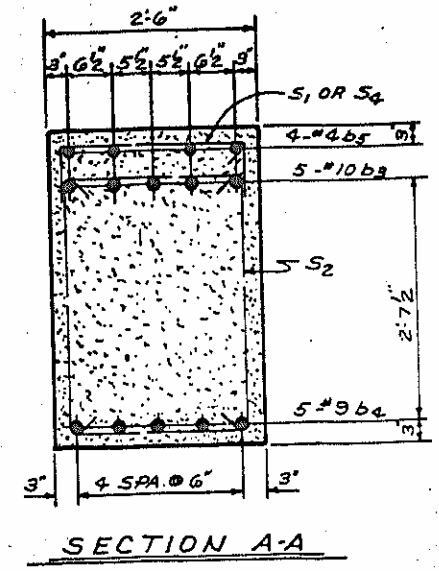
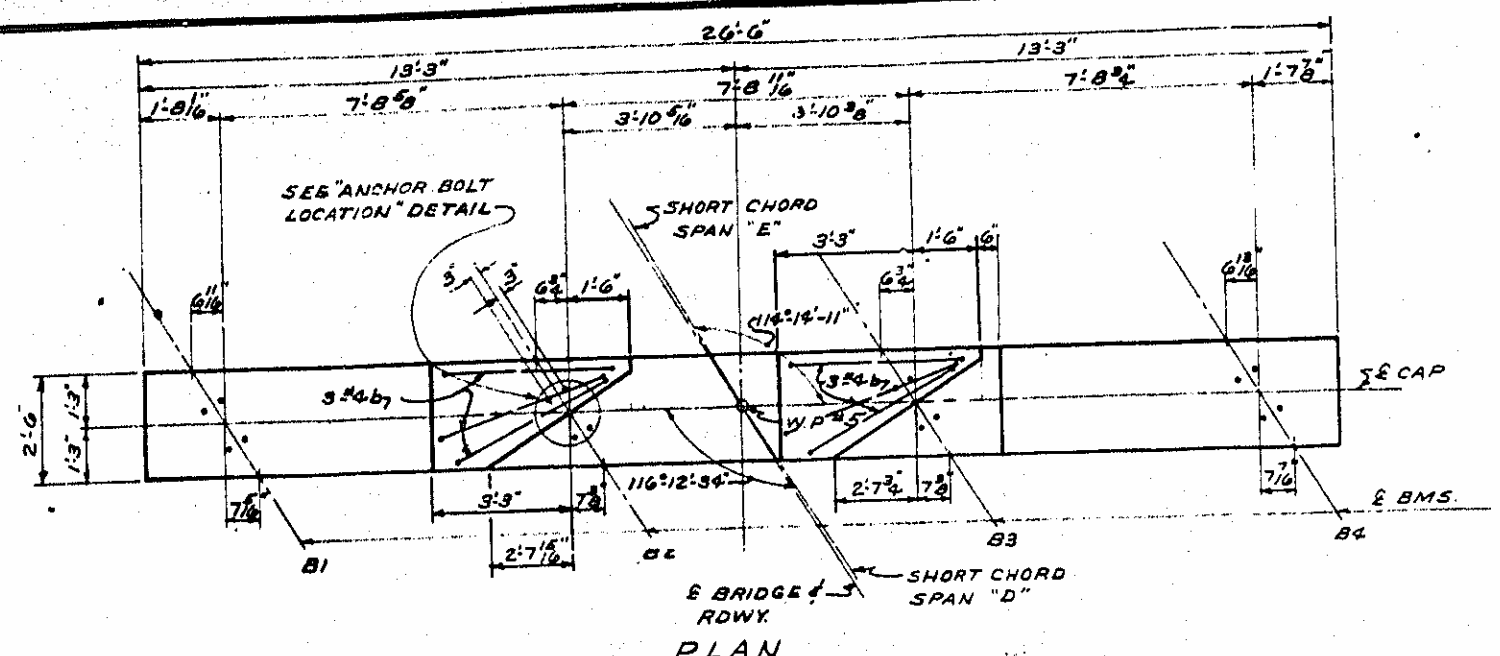
TABLE OF VARIABLE DIMENSIONS		
BEAM	"X"	"Y"
1	6 3/4	6 3/4
2	6 1/4	6 3/4
3	6 1/4	6 3/4
4	6 1/4	6 3/4

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
RALEIGH

BENT #2

NOVEMBER 1964

DRAWN BY G.T. Phillips DATE NOV. 1964
CHECKED BY GEORGE B. WYNNIE DATE NOV. 1964



BILL OF MATERIALS FOR BENT NO. 4

BAR	NO	SIZE	TYPE	LENGTH	WEIGHT
b1	5	#10	2	16'-11"	500
b2	2	#10	2	12'-5"	107
b3	5	#10	2	15'-5"	300
b4	4	#9	STR	26'-2"	225
b5	4	#4	STR	5'-9"	75
b6	4	#4	STR	5'-11"	76
b7	6	#4	5	5'-11"	200
b8	12	#5	1	6'-2"	77
v1	16	#10	2	21'-6"	1960
m	16	#10	2	6'-0"	413
e1	56	#8	1	7'-6"	423
s1	16	#4	4	2'-11"	81
s2	13	#4	3	8'-6"	76
s3	23	#4	3	7'-3"	111
s4	8	#4	5	3'-8"	20

REINFORCING STEEL LBS 3827
 CLASS A CONC CU. YDS. 82.8
 POUR #1 8.2
 POUR #2 7.9
 POUR #3 8.8
 TOTAL 24.8

12" PRESTRESS CONC PILES LBS
 NO. 10 LIN. FT. 22512

* NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN DEDUCTED.

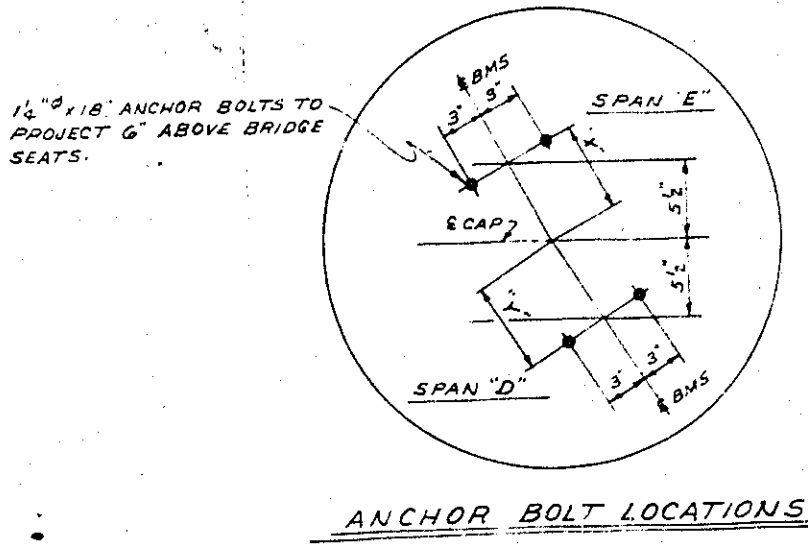
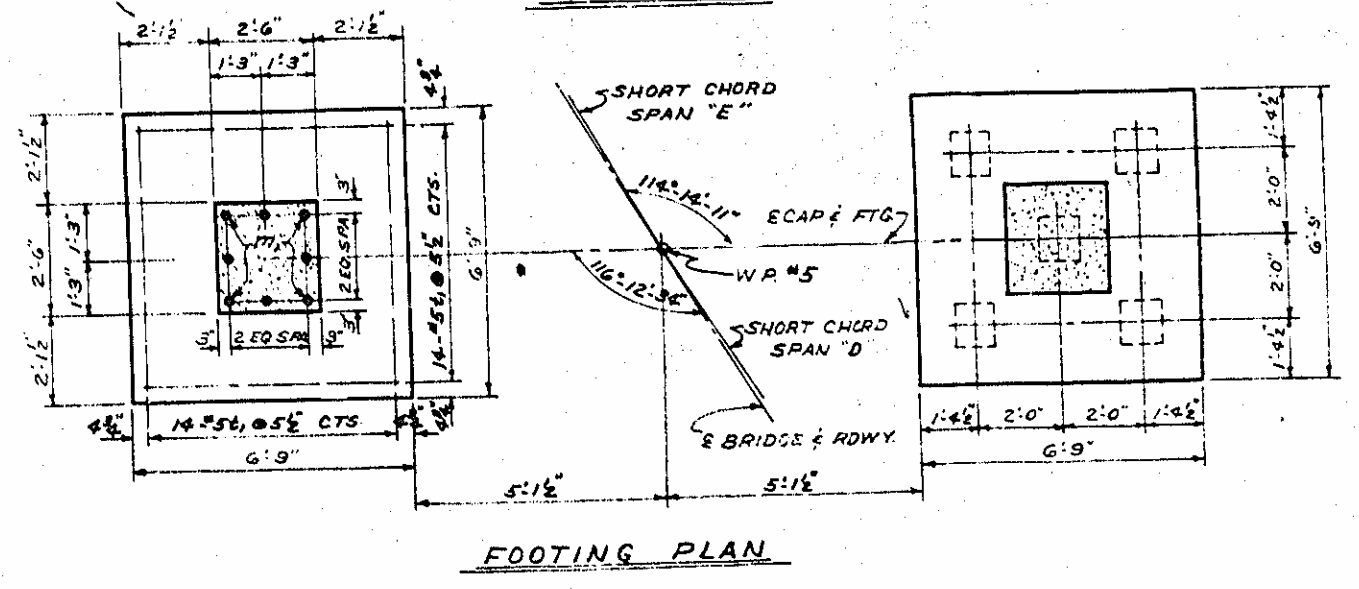


TABLE OF VARIABLE DIMENSIONS

BEAM	X	Y
1	6'	6 3/4'
2	6'	6 3/4'
3	6 1/4'	6 3/4'
4	6 1/4'	6 3/4'

PROJECT NO. 8.11618
 NASH COUNTY
 STATION 377+9.110 N.B.L.

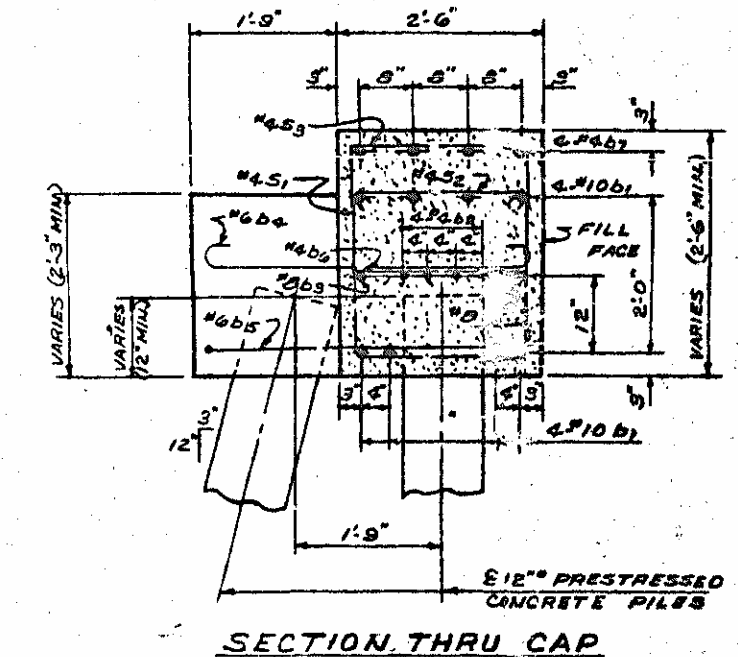
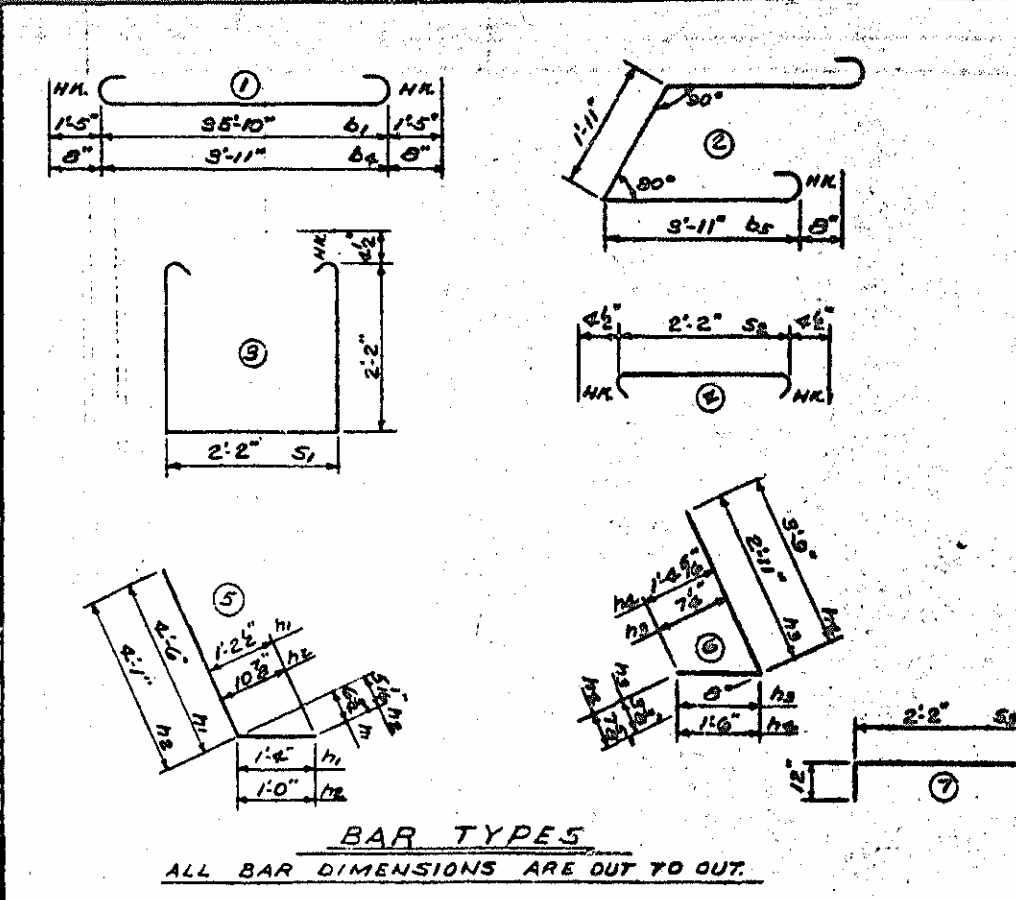
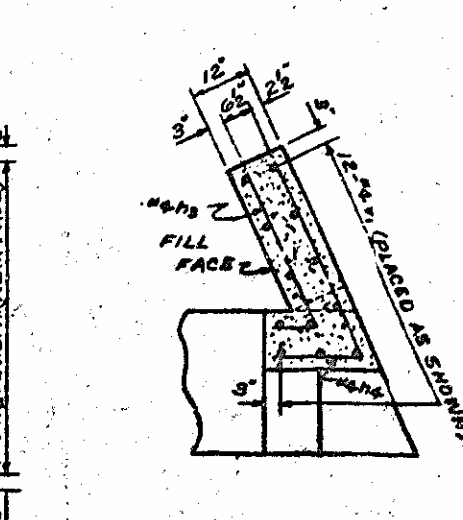
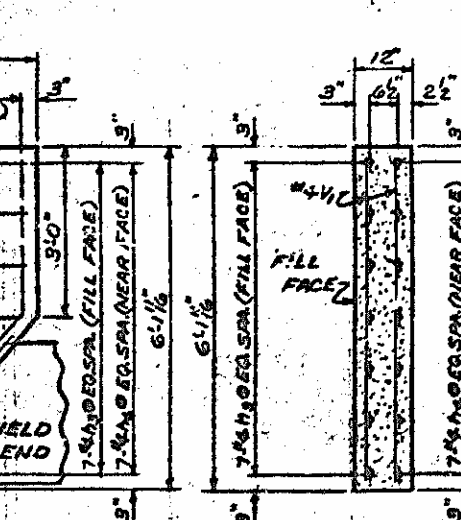
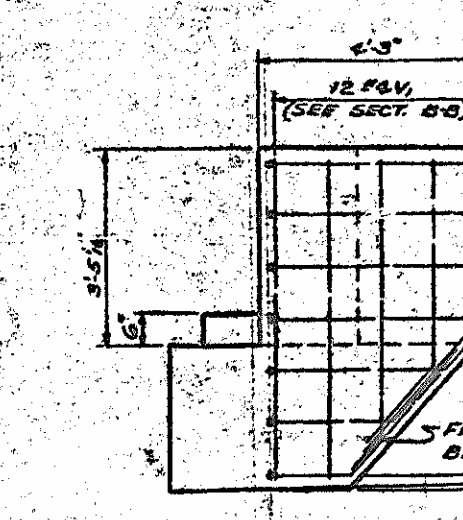
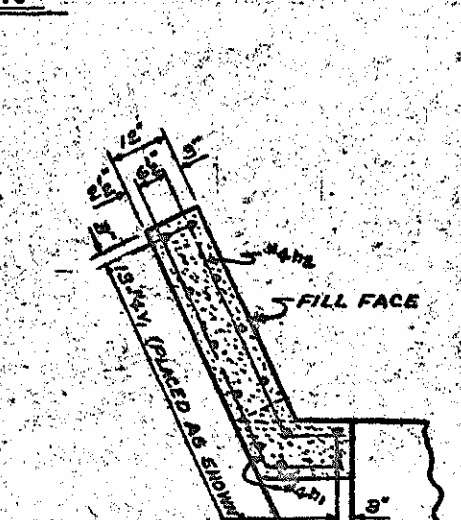
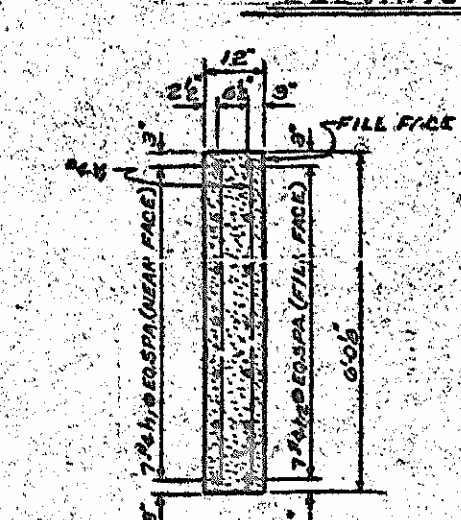
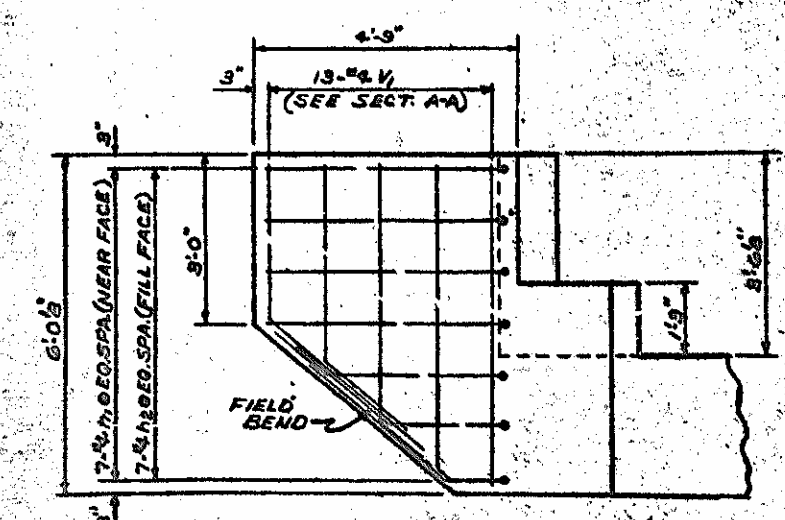
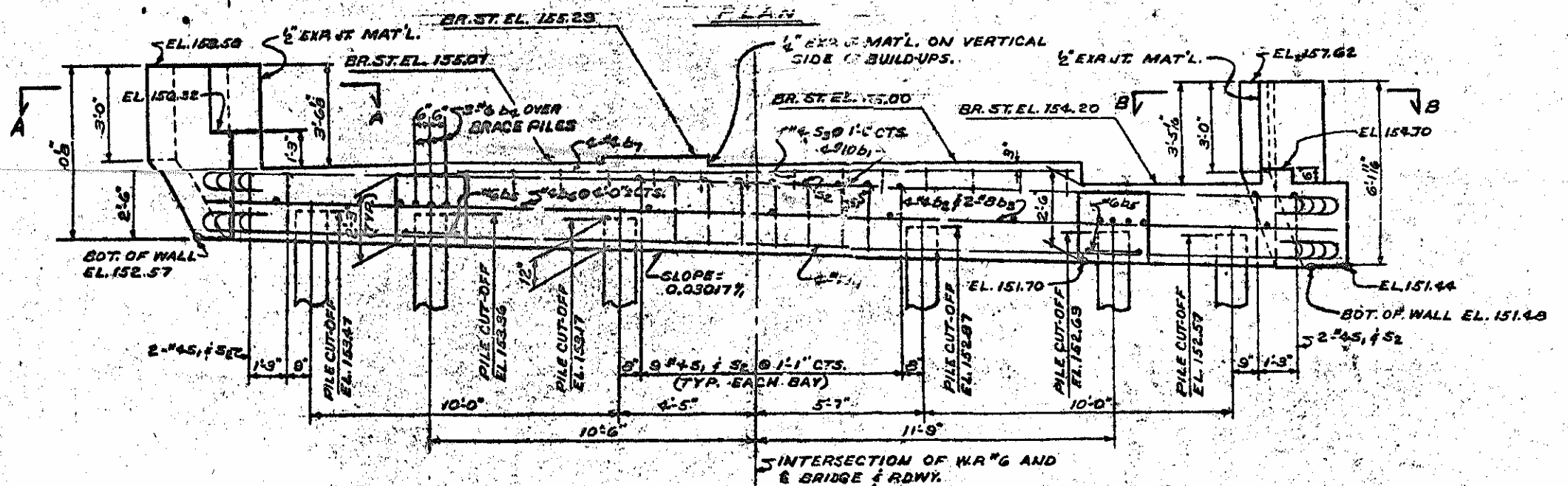
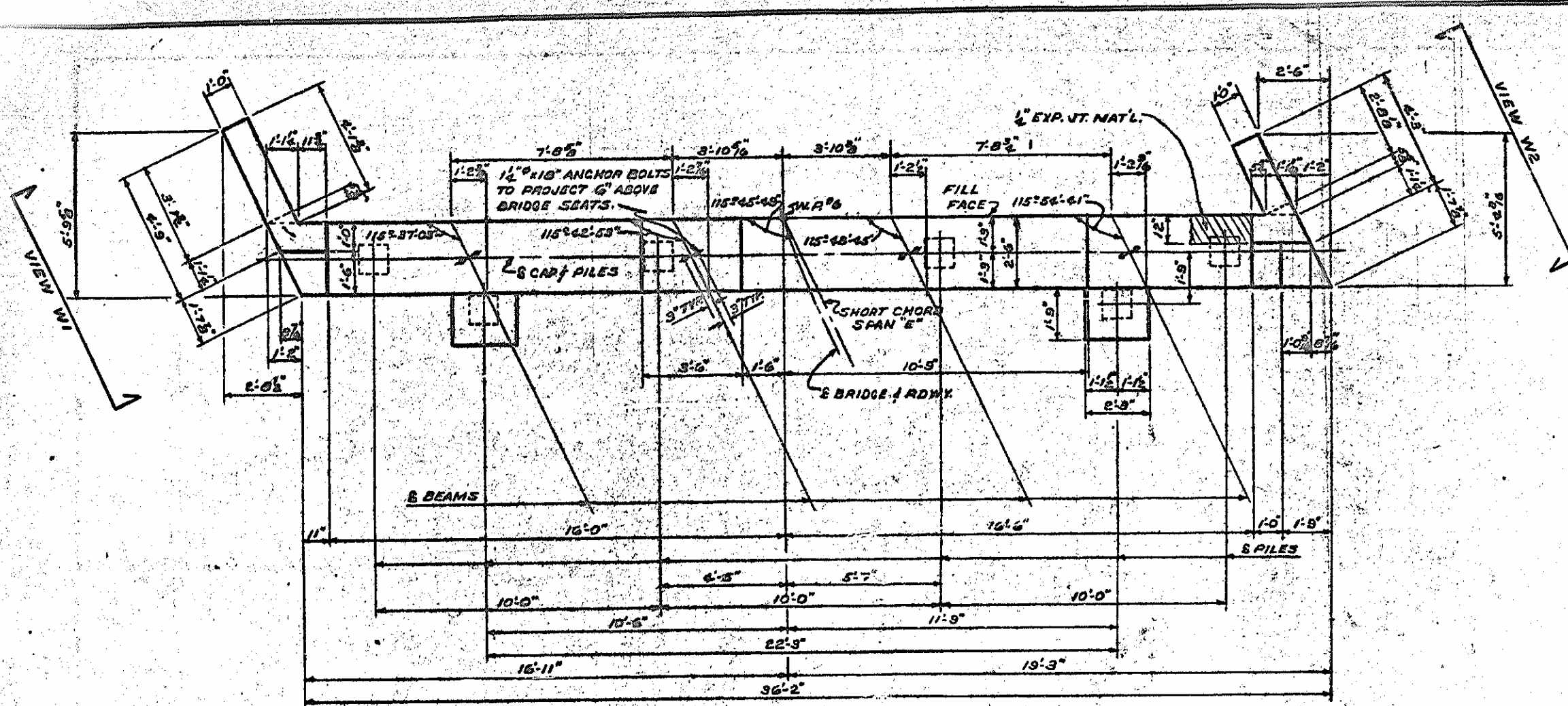
STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 RALEIGH

BENT #4

NOVEMBER 1964

DRAWN BY G. T. Phillips DATE NOV, 1964
 CHECKED BY GEORGE B. WYNNIE DATE NOV, 1964

KODAK SAFETY FILM



NO.	DATE	BY	REVISIONS
1			

BILL OF MATERIALS
FOR END BENT #2

BAR NO.	SIZE	TYPE	LENGTH	QTY
B1	8	#10	1	35.0
B2	8	#6	STR.	10.0
B3	2	#8	STR.	35.0
B4	6	#6	1	6.0
B5	2	#6	2	11.0
B6	9	#6	STR.	6.0
B7	4	#6	STR.	20.0
V1	25	#6	STR.	6.0
S1	31	#6	3	7.0
S2	31	#6	4	8.0
S3	12	#6	7	6.0
H1	7	#6	5	6.0
H2	7	#6	5	6.0
H3	7	#6	6	5.0
H4	7	#6	6	5.0

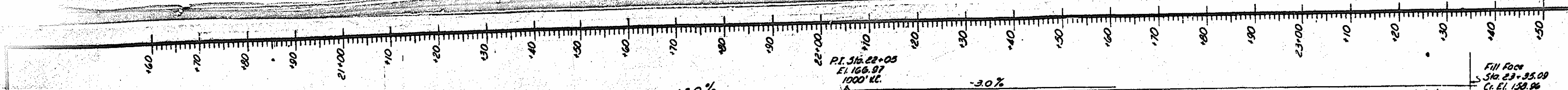
REINFORCING STEEL LBS. = 8200
CLASS A CONC. CU. YDS. = 11.7
12" PRESTRESS CONC. PILES
NO. 6 LIN FT. = 672.6

NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN DEDUCTED.
PROJECT NO. B11612
NASH COUNTY
STATION: 377+910, N.B.L.

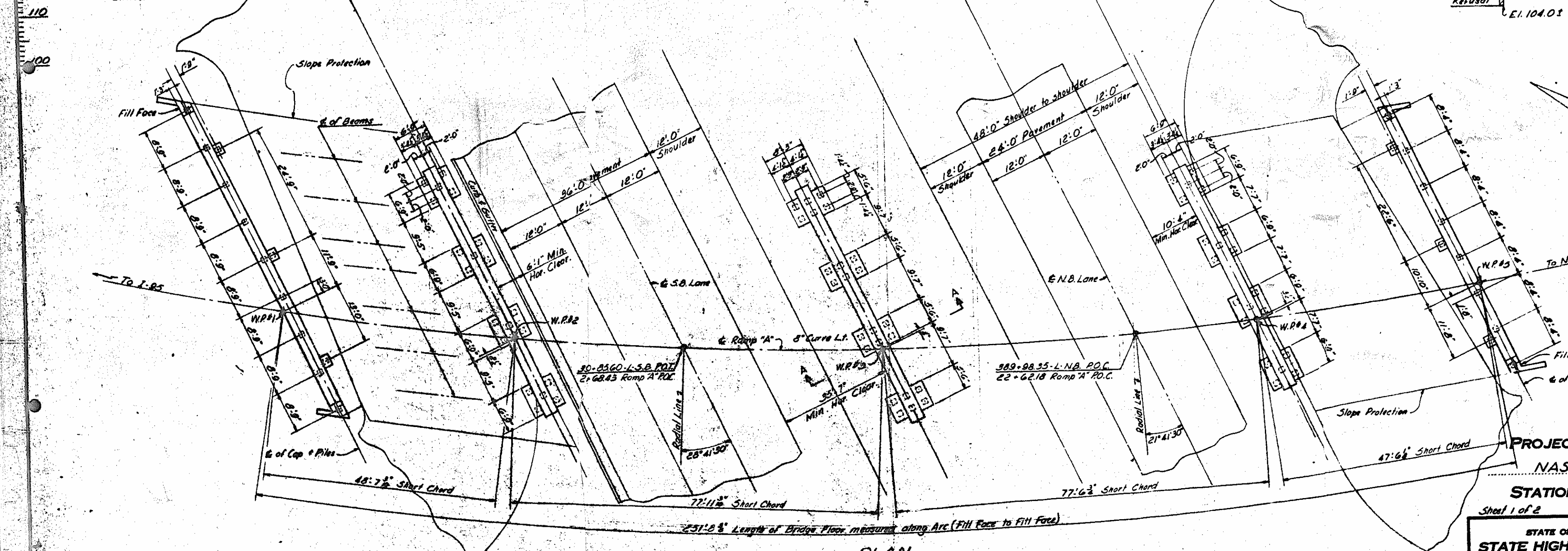
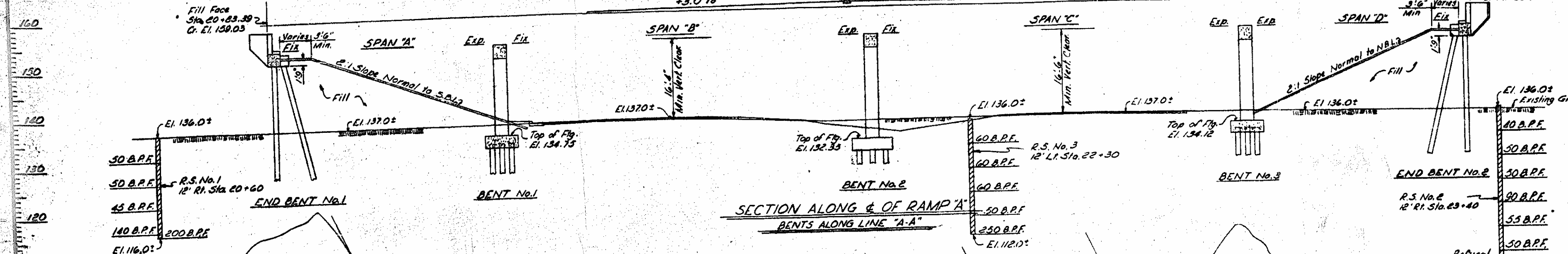
STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
END BENT #2

OCTOBER 1956	
NO.	DATE
1	
2	

PHILLIPS
BY GEORGE B. JAMES



NO. ROAD DIST. NO.	CLASS	DATE
3	H.C.	6/1/64
S.A. Proj. I-95-9(13) 149		



PLAN

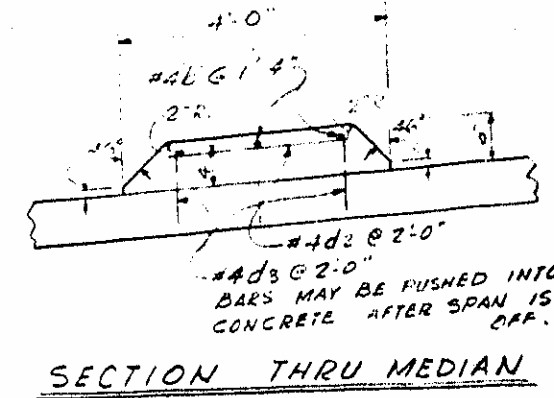
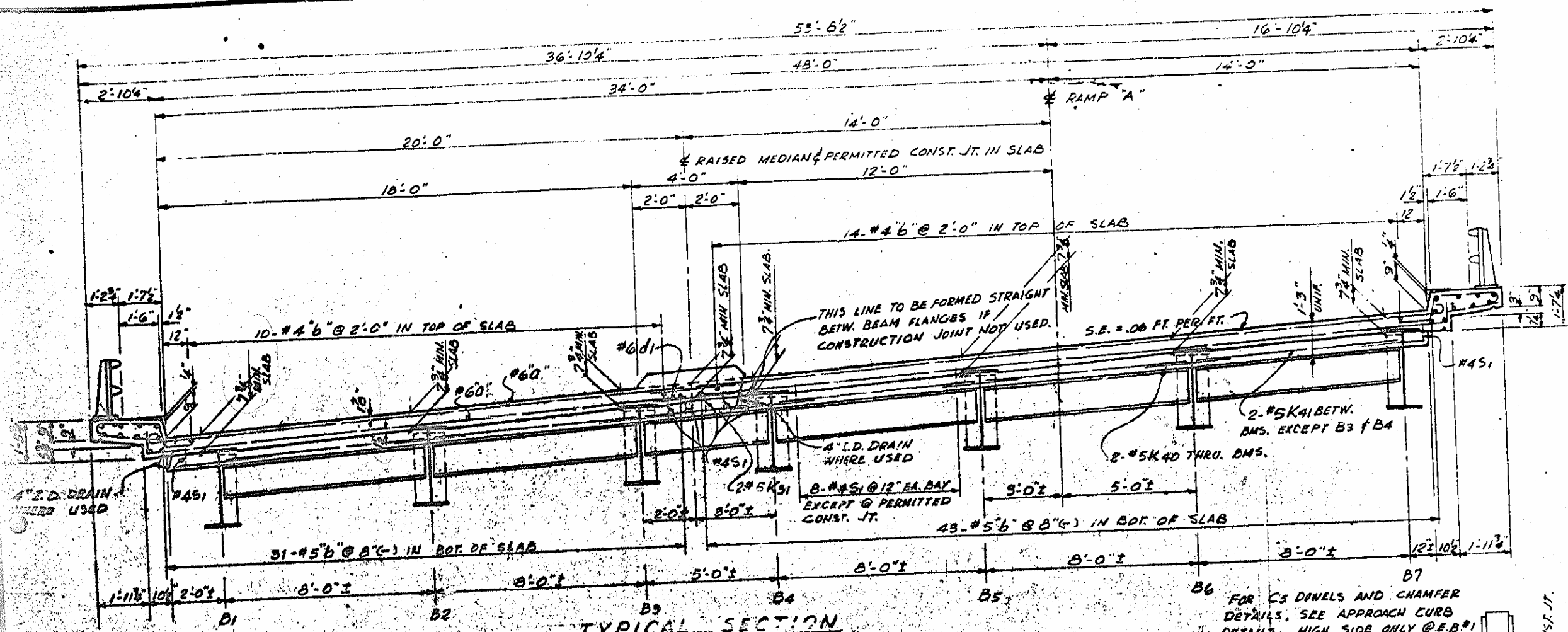
PROJECT No. 8-116.12
 NASH COUNTY
 STATION: 389+98.35 N.B.L.
 22+62.18 RAMP 'A'
 Sheet 1 of 2

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 GENERAL DRAWING
 FOR BRIDGE OVER PROJECT
 I-95 ON RAMP 'A' BETWEEN
 PROJECT AND N.C. 48
 DECEMBER 1964

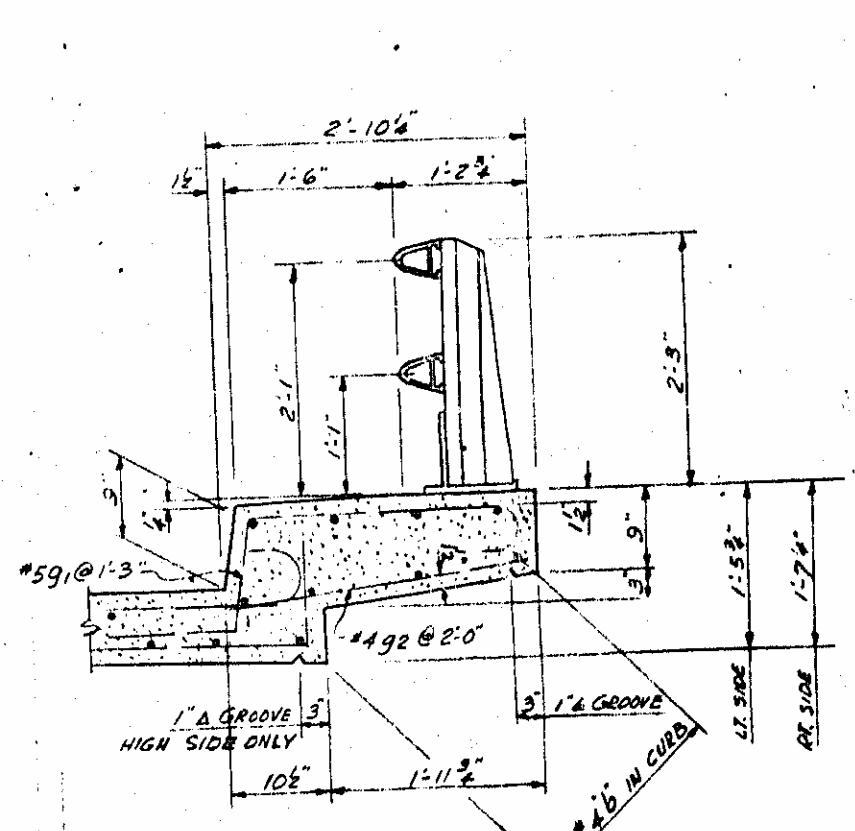
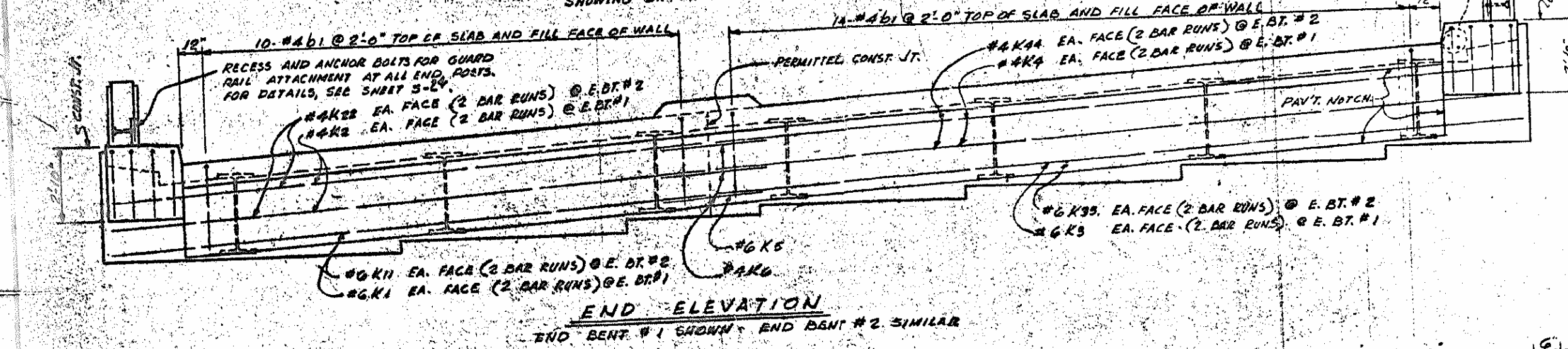
REVISIONS			
NO.	DATE	BY	REVISION
1			
2			
3			
4			

DESIGNED BY	S.L. SANTO-TOMAS	DATE	DEC. 1964
CHECKED BY		DATE	
APPROVED BY		DATE	

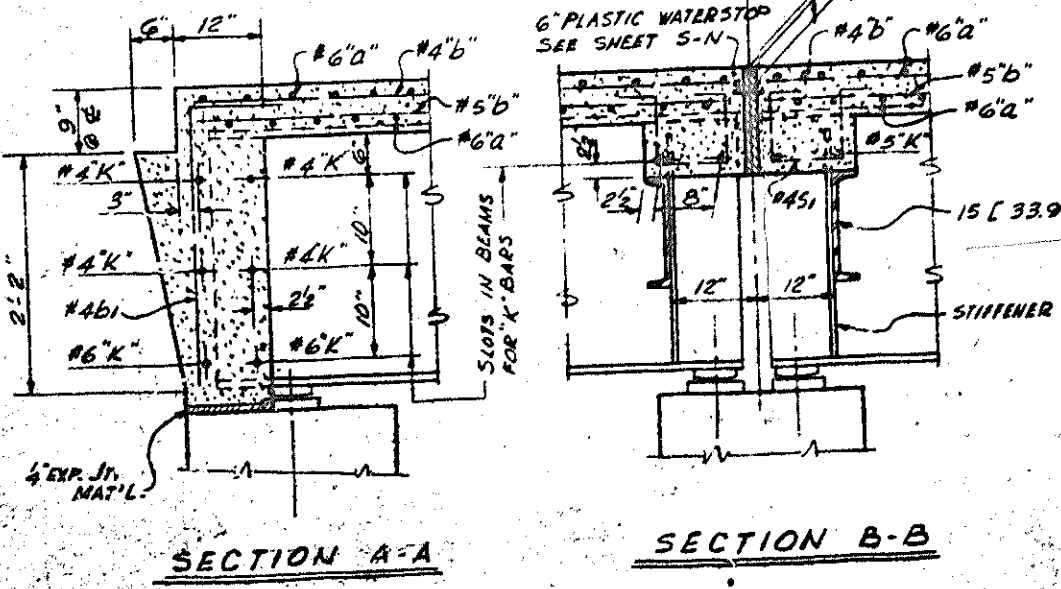
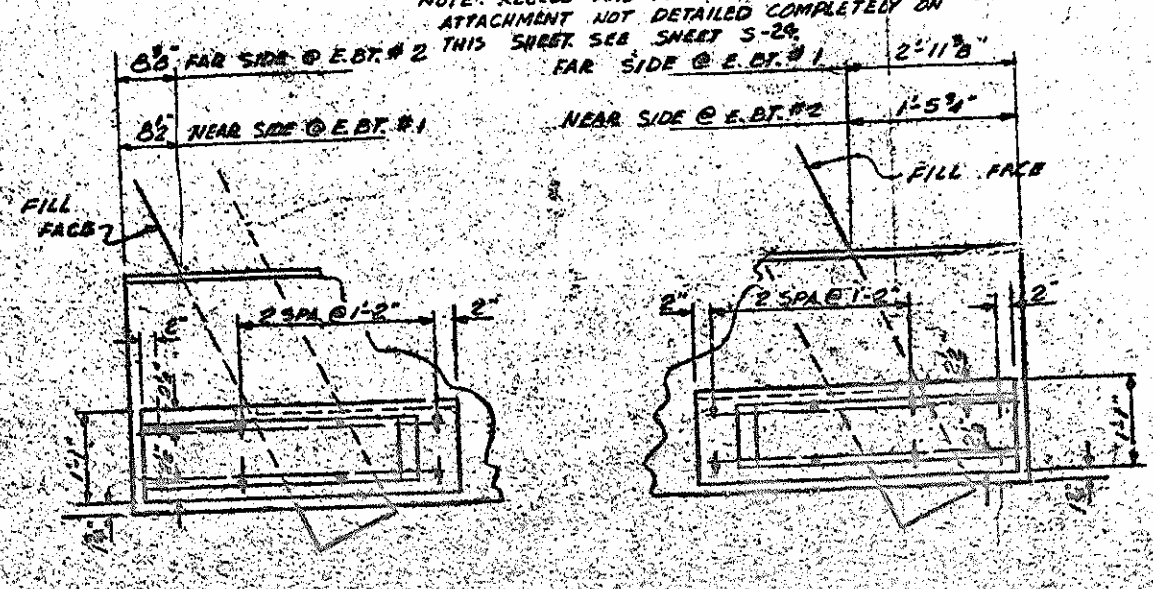
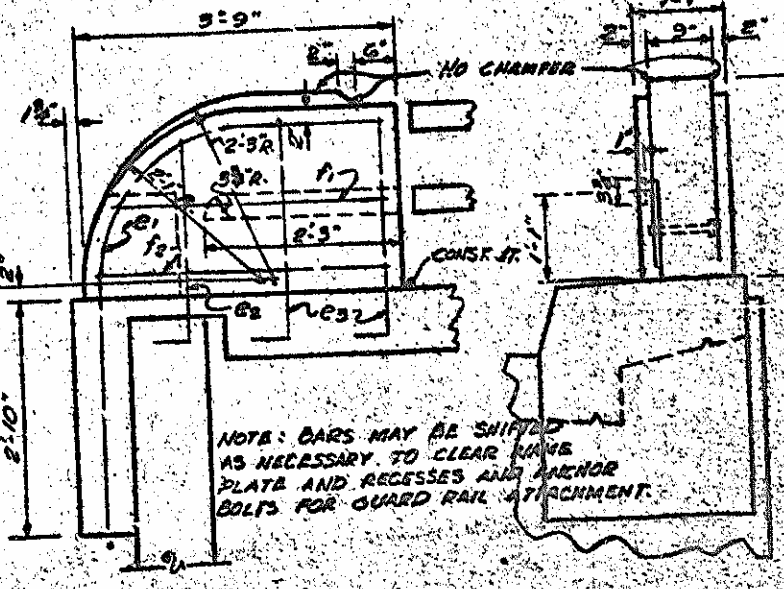
DESIGN NO.	DWG NO.
3	N.C. 8.116
P.A. DATE: 8-95-3052.102	



NOTES
FOR BARS INDICATED AND NO BAR MADE SHOWN, SEE CONCRETE PLAN FOR THE DIFFERENT SPANS.
EXPANSION JOINTS TO BE KEPT FREE OF CONCRETE AND SEALED WITH APS ASPHALT CEMENT.



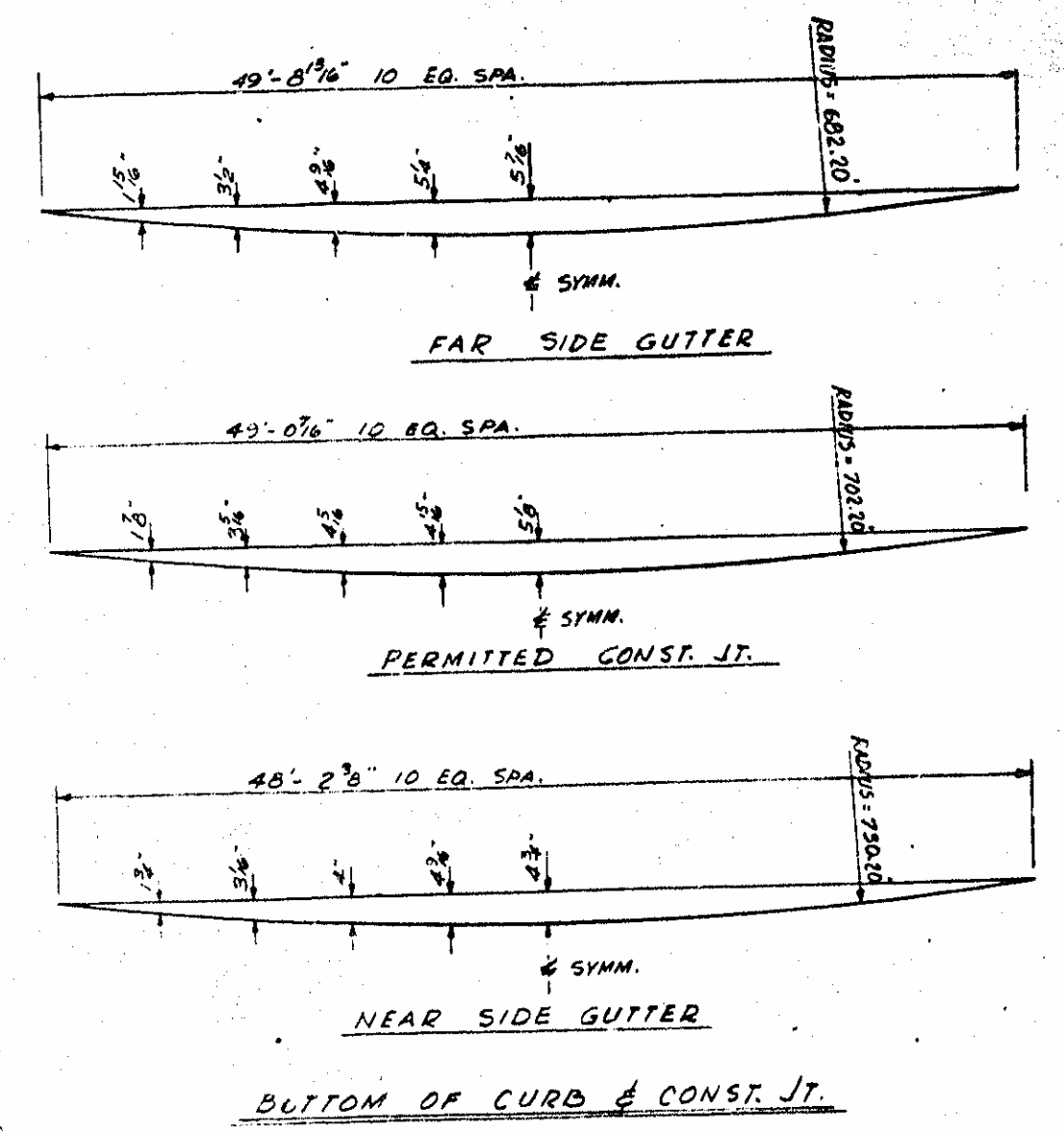
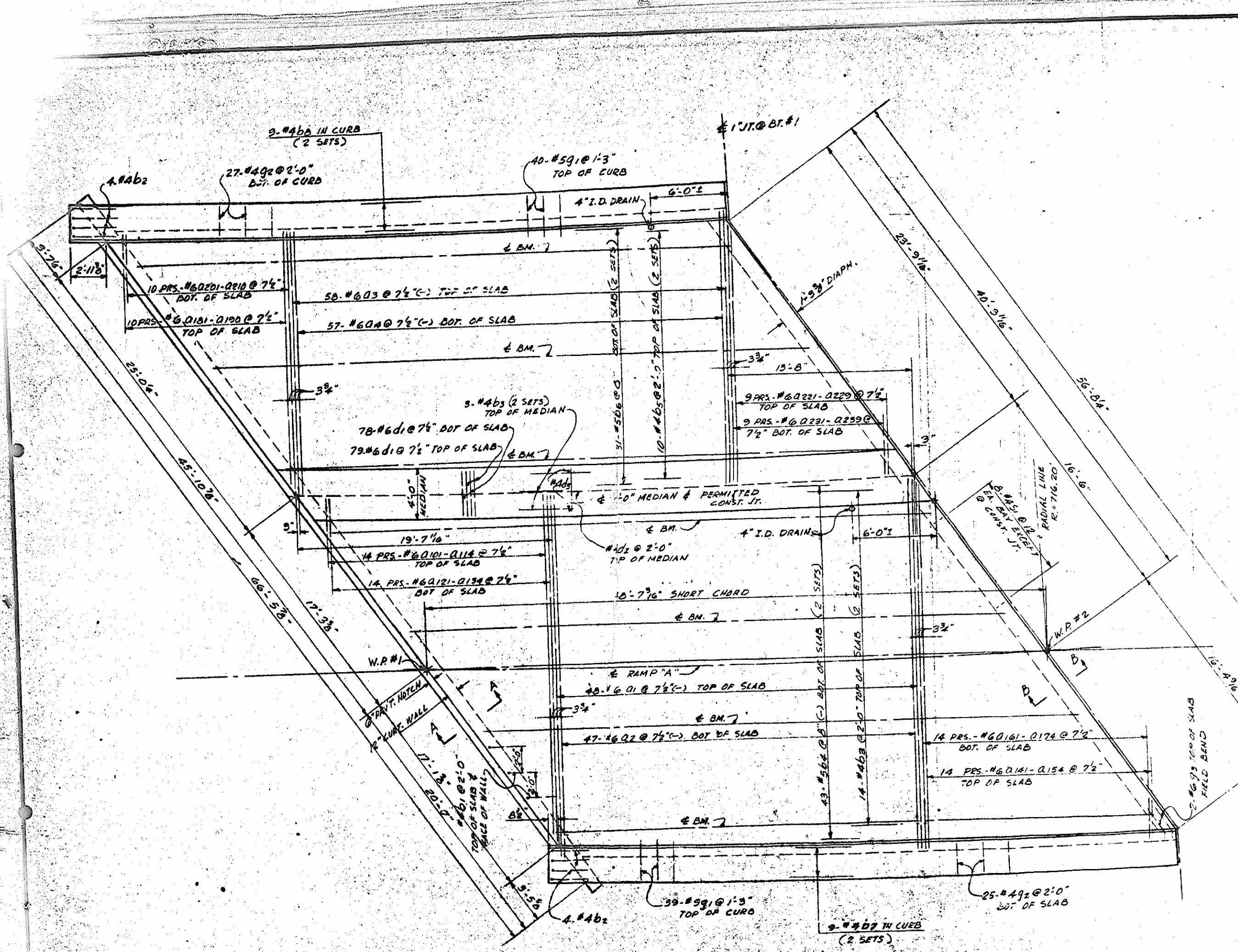
DRAIN DETAIL
DRAINS TO BE STANDARD WEIGHT GALV. STEEL PIPE. TOPS OF DRAINS TO BE SET 1/8" BELOW SURFACE OF SLAB.
(4 REQUIRED)



PROJECT No. 8.11619
NASH COUNTY
STATION: 389 + 98.35 (M.L.)

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SECTIONS

NOVEMBER 1958			
REV.	BY	CHKD.	DATE
1			
2			



NOTE: SHORT CHORD IS MEASURED FROM FILL FACE E. BENT #1 TO E. BENT #1.

NOTE: "a" BARS SHALL BE PLACED PERPENDICULAR TO THE PERMITTED CONST. JT. SHORT CHORD AND SPACED ALONG THE SHORT CHORD.

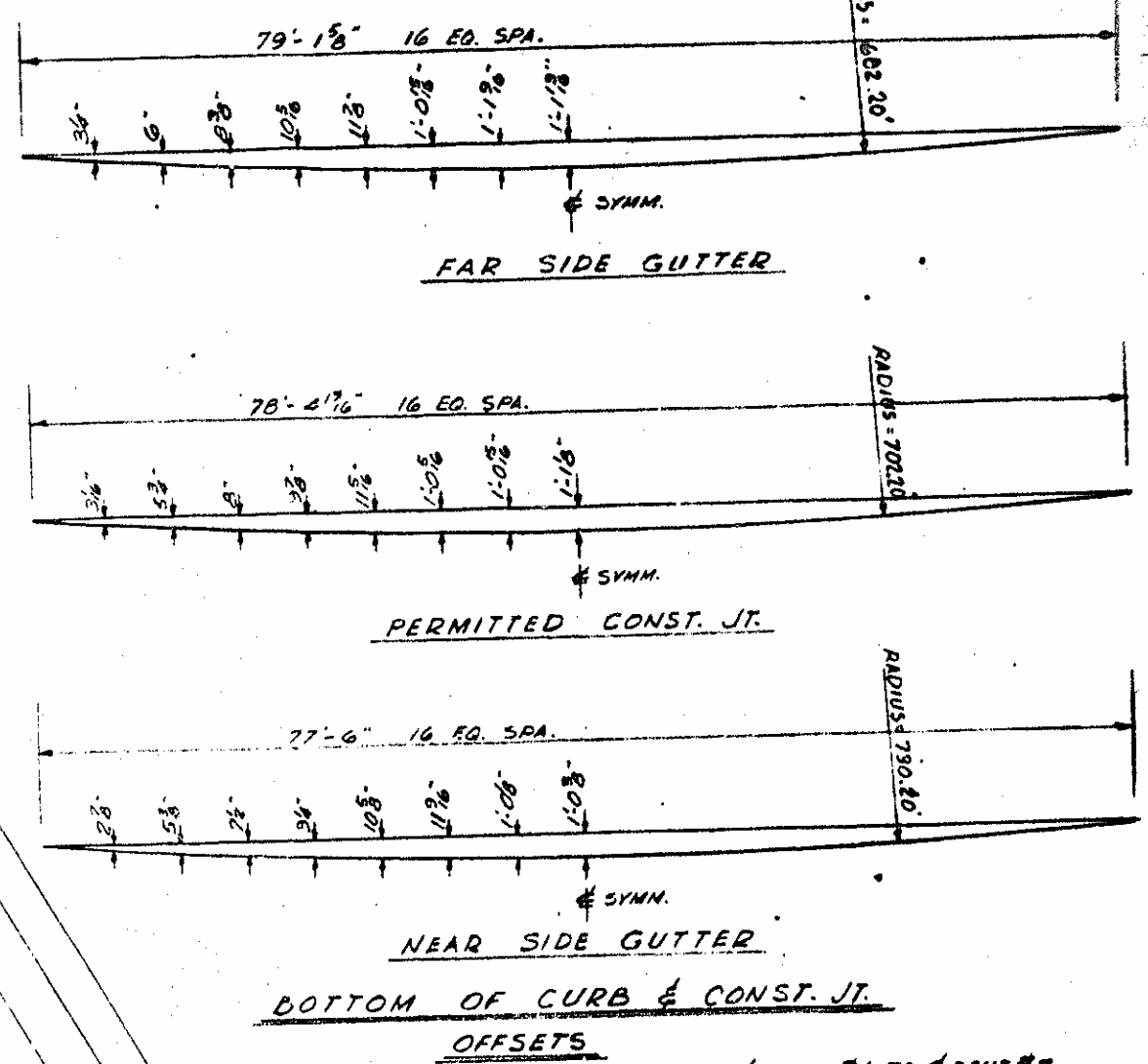
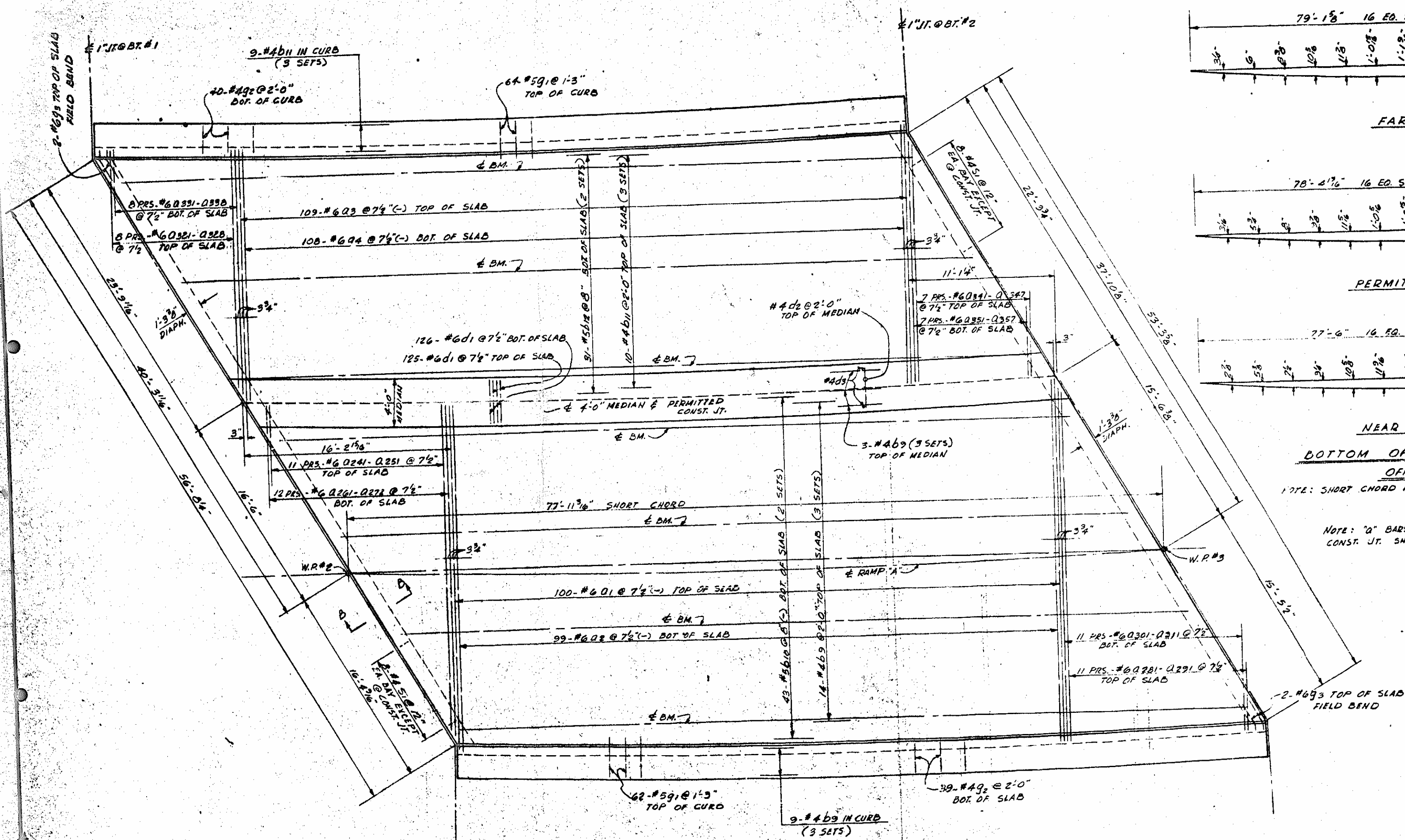
PLAN - SPAN A
NOTE: END POSTS AND RAIL ARE NOT SHOWN.

PROJECT NO. B.11618
NASH COUNTY
STATION: 382 + 98.35 N.B.L.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SPAN A
NOVEMBER 1968

REVISIONS					
NO.	BY	DATE	ED.	BY	DATE
1					
2					

DESIGNED BY: W. G. REARD, JR.
CHECKED BY: GEORGE B. WILSON
DATE: NOV. 1968



NOTE: SHORT CHORD IS MEASURED FROM C BENT #1 TO C BENT #2.

NOTE: 2\"/>

PLAN - SPAN B
NOTE: RAIL IS NOT SHOWN.

PROJECT NO. 8.11618
NASH COUNTY
STATION: 389 + 98.35 M.D.L.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SPAN B
NOVEMBER 1964

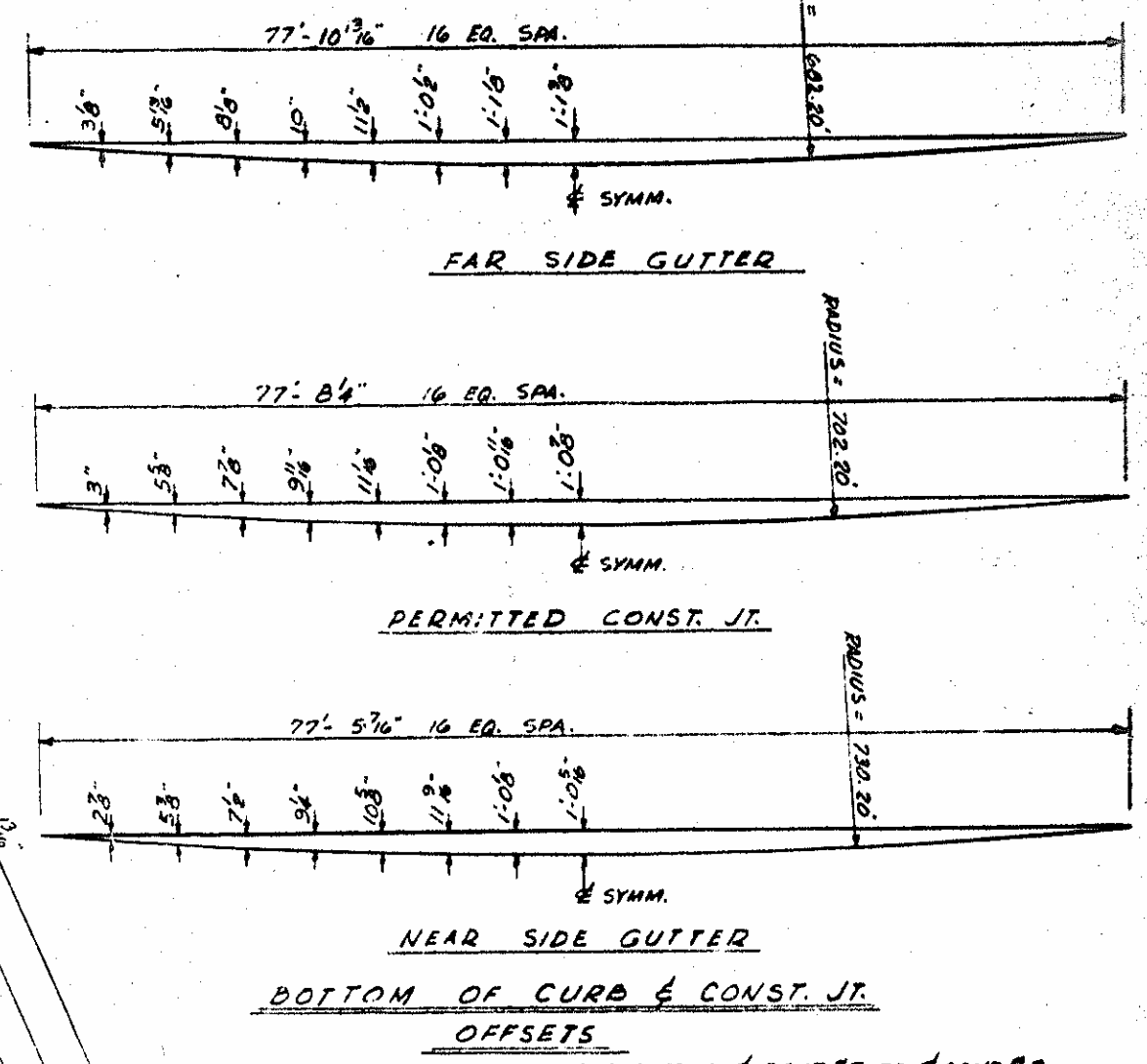
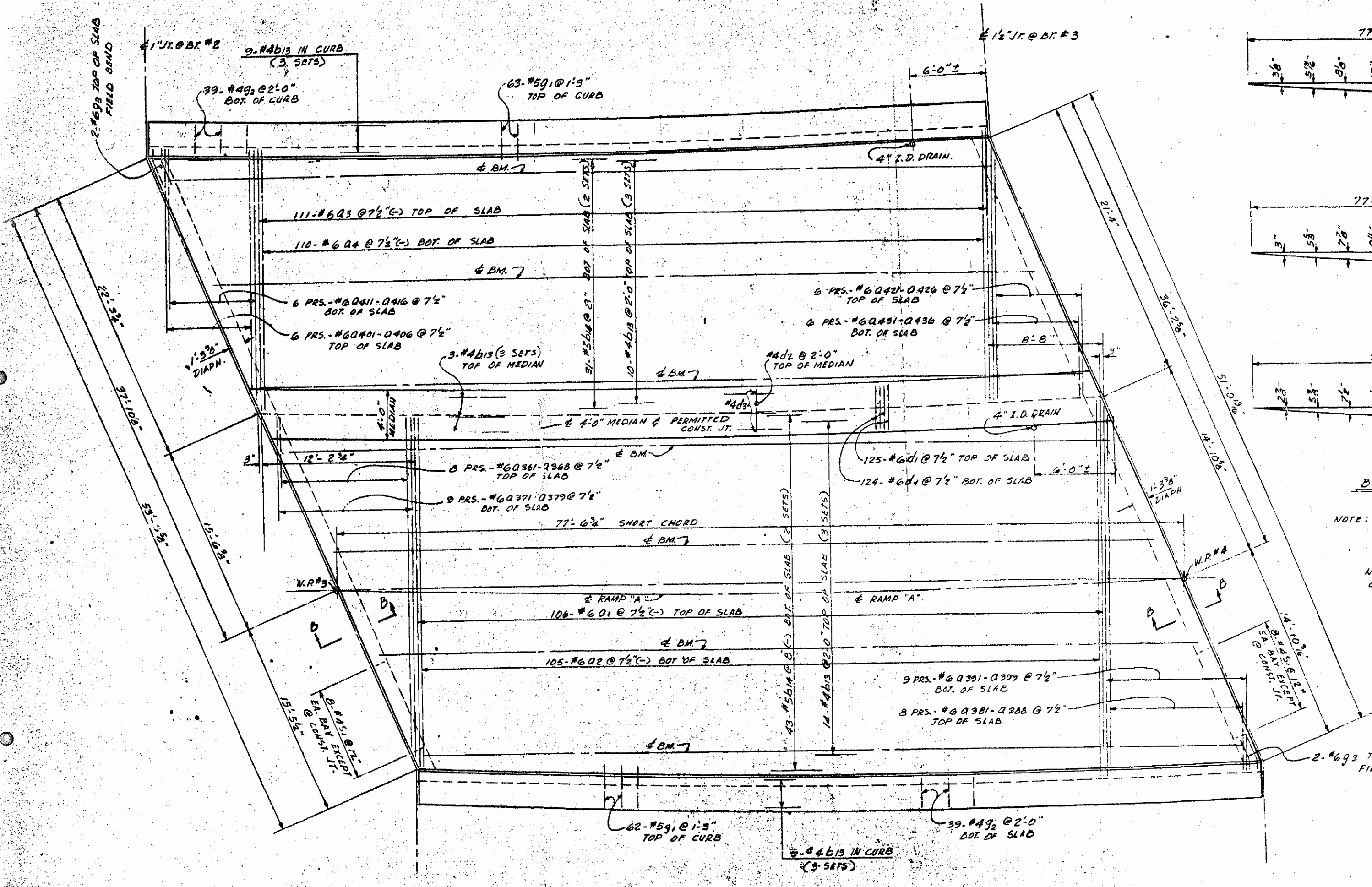
REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

DESIGNED BY: E. G. ANFORD, JR.
CHECKED BY: GEORGE B. HARRIS

1007 NO. 1078

NO.	DATE	BY
1	11.10.64	W.C. DAVIS
2		

FA. Proj. E-25-9101-122



NOTE: SHORT CHORD IS MEASURED FROM BENT #2 TO BENT #3.
NOTE: "A" BARS SHALL BE PLACED PERPENDICULAR TO THE PERMITTED CONST. JT. SHORT CHORD AND SPACED ALONG THE SHORT CHORD.

PLAN - SPAN C
NOTE: RAIL IS NOT SHOWN.

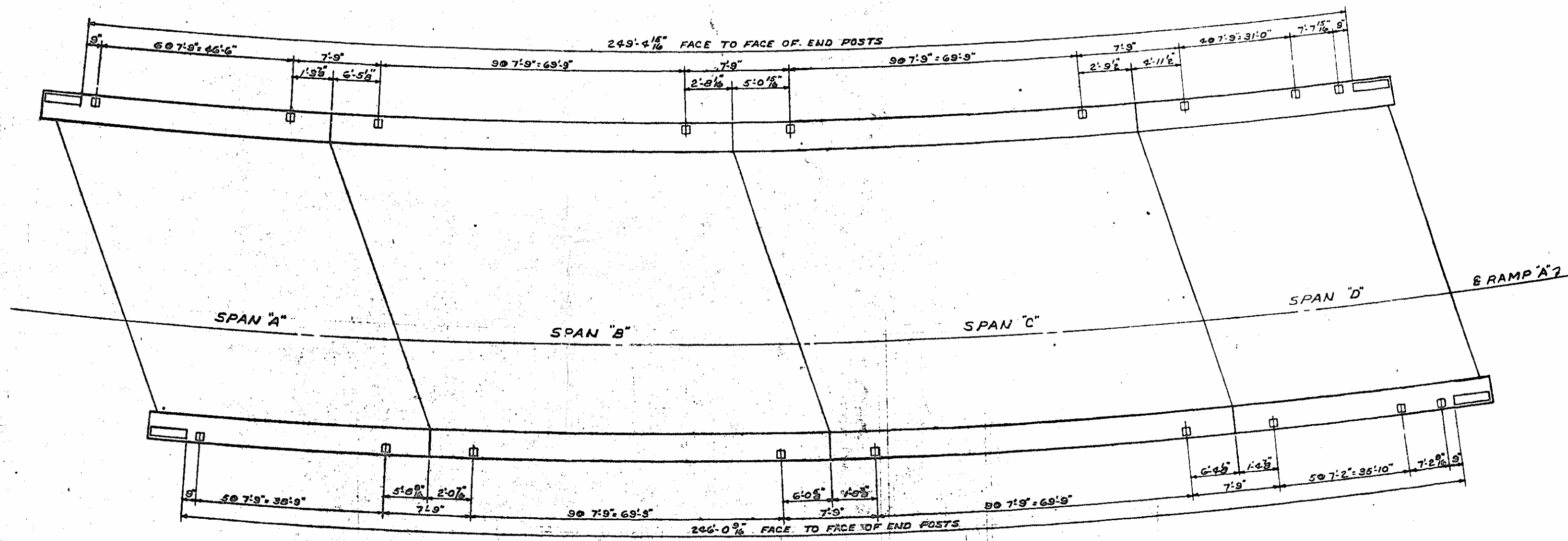
PROJECT No. B.11618
NASH COUNTY
STATION: 502+20.35 N.B.L.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SPAN C
NOVEMBER, 1964

NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

DESIGNED BY: G.E. ALFORD, JR.
CHECKED BY: GEORGE B. HUNNING
DATE: NOV 1964

218
ITY
35 N.A.L.
MISSION



PLAN SHOWING RAIL POST SPACING

NOTE: DIMENSIONS ARE MEASURED ALONG OUTSIDE OF CURB
 16'-10 1/2" FROM RAMP A NEAR SIDE AND 86'-10 1/2" FROM
 RAMP A FAR SIDE.

PROJECT NO. 811618
 MASH COUNTY
 STATION: 389+99.35 N.B.L.

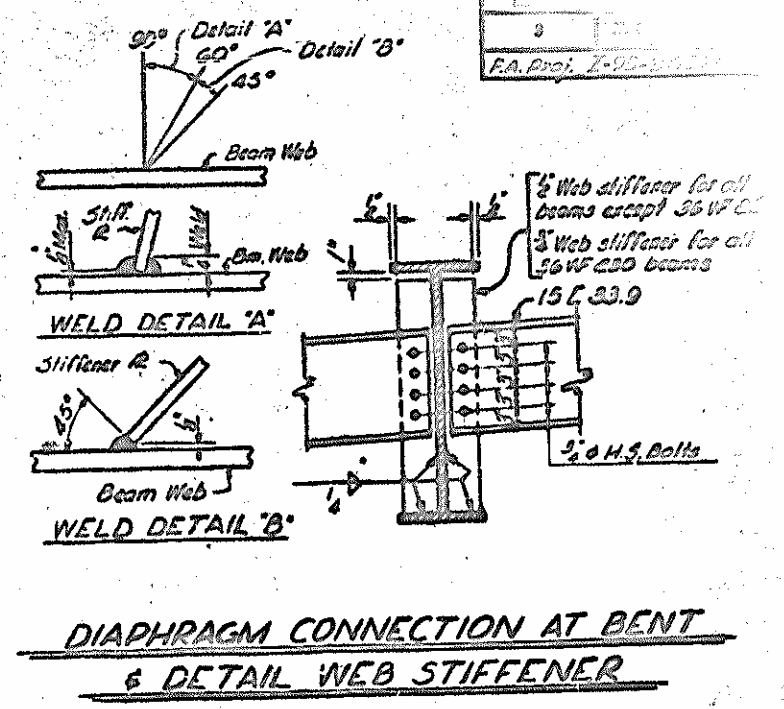
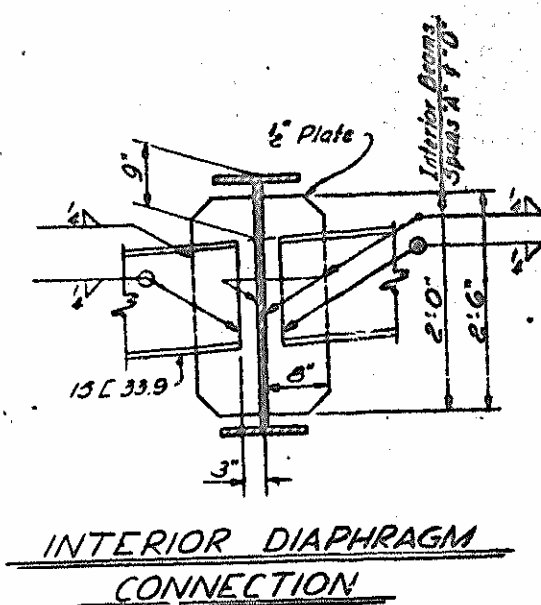
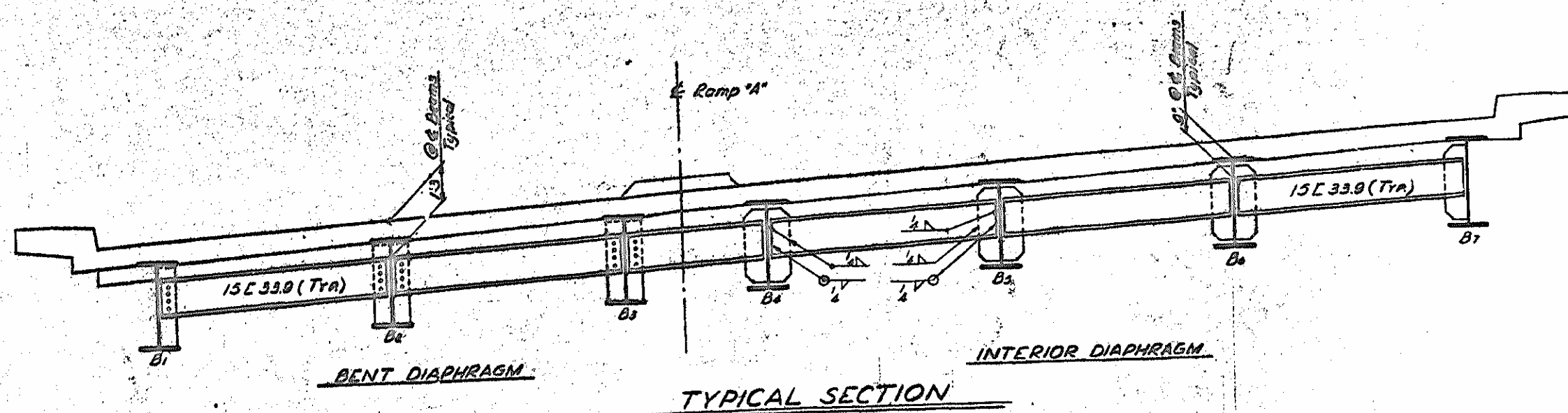
STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION

RAIL POST SPACING

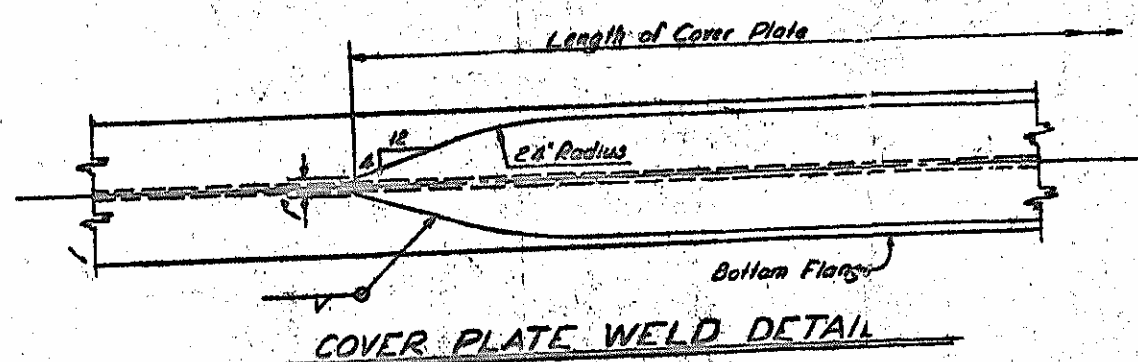
NOVEMBER 1966

REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

DESIGNED BY: G.T. PHILLIPS
 DRAWN BY: [Signature]
 CHECKED BY: [Signature]
 DATE: NOV. 1966

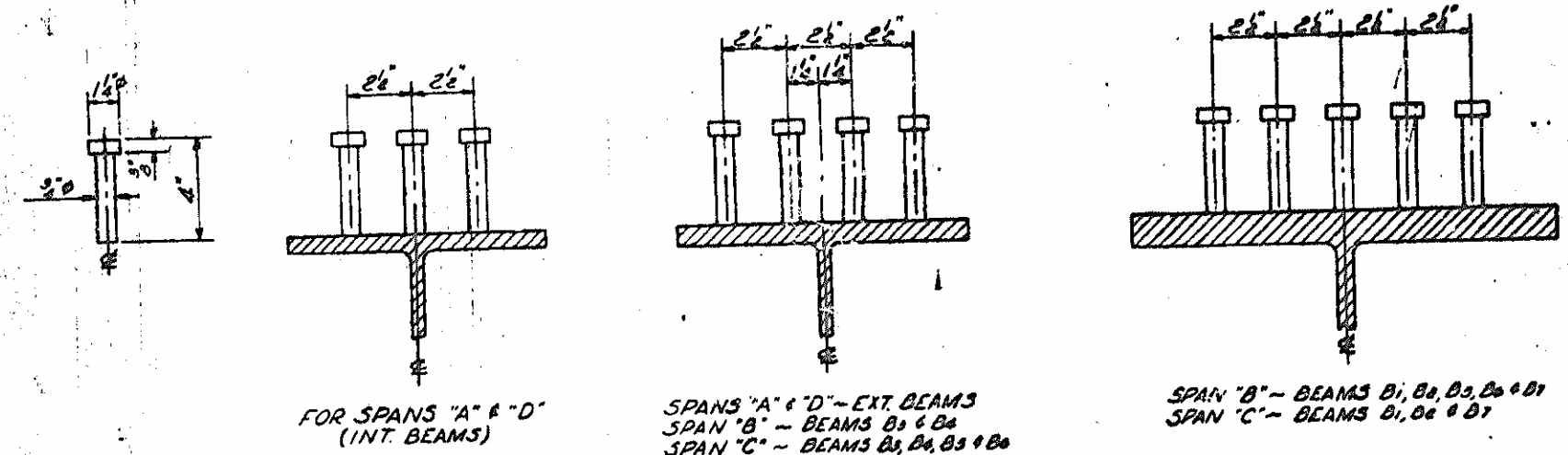


NOTE
 In lieu of the welding procedure for stop and field welds indicated for the intermediate diaphragm connections, the contractor may, at his option, stop weld the connector to plates to the beam webs and field weld the channels to the connector plates. Special care in handling the beams must be observed if connector plates are stop welded to the beam webs.



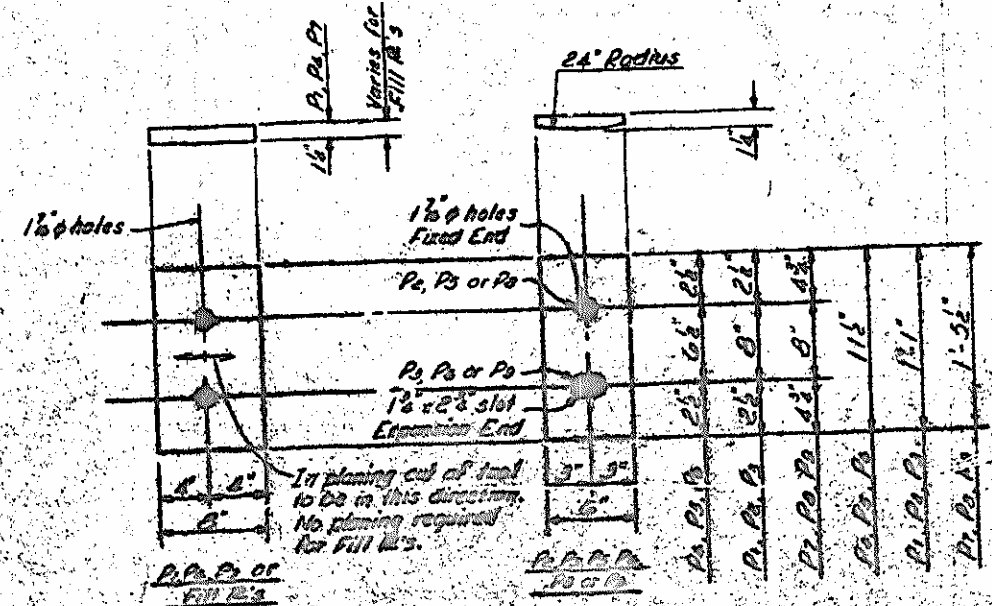
COVER PLATE WELD DETAIL

NOTE:
 Cover plate lengths are symmetrical about the center of bearings and for bottom flange only.

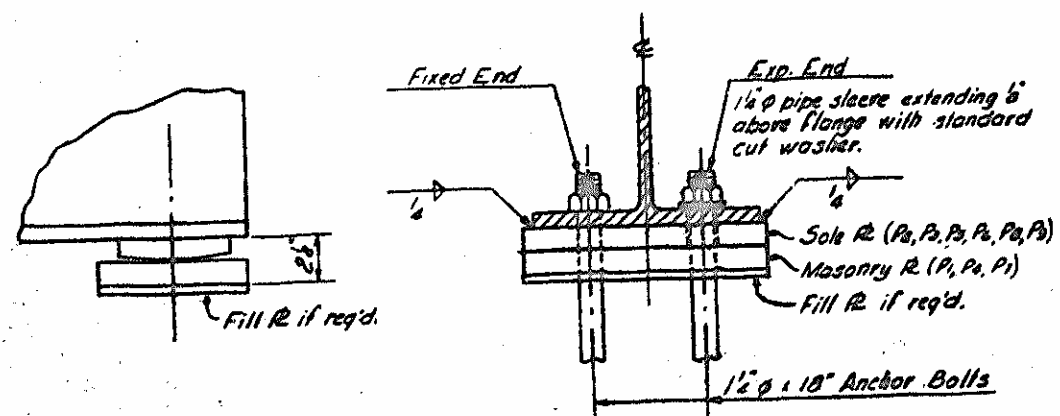


STUD DETAILS

BEARING PLATES REQUIRED		FILL PLATES REQUIRED	
20	P ₁ - 8" x 1 1/2" x 1-1"	Plane finish to 1/4"	1 - 8" x 5" x 1-1"
10	P ₂ - 6" x 1 1/2" x 1-1"	As detailed	1 - 8" x 1/2" x 1-3/8"
10	P ₃ - 6" x 1 1/2" x 1-1"	As detailed	2 - 8" x 1/2" x 1-3/8"
20	P ₄ - 8" x 1 1/2" x 1 1/2"	Plane finish to 1/4"	1 - 8" x 3" x 1 1/2"
10	P ₅ - 6" x 1 1/2" x 1 1/2"	As detailed	2 - 8" x 3" x 1 1/2"
10	P ₆ - 6" x 1 1/2" x 1 1/2"	As detailed	1 - 8" x 1/2" x 1 1/2"
16	P ₇ - 8" x 1 1/2" x 1-3/8"	Plane finish to 1/4"	1 - 8" x 5" x 1 1/2"
8	P ₈ - 6" x 1 1/2" x 1-3/8"	As detailed	
8	P ₉ - 6" x 1 1/2" x 1-3/8"	As detailed	



BEARING PLATE DETAILS



DETAILS AT BEARINGS

STRUCTURAL STEEL APPROXIMATE WEIGHT - 325,900 LBS.

PROJECT No. 811618
 NASH COUNTY
 STATION: 389 - 90.35 N.P.L.
 Sheet 3 of 3

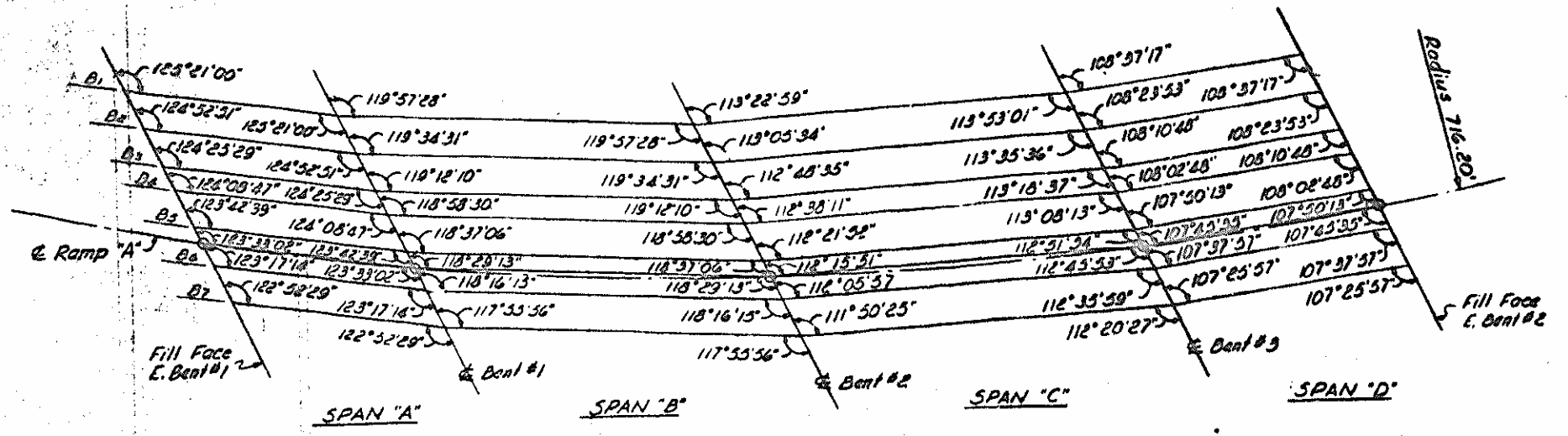
STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 STRUCTURAL STEEL
 DETAILS
 NOVEMBER 1954

REV.	BY	CHKD.	DATE
1			
2			
3			

DESIGNED BY: S.A. SANTO-TOMAS
 CHECKED BY: GEORGE B. FLYNN
 DATE: APRIL 1954
 DATE: DEC 1954

of Deflection Bearings

SPAN "A"	B1	61 rows - 4" x 4" studs each row - 244 studs each beam 4 spms @ 6" x 6", 10 spms @ 7" x 6", 4 spms @ 8" x 6", 4 spms @ 10" x 6", 5 spms @ 12" x 6", 2 spms @ 15" x 6", 1 spm @ 18" x 6"
	B2	61 rows - 4" x 4" studs each row - 244 studs each beam
	B3 or B4	13 spms @ 4" x 7" x 11", 11 spms @ 6" x 5" x 6", 7 spms @ 7" x 4" x 6", 4 spms @ 8" x 5" x 6", 4 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B5 or B6	14 spms @ 4" x 6" x 9", 8 spms @ 6" x 4" x 6", 4 spms @ 7" x 5" x 6", 4 spms @ 8" x 5" x 6", 4 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B7	25 spms @ 3" x 6" x 9", 16 spms @ 4" x 6" x 9", 10 spms @ 6" x 5" x 6", 6 spms @ 7" x 5" x 6", 3 spms @ 8" x 5" x 6", 2 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B8	4 spms @ 7" x 8" x 11", 6 spms @ 9" x 4" x 6", 4 spms @ 10" x 3" x 6", 5 spms @ 12" x 5" x 6", 3 spms @ 15" x 3" x 6", 2 spms @ 18" x 3" x 6"
	B9	73 rows - 5" x 4" studs each row - 363 studs each beam
	B10	73 rows - 5" x 4" studs each row - 363 studs each beam
	B11 or B12	17 spms @ 7" x 10" x 11", 9 spms @ 8" x 8" x 9", 7 spms @ 10" x 6" x 6", 5 spms @ 12" x 6" x 6", 5 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B13 or B14	12 spms @ 7" x 7" x 6", 9 spms @ 8" x 6" x 9", 5 spms @ 10" x 4" x 6", 9 spms @ 12" x 6" x 6", 5 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B15	63 rows - 5" x 4" studs each row - 313 studs each beam
	B16	63 rows - 5" x 4" studs each row - 313 studs each beam
	B17	71 rows - 5" x 4" studs each row - 335 studs each beam
	B18	73 rows - 5" x 4" studs each row - 363 studs each beam
	B19	8 spms @ 9" x 6" x 9", 8 spms @ 10" x 7" x 9", 7 spms @ 12" x 7" x 9", 6 spms @ 15" x 6" x 6", 4 spms @ 18" x 6" x 6", 3 spms @ 21" x 6" x 6"
	B20 or B21	17 spms @ 7" x 10" x 11", 9 spms @ 8" x 8" x 9", 7 spms @ 10" x 6" x 6", 5 spms @ 12" x 6" x 6", 5 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B22 or B23	12 spms @ 7" x 7" x 6", 9 spms @ 8" x 6" x 9", 5 spms @ 10" x 4" x 6", 9 spms @ 12" x 6" x 6", 5 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B24	63 rows - 5" x 4" studs each row - 313 studs each beam
	B25	63 rows - 5" x 4" studs each row - 313 studs each beam
	B26	71 rows - 5" x 4" studs each row - 335 studs each beam
	B27	73 rows - 5" x 4" studs each row - 363 studs each beam
	B28	8 spms @ 9" x 6" x 9", 8 spms @ 10" x 7" x 9", 7 spms @ 12" x 7" x 9", 6 spms @ 15" x 6" x 6", 4 spms @ 18" x 6" x 6", 3 spms @ 21" x 6" x 6"
	B29 or B30	10 spms @ 7" x 11" x 11", 8 spms @ 8" x 11" x 11", 8 spms @ 12" x 8" x 11", 5 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B31 or B32	10 spms @ 6" x 13" x 10", 8 spms @ 7" x 13" x 10", 8 spms @ 9" x 11" x 10", 6 spms @ 12" x 8" x 10", 4 spms @ 15" x 6" x 6", 3 spms @ 18" x 6" x 6", 2 spms @ 21" x 6" x 6"
	B33	63 rows - 5" x 4" studs each row - 313 studs each beam
	B34	63 rows - 5" x 4" studs each row - 313 studs each beam
	B35	61 rows - 4" x 4" studs each row - 244 studs each beam
	B36	73 rows - 5" x 4" studs each row - 363 studs each beam
	B37	89 rows - 5" x 4" studs each row - 427 studs each beam
	B38	10 spms @ 4" x 7" x 11", 11 spms @ 6" x 5" x 6", 6 spms @ 7" x 5" x 6", 4 spms @ 8" x 5" x 6", 4 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B39 or B40	17 spms @ 4" x 6" x 9", 10 spms @ 6" x 5" x 6", 6 spms @ 7" x 5" x 6", 4 spms @ 8" x 5" x 6", 4 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B41 or B42	25 spms @ 3" x 6" x 9", 16 spms @ 4" x 6" x 9", 10 spms @ 6" x 5" x 6", 6 spms @ 7" x 5" x 6", 3 spms @ 8" x 5" x 6", 2 spms @ 10" x 4" x 6", 1 spm @ 11" x 5" x 6"
	B43	51 rows - 4" x 4" studs each row - 204 studs each beam
	B44	5 spms @ 7" x 3" x 11", 6 spms @ 8" x 4" x 6", 4 spms @ 10" x 3" x 6", 5 spms @ 12" x 5" x 6", 3 spms @ 15" x 3" x 6", 2 spms @ 18" x 3" x 6"



PLAN SHOWING ANGLES AT BENTS

DEAD LOAD DEFLECTIONS AND BEAM CAMBER							
Span	Beam	Deflection due to weight of beam	Deflection due to superimposed dead load	Total dead load deflection	Vertical curve ordinate	Super elevation	Camber
"A"	B1	1/8"	1/8"	1/4"	1/8"	0"	0"
	B2	1/8"	1/8"	1/4"	1/8"	0"	0"
	B3	1/8"	1/8"	1/4"	1/8"	0"	0"
	B4	1/8"	1/8"	1/4"	1/8"	0"	0"
	B5	1/8"	1/8"	1/4"	1/8"	0"	0"
	B6	1/8"	1/8"	1/4"	1/8"	0"	0"
"B"	B7	1/8"	1/8"	1/4"	1/8"	0"	0"
	B8	1/8"	1/8"	1/4"	1/8"	0"	0"
	B9	1/8"	1/8"	1/4"	1/8"	0"	0"
	B10	1/8"	1/8"	1/4"	1/8"	0"	0"
	B11	1/8"	1/8"	1/4"	1/8"	0"	0"
	B12	1/8"	1/8"	1/4"	1/8"	0"	0"
"C"	B13	1/8"	1/8"	1/4"	1/8"	0"	0"
	B14	1/8"	1/8"	1/4"	1/8"	0"	0"
	B15	1/8"	1/8"	1/4"	1/8"	0"	0"
	B16	1/8"	1/8"	1/4"	1/8"	0"	0"
	B17	1/8"	1/8"	1/4"	1/8"	0"	0"
	B18	1/8"	1/8"	1/4"	1/8"	0"	0"
"D"	B19	1/8"	1/8"	1/4"	1/8"	0"	0"
	B20	1/8"	1/8"	1/4"	1/8"	0"	0"
	B21	1/8"	1/8"	1/4"	1/8"	0"	0"
	B22	1/8"	1/8"	1/4"	1/8"	0"	0"
	B23	1/8"	1/8"	1/4"	1/8"	0"	0"
	B24	1/8"	1/8"	1/4"	1/8"	0"	0"

of Bearing 2

SPAN "A"	B1	Cover B - 6" x 7" x 23" 0"
	B2	Cover B - 9" x 7" x 34" 6"
	B3 or B4	Cover B - 6" x 7" x 34" 0"
SPAN "B"	B1	Cover B - 11" x 15" x 46" 0"
	B2	Cover B - 10" x 16" x 52" 6"
	B3 or B4	Cover B - 9" x 15" x 45" 6"
SPAN "C"	B1	Cover B - 11" x 15" x 45" 0"
	B2	Cover B - 10" x 16" x 52" 0"
	B3 or B4	Cover B - 10" x 16" x 52" 0"
SPAN "D"	B1	Cover B - 11" x 15" x 44" 6"
	B2	Cover B - 9" x 8" x 33" 0"
	B3 or B4	Cover B - 6" x 8" x 29" 0"
B5 or B6	Cover B - 9" x 8" x 32" 6"	

BEAM ELEVATION AND STUD SPACING

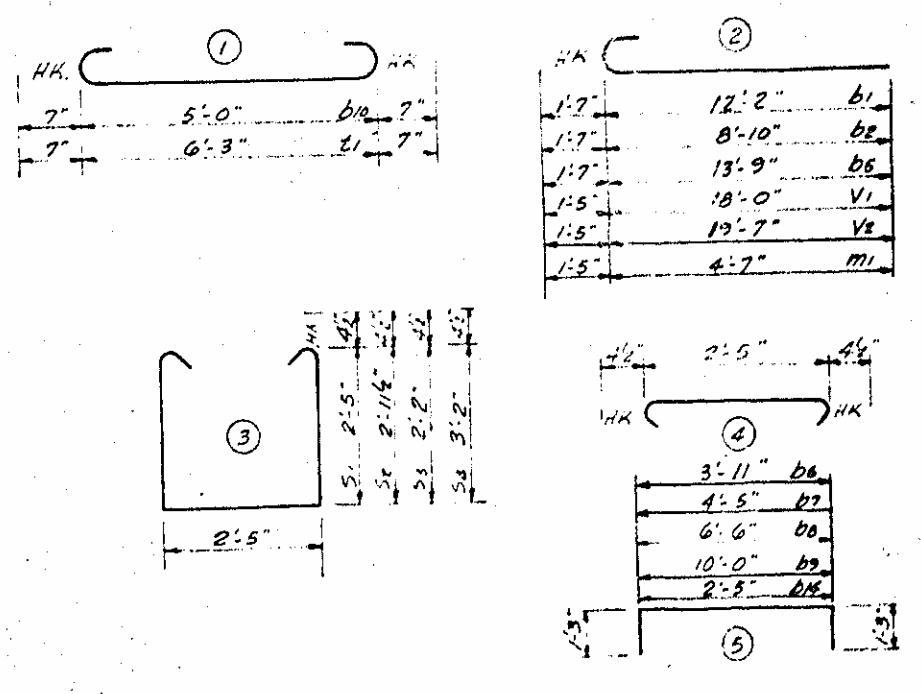
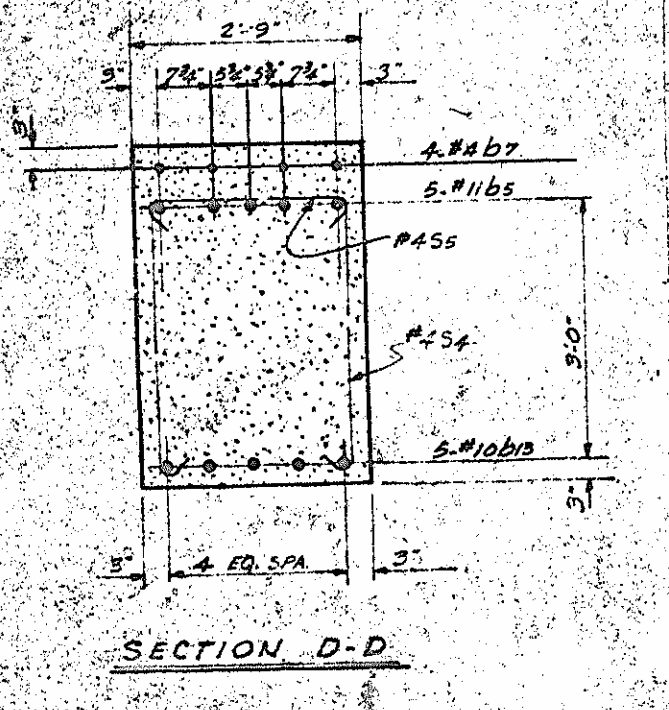
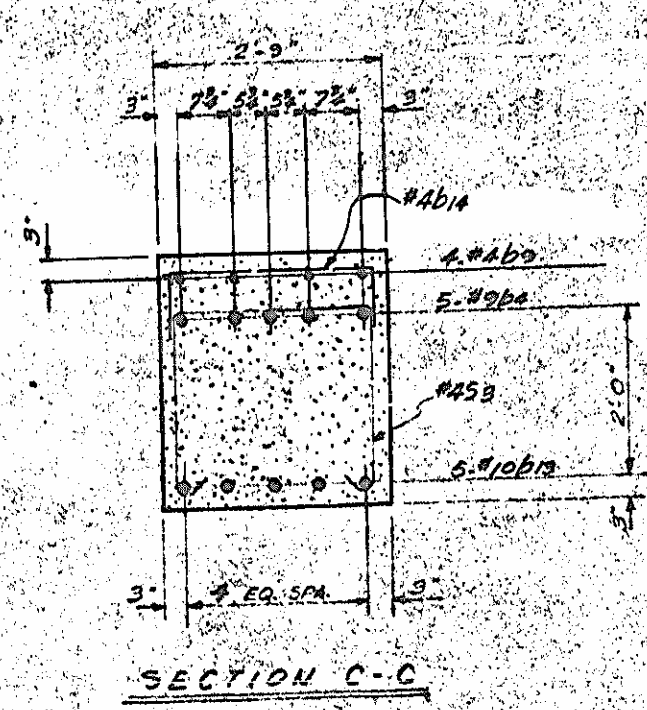
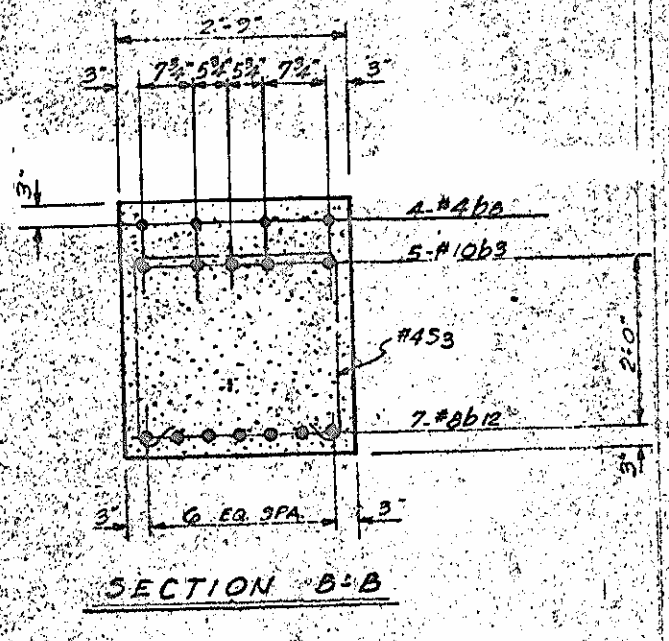
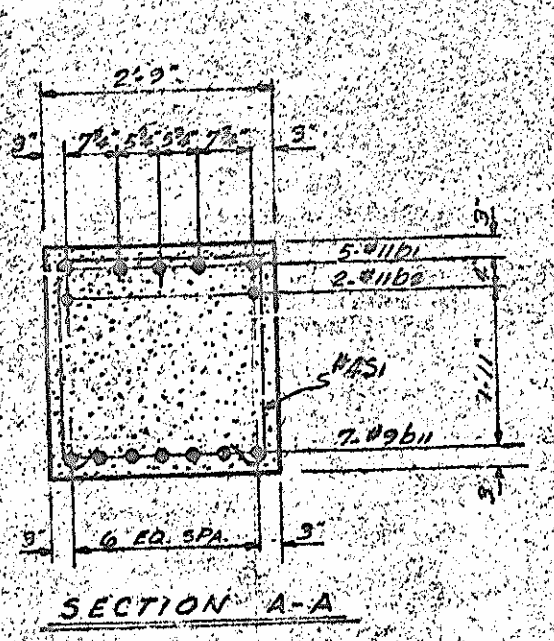
PROJECT No. 8.11618
 NASH COUNTY
 STATION: 389+99.35 N.B.L.
 Sheet 2 of 3

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE
 STRUCTURAL STEEL

NOVEMBER 1964

REVISIONS					
NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

DESIGNED BY S. L. SANTO - TOMAS DATE NOV. 1964
 CHECKED BY GEORGE B. HUNNING DATE DEC. 1964



BAR TYPES
 ALL BAR DIMENSIONS ARE CUT TO OUT

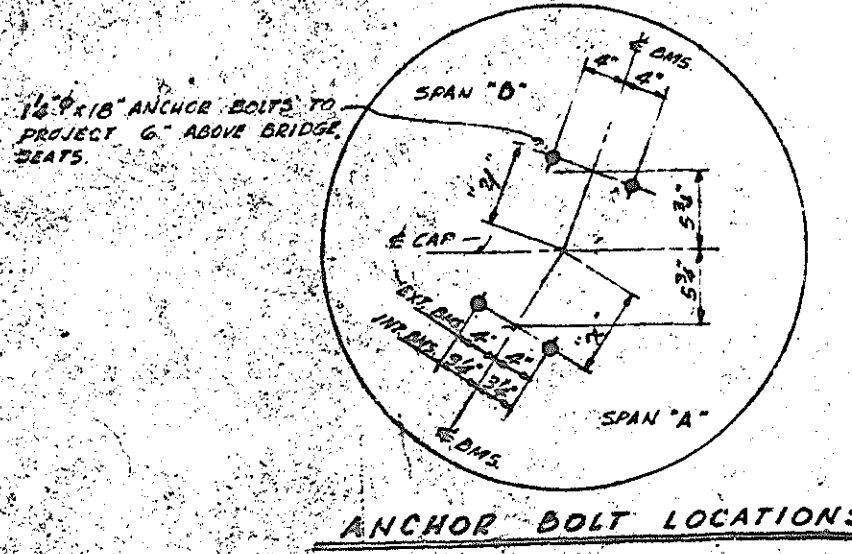


TABLE OF VARIABLE DIMENSIONS

DEAN	X	Y
1	7 1/2"	6 1/2"
2	7"	6 1/2"
3	7"	6 1/2"
4	6 1/2"	6 1/2"
5	6 1/2"	6 1/2"
6	6 1/2"	6 1/2"
7	6 1/2"	6 1/2"

BILL OF MATERIAL
 FOR ONE BENT

BAR NO.	NO.	SIZE	TYPE	LENGTH	QTY.
D1	5	#11	2	15'-9"	515
D2	2	#11	2	10'-5"	111
D3	5	#10	57R	18'-8"	662
D4	5	#9	57R	21'-2"	367
D5	5	#11	2	15'-4"	407
D6	15	#4	5	6'-5"	61
D7	4	#4	5	6'-4"	19
D8	4	#4	5	9'-0"	24
D9	4	#4	5	11'-6"	33
D10	24	#5	1	6'-2"	154
D11	7	#9	57R	21'-0"	300
D12	7	#6	57R	17'-8"	320
D13	5	#10	57R	21'-0"	405
D14	3	#4	5	4'-11"	10
V1	16	#10	2	19'-5"	1227
V2	16	#10	2	21'-0"	1426
M1	32	#10	2	6'-0"	826
L1	120	#5	1	7'-5"	928
S1	19	#4	3	8'-0"	69
S2	17	#4	3	9'-1"	123
S3	8	#4	3	7'-6"	30
S4	11	#4	3	9'-6"	70
S5	26	#4	4	8'-2"	59

REINFORCING STEEL LOS B105

CLASS	AREA	CU. YDS.
MUR #1	16.3	8
MUR #2	14.9	
MUR #3	22.9	
TOTAL	54.1	

12" Ø PRESTRESS CONC. PILES
 NO. 20 L.N. FT. 4000

NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN REJECTED.

PROJECT NO. B-11618
 NASH COUNTY
 STATION: 389 + 98.35 N.B.L.

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 Raleigh

BENT No 1

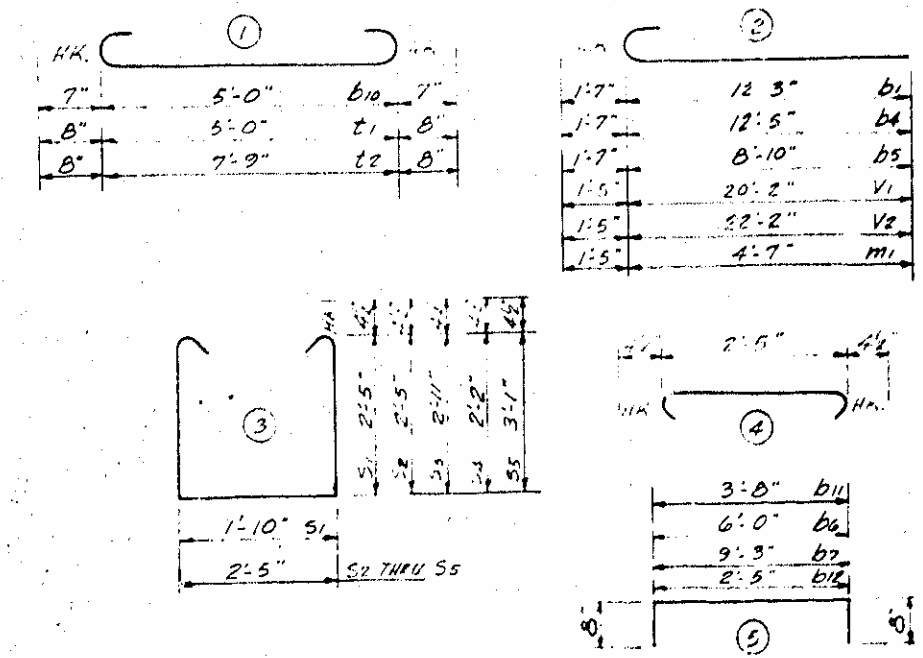
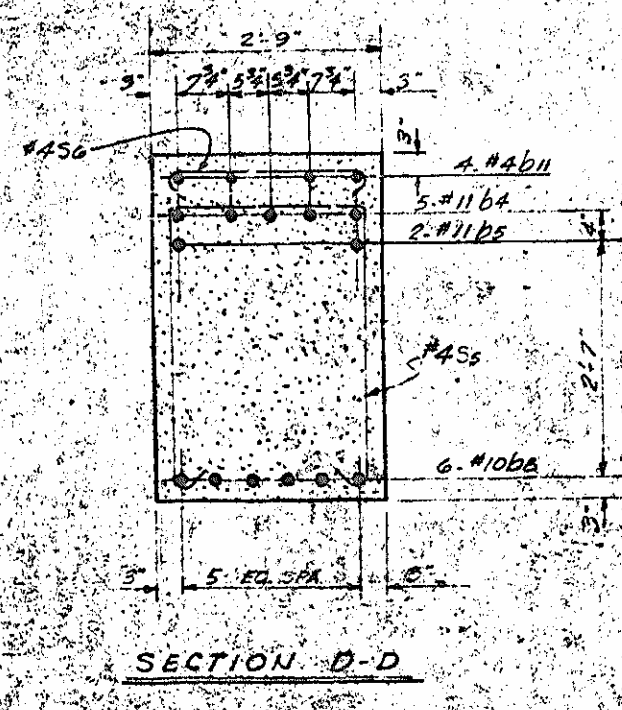
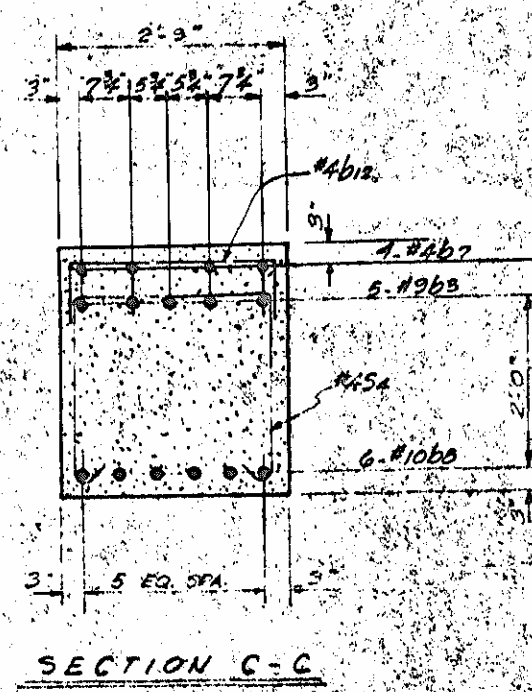
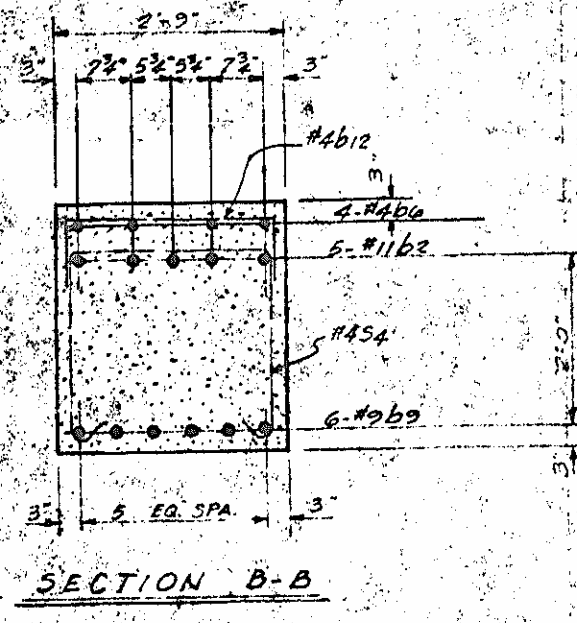
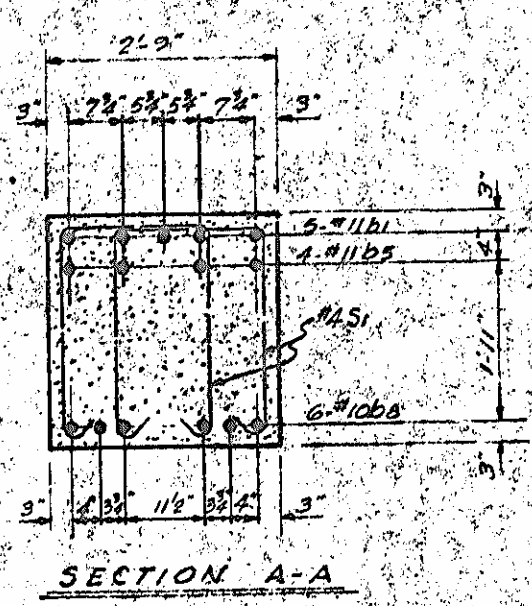
DECEMBER 1964

REVISIONS

NO.	BY	DATE	NO.	BY	DATE
1			2		
3					

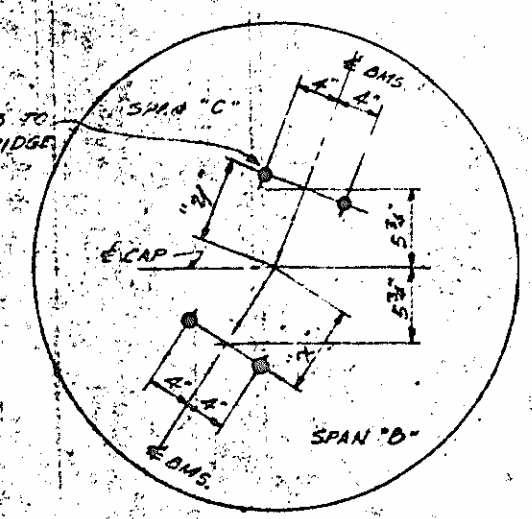
KODAK SAFETY FILM

NO. ROAD DIST. NO.	STATE	PROJECT NO.
2	N.C.	8.11618
PROJECT 1-95-3(2) 100		



BAR TYPES
ALL BAR DIMENSIONS ARE CUT TO CUT

1/2" DIA ANCHOR BOLTS TO PROJECT 6" ABOVE BRIDGE SEATS



ANCHOR BOLT LOCATIONS

DEAN	X	Y
1	6 3/4"	6 1/4"
2	6 3/4"	6 1/4"
3	6 3/4"	6 1/4"
4	6 3/4"	6 1/4"
5	6 3/4"	6 1/4"
6	6 3/4"	6 1/4"
7	6 3/4"	6 1/4"

BILL OF MATERIAL FOR ONE BENT

BAR NO	SIZE	TYPE	LENGTH	WEIGHT
b1	5 #11	2	13'-10"	867
b2	5 #11	STR	18'-0"	870
b3	5 #9	STR	20'-6"	900
b4	5 #11	2	14'-0"	872
b5	6 #11	2	10'-5"	882
b6	4 #4	5	7'-4"	20
b7	4 #4	5	11'-7"	20
b8	6 #9	STR	16'-7"	850
b9	12 #10	STR	20'-0"	1038
b10	6 #9	STR	16'-7"	850
b11	4 #4	5	6'-2"	154
b12	8 #4	5	5'-0"	13
V1	16 #10	2	21'-7"	1485
V2	16 #10	2	23'-7"	1625
M1	32 #10	2	6'-0"	825
C1	64 #6	1	6'-4"	797
C2	48 #6	1	9'-1"	655
S1	14 #4	3	7'-5"	67
S2	4 #4	3	8'-0"	31
S3	10 #4	3	9'-0"	120
S4	9 #4	3	7'-6"	45
S5	14 #4	3	9'-4"	87
S6	28 #4	4	3'-2"	59

REINFORCING STEEL	LD: 9,209
CLASS A	CO: 1,000
BAR #1	16.1 #
BAR #2	17.5
BAR #3	20.6
TOTAL	54.2 #

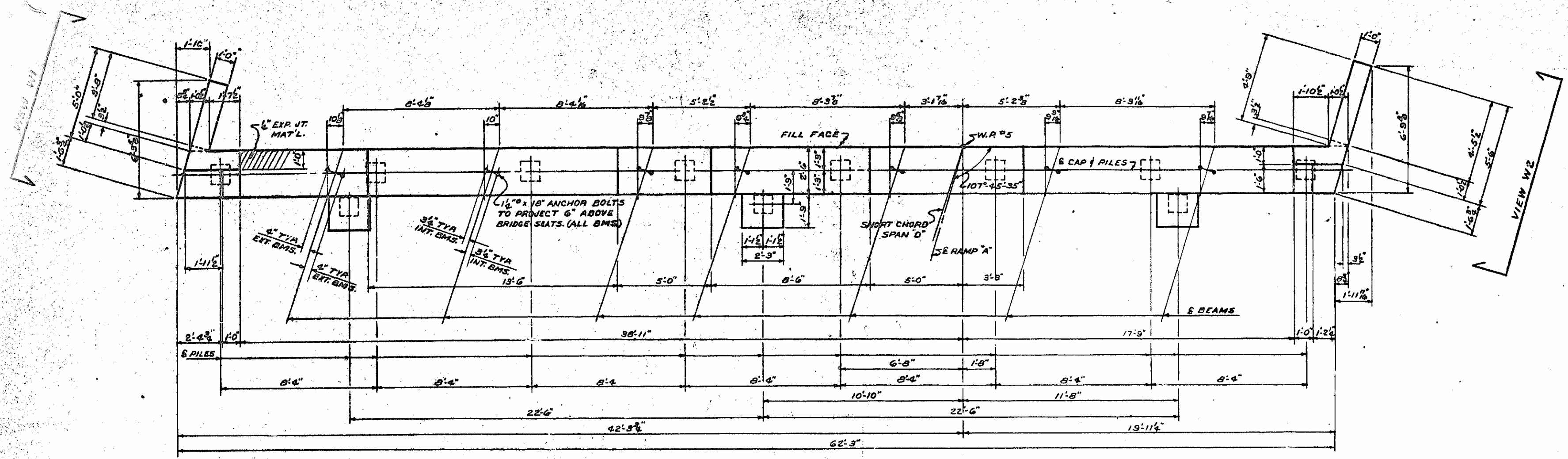
PROJECT NO. 8.11618
NASH COUNTY
STATION: 389 + 98.95 N.B.L.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
BALDWIN
BENT NO. 2

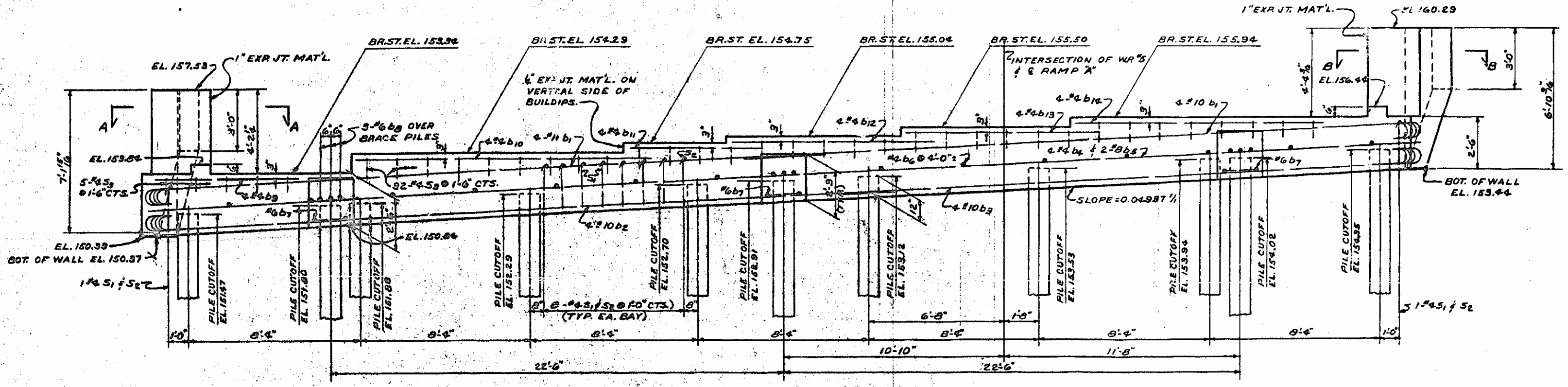
DECEMBER 1964

NO.	BY	DATE	NO.	BY	DATE
1			2		
2			3		

5. ALWOOD, JR. DATE 1/28/64
7. PROCTOR, JR. DATE 3/11/64



PLAN



ELEVATION

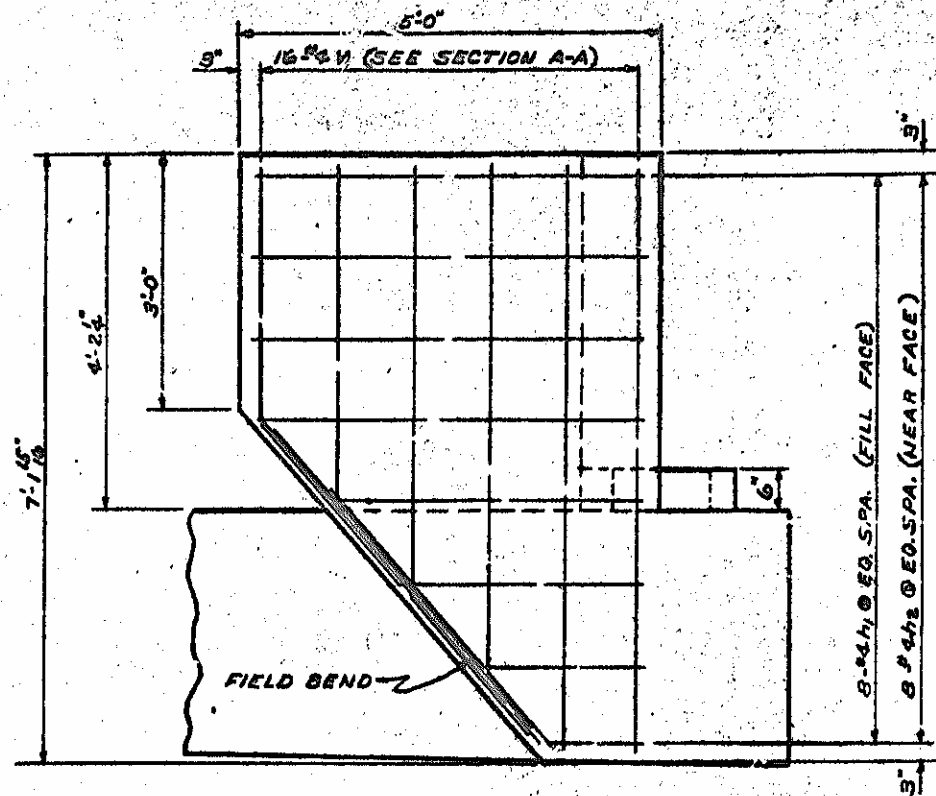
PROJECT No. B.11618
 NASH COUNTY
 STATION: 389+98.35 N.B.L.

STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION

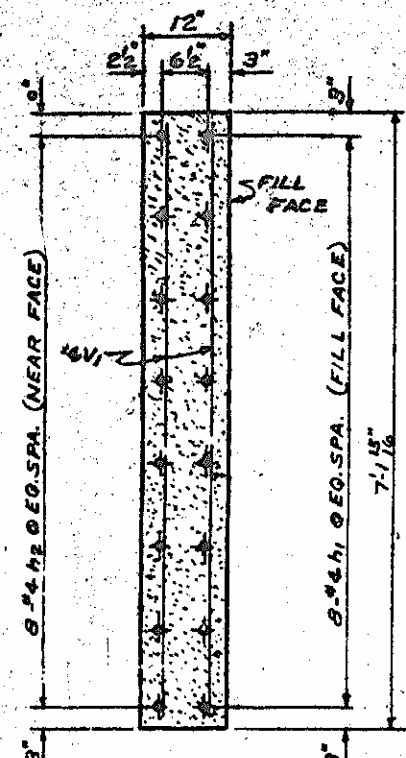
END BENT #2

NOVEMBER		1924	
REV.	BY	DATE	REVISIONS
1			
2			

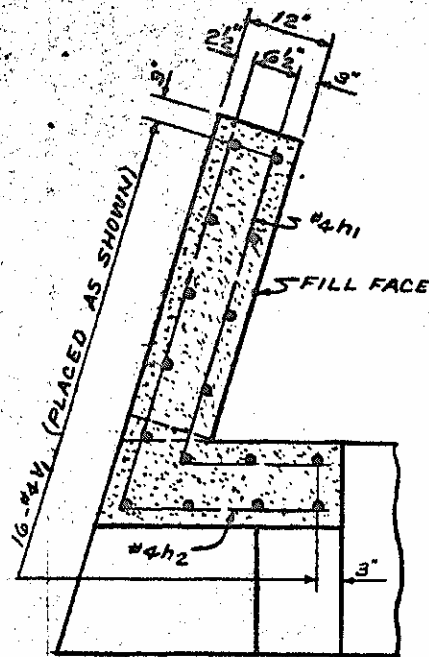
DESIGNED BY: G.T. PHILLIPS
 DRAWN BY: G.T. PHILLIPS
 CHECKED BY: GEORGE B. WYNNIE



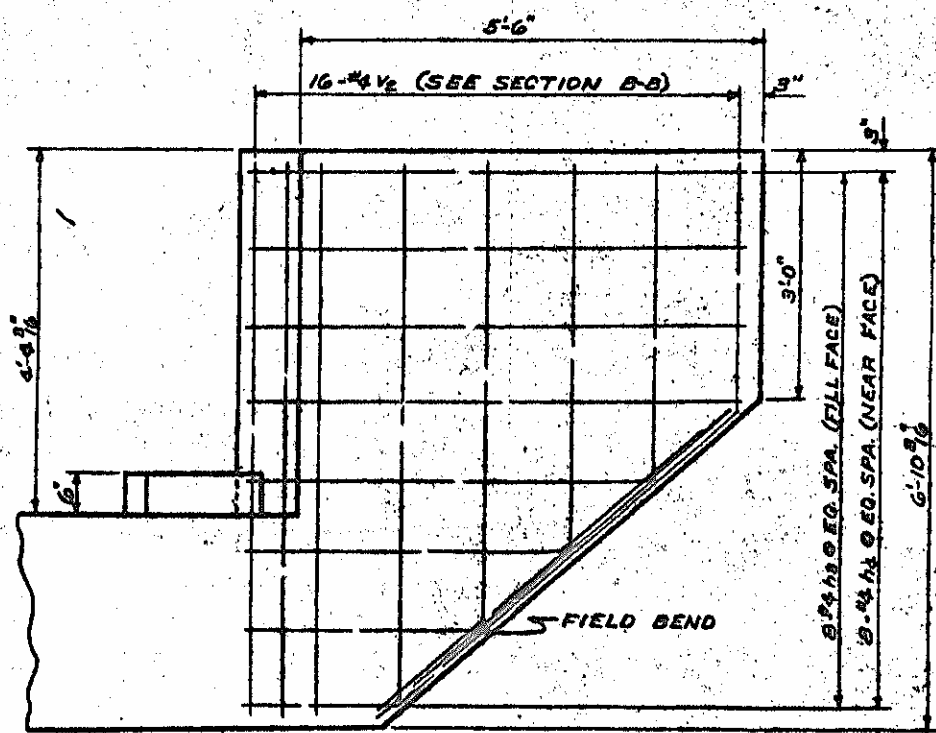
WING W1



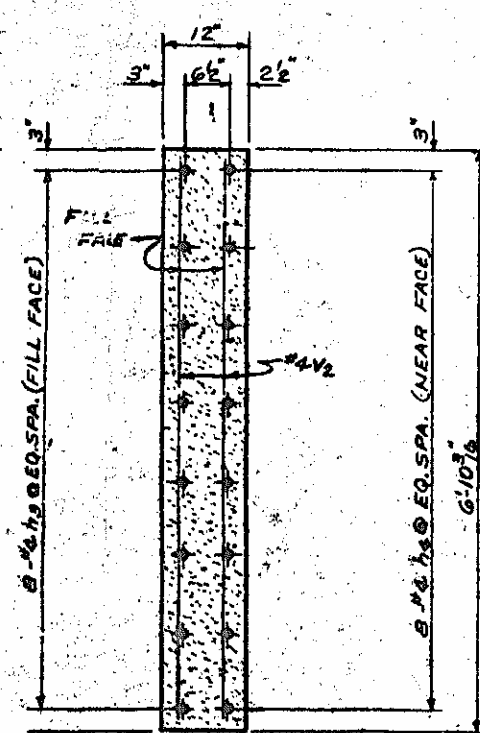
SECTION THRU WING W1



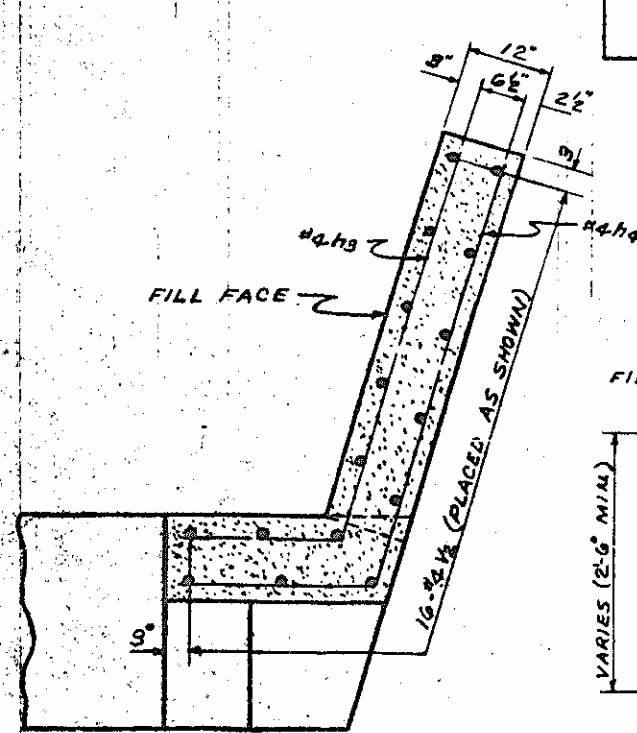
SECTION A-A



WING W2



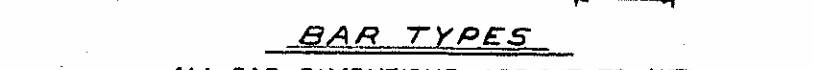
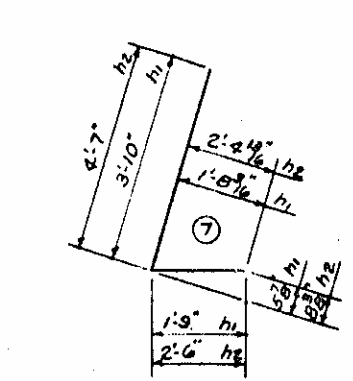
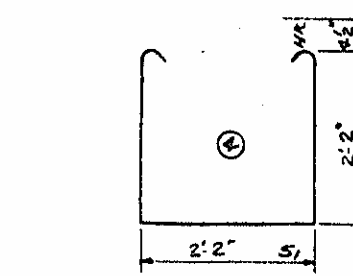
SECTION THRU WING W2



SECTION B-B

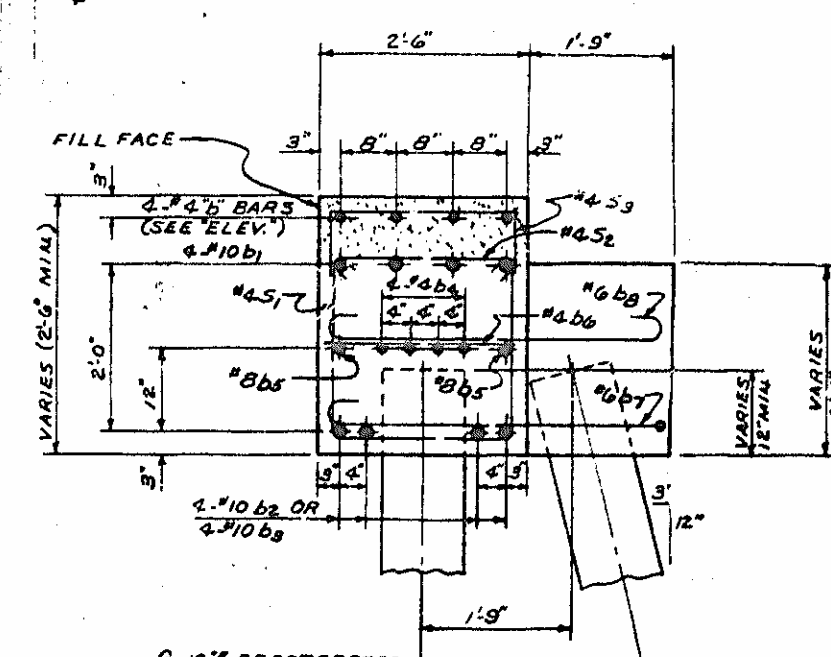
NR.	1	32'-7"	b1
	1-6"	36'-9"	b2
	1-5"	28'-5"	b3

NR.	2	NR.
	5'-11"	b4



BAR TYPES

ALL BAR DIMENSIONS ARE OUT TO OUT.



SECTION THRU CAP

BILL OF MATERIAL

BAR NO.	SIZE	TYPE	LENGTH	QUANTITY
b1	#4	1	32'-7"	172
b2	#4	1	36'-9"	657
b3	#4	1	28'-5"	513
b4	#4	STR	28'-5"	172
b5	#4	STR	32'-3"	302
b6	#4	STR	2'-2"	20
b7	#4	3	11'-1"	20
b8	#4	2	5'-3"	71
b9	#4	STR	7'-0"	10
b10	#4	STR	14'-0"	30
b11	#4	STR	6'-9"	17
b12	#4	STR	5'-0"	25
b13	#4	STR	5'-6"	25
b14	#4	STR	12'-0"	32
S1	#4	4	7'-8"	261
S2	#4	5	2'-11"	113
S3	#4	6	4'-2"	103
V1	#4	STR	6'-0"	73
V2	#4	STR	6'-0"	69
H1	#4	7	5'-7"	80
H2	#4	7	7'-1"	80
H3	#4	8	6'-0"	35
H4	#4	8	7'-6"	60

REINFORCING STEEL LBS.: 5,020
CLASS "A" CONC. CU. YDS.: 20.5
12" PRESTRESSED CONC. PILES
NO. 111 LIM. FT.: 220

* NOTE: CONCRETE DISPLACED BY PILE HEADS HAS BEEN DEDUCTED.

PROJECT NO. 8.11618
NASH COUNTY
STATION: 389+9835 T.B.L.

STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION

END BENT #2

NOVEMBER 1966

NO.	BY	DATE	NO.	BY	DATE
1			3		
2			4		

DRAWN BY: J. CHILLER
CHECKED BY: J. CHILLER

ASSEM.
CHECK.
DRAW.
CHECK.