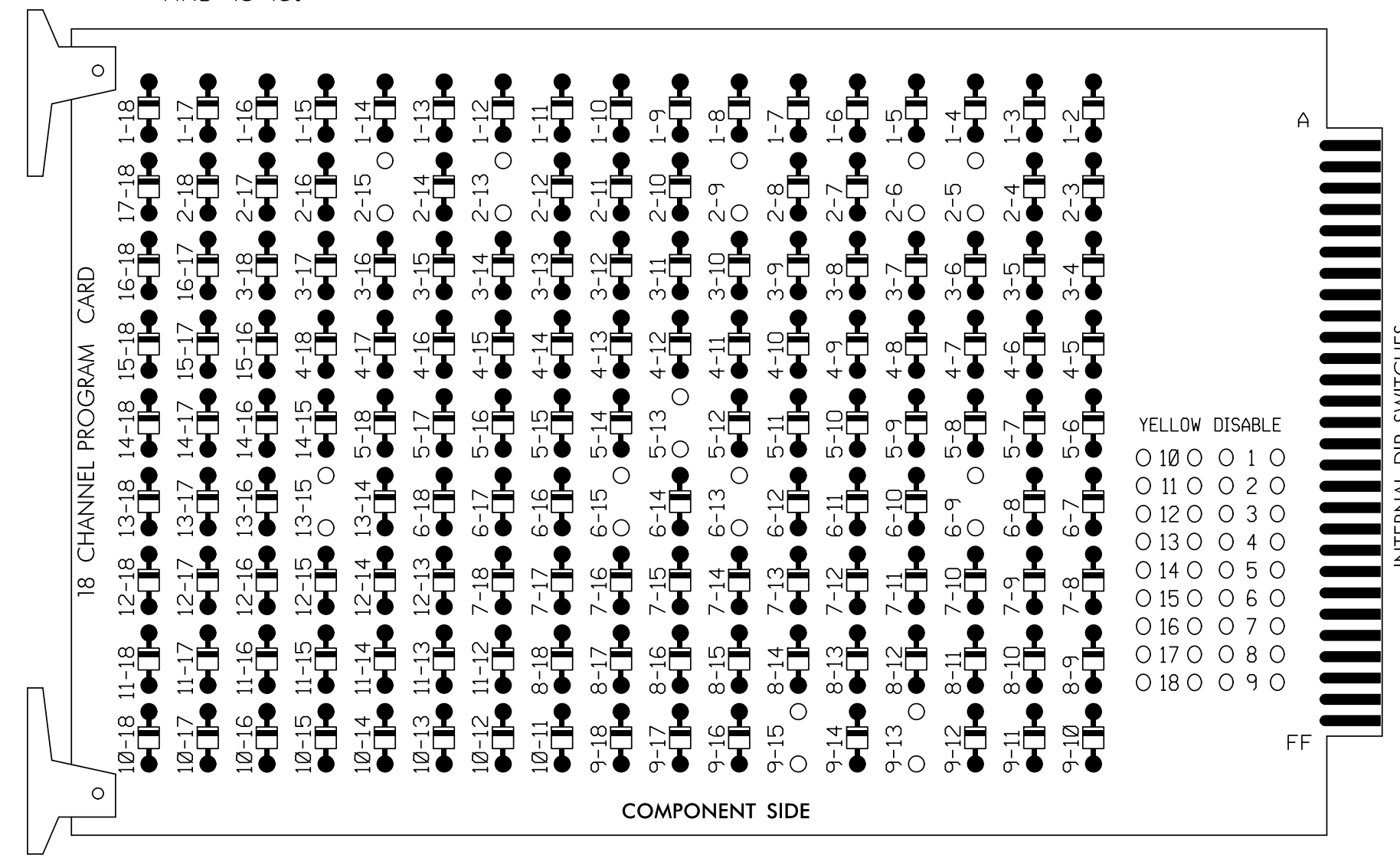


EDI MODEL 2018ECLip-NC CONFLICT MONITOR PROGRAMMING DETAIL

(remove jumpers and set switches as shown)

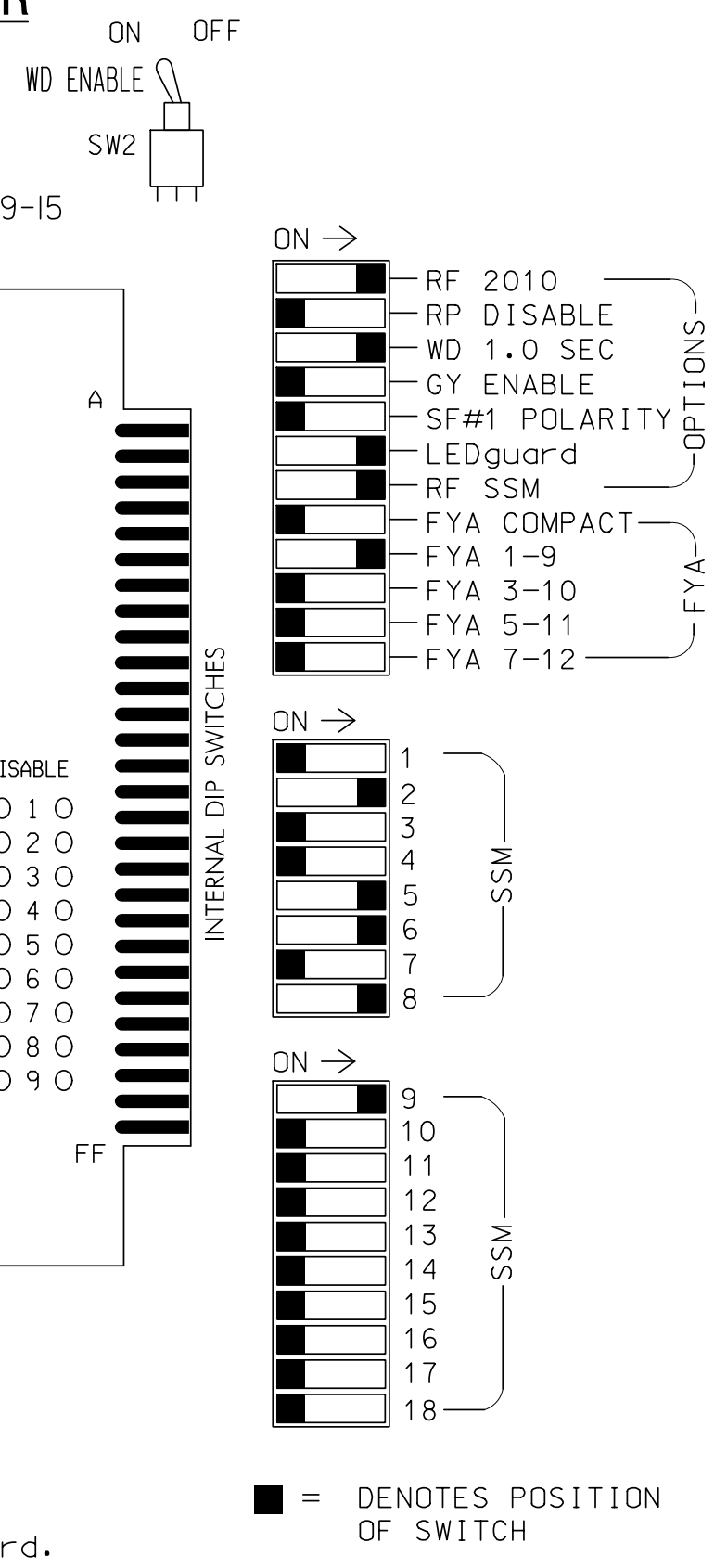
REMOVE DIODE JUMPERS 2-5, 2-6, 2-9, 2-13, 2-15, 5-13, 6-9, 6-13, 6-15, 9-13, 9-15 AND 13-15.



REMOVE JUMPERS AS SHOWN

NOTES:

- Card is provided with all diode jumpers in place. Removal of any jumper allows its channels to run concurrently.
- Ensure jumpers SEL2-SEL5 and SEL9 are present on the monitor board.
- Ensure that Red Enable is active at all times during normal operation.
- Integrate monitor with Ethernet network in cabinet.



NOTES

- To prevent "flash-conflict" problems, insert red flash program blocks for all unused vehicle load switches in the output file. The installer shall verify that signal heads flash in accordance with the Signal Plans.
- Program controller to start up in phase 2 Walk and phase 6 Walk.
- The cabinet and controller are part of the SR 1007 (Mebane Oaks Rd) Closed Loop System.

EQUIPMENT INFORMATION

CONTROLLER.....2070
 CABINET.....332 W/ AUX
 SOFTWARE.....ECONOLITE ASC/3-2070
 CABINET MOUNT.....BASE
 OUTPUT FILE POSITIONS...18 WITH AUX. OUTPUT FILE
 LOAD SWITCHES USED.....S2,S3,S7,S8,S9,S11
 AUX S1
 PHASES USED.....2,2PED,5,6,6PED,8
 OVERLAP 'A'.....*
 OVERLAP 'B'.....NOT USED
 OVERLAP 'C'.....NOT USED
 OVERLAP 'D'.....NOT USED
 * See overlap programming detail on sheet 2

SIGNAL HEAD HOOK-UP CHART

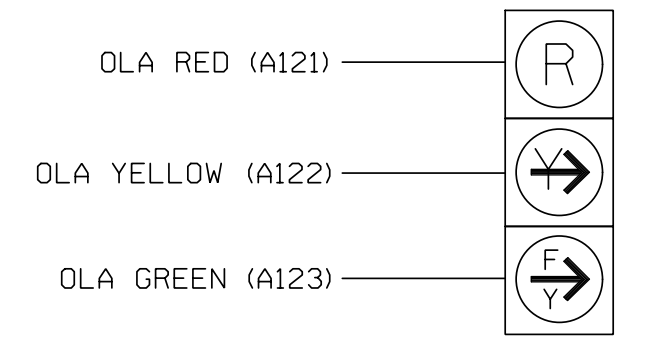
LOAD SWITCH NO.	S1	S2	S3	S4	S5	S6	S7	S8	S9	S10	S11	S12	AUX S1	AUX S2	AUX S3	AUX S4	AUX S5	AUX S6
CMU CHANNEL NO.	1	2	13	3	4	14	5	6	15	7	8	16	9	10	17	11	12	18
PHASE	1	2	2 PED	3	4	4 PED	5	6	6 PED	7	8	8 PED	OLA	OLB	SPARE	OLC	OLD	SPARE
SIGNAL HEAD NO.	NU	21,22	P21, P22	NU	NU	NU	51, 52 53	61,62 63	65	P61, P62	NU	81,82 83,84 85	64	NU	NU	NU	NU	NU
RED		128						134	134		107	107	A121					
YELLOW		129						135	135		108							
GREEN								136			109							
RED ARROW								131										
YELLOW ARROW								132			108		A122					
FLASHING YELLOW ARROW													A123					
GREEN ARROW		130						133	136		109							
Hand icon														119				
Walking person icon																		121

NU = Not Used

★ See pictorial of head wiring in detail this sheet.

FYA SIGNAL WIRING DETAIL

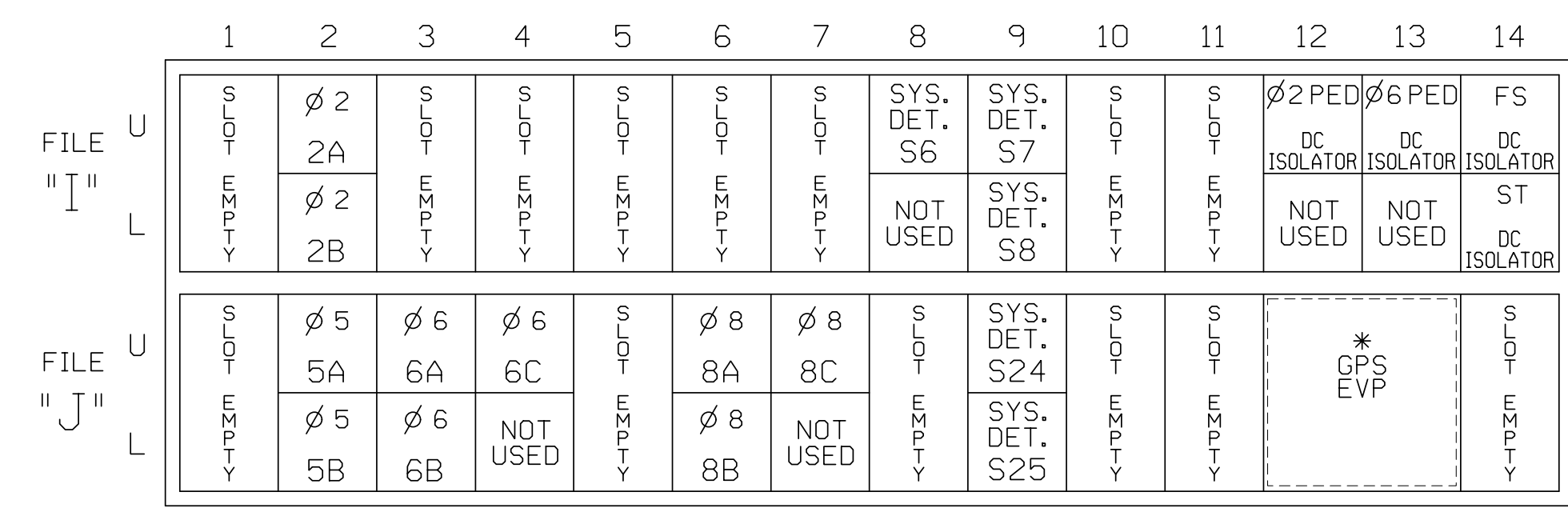
(wire signal heads as shown)



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INPUT FILE POSITION LAYOUT

(front view)



EX.: 1A, 2A, ETC. = LOOP NO.'S FS = FLASH SENSE ST = STOP TIME See GPS Preemption Installation Note Below

SPECIAL DETECTOR NOTE

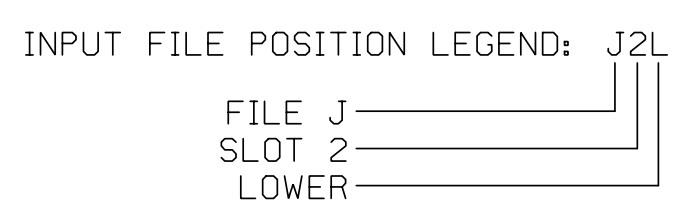
Install a GPS preemption system. Perform installation according to manufacturer's directions and NCDOT engineer approved mounting location to accomplish the preemption schemes shown on the Signal Design Plans.

INPUT FILE CONNECTION & PROGRAMMING CHART

LOOP NO.	LOOP TERMINAL	INPUT FILE POS.	PIN NO.	DETECTOR NO.	NEMA PHASE	CALL	EXTEND TIME	DELAY TIME	ADDED INITIAL	DETECTOR TYPE
2A	TB2-5,6	I2U	39	2	2	YES				S
2B	TB2-7,8	I2L	43	12	2	YES				S
5A	TB3-5,6	J2U	40	6	5	YES				S
5B	TB3-7,8	J2L	44	16	5	YES				S
6A	TB3-9,10	J3U	64	36	6	YES				S
6B	TB3-11,12	J3L	77	46	6	YES				S
6C	TB5-1,2	J4U	48	26	6	YES				S
8A	TB5-9,10	J6U	42	8	8	YES				S
8B	TB5-11,12	J6L	46	18	8	YES		15		S
8C	TB7-1,2	J7U	66	38	8	YES		15		S
* S6	TB6-5,6	I8U	49	24	SYS	NO				N
* S7	TB6-9,10	I9U	60	11	SYS	NO				N
* S8	TB6-11,12	I9L	62	13	SYS	NO				N
* S24	TB7-9,10	J9U	59	15	SYS	NO				N
* S25	TB7-11,12	J9L	61	17	SYS	NO				N
PED PUSH BUTTONS										
P21,P22	TB8-4,6	I12U	67	PED 2	2 PED					
P61,P62	TB8-7,9	I13U	68	PED 6	6 PED					

NOTE: INSTALL DC ISOLATORS IN INPUT FILE SLOTS I12 AND I13.

* System detector only. Remove any assigned vehicle phase.

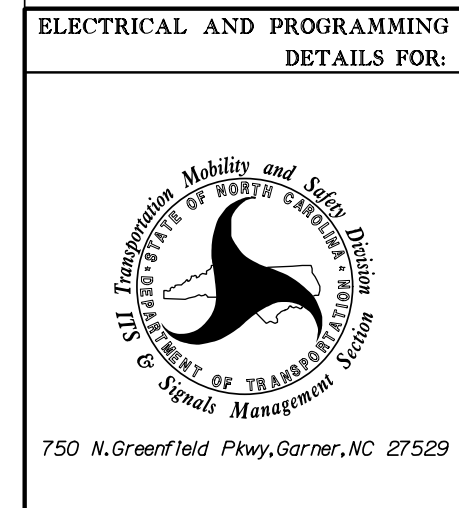


COUNTDOWN PEDESTRIAN SIGNAL OPERATION

Countdown Ped Signals are required to display timing only during Ped Clearance Interval. Consult Ped Signal Module user's manual for instructions on selecting this feature.

THIS ELECTRICAL DETAIL IS FOR THE SIGNAL DESIGN: 07-2024 DESIGNED: November 2019 SEALED: 12/17/2019 REVISED: N/A

Final Design Electrical Detail - Sheet 1 of 2



SR 1007 (Mebane Oaks Road) at I-40 WB / I-85 SB Ramps

Division 7 Alamance County Mebane PLAN DATE: November 2019 REVIEWED BY: Z. "Gavin" Teng PREPARED BY: Z. "Gavin" Teng REVIEWED BY:

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL NORTH CAROLINA PROFESSIONAL ENGINEER ZHAOLONG TENG 032179

REVISIONS INIT. DATE 12/17/2019 DocuSigned by: Zhaolong Teng DATE: 12/17/2019 SIG. INVENTORY NO. 07-2024

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