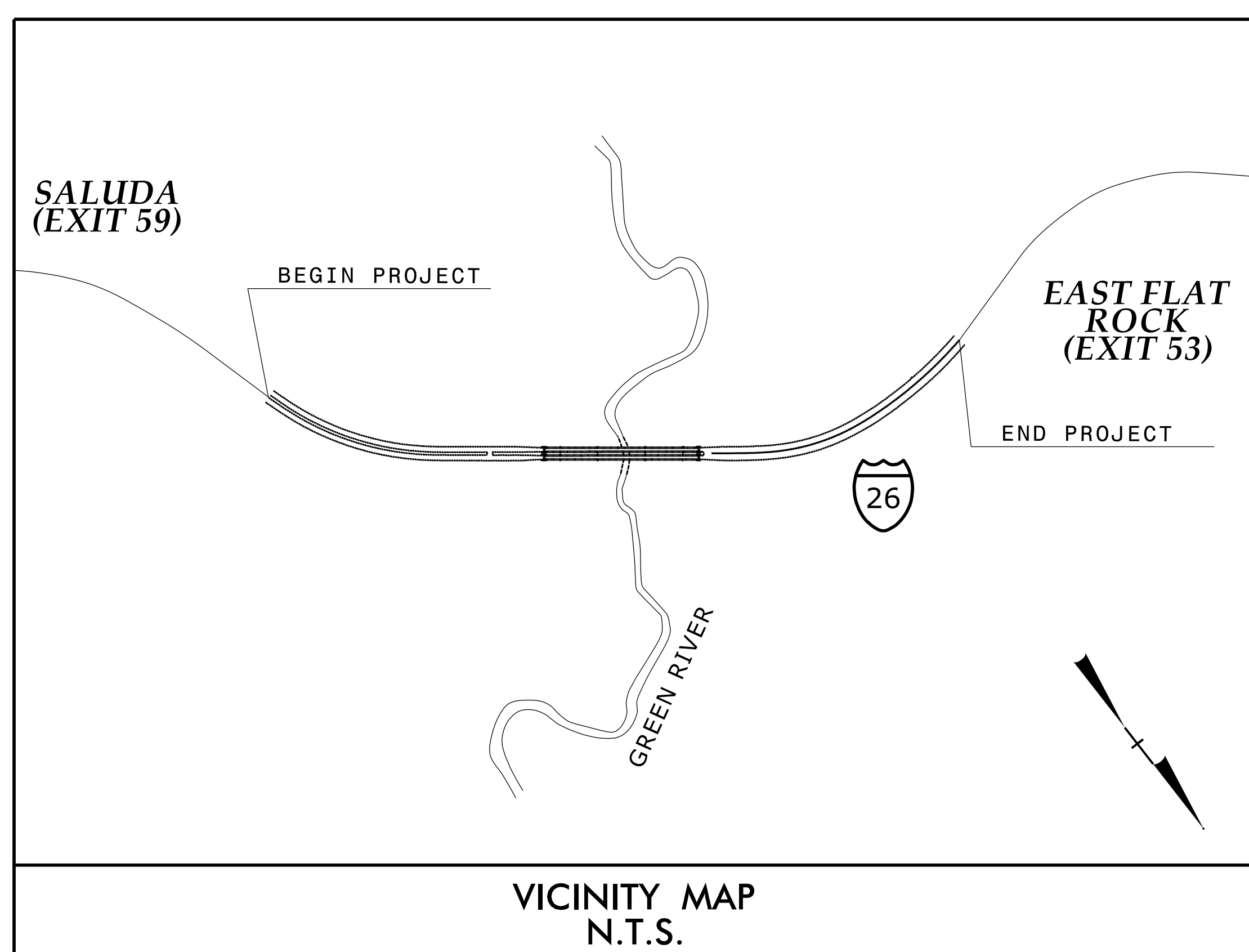
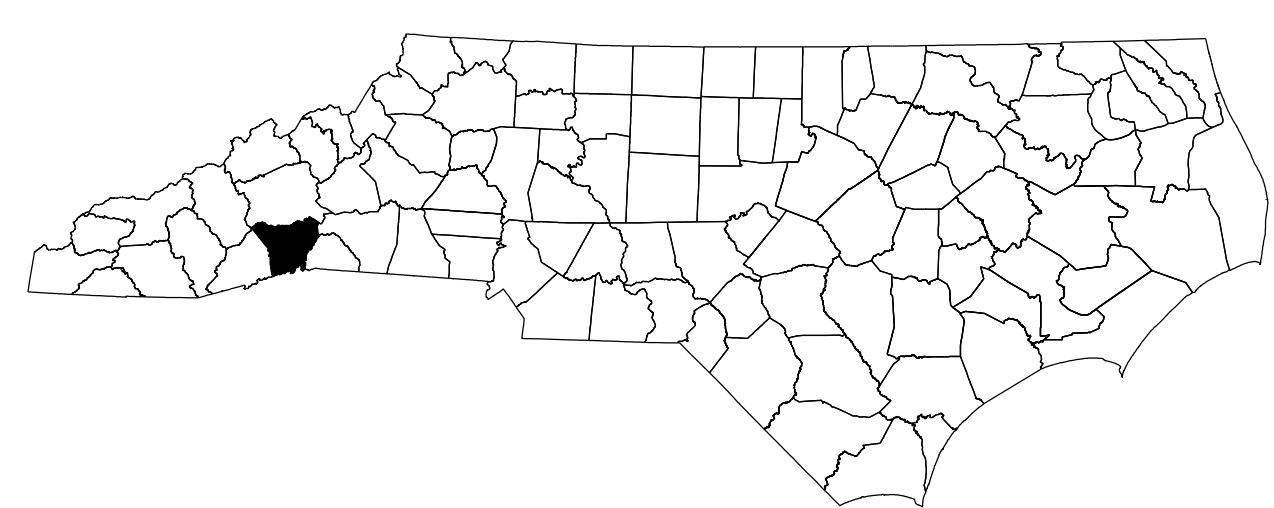


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

HENDERSON COUNTY

TRANSPORTATION MANAGEMENT PLAN



LOCATION: I-26 INTERSTATE BRIDGES OVER GREEN RIVER
TYPE OF WORK: TRAFFIC CONTROL FOR BRIDGE CONSTRUCTION

<u>SHEET NO.</u>	<u>TITLE</u>
TMP-01	TITLE SHEET AND INDEX OF SHEETS
TMP-01A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, TEMPORARY PAVEMENT MARKING SCHEDULE
TMP-02	PROJECT NOTES
TMP-02A	DYNAMIC ZIPPER MERGE SYSTEM LAYOUT
TMP-03	PROJECT PHASING
TMP-04 THRU TMP-08	STAGE I OVERVIEW & DETAILS
TMP-09 THRU TMP-13	STAGE II OVERVIEW & DETAILS
TMP-14 THRU TMP-18	STAGE III OVERVIEW & DETAILS
TMP-19 THRU TMP-23	STAGE IV OVERVIEW & DETAILS

03-02-2020 DATE SUBMITTED

SUBMITTAL:

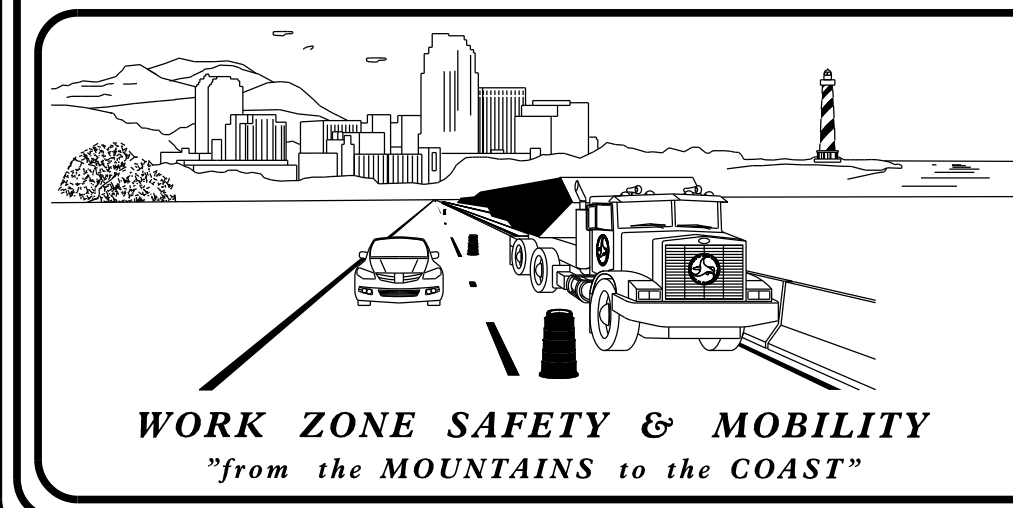
STAGING CONCEPT

MIDPOINT

PRE-FINAL

FINAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



PLANS PREPARED BY: AECOM

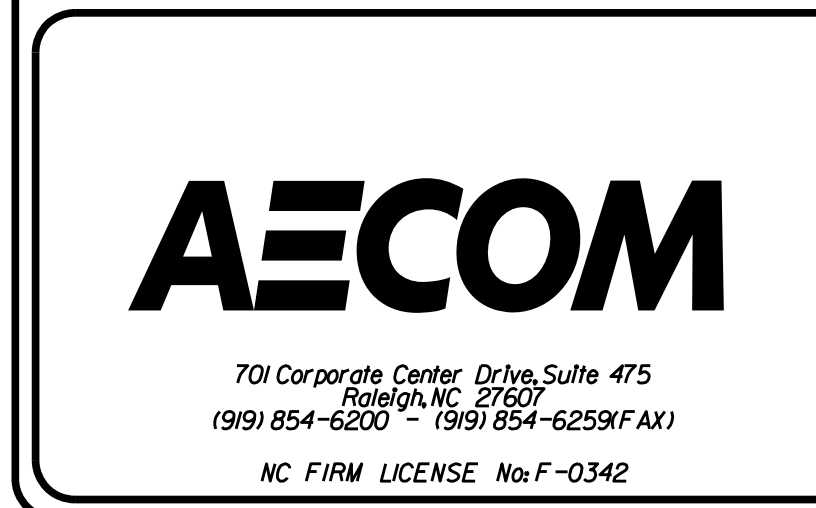
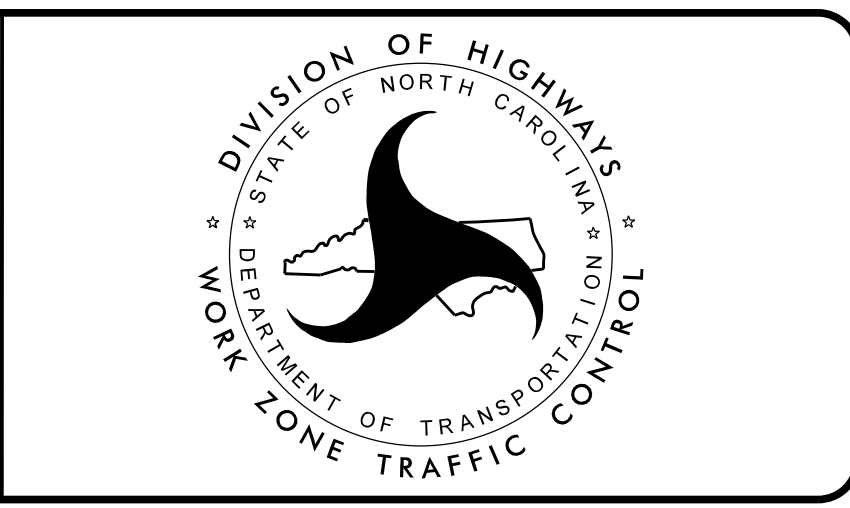
ED EDENS, P.E.
PROJECT ENGINEER

KEVIN VAN METRE, P.E.
PROJECT DESIGN ENGINEER

NCDOT CONTACTS:

DON PARKER, P.E.
PROJECT ENGINEER

KARMEN DAIS, P.E.
PROJECT DESIGN ENGINEER



APPROVED: Kevin J. Van Metre
DATE: 3/10/2020

SEAL

TIP PROJECT: 15BPR.20

\$\$\$\$\$SYTIME\$\$\$\$\$
\$\$\$\$\$DGN\$\$\$\$\$
\$\$\$\$\$USERNAME\$\$\$\$\$

PROJ. REFERENCE NO.	SHEET NO.
15BPR.20	TMP-1A

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

<u>STD. NO.</u>	<u>TITLE</u>
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGERS
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY - DRUMS
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES

GENERAL

- DIRECTION OF TRAFFIC FLOW

- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.

- WORK AREA

- WEDGING & WIDENING

- TEMPORARY PAVEMENT

- PAVEMENT / BRIDGE REMOVAL

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)

- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW PANEL (TYPE C)
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- CHANGEABLE MESSAGE SIGN
- PORTABLE CONCRETE BARRIER (ANCHORED)

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

PAVEMENT MARKERS

- CRYSTAL / CRYSTAL
- CRYSTAL / RED
- YELLOW / YELLOW

LEGEND

TEMPORARY PAVEMENT MARKING

WORK ZONE PERFORMANCE *	
Z7	10' WHITE SKIP LINE (6")
Z8	WHITE SOLID LINE (6")
Z9	YELLOW SOLID LINE (6")

* INSTALL WORK ZONE PERFORMANCE COLD APPLIED PLASTIC (TYPE IV) ON BRIDGE DECK AREAS (SEE TMP SHEET 02 LOCAL NOTE 3)

NOTE: SEE WORK ZONE PERFORMANCE MARKING SPECIAL PROVISION

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APPROVED: DATE: 6/10/2019 SEAL			<h1 style="margin: 0;">ROADWAY STANDARD DRAWINGS & LEGEND</h1>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED			

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

A) TIME RESTRICTIONS - INTERMEDIATE CONTRACT TIME PROJECT SPECIAL PROVISIONS

THE CONTRACTOR SHALL COMPLETE THE REQUIRED WORK OF INSTALLING, MAINTAINING, AND REMOVING THE TRAFFIC CONTROL DEVICES FOR LANE CLOSURES AND RESTORING TRAFFIC TO THE EXISTING PATTERN. EXCEPT FOR CONCRETE POURS ONLY, THE CONTRACTOR SHALL NOT CLOSE OR NARROW A LANE OF TRAFFIC ON I-26 DURING THE FOLLOWING TIME RESTRICTIONS:

DAY AND TIME RESTRICTIONS

MONDAY THRU SUNDAY 7:00 AM TO 9:00 PM

FOR LANE CLOSURES SPECIFICALLY REQUIRED FOR CONCRETE POURS, THE CONTRACTOR SHALL NOT CLOSE OR NARROW A LANE OF TRAFFIC ON EITHER DIRECTION OF I-26 DURING THE FOLLOWING TIME RESTRICTIONS:

DAY AND TIME RESTRICTIONS

MONDAY THRU SUNDAY 9:00 AM TO 7:00 PM

NOTE: NEITHER OF THE ABOVE TIME RESTRICTIONS APPLY TO I-26 WESTBOUND DURING THE LONG TERM LANE CLOSURE PERMITTED AND DESCRIBED IN THE RESPECTIVE ICT. SEE PROJECT PROPOSAL.

IN ADDITION, THE CONTRACTOR SHALL NOT CLOSE OR NARROW A LANE OF TRAFFIC ON I-26, DETAIN AND/OR ALTER THE TRAFFIC FLOW ON OR DURING HOLIDAY WEEKENDS, SPECIAL EVENTS, OR ANY OTHER TIME WHEN TRAFFIC IS UNUSUALLY HEAVY, INCLUDING THE FOLLOWING SCHEDULES:

B) HOLIDAY AND HOLIDAY WEEKEND LANE CLOSURE RESTRICTIONS

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S DAY, BETWEEN THE HOURS OF 7:00 AM DECEMBER 31ST AND 9:00 PM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN UNTIL 9:00PM THE FOLLOWING TUESDAY. (THIS RESTRICTION DOES NOT APPLY TO I-26 WB DURING THE LONG TERM LANE CLOSURE PERMITTED AND DESCRIBED IN THE INTERMEDIATE CONTRACT TIME. SEE PROJECT PROPOSAL.)
- FOR EASTER, BETWEEN THE HOURS OF 7:00 AM THURSDAY AND 9:00 PM MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00 AM FRIDAY AND 9:00 PM TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00 AM THE DAY BEFORE INDEPENDENCE DAY AND 9:00 PM THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN BETWEEN THE HOURS OF 7:00 AM THE THURSDAY BEFORE INDEPENDENCE DAY AND 9:00 PM THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 7:00 AM FRIDAY AND 9:00 PM TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 7:00 TUESDAY AND 9:00 PM MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00 AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 9:00 PM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS DAY. (THIS RESTRICTION DOES NOT APPLY TO I-26 WB DURING THE LONG TERM LANE CLOSURE PERMITTED AND DESCRIBED IN ICT. SEE PROJECT PROPOSAL.)
- FOR THE CHRISTMAS RETAIL SEASON (BEGINS THE FIRST THURSDAY FOLLOWING THANKSGIVING DAY THROUGH DECEMBER 18TH), THURSDAYS THROUGH SUNDAYS BETWEEN THE HOURS OF 7:00 AM THURSDAY AND 9:00 PM THE FOLLOWING SUNDAY.
- FOR THE NORTH CAROLINA MOUNTAIN STATE FAIR (TYPICALLY HELD FOR 10 DAYS STARTING THE FRIDAY AFTER LABOR DAY), BETWEEN THE HOURS OF 7:00 AM THE FRIDAY FOLLOWING LABOR DAY AND 9:00 PM THE FOLLOWING MONDAY AFTER THE FAIR CONCLUDES.

HOLIDAYS AND HOLIDAY WEEKENDS SHALL INCLUDE NEW YEAR'S, EASTER, MEMORIAL DAY, INDEPENDENCE DAY, LABOR DAY, THANKSGIVING, AND CHRISTMAS. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT LANE CLOSURES WILL NOT BE REQUIRED DURING THESE PERIODS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE TIME OF AVAILABILITY FOR THIS INTERMEDIATE CONTRACT WORK SHALL BE THE TIME THE CONTRACTOR BEGINS TO INSTALL ALL TRAFFIC CONTROL DEVICES FOR LANE CLOSURES ACCORDING TO THE TIME RESTRICTIONS LISTED HEREIN.

THE COMPLETION TIME FOR THIS INTERMEDIATE CONTRACT WORK SHALL BE THE TIME THE CONTRACTOR IS REQUIRED TO COMPLETE THE REMOVAL OF ALL TRAFFIC CONTROL DEVICES FOR LANE CLOSURES ACCORDING TO THE TIME RESTRICTIONS STATED ABOVE AND PLACE TRAFFIC IN THE EXISTING TRAFFIC PATTERN.

C) DO NOT CONDUCT SINGLE VEHICLE HAULING AS FOLLOWS; INGRESS AND EGRESS FROM RAMPS WILL NOT BE ALLOWED:

ROAD NAME	DAY AND TIME RESTRICTIONS
I-26	MON-FRI 7:00 AM-9:00 AM & 4:00 PM - 6:00 PM

D) DO NOT CONDUCT MULTI-VEHICLE HAULING AS FOLLOWS; INGRESS AND EGRESS FROM RAMPS WILL NOT BE ALLOWED:

ROAD NAME	DAY AND TIME RESTRICTIONS
I-26	MON-FRI 7:00 AM-9:00 AM & 4:00 PM - 6:00 PM

E) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER. LARGE HAULING OPERATIONS SHALL OCCUR DURING ALLOWABLE NIGHTTIME HOURS USING TEMPORARY SINGLE LANE CLOSURE OR AS DIRECTED BY THE ENGINEER. HAULING FOR DELIVERY OF CONCRETE MAY OCCUR DURING PEAK TRAVEL HOURS UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

F) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.

H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

J) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

L) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

M) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

N) FOR STATIONARY AND PORTABLE WORK ZONE SIGNS, SEE HIGH VISIBILITY DEVICES SPECIAL PROVISION.

O) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

P) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE OR SHIFT LANES ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

Q) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE OR SHIFT LANES WHEN ROAD CLOSURE OR LANE SHIFT IS NOT IN OPERATION.

R) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

S) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 350 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

T) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

U) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

V) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS, AND HIGH VISIBILITY DEVICES SPECIAL PROVISION.

W) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY WIDTH REQUIRED.

X) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

Y) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
I-26	WORK ZONE PERFORMANCE	TEMPORARY RAISED
I-26	W.Z.PERF. COLD APPLIED PLASTIC (IV)-BRIDGE DECKS	TEMPORARY RAISED

Z) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

AA) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

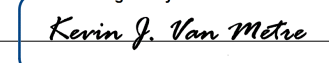
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BB) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AS DIRECTED BY THE ENGINEER.

LOCAL NOTES

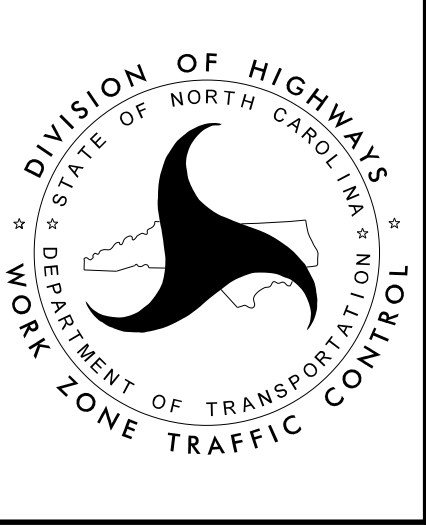
- COORDINATE WITH THE ENGINEER, TRAFFIC MANAGEMENT CENTER (TMC) AND EMERGENCY SERVICE PERSONNEL REGARDING ALL TRAFFIC ALTERATIONS.
- COORDINATE WITH THE ENGINEER TO ENSURE THAT THE OVERSIZE/OVERWEIGHT PERMIT UNIT IS AWARE OF ONGOING TRAFFIC OPERATIONS AND CLEAR ROADWAY WIDTHS THROUGHOUT CONSTRUCTION.
- WHEN USING REMOVABLE TAPE ON BRIDGE DECK, INSTALL TEMPORARY RAISED MARKERS HALF-ON/ HALF-OFF AT 20' SPACING TO HOLD TAPE IN PLACE.
- REFER TO ROADWAY PLAN SHEETS FOR ALL PAVEMENT DETAILS.
- FOR LANE CLOSURES ON I-26, USE CONNECTED LANE CLOSURE DEVICES, (SEE SPECIAL PROVISION).
- FOR DRUMS IN MERGE TAPERS, SEE SEQUENTIAL FLASHING WARNING LIGHTS SPECIAL PROVISION.

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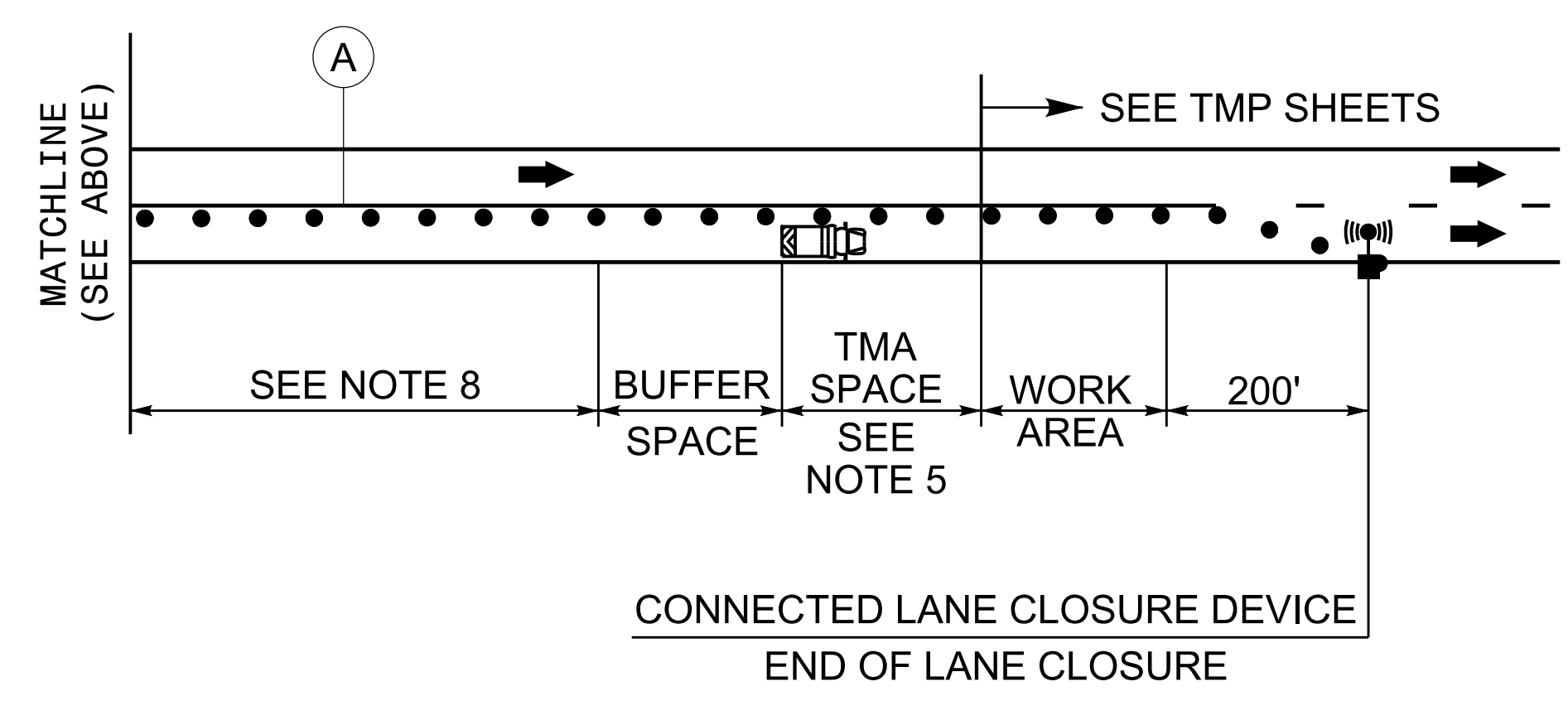
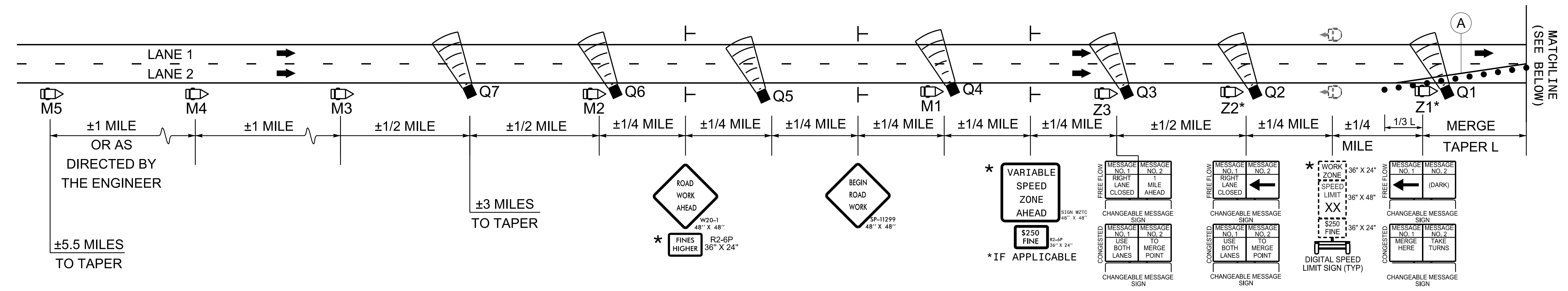
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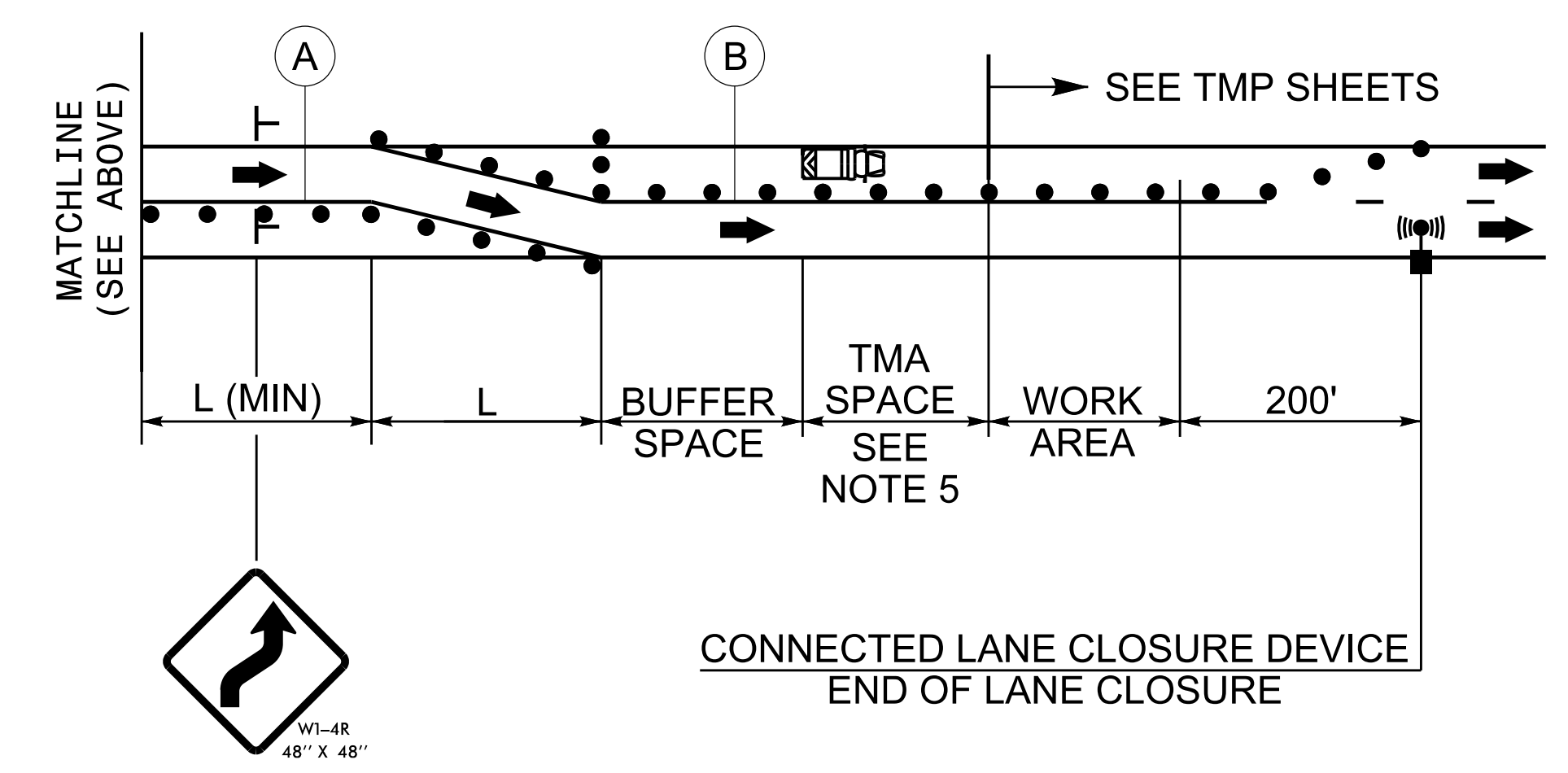
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PROJECT NOTES



RIGHT LANE WORK AREA



LEFT LANE WORK AREA (IF APPLICABLE)

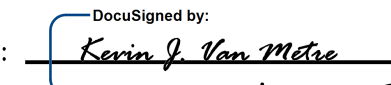
GENERAL NOTES

- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE WORK AREA AT THE MAXIMUM SPACING EQUAL TO 2 TIMES THE POSTED SPEED LIMIT.
- REFER TO RSD. 1101.11, SHEETS 1 & 2, FOR "L" DISTANCE AND BUFFER SPACE.
- REFER TO RSD. 1101.02, SHEETS 9 & 10, FOR TREATMENT OF LANE CLOSURES THRU INTERCHANGES.
- INSTALL LANE CLOSURES WITH THE FLOW OF TRAFFIC, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE FLOW OF TRAFFIC, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- POSITION THE TMAS TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER AND CONTINUOUSLY ADVANCE TMAS AS WORK PROGRESSES. USE TMAS AS REQUIRED TO SHIELD WORKERS FROM ERRANT VEHICLES AND TRAFFIC FROM POTENTIAL HAZARDS IN THE WORK AREA.
- REMOVE OR COVER EXISTING ADVANCED WARNING SIGNS PREVIOUSLY INSTALLED USING RSD 1101.01, SHEET 2 OF 3.
- IN THE EVENT OF A SYSTEM MALFUNCTION, IMMEDIATELY INSTALL WORK ZONE SIGNS ACCORDING TO RSD 1101.02, SHEET 4. SIGNAGE SHOULD REMAIN IN PLACE UNTIL SYSTEM OPERATION IS FULLY RESTORED.
- IF A LEFT LANE WORK AREA WILL BE NEEDED AT ANY TIME DURING THE PROJECT, USE 2L. IF NO LEFT LANE WORK AREA IS NEEDED, OMIT THIS SPACE.
- REMOVE ANY CONFLICTING PAVEMENT MARKINGS & MARKERS.

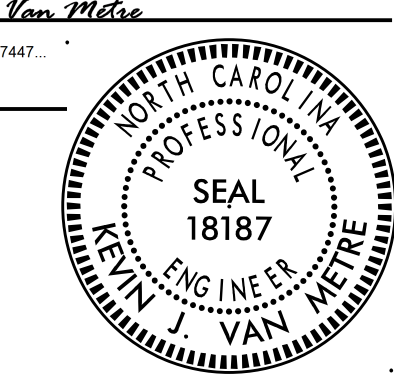
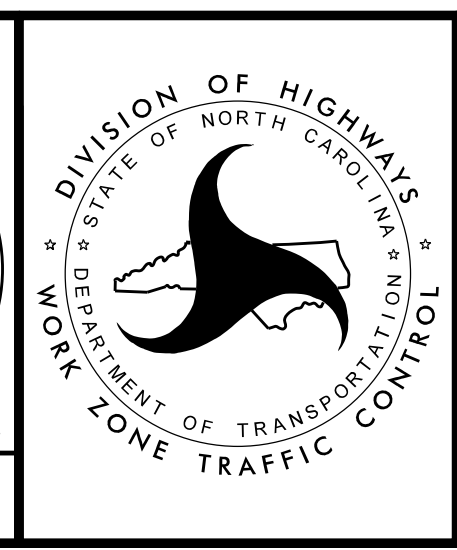
LEGEND

- SPEED SENSOR Q# (1-7)
- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)
M# (1-5) = MAINLINE
Z# (1-3) = ZIPPER MERGE
*Z1 & Z2 SHALL BE FULL MATRIX
- CONNECTED LANE CLOSURE DEVICE
- DRUM
- TRUCK MOUNTED ATTENUATOR (TMA)
- STATIONARY SIGN

- (A) 6" WHITE EDGELINE
- (B) 6" YELLOW EDGELINE

APPROVED: 
DATE: 3/10/2020

SEAL

**DYNAMIC ZIPPER MERGE
SYSTEM LAYOUT**

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PROJ. REFERENCE NO.	SHEET NO.
15BPR.20	TMP-03

STAGE I

STAGE IA TEMPORARY OUTSIDE SHOULDER RECONSTRUCTION

- PRIOR TO ANY CONSTRUCTION ACTIVITY, INSTALL WORK ZONE ADVANCED WARNING SIGNS ON I-26. FOR SIGN LOCATIONS, SEE TMP DETAILS. FOR ADDITIONAL INFORMATION, SEE RSD 1101.01, SHEET 2 AND THE HIGH VISIBILITY DEVICES SPECIAL PROVISION.
- DURING ALLOWABLE HOURS ONLY CLOSE OUTSIDE LANE TO TRAFFIC (UTILIZE FIGURE 1101.02).
- SAW CUT EXISTING SHOULDER PAVEMENT ADJACENT TO TRAVEL WAY.
- REMOVE EXISTING PAVMENT AND CONSTRUCT TEMPORARY PAVEMENT AS SHOWN TO FULL DEPTH TO ACCOMMODATE SHIFTING TRAFFIC IN PROCEEDING STAGE.
- APPLY TEMPORARY PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS.

STAGE IB TEMPORARY INSIDE SHOULDER & TEMPORARY PAVEMENT CONSTRUCTION

- DURING ALLOWABLE HOURS ONLY CLOSE INSIDE LANE TO TRAFFIC (UTILIZE FIGURE 1101.02).
- SAW CUT EXISTING SHOULDER PAVEMENT ADJACENT TO TRAVEL WAY.
- DEMOLISH ALL APPLICABLE FEATURES AND CONSTRUCT TEMPORARY DRAINAGE.
- REMOVE EXISTING PAVEMENT AND CONSTRUCT TEMPORARY PAVEMENT AS SHOWN TO ACCOMMODATE TEMPORARY LANE SHIFTS. BASED UPON THE CONTRACTOR'S MEANS AND METHODS, TEMPORARY DIVERSION OF EXISTING ROADWAY SURFACE RUNOFF AWAY FROM THE MEDIAN AREA MAY BE REQUIRED. CONSTRUCTION OF PAVEMENT MILLING AND TEMPORARY OVERLAY WEDGING ALONG THE INSIDE EB AND WB TRAVEL LANES, BETWEEN THE EXISTING TRAVELWAY CROWN POINT TO THE TEMPORARY PAVEMENT EDGE. CONSTRUCTION OF PAVEMENT MILLING AND TEMPORARY OVERLAY WEDGING WOULD REQUIRE USING ONE LANE TRAFFIC CONTROL DURING ALLOWABLE LANE CLOSURE HOURS.
- INSTALL TEMPORARY BARRIER AS SHOWN.
- APPLY TEMPORARY PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS.

STAGE IC SUPERSTRUCTURE DEMOLITION & CONSTRUCTION

- DURING ALLOWABLE HOURS ONLY CLOSE INSIDE LANE TO TRAFFIC (UTILIZE FIGURE 1101.02).

THE CONTRACTOR SHALL NOT PLACE BARRIERS ON THE BRIDGE UNTIL FEBRUARY 18, 2022.

- INSTALL TEMPORARY TRAFFIC BARRIER ON EXISTING BRIDGE DECK (BAY 1 & BAY 3) AS SHOWN (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IA) AND CONNECT TO TEMPORARY BARRIER INSTALLED IN STAGE IA. CONTRACTOR SHALL COORDINATE AND ADJUST THE LOCATION OF THE DRAINAGE SLOT UNDER THE TEMPORARY BARRIER TO SPAN OVER THE EXISTING SCUPPER HOLES IN THE BRIDGE DECK.
- APPLY TEMPORARY PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS.
- SHIFT TRAFFIC AS SHOWN (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IA). CONTRACTOR SHALL MAINTAIN TWO LANES OF TRAFFIC IN EACH DIRECTION DURING REQUIRED HOURS.
- DEMOLISH ALL APPLICABLE FEATURES IN BAY 1 & BAY 3 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IA).

STAGE ID SUPERSTRUCTURE DEMOLITION & CONSTRUCTION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF TRAFFIC IN BAY 1 AND BAY 3 IN EACH DIRECTION DURING REQUIRED HOURS.
- INSTALL BRIDGE STRINGERS AND CONCRETE DECK IN BAY 2 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIA).
- UPON COMPLETION OF CONSTRUCTING BAY 2 CONCRETE DECK CONTRACTOR SHALL INSTALL TEMPORARY BARRIER AS SHOWN (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIA).

THE CONTRACTOR SHALL ONLY SHIFT TO A SINGLE LANE CONFIGURATION IN THE WESTBOUND DIRECTION IN ACCORDANCE WITH THE ICT.

- APPLY TEMPORARY PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS INCLUDING IN LANE CHARACTERS. COVER EXISTING SIGNS AS DESCRIBED IN SPECIAL PROVISIONS. INSTALL DYNAMIC ZIPPER MERGE SYSTEM. (SEE SPECIAL PROVISIONS).
- ACTIVATE DYNAMIC LANE MERGE SYSTEM AND SHIFT TRAFFIC AS SHOWN (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIA).

STAGE II

STAGE IIA SUPERSTRUCTURE DEMOLITION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 1 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN ONE LANE OF WB TRAFFIC IN BAY 2 ON I-26 IN THE NEWLY CONSTRUCTED BAY 2 LOCATION.
- DEMOLISH ALL APPLICABLE FEATURES IN BAY 3 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIA).

STAGE IIB SUPERSTRUCTURE CONSTRUCTION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 1 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN ONE LANE OF WB TRAFFIC IN BAY 2 ON I-26 IN THE NEWLY CONSTRUCTED BAY 2 LOCATION.
- CONSTRUCT ALL APPLICABLE FEATURES IN BAY 3 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIB).
- CONSTRUCT WESTBOUND ROADWAY BRIDGE APPROACH PAVEMENT MILLING, PAVING AND WIDENING WHILE MAINTAINING POSITIVE DRAINAGE RUNOFF AND CONSTRUCTION ACCESS NEEDS.

STAGE IIC SUPERSTRUCTURE CONSTRUCTION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 1 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN ONE LANE OF WB TRAFFIC IN BAY 2 ON I-26 IN THE NEWLY CONSTRUCTED BAY 2 LOCATION.
- INSTALL ALL APPLICABLE FEATURES IN THE BAY 3 LOCATION (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIC).
- CONSTRUCT ALL REMAINING FEATURES IN BAY 3 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIC).
- UPON COMPLETION OF CONSTRUCTING BAY 3 CONCRETE DECK CONTRACTOR SHALL INSTALL BRIDGE PARAPET, TEMPORARY BARRIER (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIC).
- SHIFT TEMPORARY BARRIERS AND SIGNING TRAFFIC BACK TO PRIOR STAGE CONFIGURATION AND CONSTRUCT EB TEMPORARY PAVEMENT AS SHOWN TO FULL DEPTH TO ACCOMODATE SHIFTING TRAFFIC ON NEXT STAGE.
- CONSTRUCT TEMPORARY MEDIAN DITCH BETWEEN THE PORTABLE CONCRETE BARRIERS BEGINNING AT STATION 20+00 +/- AND ENDING AT STATION 21+50 +/-.
- CONSTRUCT TEMPORARY PAVEMENT WEDGE TO CREATE LOW POINT AT FACE OF PORTABLE CONCRETE BARRIER FOR TEMPORARY INLET IN THE VICINITY OF STATION 43+00.
- APPLY TEMPORARY PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS.
- SHIFT TEMPORARY BARRIER, SIGNING AND TRAFFIC AS SHOWN (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIIA).

THE CONTRACTOR SHALL COMPLETE THE STAGE II WORK AND SHIFT WESTBOUND TRAFFIC BACK ONTO BAY 3 IN ACCORDANCE WITH THE ICT.

- REMOVE DYNAMIC ZIPPER MERGE SYSTEM. UNCOVER SIGNS AND REPLACE IN LANE PAVEMENT MARKING CHARACTERS PREVIOUSLY REMOVED.

STAGE III

STAGE IIIA SUPERSTRUCTURE DEMOLITION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 2 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN TWO LANES OF WB TRAFFIC IN BAY 3 ON I-26 IN THE NEWLY CONSTRUCTED BAY 3 LOCATION.
- DEMOLISH ALL APPLICABLE FEATURES IN BAY 1 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIIA).

STAGE IIIB SUPERSTRUCTURE CONSTRUCTION

- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 2 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN TWO LANES OF WB TRAFFIC IN BAY 3 ON I-26 IN THE NEWLY CONSTRUCTED BAY 3 LOCATION.
- CONSTRUCT ALL APPLICABLE FEATURES IN BAY 1 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIIB).
- CONSTRUCT EASTBOUND ROADWAY BRIDGE APPROACH PAVEMENT MILLING, PAVING AND WIDENING WHILE MAINTAINING POSITIVE DRAINAGE RUNOFF AND CONSTRUCTION ACCESS NEEDS.

STAGE IIIC SUPERSTRUCTURE CONSTRUCTION

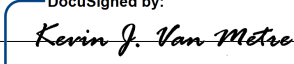
- CONTRACTOR SHALL MAINTAIN TWO LANES OF EB TRAFFIC IN BAY 2 ON I-26 DURING REQUIRED HOURS.
- CONTRACTOR SHALL MAINTAIN TWO LANES OF WB TRAFFIC IN BAY 3 ON I-26 IN THE NEWLY CONSTRUCTED BAY 3 LOCATION.
- CONSTRUCT ALL REMAINING FEATURES IN BAY 1 (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIIC).
- UPON COMPLETION OF CONSTRUCTING BAY 1 CONCRETE DECK CONTRACTOR SHALL INSTALL BRIDGE PARAPET AND REMOVE TEMPORARY BARRIER (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE STAGE IIIC).
- APPLY TEMPORARY PAVEMENT MARKINGS.
- SHIFT TRAFFIC AS SHOWN IN STAGE IV.

STAGE IV

STAGE IV BRIDGE MEDIAN BARRIER AND OVERLAY CONSTRUCTION)

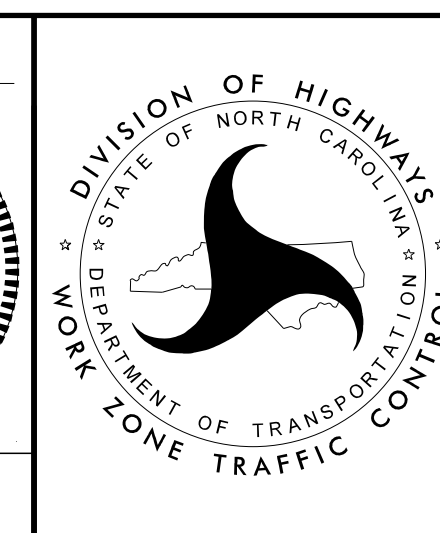
- INSTALL PERMANENT MEDIAN BARRIER (SEE SUPERSTRUCTURE DEMOLITION & CONSTRUCTION SEQUENCE FINAL STAGE).
- REMOVE TEMPORARY PAVEMENT WITHIN APPROACH MEDIAN AREAS.
- CONSTRUCT ROADWAY APPROACH MEDIAN AREAS AND BRIDGE PPC OVERLAY (UTILIZE FIGURE 1101.02).
- APPLY FINAL PAVEMENT MARKINGS AND ERADICATE ALL EXISTING CONFLICTING PAVEMENT MARKINGS AND OPEN ROAD TO FINAL PATTERN.

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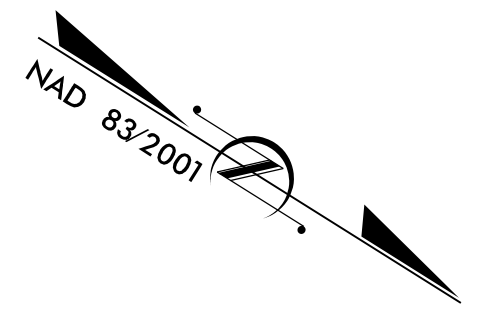
APPROVED: 
 DATE: 11/16/2020

SEAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



PROJECT PHASING



END ROAD WORK

500' +/- IN ADVANCE OF STATION 10+00 +/-
THIS SIGN SHALL REMAIN IN-PLACE THROUGHOUT CONSTRUCTION

G20-2 A
48" X 24"

LEFT SHOULDER CLOSED

W21-5A L
48" X 48"

1500 FEET

W16-2
30" X 24"

BEGIN ROAD WORK

SP-11299
48" X 48"

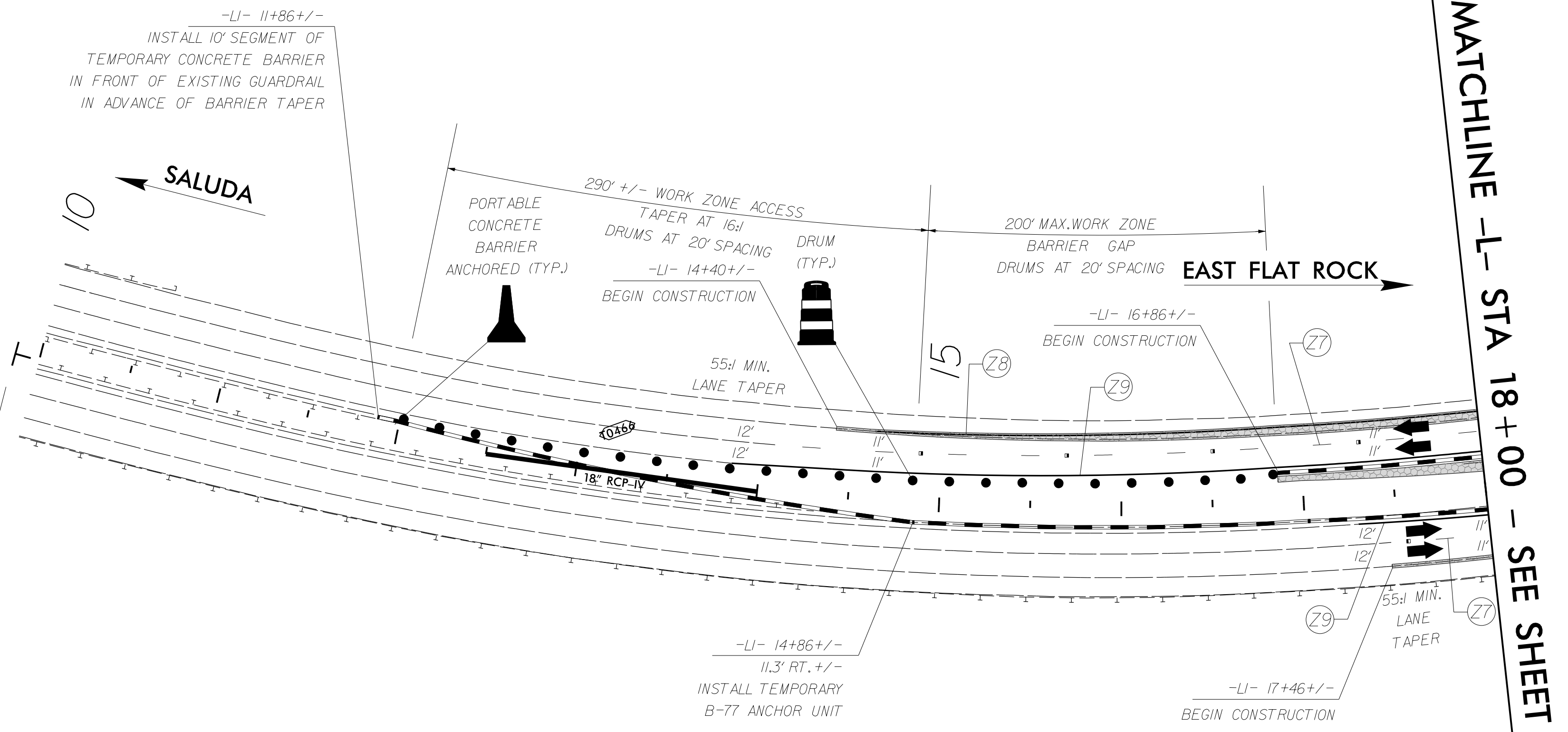
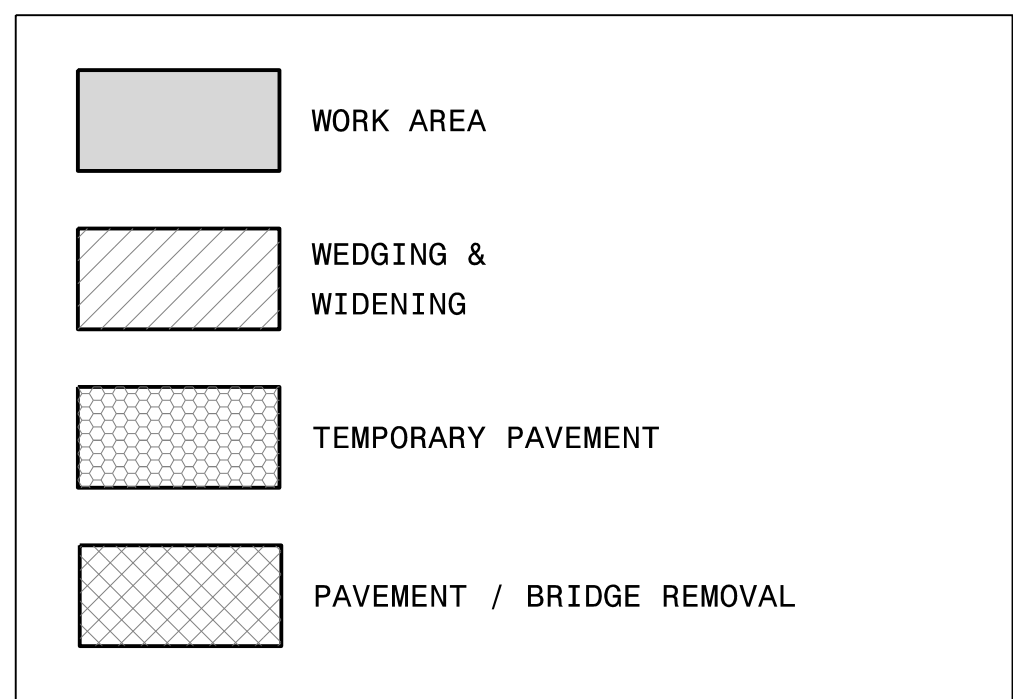
1380' +/- IN ADVANCE OF STATION 10+00 +/-
THIS SIGN SHALL REMAIN IN-PLACE THROUGHOUT CONSTRUCTION

LEFT SHOULDER CLOSED

W21-5A L
48" X 48"

NEXT 4600 FT

W16-4
30" X 24"



MATCHLINE -L- STA 18+00 - SEE SHEET 5

6/7/2019
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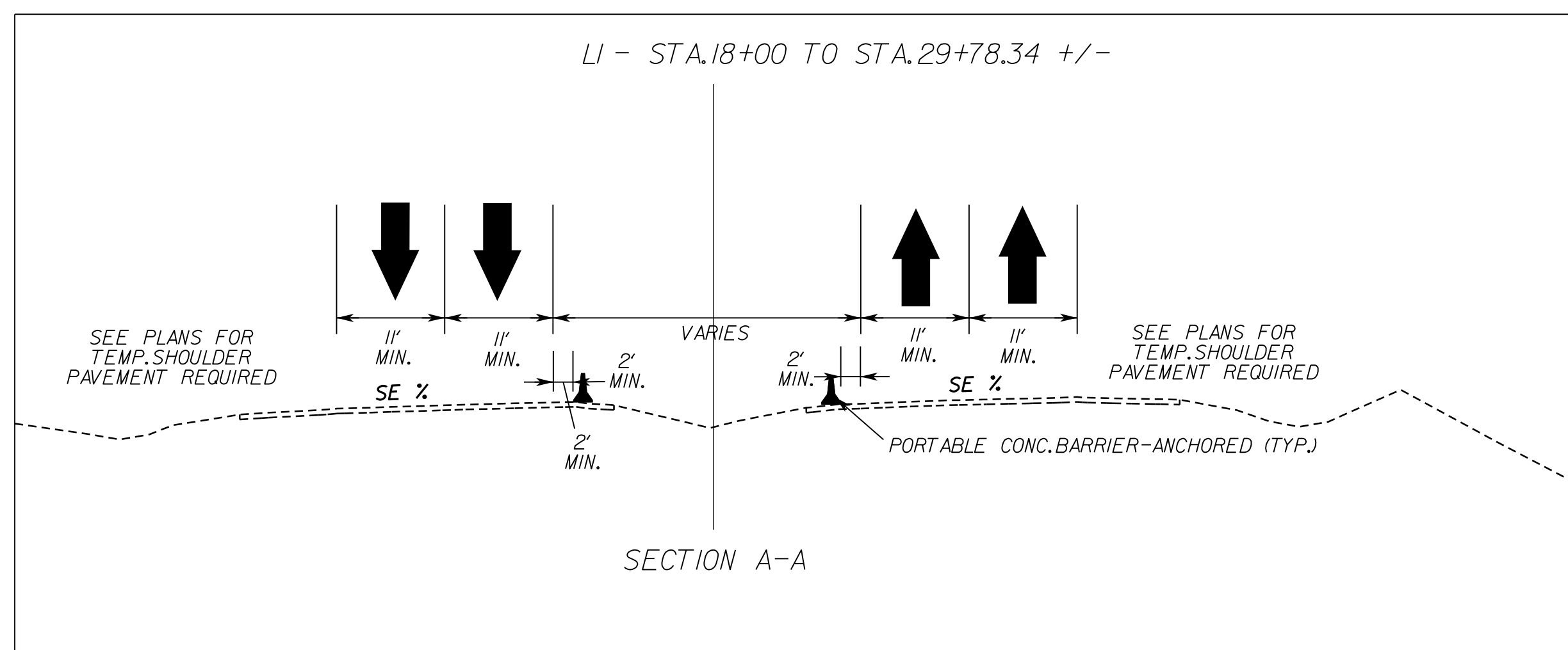
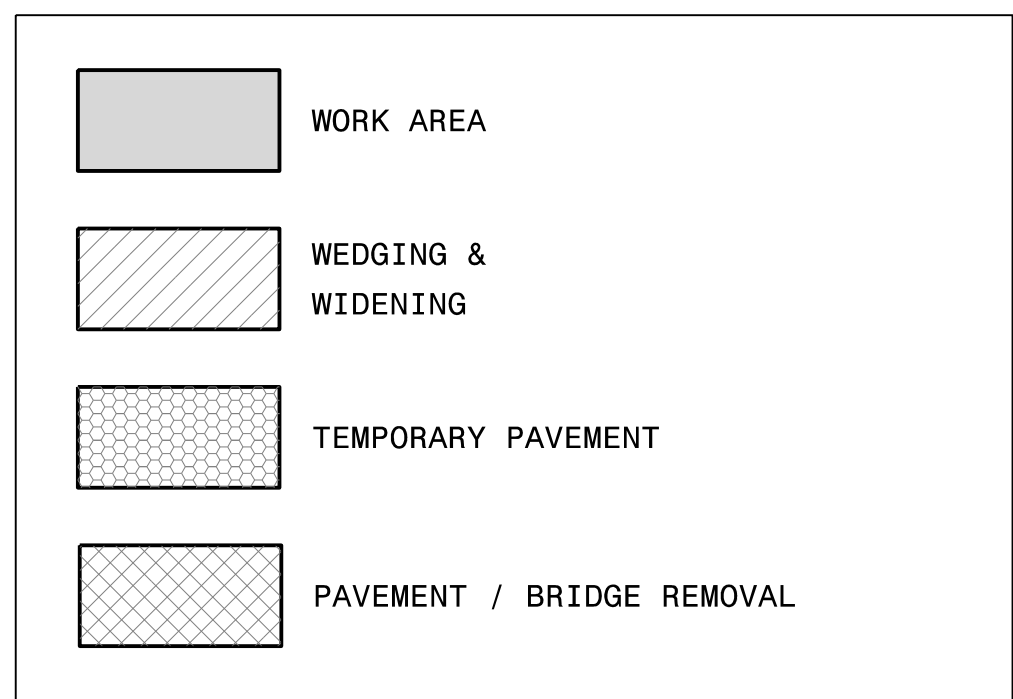
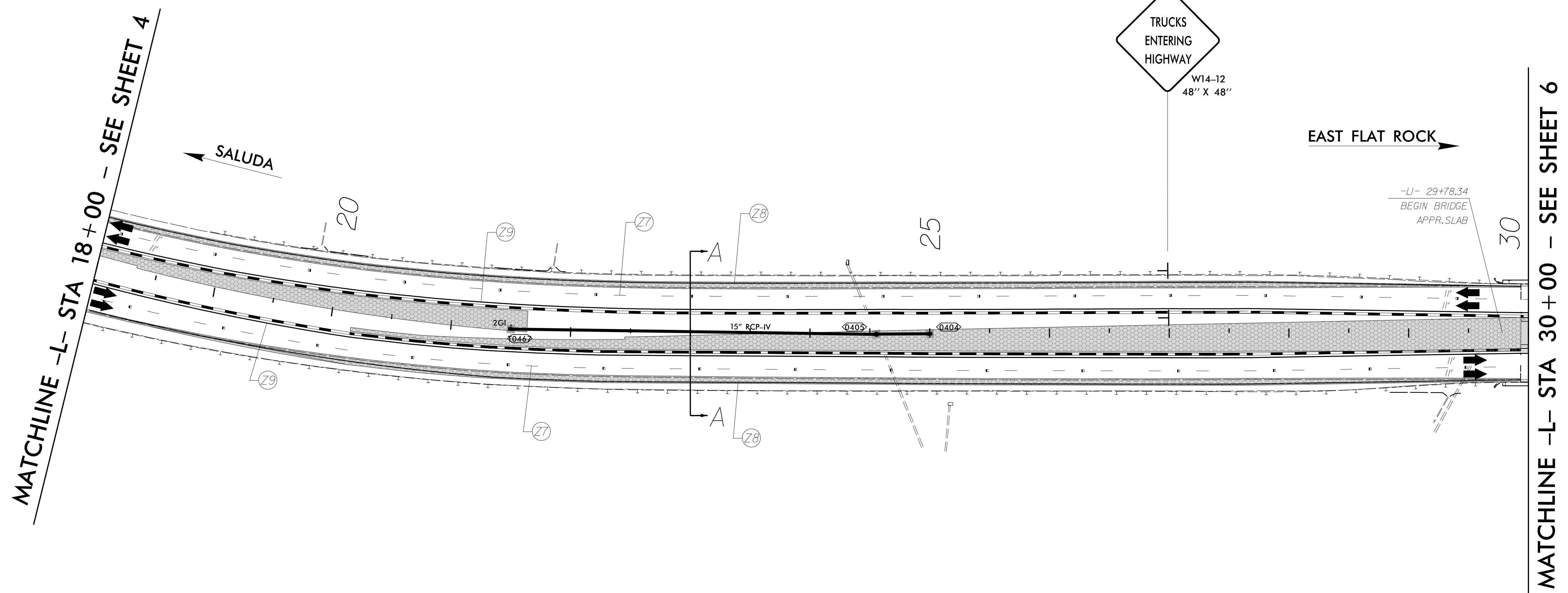
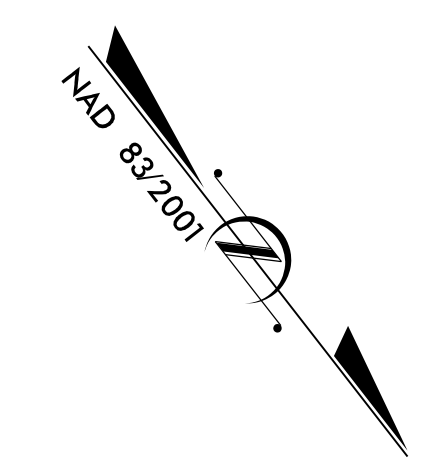
APPROVED: *Kevin J. VanMetre*
DATE: 6/10/2019

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STAGE I OVERVIEW & DETAILS

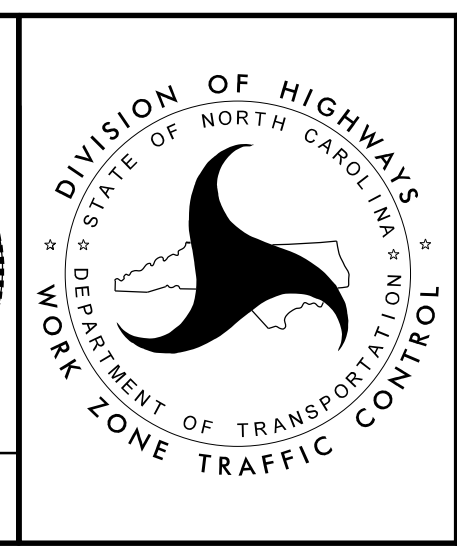


APPROVED: *Kevin J. VanMetre*
FFDESAT789337447

DATE: 6/10/2019

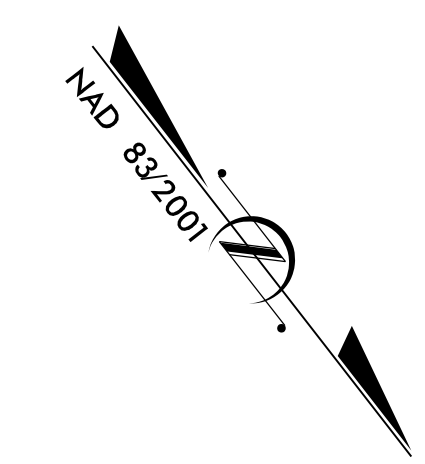
SEAL

SEAL
 18187
 KEVIN J. VAN METRE
 PROFESSIONAL ENGINEER
 NORTH CAROLINA



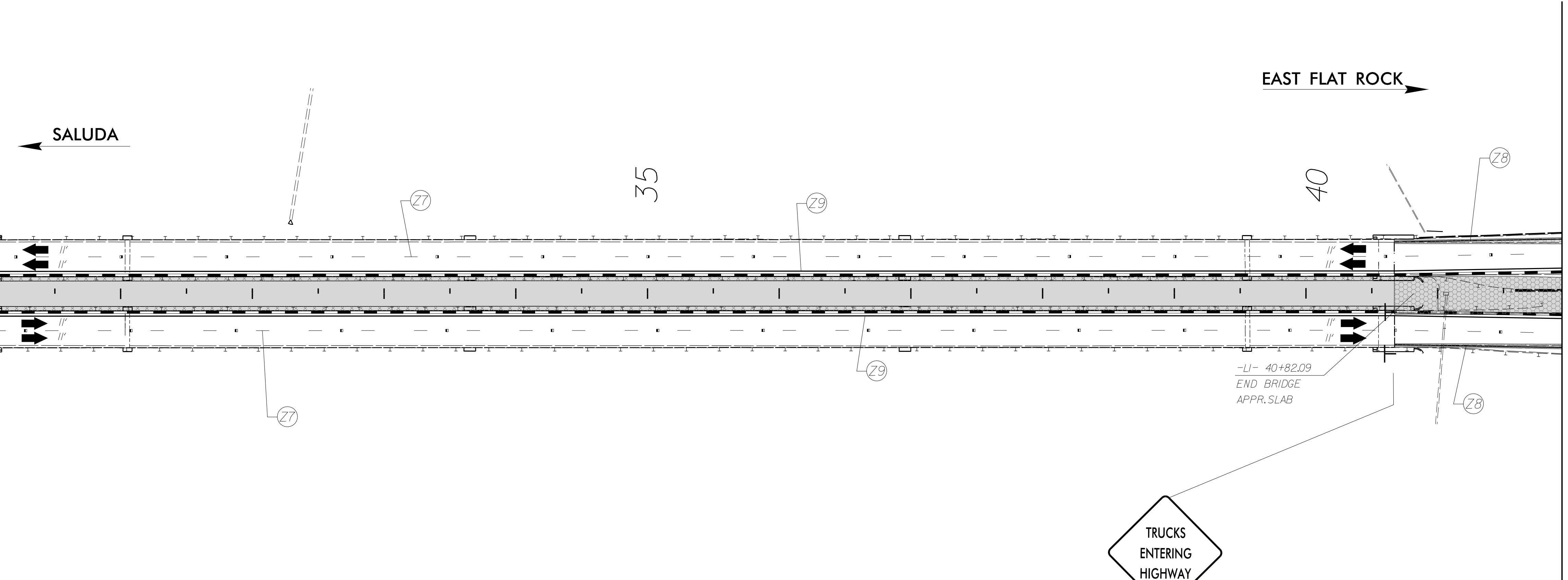
**STAGE I
OVERVIEW
& DETAILS**

6/7/2019
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 User:Jeffrey.Thurston



MATCHLINE -L- STA 30+00 - SEE SHEET 5

MATCHLINE -L- STA 42+00 - SEE SHEET 7

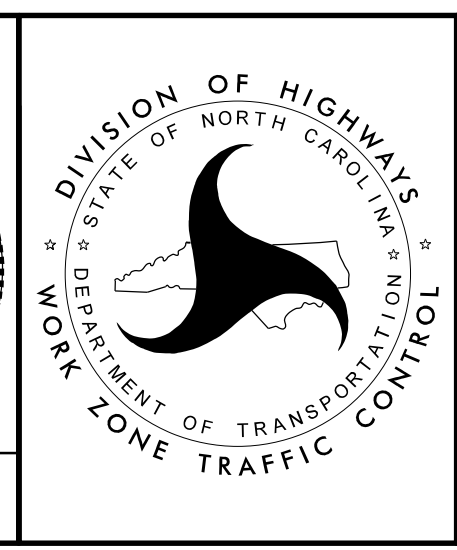


	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

6/7/2019
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APPROVED: *Kevin J. VanMetre*
 DATE: 6/10/2019

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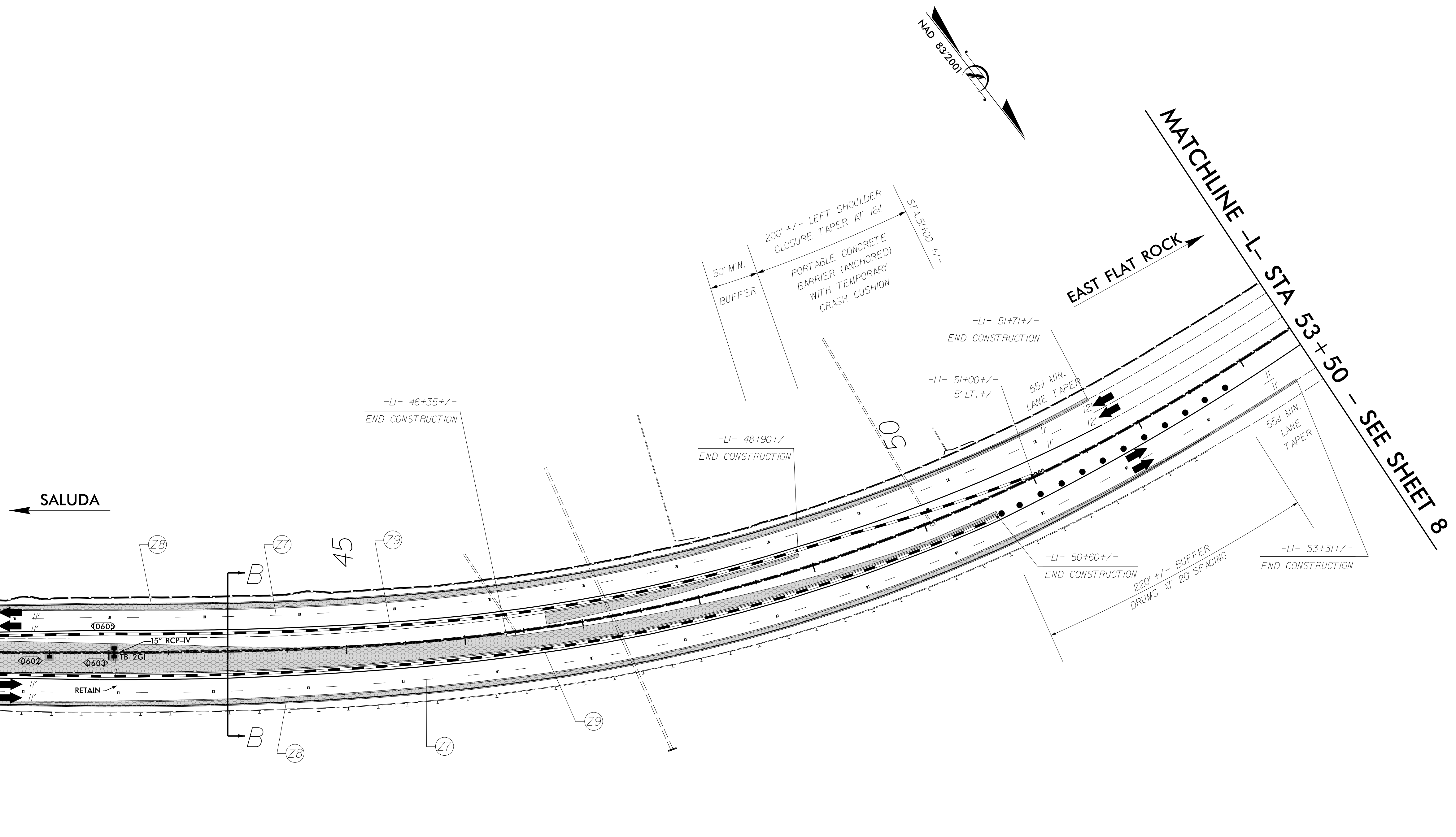


**STAGE I
 OVERVIEW
 & DETAILS**

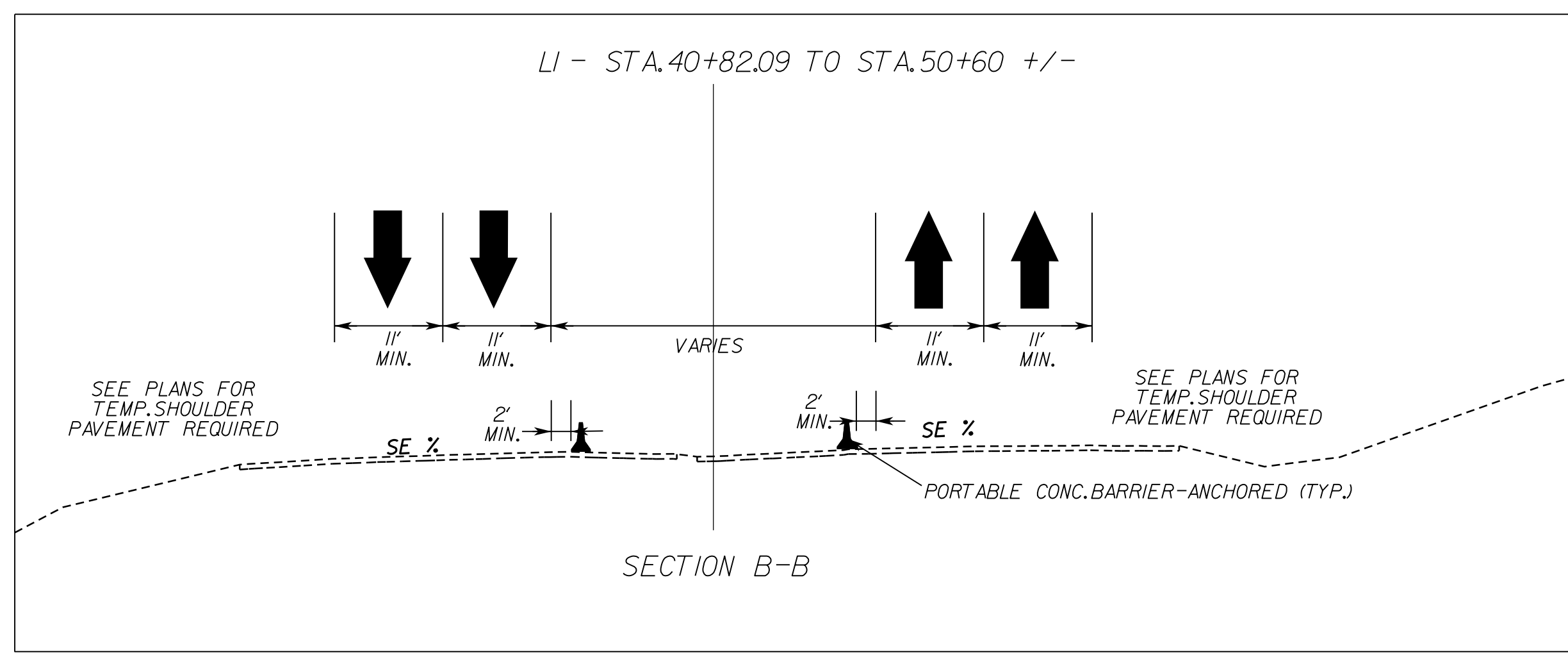
**DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED**

MATCHLINE -L- STA 42+00 - SEE SHEET 6

MATCHLINE -L- STA 53+50 - SEE SHEET 8



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

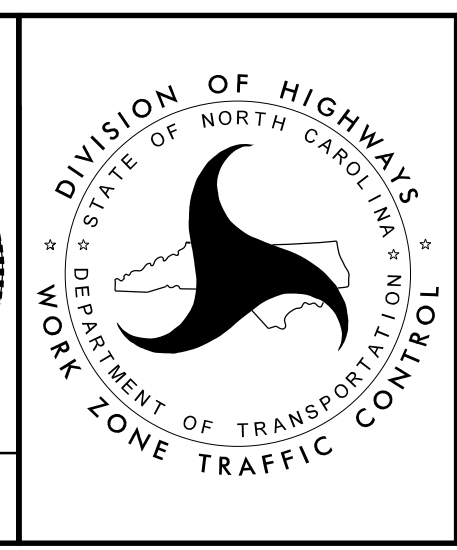


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DATE: 6/10/2019

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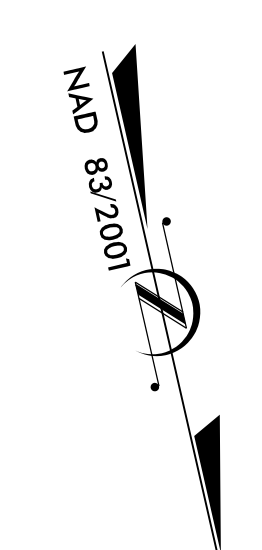
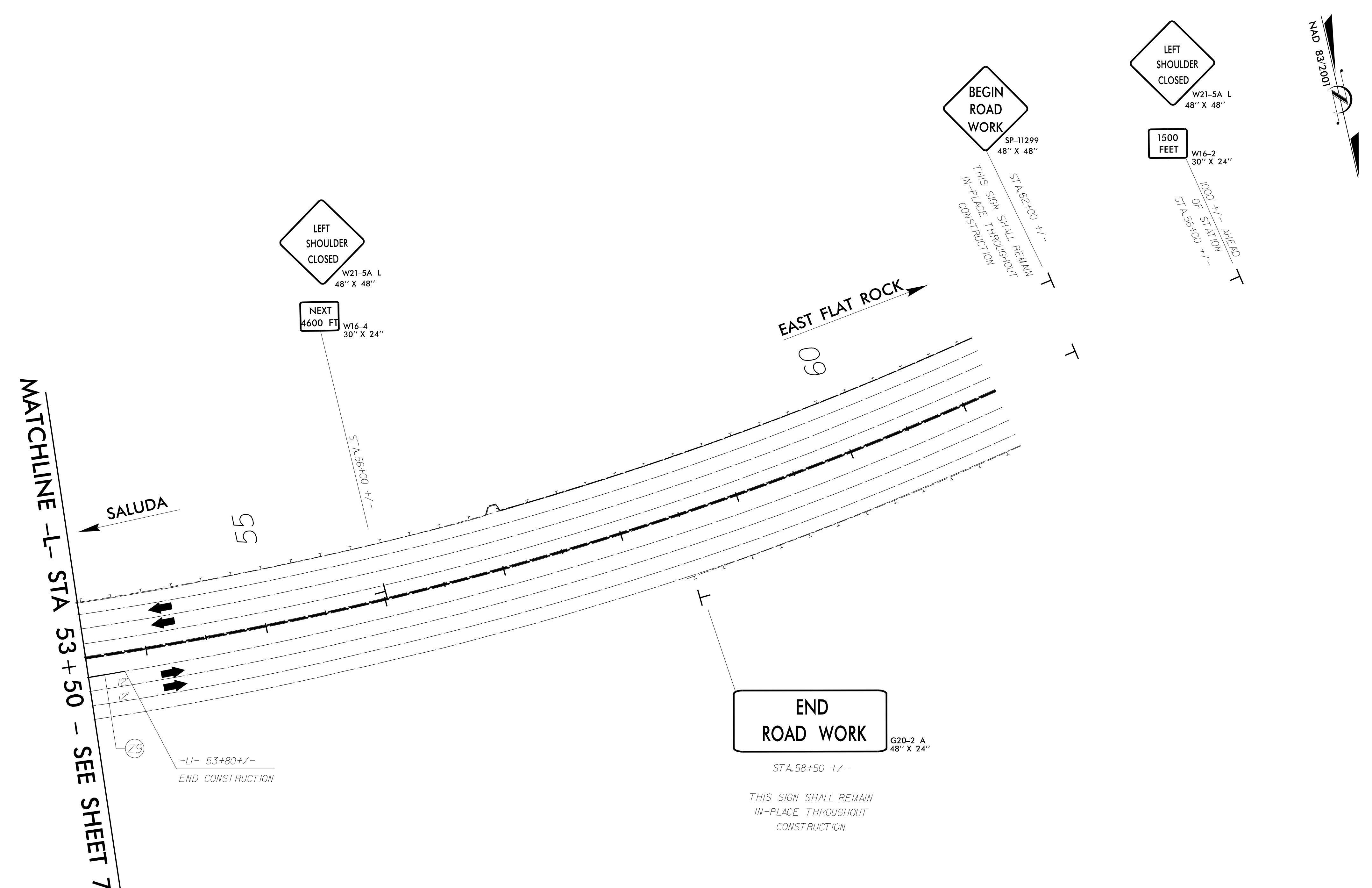
PROFESSIONAL ENGINEER
SEAL 18187
KEVIN J. VANMETRE

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STAGE I OVERVIEW & DETAILS

6/7/2019
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 User:Jeffrey.Thurston



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

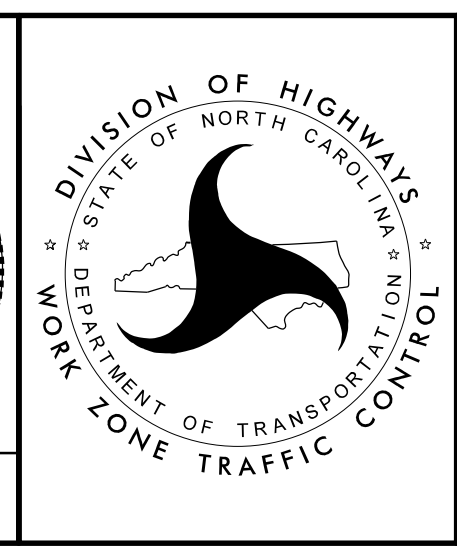
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DATE: 6/10/2019

SEAL

SEAL 18187
 KEVIN J. VANMETRE
 PROFESSIONAL ENGINEER
 NORTH CAROLINA

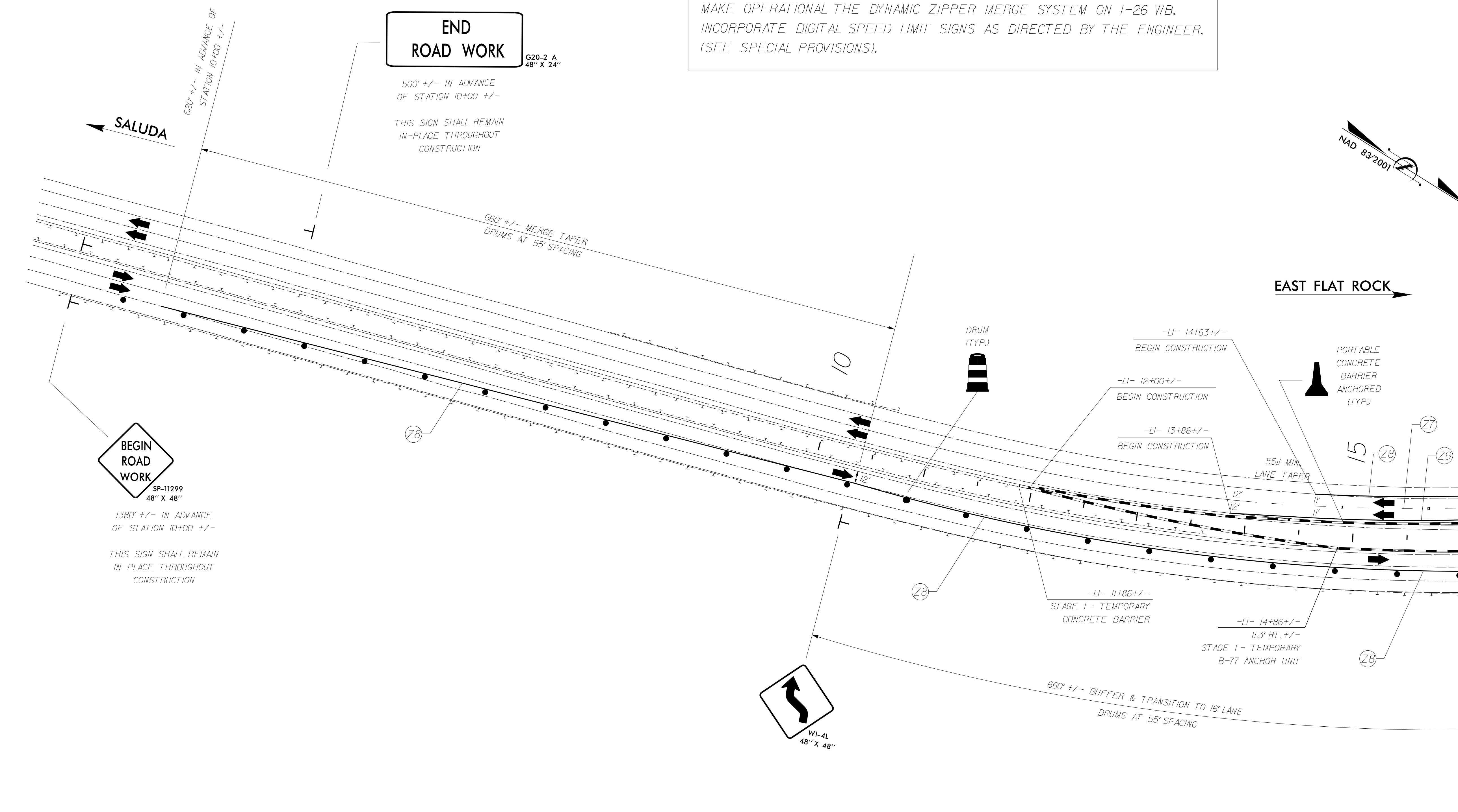
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**STAGE I
 OVERVIEW
 & DETAILS**

6/7/2019
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 User:Jeffrey.Thurston

PRIOR TO IMPLEMENTING STAGE II TRAFFIC PATTERN, INSTALL AND MAKE OPERATIONAL THE DYNAMIC ZIPPER MERGE SYSTEM ON I-26 WB. INCORPORATE DIGITAL SPEED LIMIT SIGNS AS DIRECTED BY THE ENGINEER. (SEE SPECIAL PROVISIONS).



BEGIN ROAD WORK
 SP-11299
 48" X 48"
 1380' +/- IN ADVANCE OF STATION 10+00 +/-
 THIS SIGN SHALL REMAIN IN-PLACE THROUGHOUT CONSTRUCTION

END ROAD WORK
 G20-2 A
 48" X 24"
 500' +/- IN ADVANCE OF STATION 10+00 +/-
 THIS SIGN SHALL REMAIN IN-PLACE THROUGHOUT CONSTRUCTION

	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

NOTES:

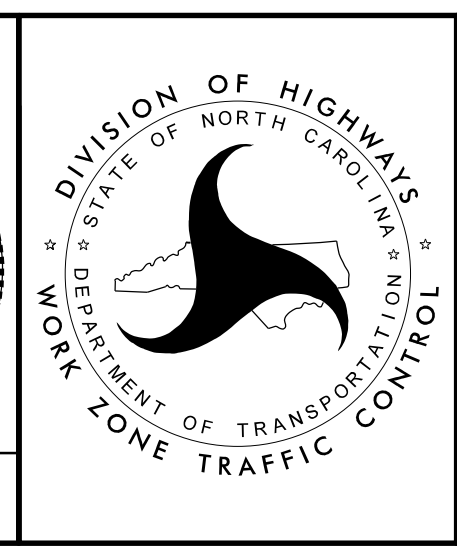
- COVER EXISTING SIGNS AS REQUIRED BY THESE PLANS AND AS DESCRIBED IN THE SPECIAL PROVISIONS.
- REMOVE ANY IN LANE PAVEMENT MARKING CHARACTERS IN CONFLICT WITH ANY TRAFFIC PATTERNS IN THESE PLANS OR DETAILS AS A PART OF THESE PLANS.

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 DATE: 3/10/2020

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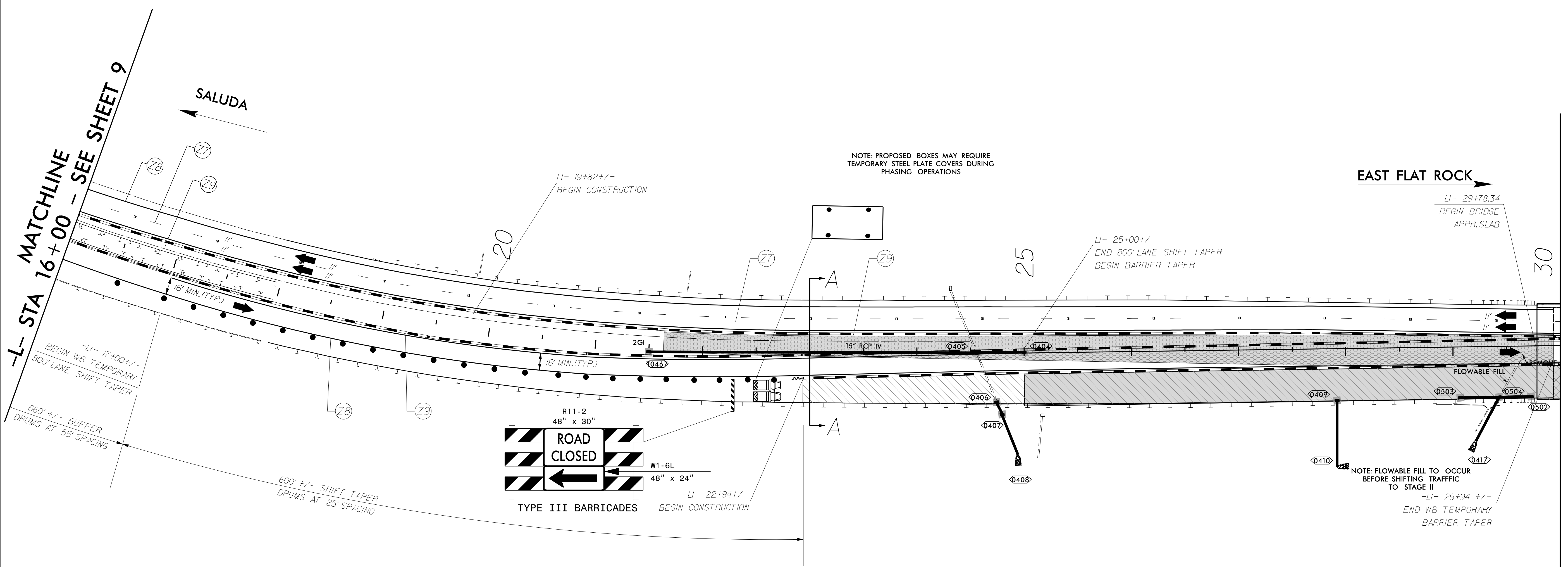
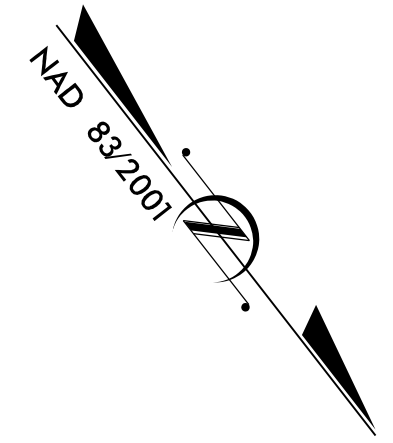
SEAL 18187
 PROFESSIONAL ENGINEER
 KEVIN J. VAN METER

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



STAGE II OVERVIEW & DETAILS

3/9/2020
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 User:Morган.Foster



NOTE: PROPOSED BOXES MAY REQUIRE TEMPORARY STEEL PLATE COVERS DURING PHASING OPERATIONS

EAST FLAT ROCK

-LI- 29+78.34
BEGIN BRIDGE APPR. SLAB

LI- 25+00+/-
END 800' LANE SHIFT TAPER
BEGIN BARRIER TAPER

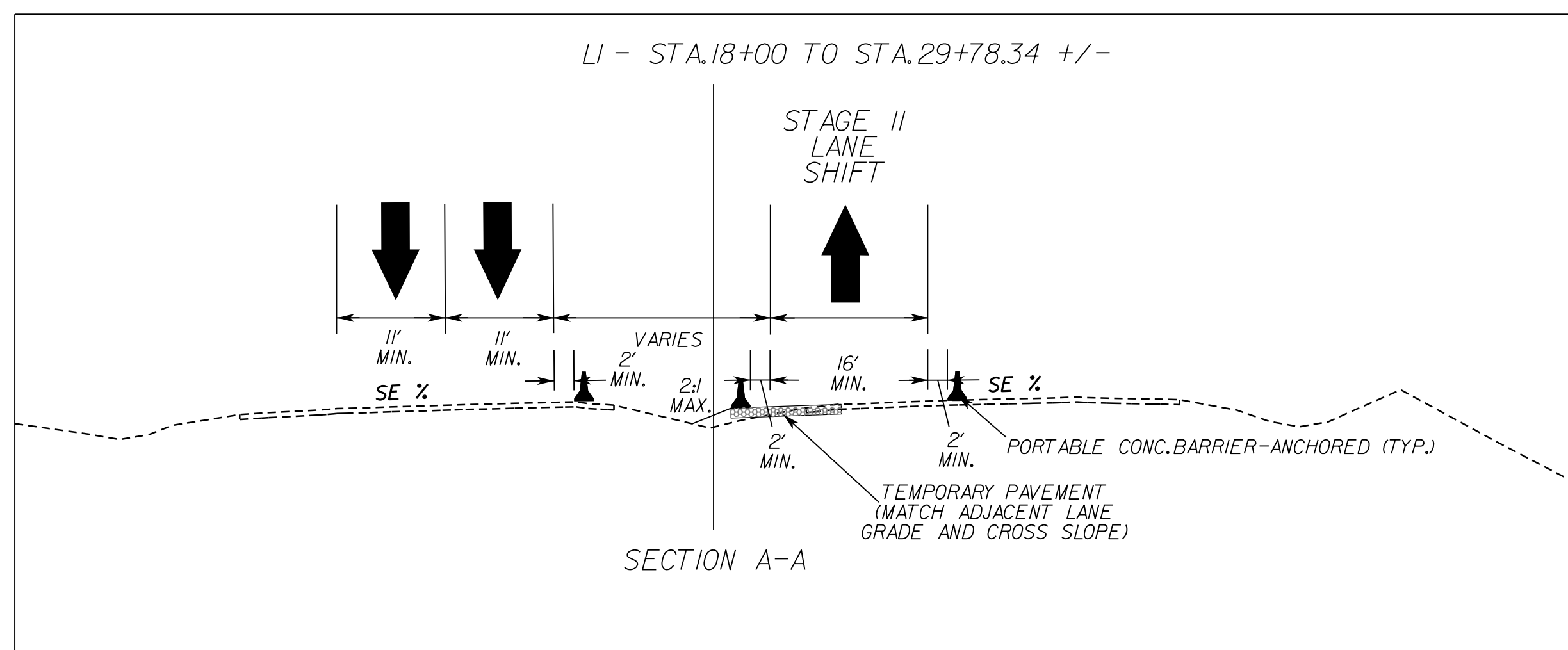
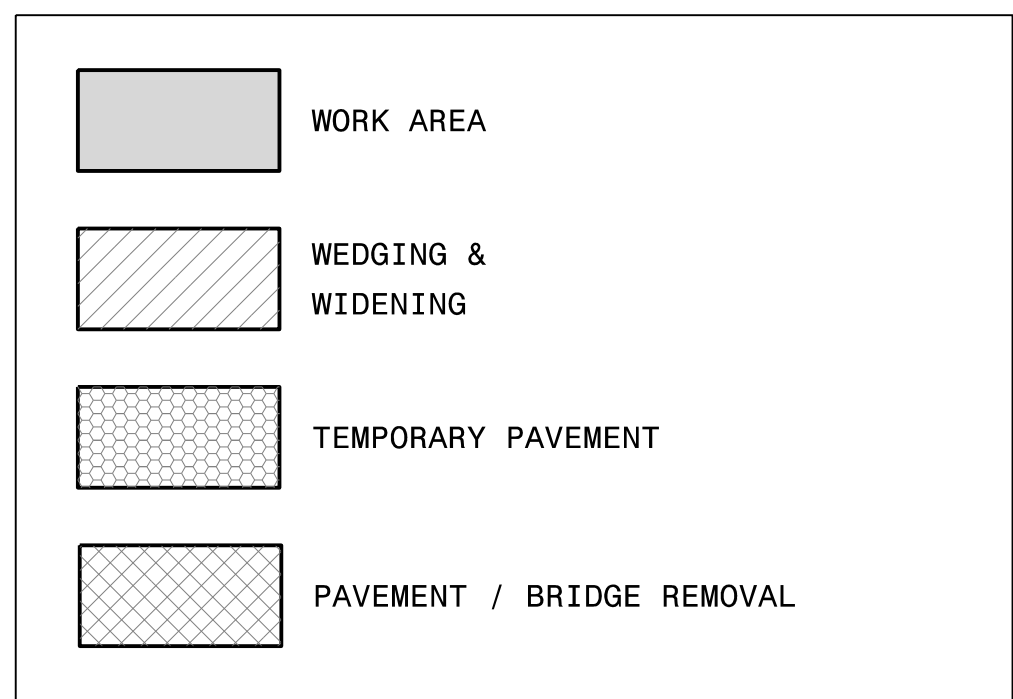
LI- 19+82+/-
BEGIN CONSTRUCTION

-LI- 17+00+/-
BEGIN WB TEMPORARY 800' LANE SHIFT TAPER

660' +/- BUFFER DRUMS AT 55' SPACING

600' +/- SHIFT TAPER DRUMS AT 25' SPACING

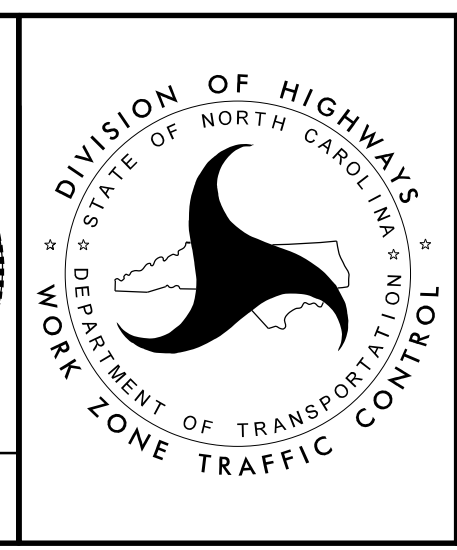
NOTE: FLOWABLE FILL TO OCCUR BEFORE SHIFTING TRAFFIC TO STAGE II
-LI- 29+94 +/-
END WB TEMPORARY BARRIER TAPER



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DATE: 3/10/2020

SEAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

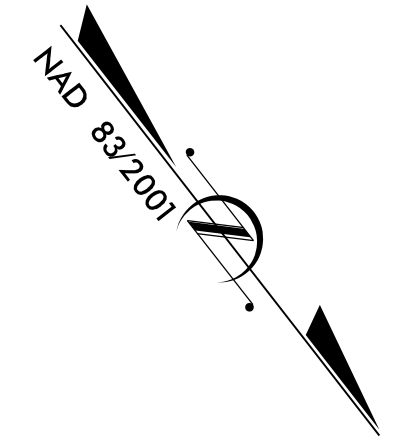


STAGE II OVERVIEW & DETAILS

3/10/2020
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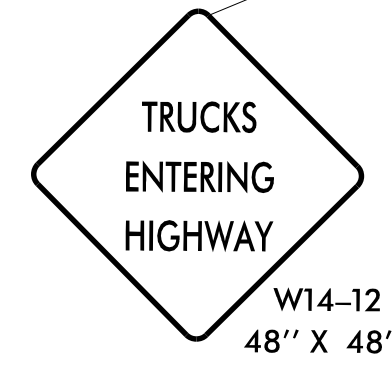
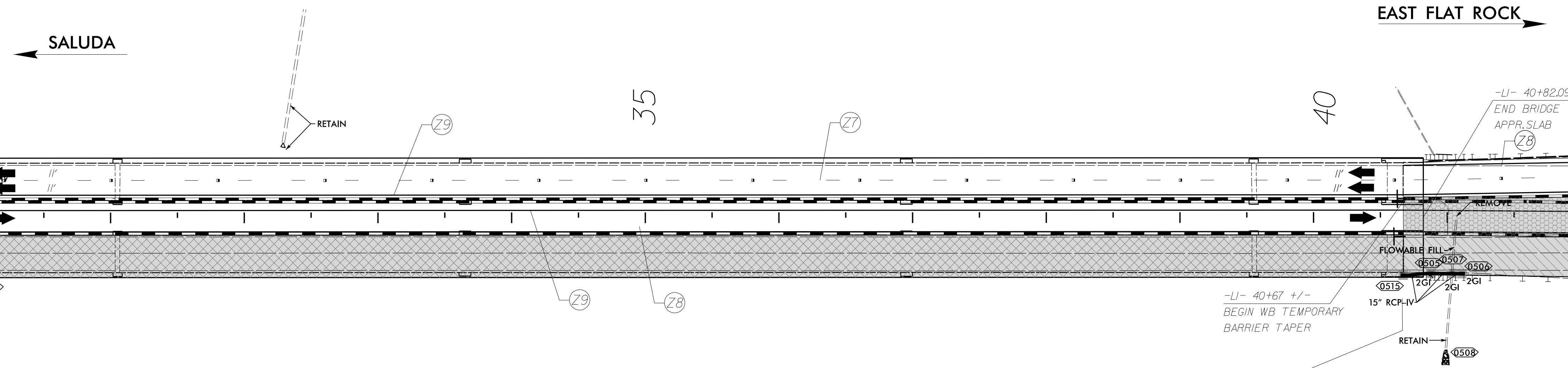
MATCHLINE -L- STA 30+00 - SEE SHEET 11

-L- STA 16+00 - SEE SHEET 9



MATCHLINE -L- STA 30+00 - SEE SHEET 10

MATCHLINE -L- STA 42+00 - SEE SHEET 12



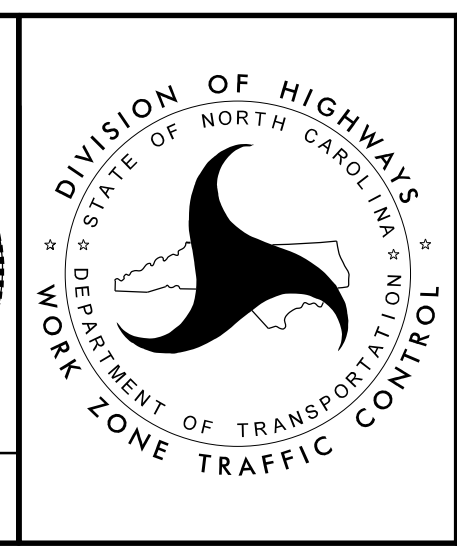
NOTE: FLOWABLE FILL TO OCCUR BEFORE SHIFTING TRAFFIC TO STAGE II

	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

6/7/2019
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User:Jeffrey.Thurston

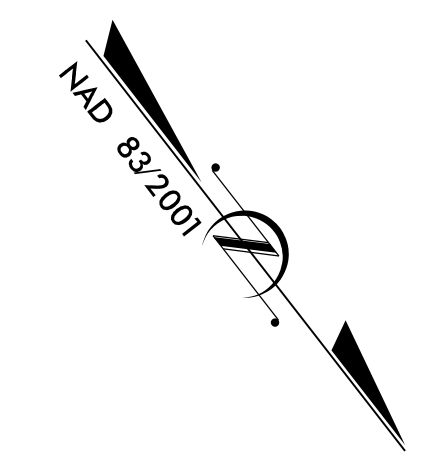
APPROVED: *Kevin J. VanMetre*
DATE: 6/10/2019

SEAL



**STAGE II
OVERVIEW
& DETAILS**

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

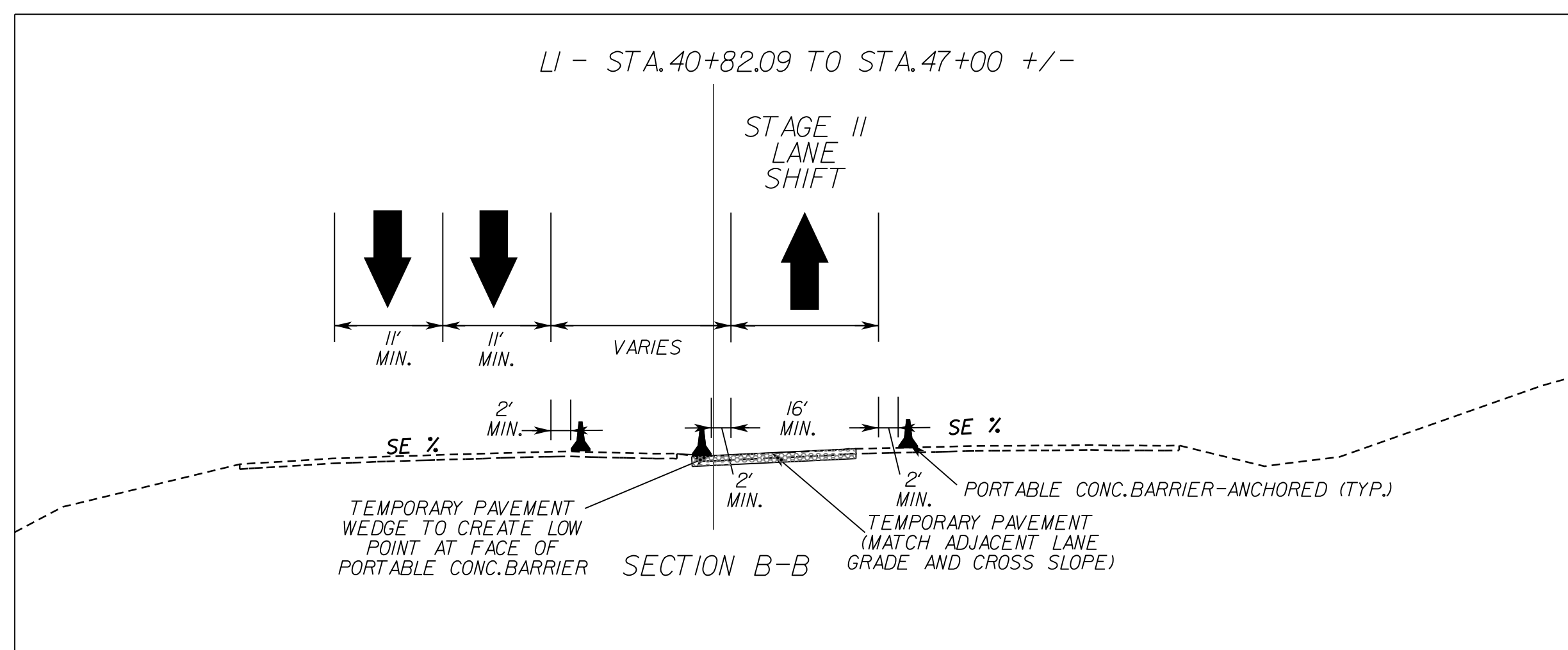
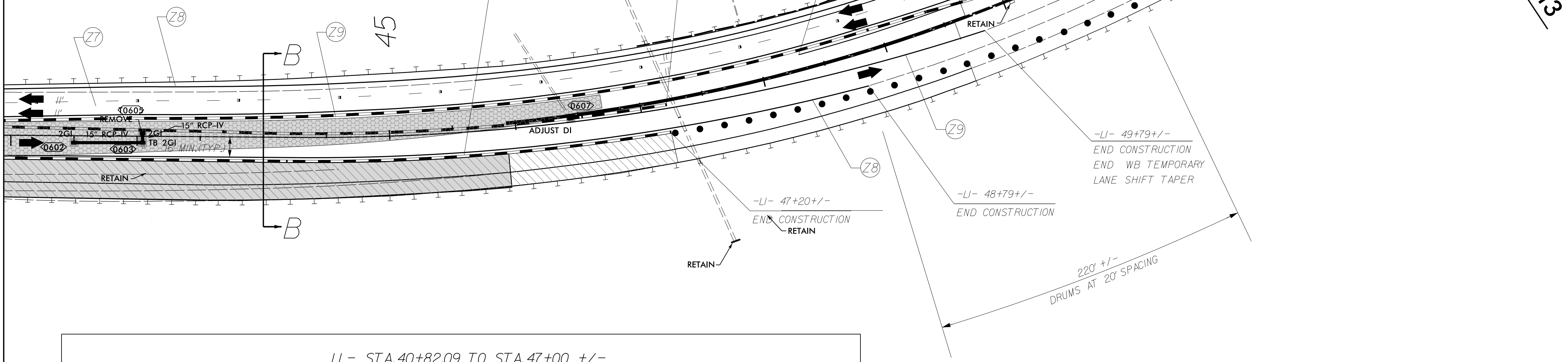


MATCHLINE -L- STA 42+00 - SEE SHEET 11

MATCHLINE -L- STA 53+50 - SEE SHEET 13

NOTE: PROPOSED BOXES MAY REQUIRE TEMPORARY STEEL PLATE COVERS DURING PHASING OPERATIONS

SALUDA



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Kevin J. Van Meter*
 DATE: 6/10/2019

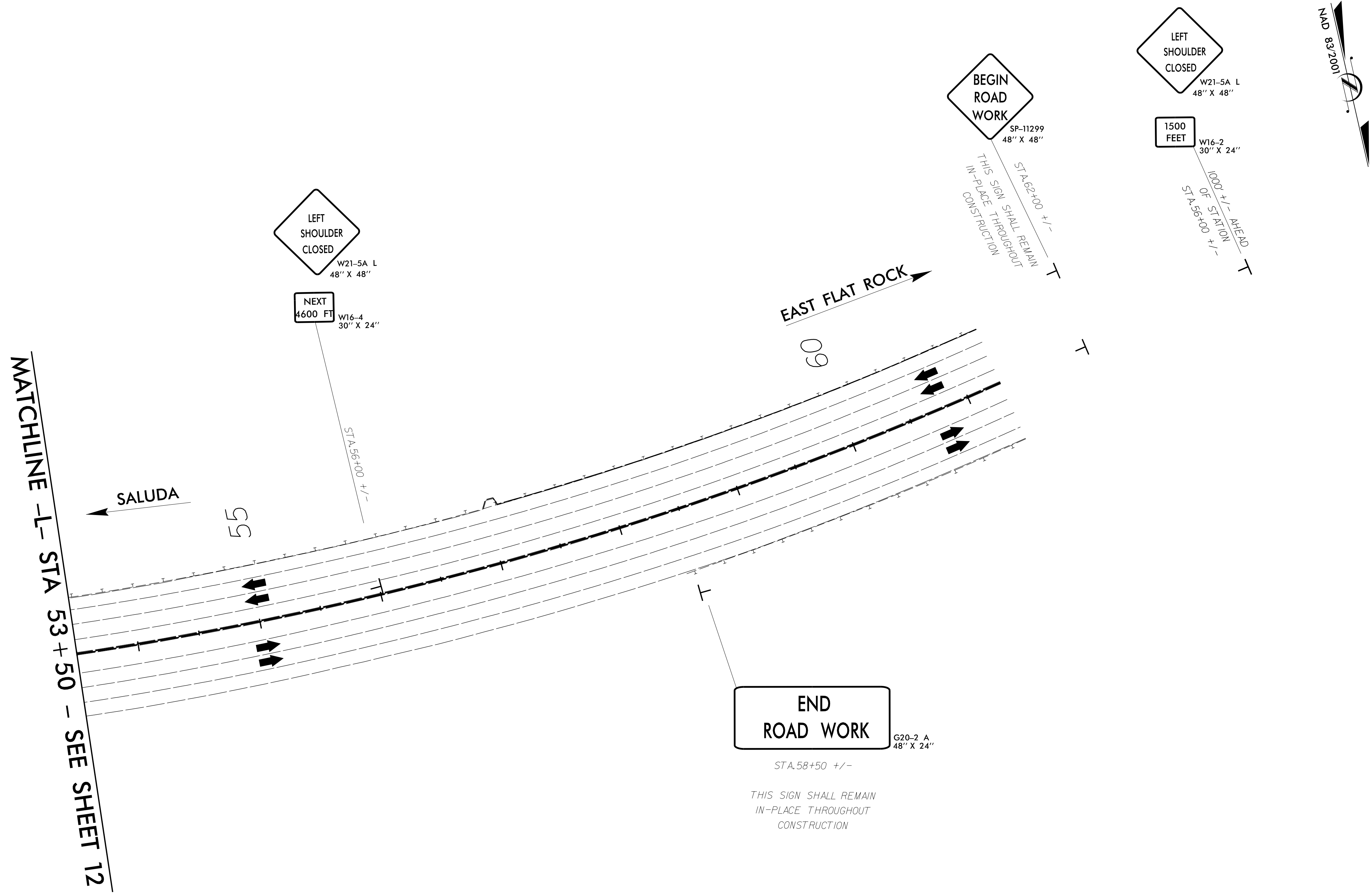
SEAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



STAGE II OVERVIEW & DETAILS

6/7/2019 L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\TrafficControl\TCP\5BPR.20_TC_TMP_12.dgn User:Jeffrey.Thurston



	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

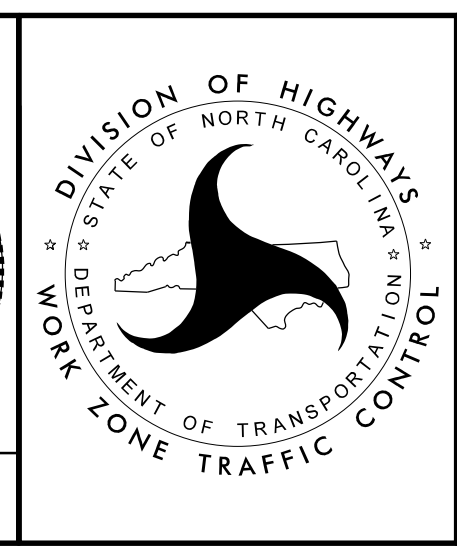
APPROVED: *Kevin J. VanMetre*
FFDE3A789337447

DATE: 6/10/2019

SEAL

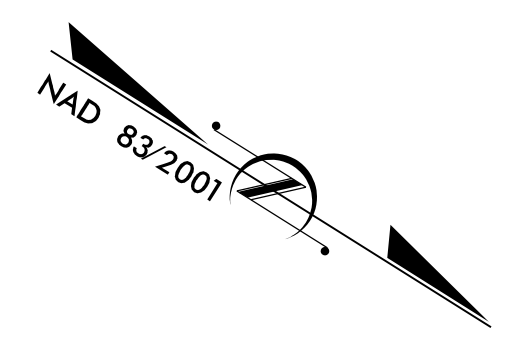
SEAL 18187
 KEVIN J. VANMETRE
 PROFESSIONAL ENGINEER
 NORTH CAROLINA

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



STAGE II OVERVIEW & DETAILS

6/7/2019
 L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\Traffic\TrafficControl\TCP\5BPR.20_TC_TMP_13.dgn
 User:Jeffrey.Thurston



END ROAD WORK
 G20-2 A
 48" X 24"

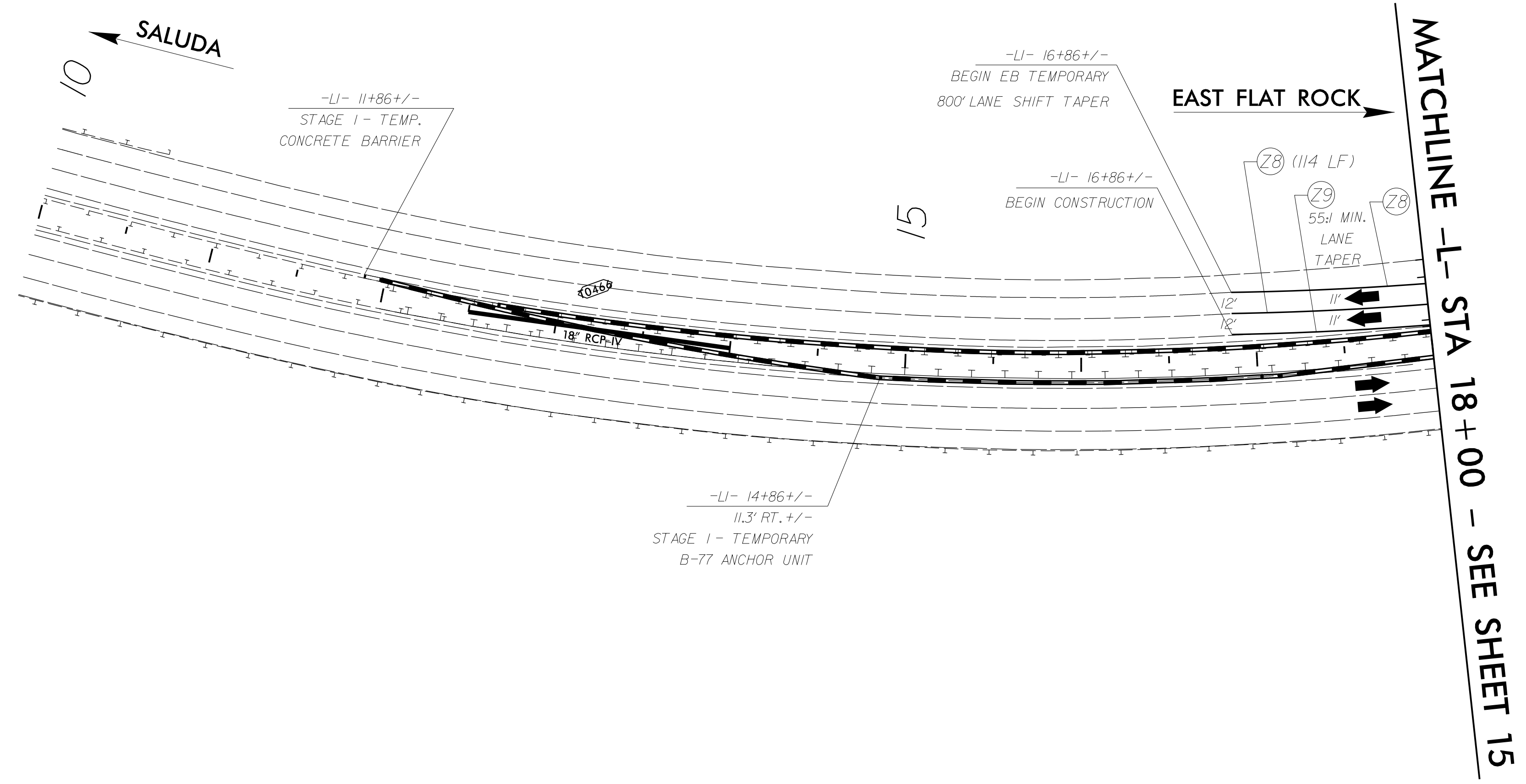
500' +/- IN ADVANCE
 OF STATION 10+00 +/-

THIS SIGN SHALL REMAIN
 IN-PLACE THROUGHOUT
 CONSTRUCTION

BEGIN ROAD WORK
 SP-11299
 48" X 48"

1380' +/- IN ADVANCE
 OF STATION 10+00 +/-

THIS SIGN SHALL REMAIN
 IN-PLACE THROUGHOUT
 CONSTRUCTION



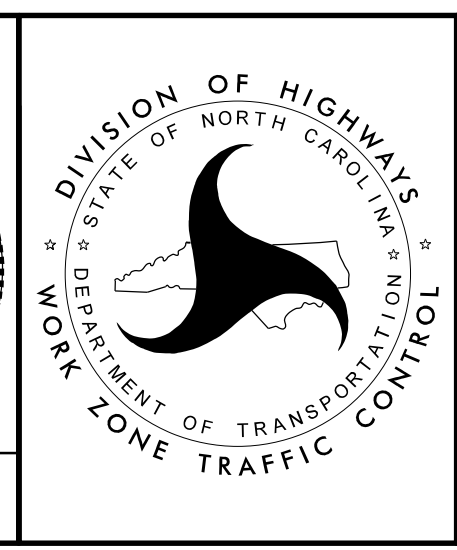
	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

6/7/2019
 L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\TrafficControl\TCP\5BPR.20_TC_TMP_14.dgn
 User:Jeffrey.Thurston

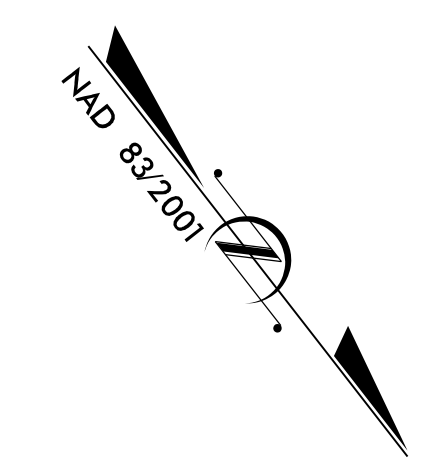
APPROVED: *Kevin J. VanMetre*
 DATE: 6/10/2019

SEAL

DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED

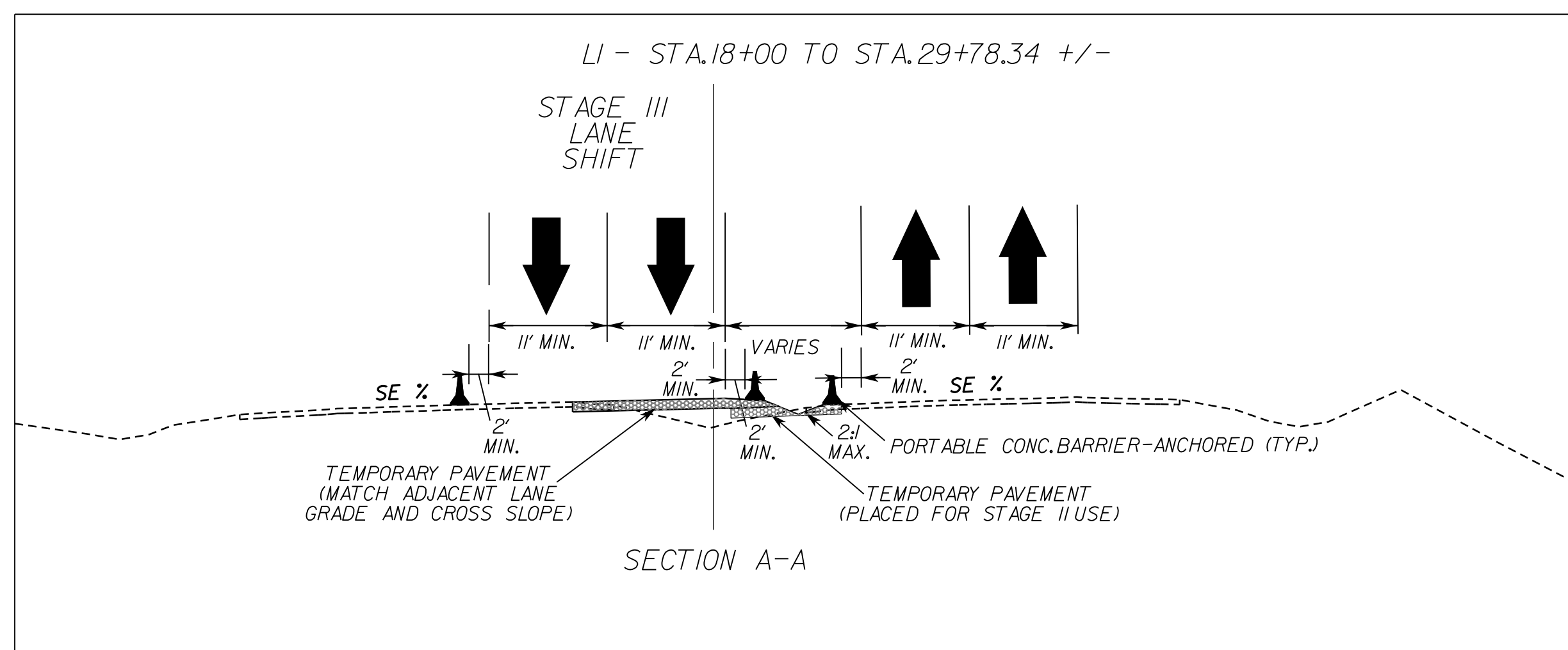
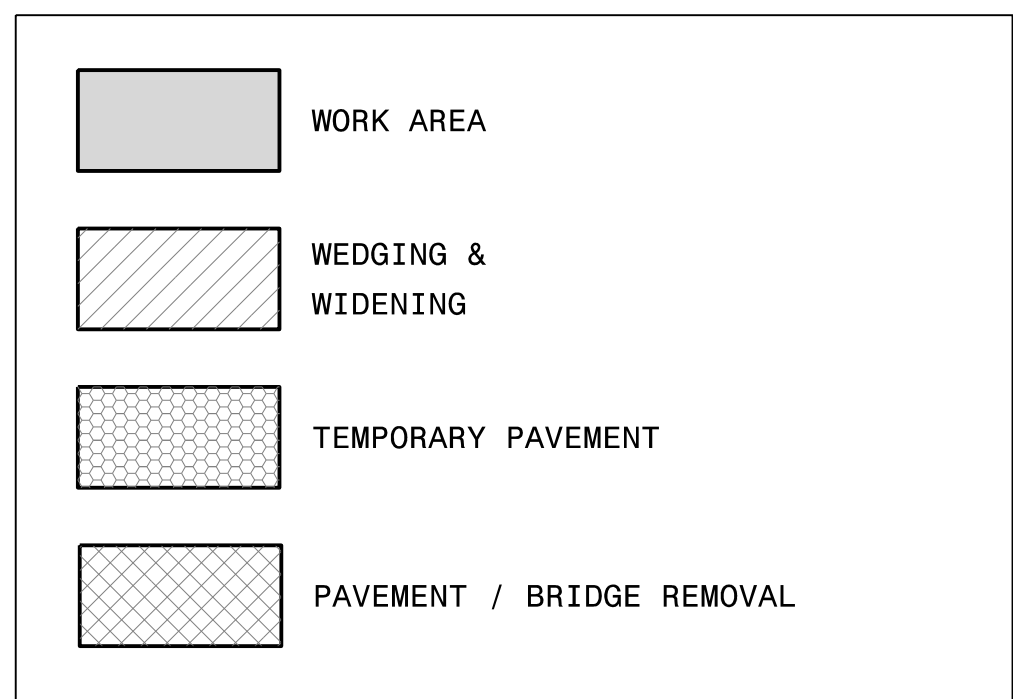
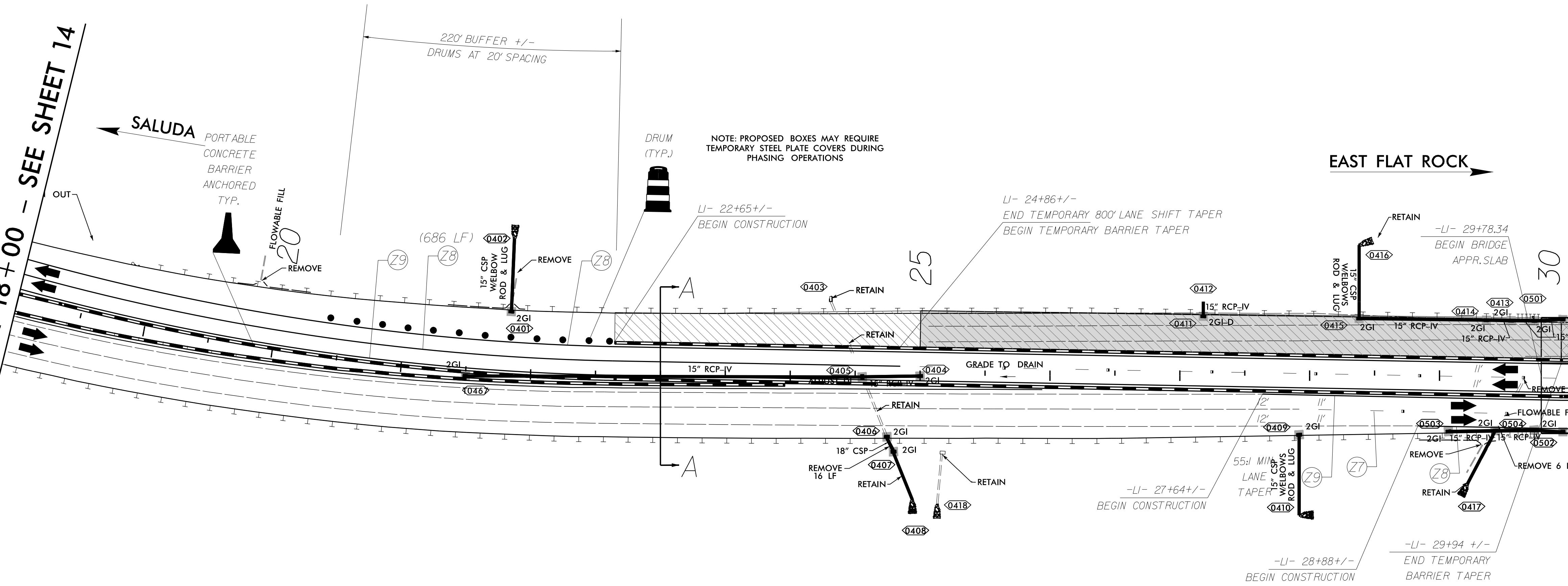


**STAGE III
 OVERVIEW
 & DETAILS**



MATCHLINE -L- STA 18+00 - SEE SHEET 14

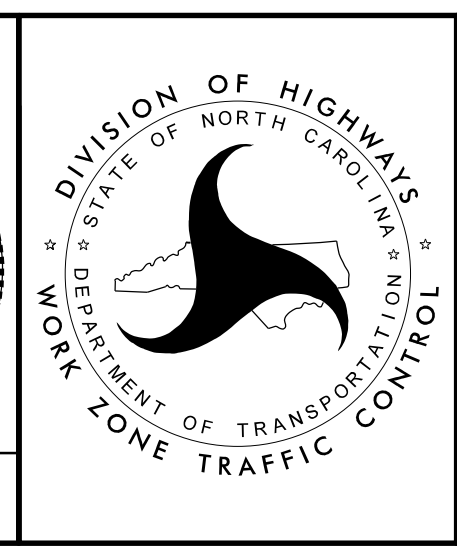
MATCHLINE -L- STA 30+00 - SEE SHEET 16



APPROVED: *Kevin J. VanMetre*
 DATE: 6/10/2019

SEAL

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



STAGE III OVERVIEW & DETAILS

6/7/2019
 L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\TrafficControl\15BPR.20_T.C.TMP_15.dgn
 User:jeffrey.thurston

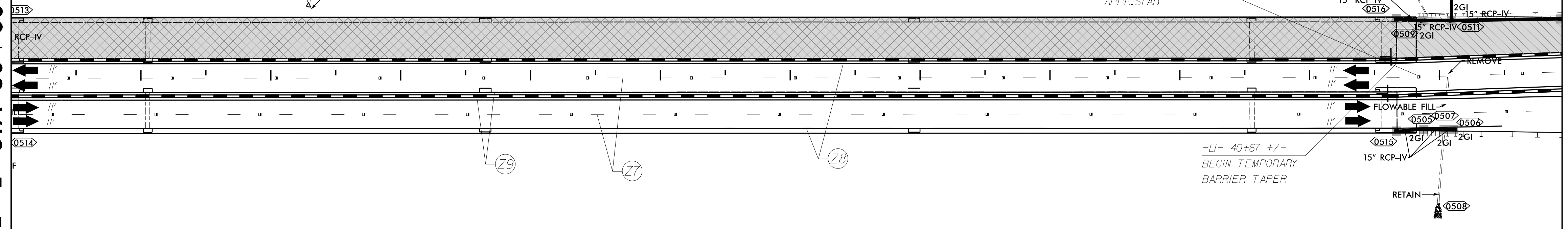
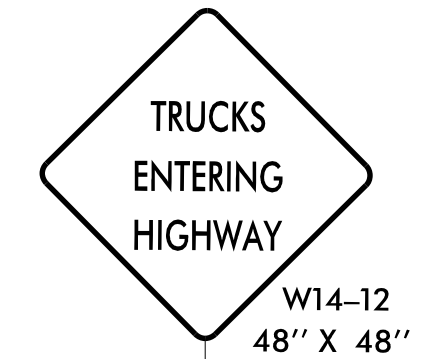


MATCHLINE -L- STA 30+00 - SEE SHEET 15

MATCHLINE -L- STA 42+00 - SEE SHEET 17

SALUDA ←

→ EAST FLAT ROCK

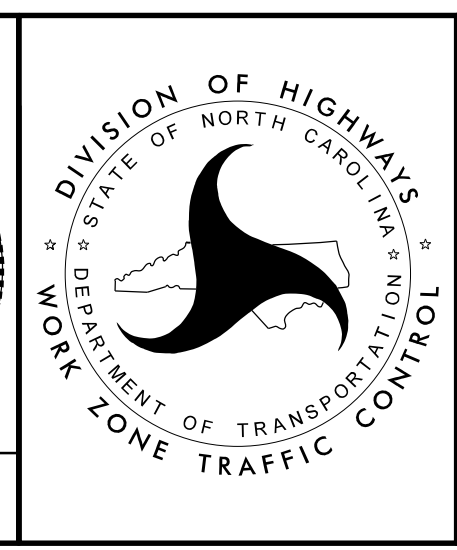


	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Kevin J. VanMetre*
FFDESAT789337447

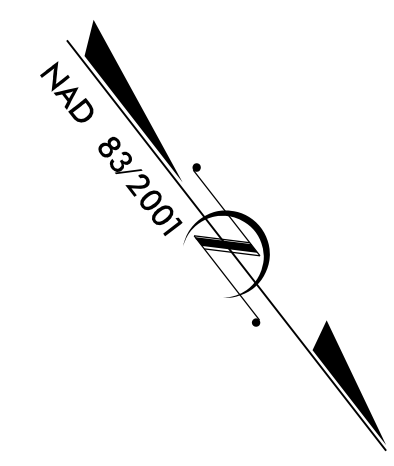
DATE: 6/10/2019

SEAL



**STAGE III
OVERVIEW
& DETAILS**

6/7/2019
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 User:Jeffrey.Thurston

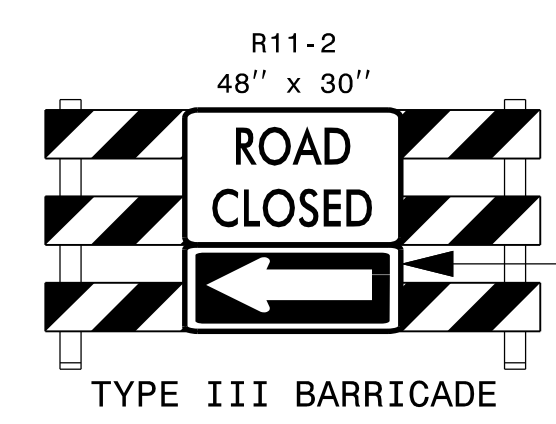


MATCHLINE -L- STA 42+00 - SEE SHEET 16

MATCHLINE -L- STA 53+50 - SEE SHEET 18

SALUDA ←

EAST FLAT ROCK →



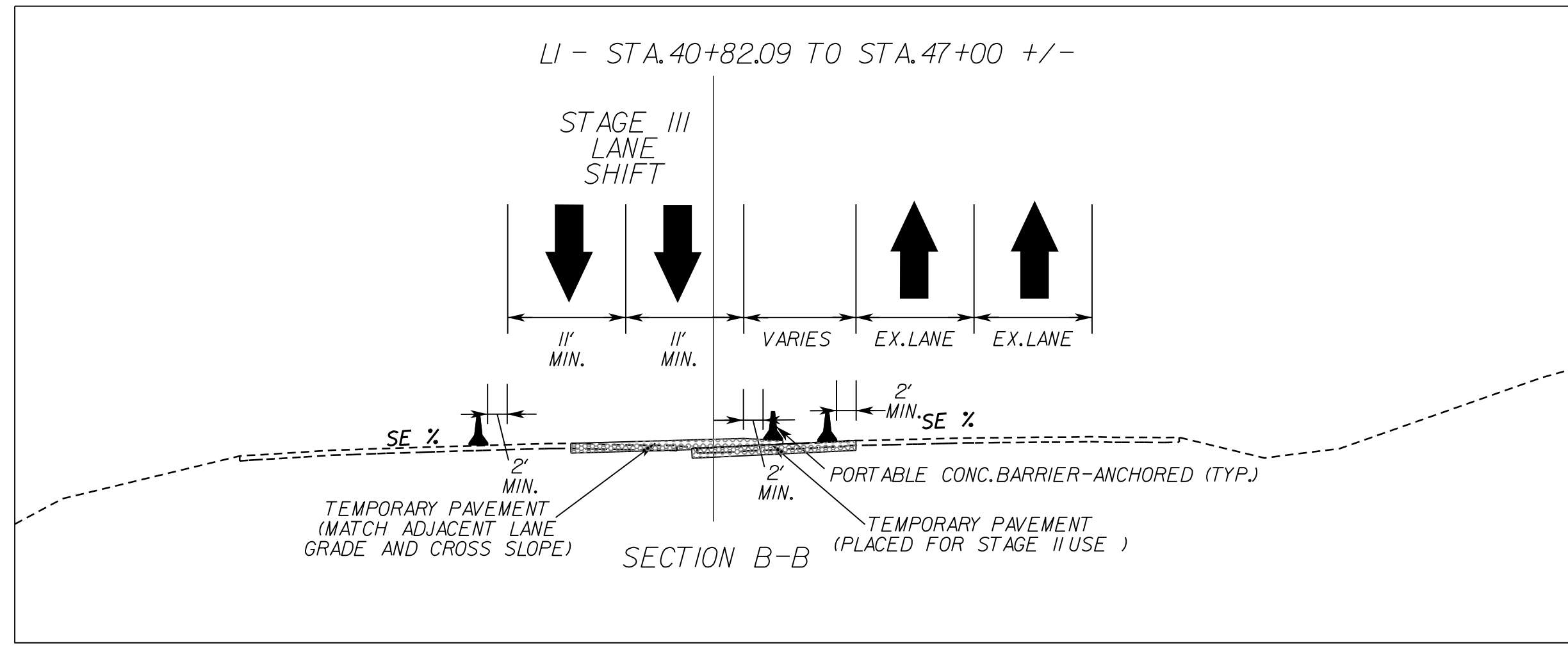
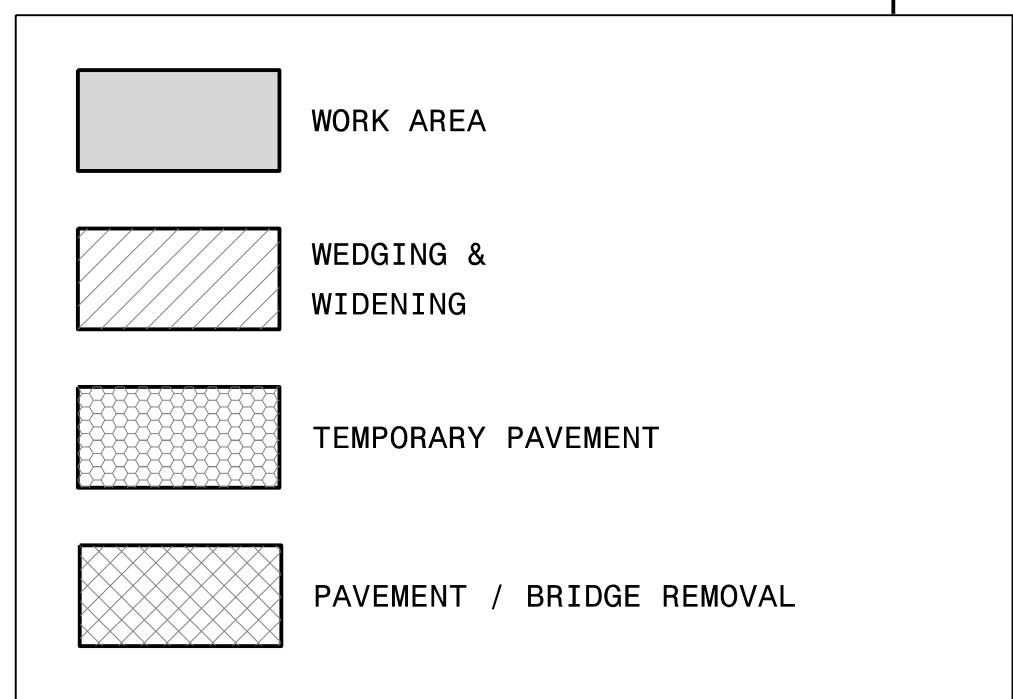
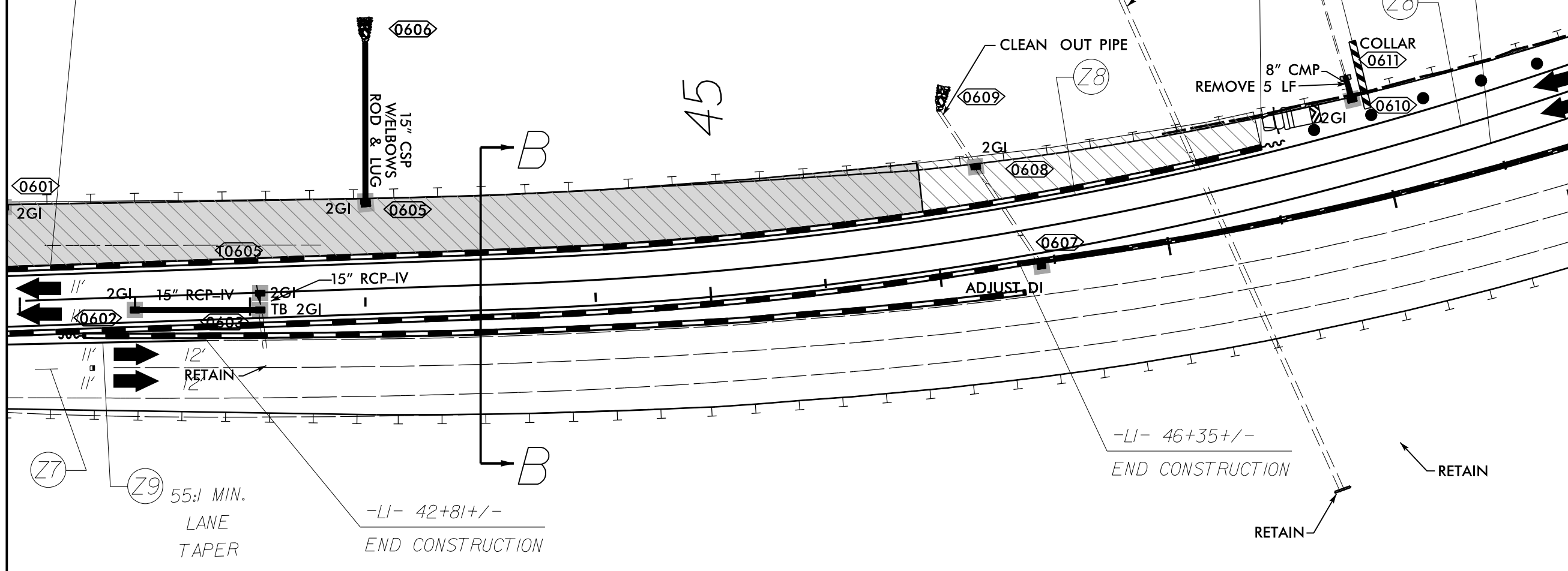
-LI- 42+08+/-
END TEMPORARY BARRIER TAPER
BEGIN TEMPORARY 800' LANE SHIFT TAPER

NOTE: PROPOSED BOXES MAY REQUIRE TEMPORARY STEEL PLATE COVERS DURING PHASING OPERATIONS

-LI- 47+48+/-
END CONSTRUCTION

-LI- 48+90+/-
END CONSTRUCTION

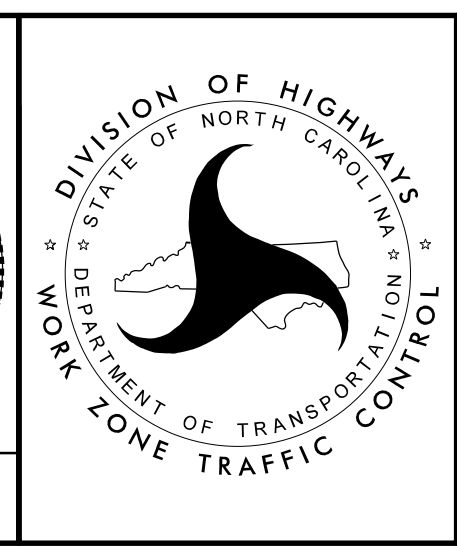
-LI- 50+08+/-
END TEMPORARY 800' LANE SHIFT TAPER



APPROVED: *Kevin J. Van Meter*
DATE: 6/10/2019

SEAL

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STAGE III OVERVIEW & DETAILS

6/7/2019 1:26:58 PM L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\TrafficControl\15BPR.20_TC_TMP_17.dgn User:jeffrey.thurston

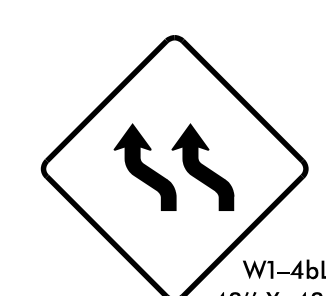
MATCHLINE -L- STA 53+50 - SEE SHEET 17

SALUDA

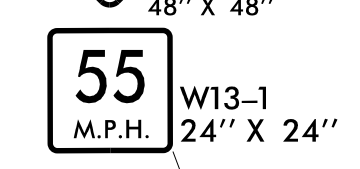
55

EAST FLAT ROCK

60



W1-4b1
48" X 48"



W13-1
24" X 24"

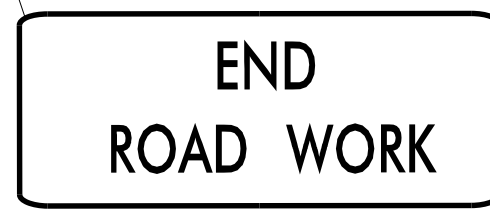
STA 60+00 +/-



SP-11299
48" X 48"

THIS SIGN SHALL REMAIN
IN-PLACE THROUGHOUT
CONSTRUCTION

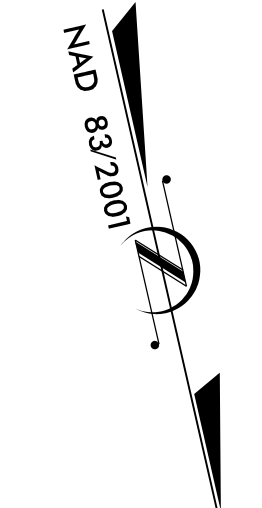
STA 62+00 +/-



G20-2 A
48" X 24"

STA 58+50 +/-

THIS SIGN SHALL REMAIN
IN-PLACE THROUGHOUT
CONSTRUCTION



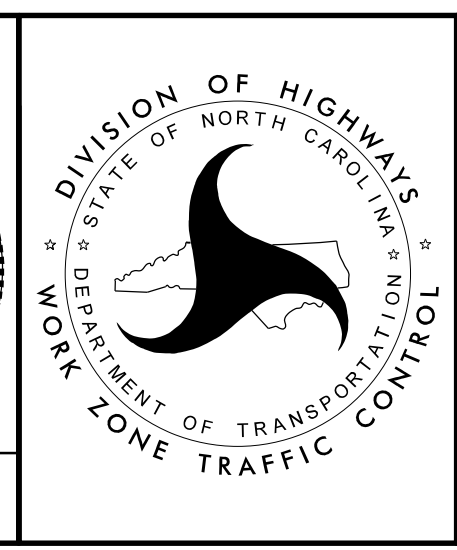
	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Kevin J. VanMetre*
FFDE3A789337447

DATE: 6/10/2019

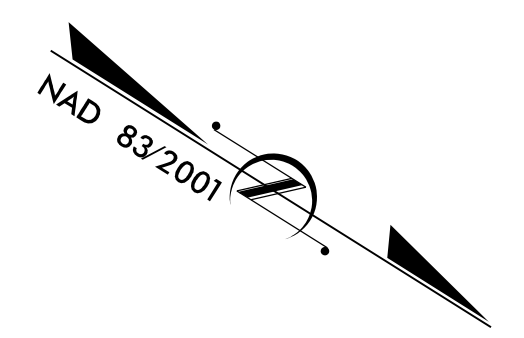
SEAL

DOCUMENT NOT CONSIDERED FINAL
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**STAGE III
OVERVIEW
& DETAILS**

6/7/2019
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User:Jeffrey.Thurston



END ROAD WORK

G20-2 A
48" X 24"

500' +/- IN ADVANCE
OF STATION 10+00 +/-

THIS SIGN SHALL REMAIN
IN-PLACE THROUGHOUT
CONSTRUCTION

BEGIN ROAD WORK

SP-11299
48" X 48"

1380' +/- IN ADVANCE
OF STATION 10+00 +/-

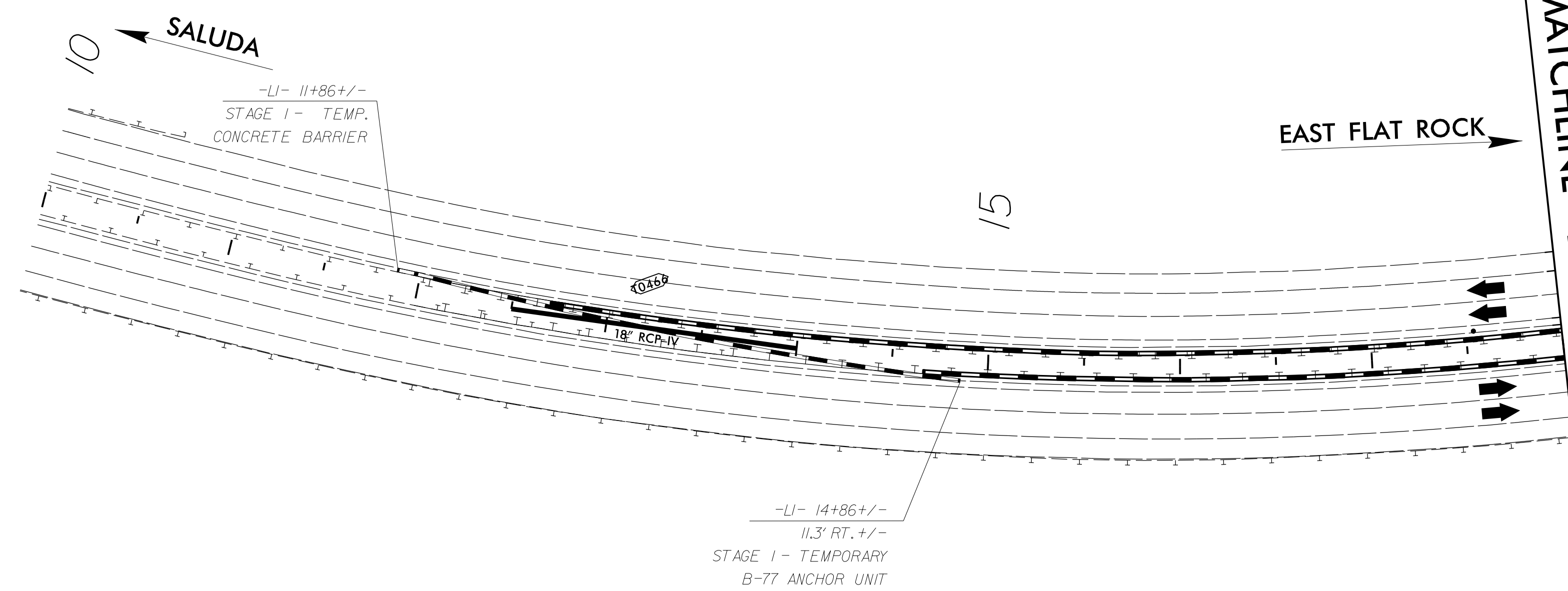
THIS SIGN SHALL REMAIN
IN-PLACE THROUGHOUT
CONSTRUCTION

LEFT SHOULDER CLOSED

1500 FEET

W21-5A L
48" X 48"

W16-2
30" X 24"

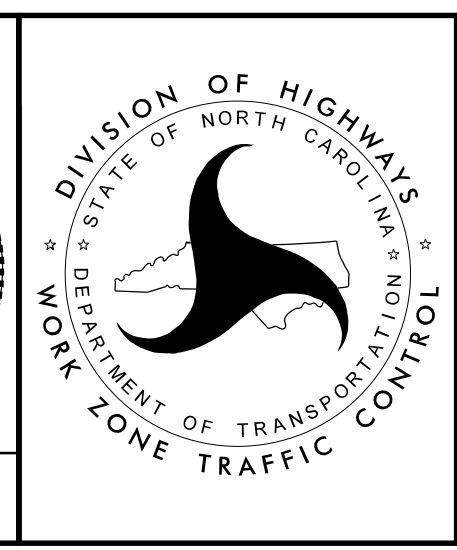


	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Kevin J. VanMetre*
DATE: 6/10/2019

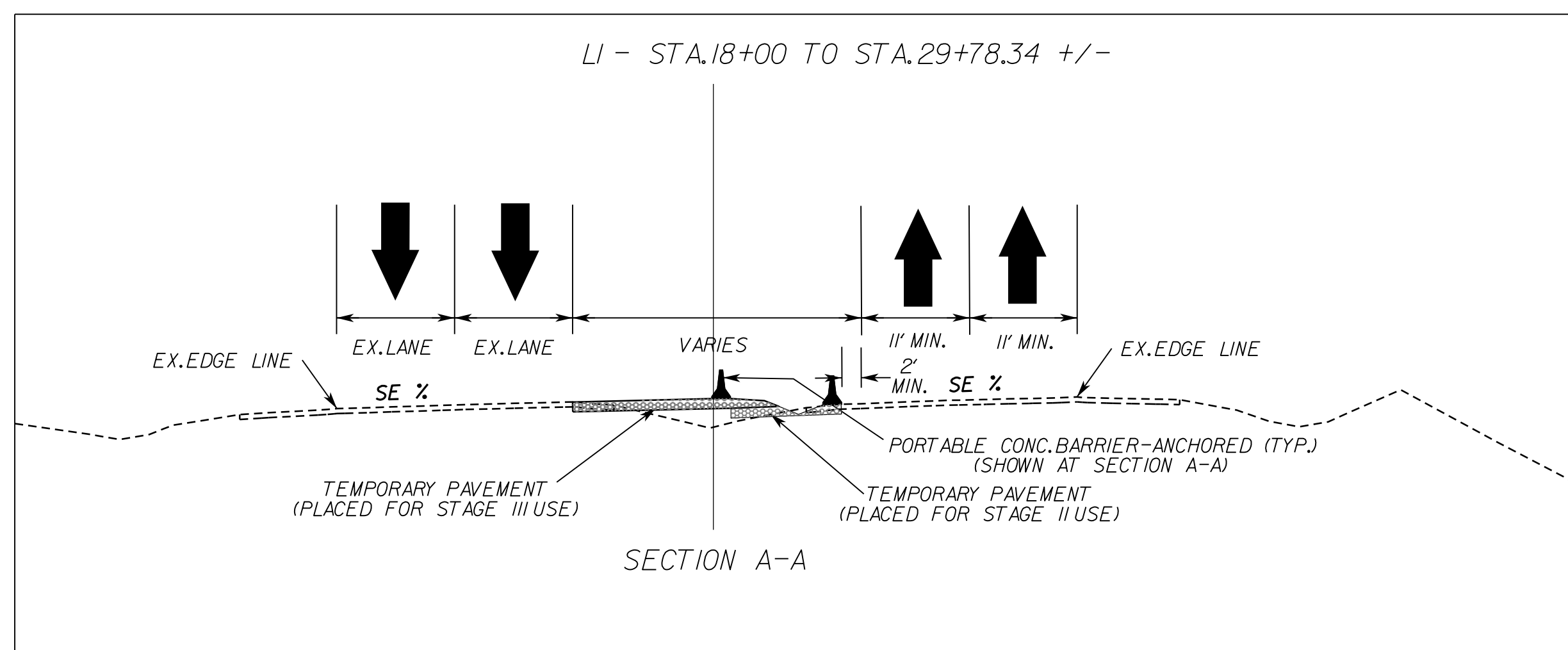
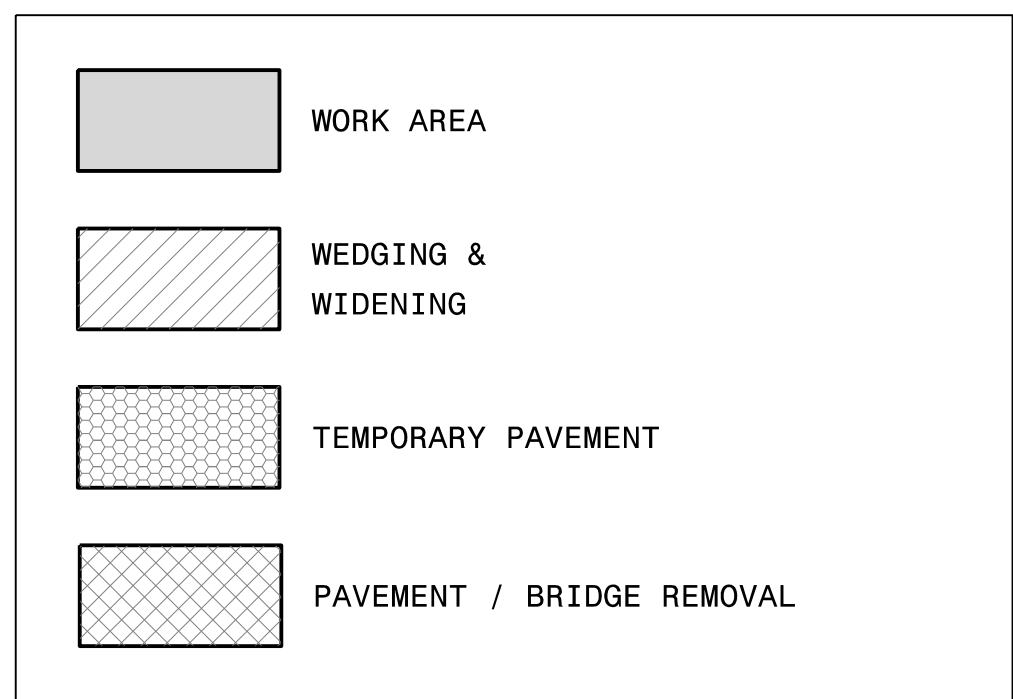
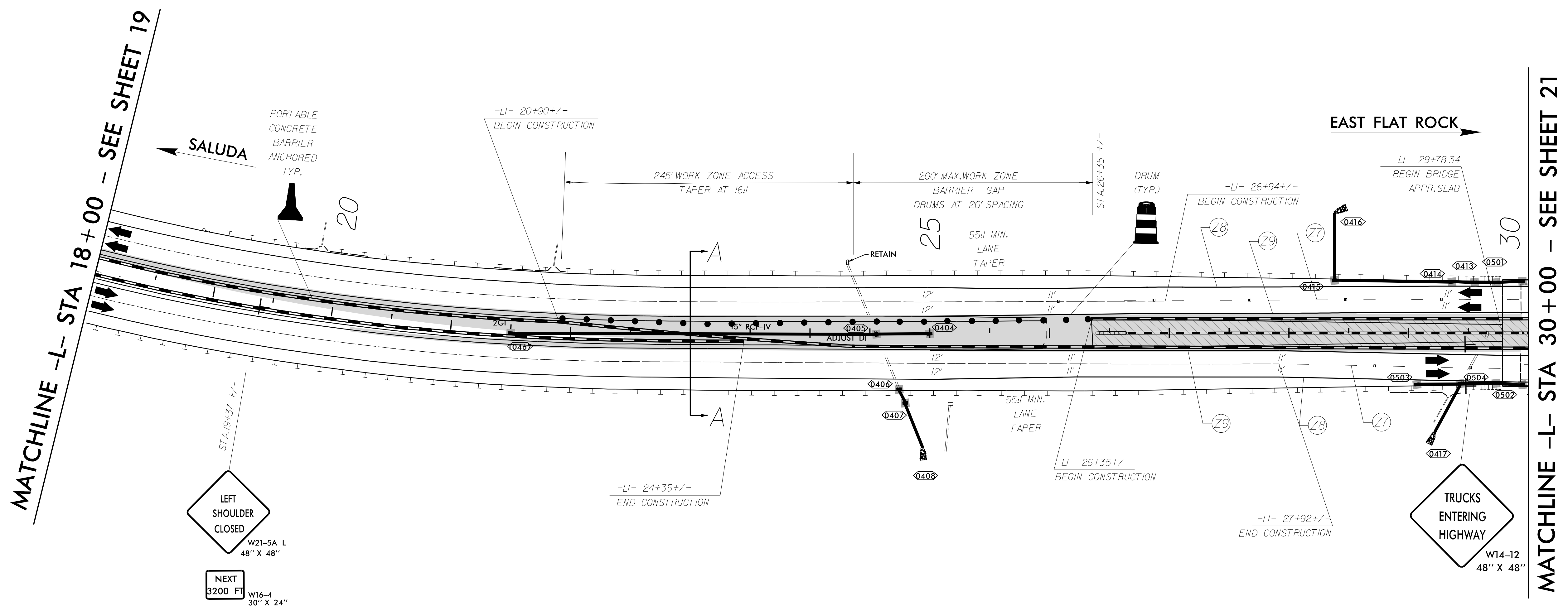
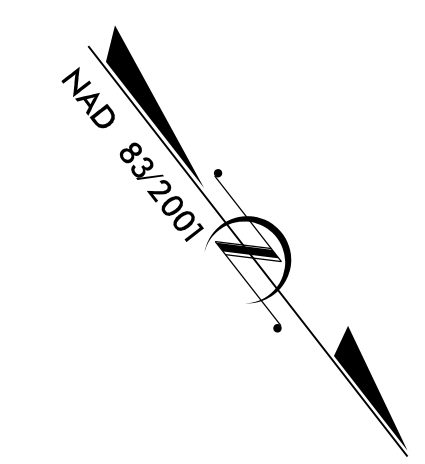
SEAL

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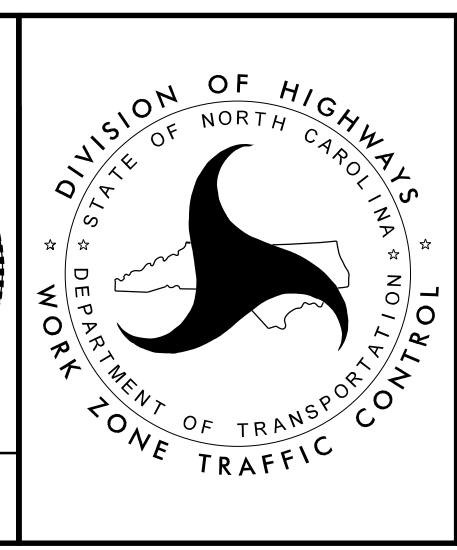
**STAGE IV
OVERVIEW
& DETAILS**

6/7/2019
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User:Jeffrey.Thurston



APPROVED: *Kevin J. VanMetre*
FFDE3A78937447
 DATE: 6/10/2019

SEAL



**STAGE IV
OVERVIEW
& DETAILS**

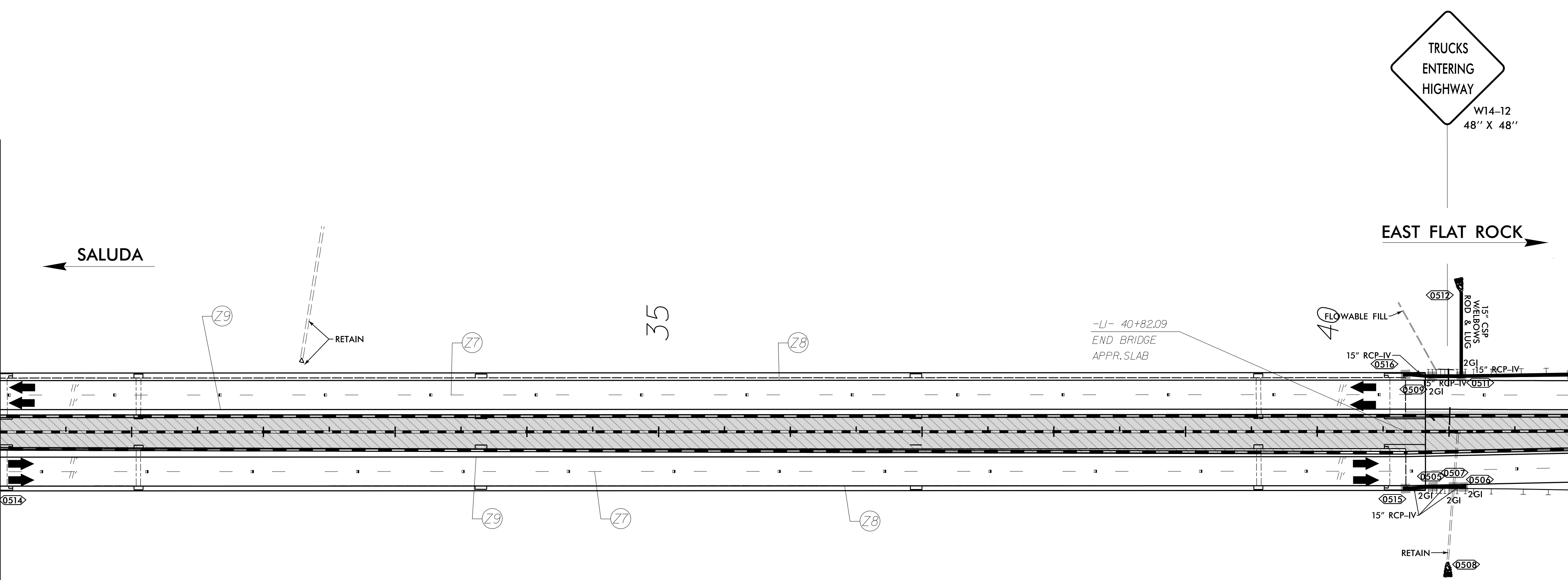
**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

6/7/2019
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 User:jeffrey.thurston



MATCHLINE -L- STA 30+00 - SEE SHEET 20

MATCHLINE -L- STA 42+00 - SEE SHEET 22

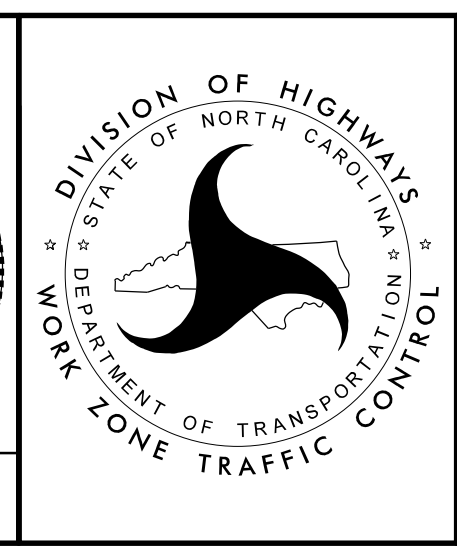


	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

6/7/2019
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User:jeffrey.thurston

APPROVED: *Kevin J. VanMetre*
DATE: 6/10/2019

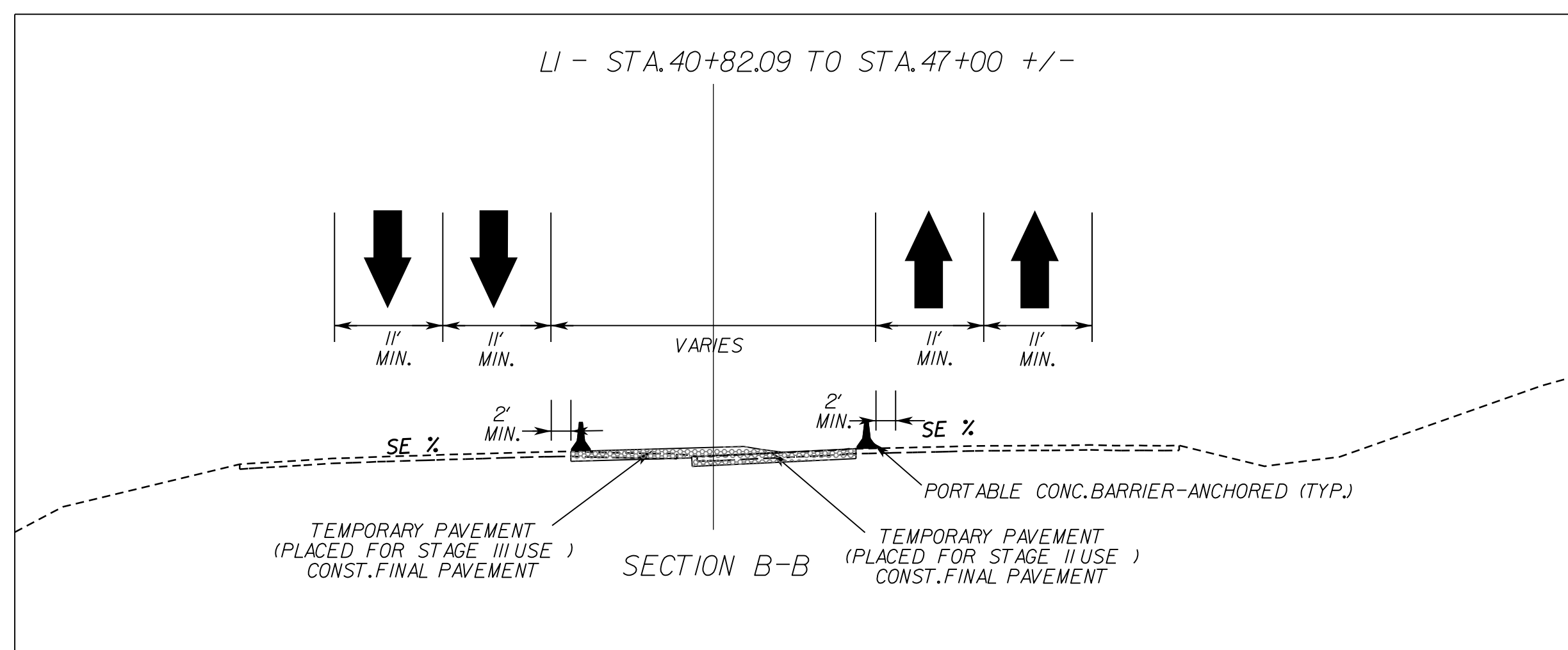
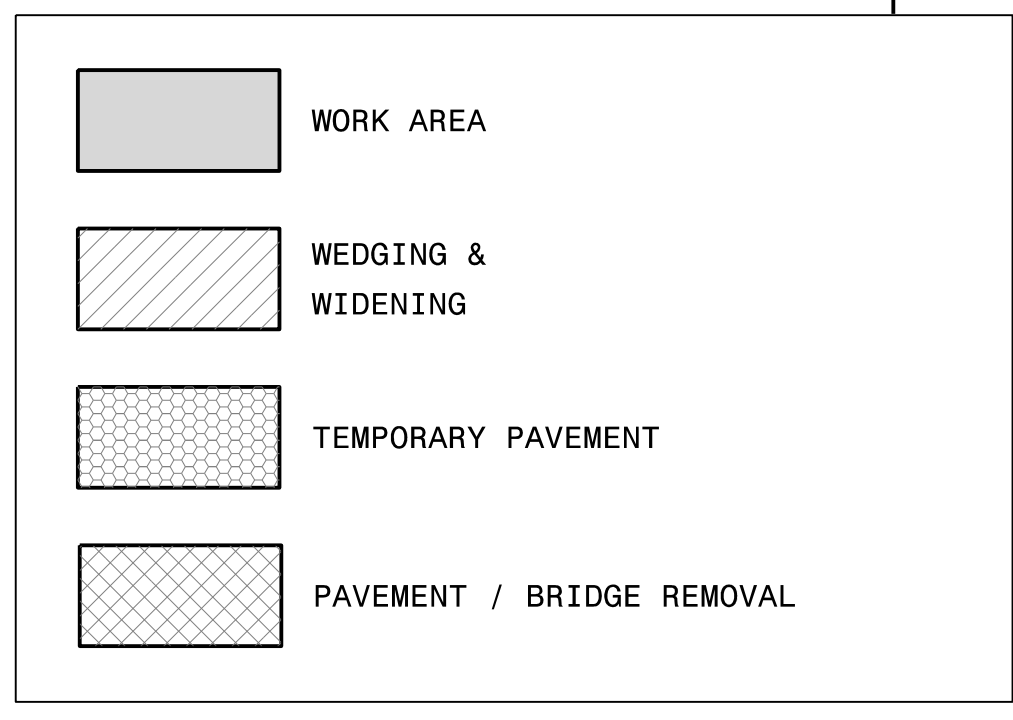
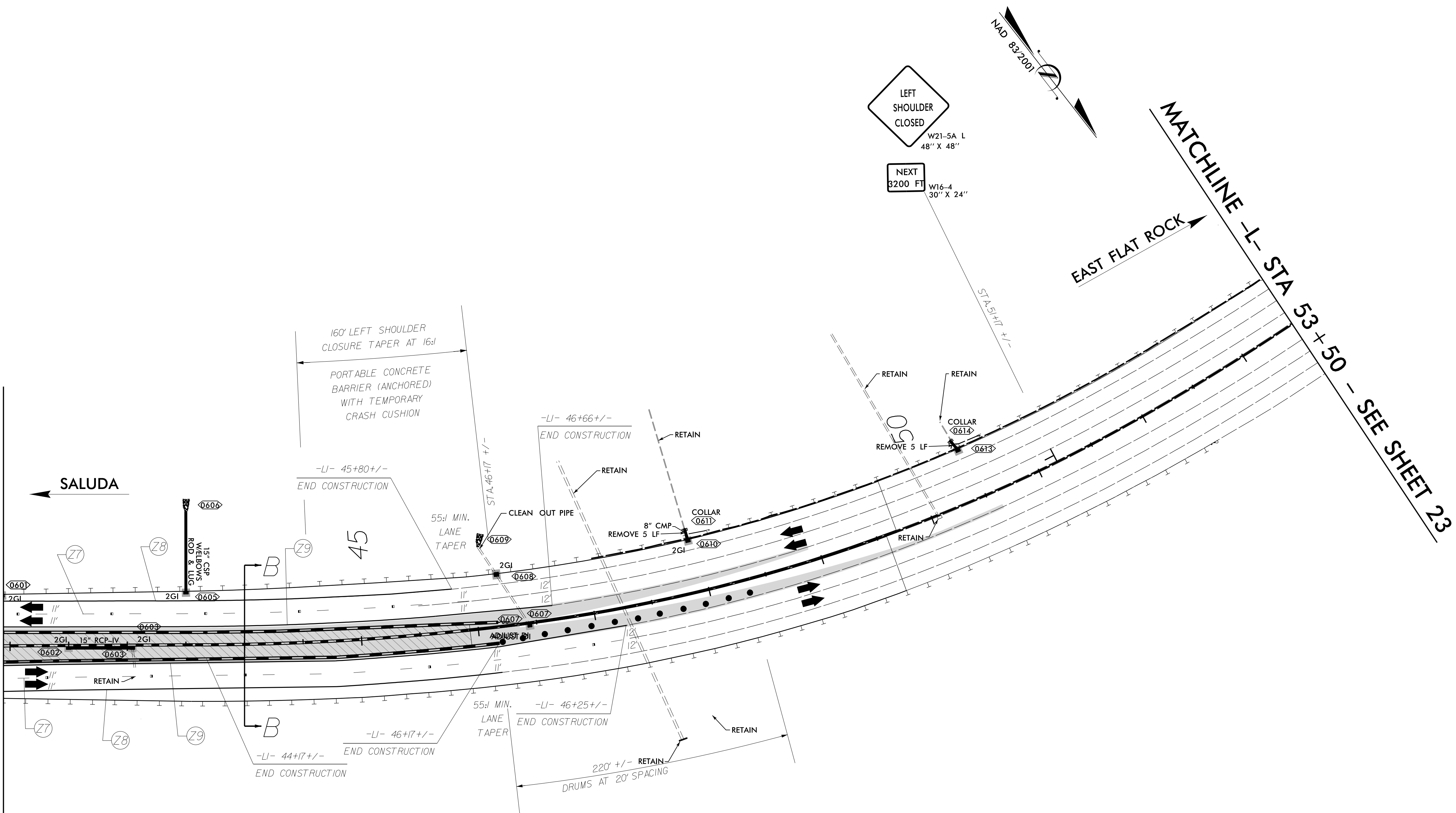
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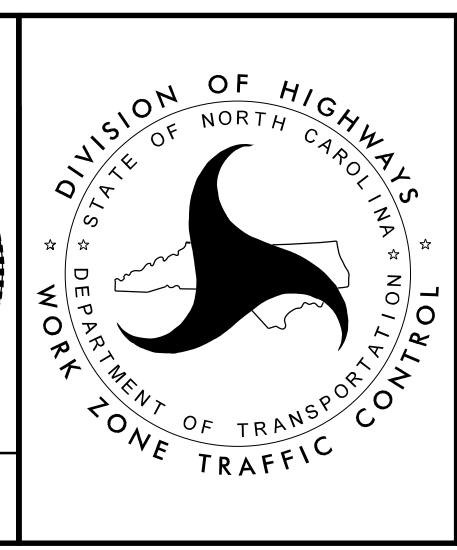
**STAGE IV
OVERVIEW
& DETAILS**

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**

MATCHLINE -L- STA 42+00 - SEE SHEET 21

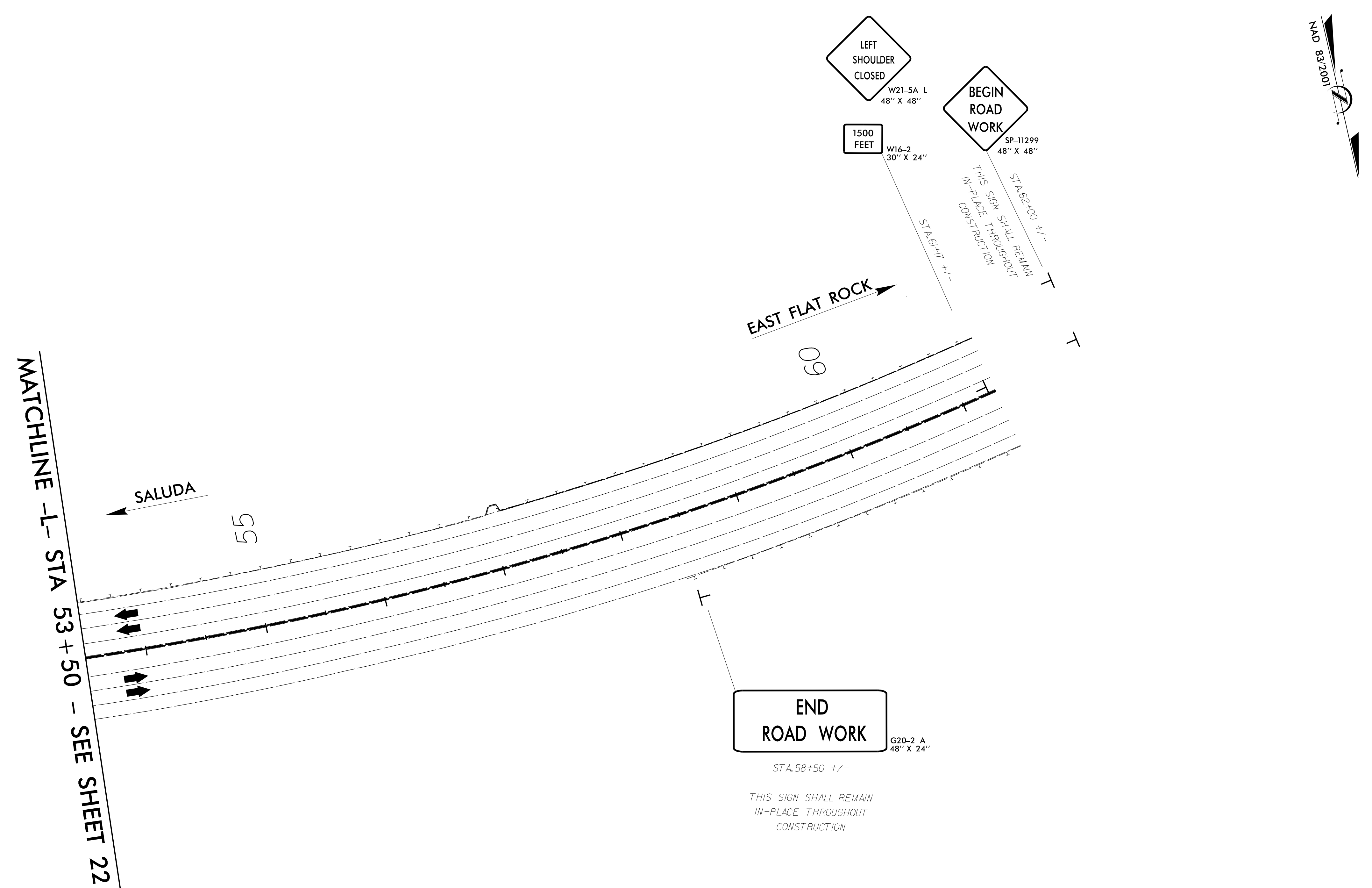


APPROVED: *Kevin J. Van Meter*
 DATE: 6/10/2019
 SEAL
 DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED



STAGE IV OVERVIEW & DETAILS

6/7/2019
 L:\60587170-1-26.GreenRiver_T02\900.CAD_GIS\910.CAD\70.NCDDOT_TIP\TrafficControl\15BPR.20_TC_TMP.22.dgn
 User:Jeffrey.Thurston



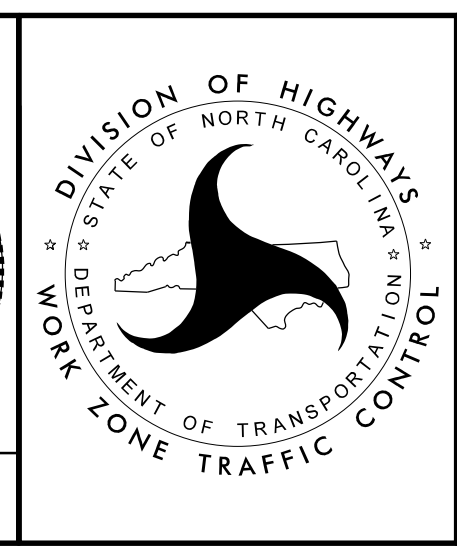
	WORK AREA
	WEDGING & WIDENING
	TEMPORARY PAVEMENT
	PAVEMENT / BRIDGE REMOVAL

APPROVED: *Kevin J. VanMetre*
FFDE3A789337447

DATE: 6/10/2019

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**DOCUMENT NOT CONSIDERED FINAL
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**STAGE IV
OVERVIEW
& DETAILS**

6/7/2019
 L:\60587170-1-26_GreenRiver_T02\900_CAD_GIS\910_CAD\70_NCDOT_TIP\Traffic\TrafficControl\TCP\15BPR.20_TC_TMP.23.dgn
 User: jeffrey.thurston