

**INDEX OF SHEETS**

Sheet No. 1. Title Page

2. Estimate of Quantities

3. Typical Cross Section of Improvement

4. Method of Shoulder Placement in Bridges Standard #303

5. Standard Slopes under drains. See Markers 51, 52, 500

6. Detail for Plain Concrete Endwall. Std #304

7. Method for Superlevation & Widening of Curves. Std #307

8. Standard for Paved Shoulder Gutter. Std #311

9. P&P Profile

10. Cross Sections

11. Structures

FED. ROAD DIST. NO.	STATE	STATE PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
18	N.C.	7424		15	
FA Proj. F-266(B)					

STATE OF NORTH CAROLINA  
STATE HIGHWAY AND PUBLIC WORKS COMMISSION  
PLAN AND PROFILE OF PROPOSED  
STATE HIGHWAY  
**FORSYTH-YADKIN COUNTIES**

Beg. of Sta. 340100 E. of N.C. 67 approx. 1565 Ft. East of Forsyth-Yadkin Co. Line.  
Thence Cross-country in a West direction to Sta. 41169 E. of N.C. 67 approx.  
4195 Ft. West of Forsyth-Yadkin Co. Line. End of Project.

**SCALES**

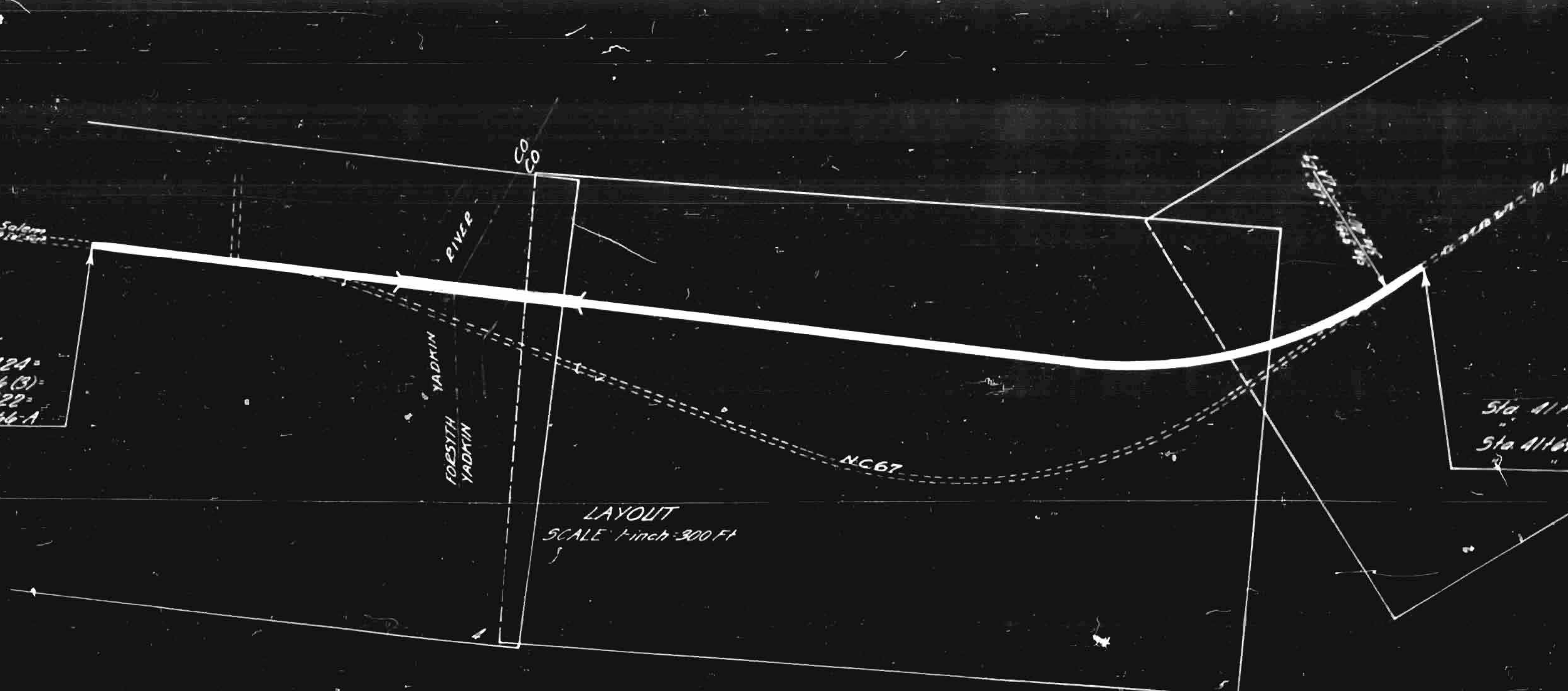
PLAN: 1-inch = 100 Ft.  
PROFILE: 1-inch = 100 Ft. HOR.  
PROFILE: 1-inch = 10 Ft. VERT.



SKETCH MAP  
SHOWING SHIPPING POINTS IN THE  
VICINITY OF STATE PROJECT  
7424, FORSYTH-YADKIN COUNTIES

Sta. 340100 Beg. State Proj. 7424  
FA Proj. F-266(B)  
Sta. 340100 on Old State Proj. 7422  
FA Proj. F-266-A

To Winston-Salem  
Rt. 251 N.W.



LAYOUT  
SCALE 1-inch = 300 Ft.

Sta. 41169 End State Proj. 7424  
FA Proj. F-266(B)  
Sta. 41169 on Old State Proj. 7424  
FA Proj. F-266-B

Prepared in Office of  
STATE HIGHWAY AND PUBLIC WORKS COMMISSION  
RALEIGH, N. C.

Surveyed by: H.C. THOMPSON  
Plans Prepared by: T.B. MADDY  
Date: July 1952

State Standard Specifications.  
Approved by Bureau Control.

Length of Roadway FA Proj. F-266(B) = 0.874 Miles  
Structures = 0.205  
Total Length of State Proj. 7424 = 1.079

Right of Way on this Project is 100 Ft. Wide

**CONVENTIONAL SIGNS**

County Line	_____
Township Line	_____
City or Town Line	_____
Right of Way Line	_____
Survey Line	_____
Property Line	_____
Wire Fence	_____
Board Fence	_____
Present Traveled Road	_____
Railroad	_____
Grade Elevation	DATUM _____
Ground Elevation	DATUM _____
Pipe Culvert	_____
Box Culvert	_____
Woods	_____
Telephone or Telegraph Pole	_____
Tower Pole and Line	_____
Power Pole	_____
Proposed No. Markers	_____

APPROVED: *W.H. Rogers*

DEPARTMENT OF COMMERCE  
BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL:

DISTRICT ENGINEER DATE

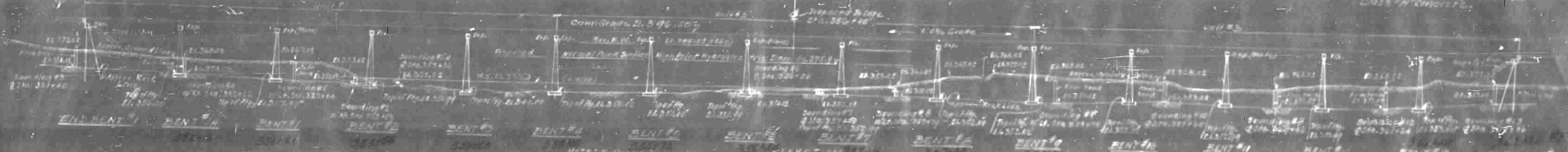
APPROVED:

DIVISION ENGINEER DATE



NOTE: This bridge shall be built on a 2% grade. The abutments, which indicate a slope conform to the grade. The elevations shown are for the bridge deck surface. The elevations shown are for the bridge deck surface. The elevations shown are for the bridge deck surface.

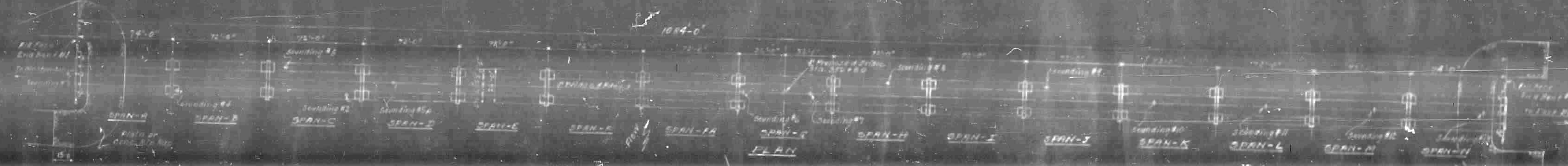
NO.	DATE	BY	REVISION
1	7-24-50	J.B.	



NOTE: The bridge engineer will furnish information as to the bridge structure.

NOTE: The bridge engineer will furnish information as to the bridge structure.

NOTE: The bridge engineer will furnish information as to the bridge structure.



**DESIGN DATA**  
 A.P.C. H.O. 1949  
 15,000 LBS. PER SQ. FT.  
 30 LBS. PER SQ. FT.  
 30 LBS. PER SQ. FT.

**REINFORCING STEEL**  
 Reinforcing steel shall be deformed bars. The diameter of bars shall conform to the requirements of A.P.C. H.O. 1949. The diameter of bars shall conform to the requirements of A.P.C. H.O. 1949.

**STRUCTURAL STEEL**  
 Structural steel shall be of the best quality obtainable. The steel shall conform to the requirements of A.P.C. H.O. 1949. The steel shall conform to the requirements of A.P.C. H.O. 1949.

**PILE FOUNDATION**  
 Pile foundations shall be of the best quality obtainable. The piles shall conform to the requirements of A.P.C. H.O. 1949. The piles shall conform to the requirements of A.P.C. H.O. 1949.

**GENERAL NOTE**  
 The bridge shall be built on a 2% grade. The abutments, which indicate a slope conform to the grade. The elevations shown are for the bridge deck surface. The elevations shown are for the bridge deck surface.

**EXPANSION JOINT MATERIAL**  
 Expansion joint material shall be of the best quality obtainable. The material shall conform to the requirements of A.P.C. H.O. 1949. The material shall conform to the requirements of A.P.C. H.O. 1949.

**HAIRY PLAYS**  
 Two hairy plays will be required. The hairy plays shall conform to the requirements of A.P.C. H.O. 1949. The hairy plays shall conform to the requirements of A.P.C. H.O. 1949.

**MATERIAL AND WORKMANSHIP**  
 All materials shall be of the best quality obtainable. The workmanship shall conform to the requirements of A.P.C. H.O. 1949. The workmanship shall conform to the requirements of A.P.C. H.O. 1949.

TOTAL BILL OF MATERIAL							
Item	Quantity	Unit	Price	Total	Remarks	Notes	Other
Superstructure	100	cu. yd.	10.00	1000.00			
Bent #1	100	cu. yd.	10.00	1000.00			
Bent #2	100	cu. yd.	10.00	1000.00			
Bent #3	100	cu. yd.	10.00	1000.00			
Bent #4	100	cu. yd.	10.00	1000.00			
Bent #5	100	cu. yd.	10.00	1000.00			
Bent #6	100	cu. yd.	10.00	1000.00			
Bent #7	100	cu. yd.	10.00	1000.00			
Bent #8	100	cu. yd.	10.00	1000.00			
Bent #9	100	cu. yd.	10.00	1000.00			
Bent #10	100	cu. yd.	10.00	1000.00			
Bent #11	100	cu. yd.	10.00	1000.00			
Bent #12	100	cu. yd.	10.00	1000.00			
<b>TOTAL</b>	<b>1000</b>	<b>cu. yd.</b>	<b>10.00</b>	<b>10000.00</b>			

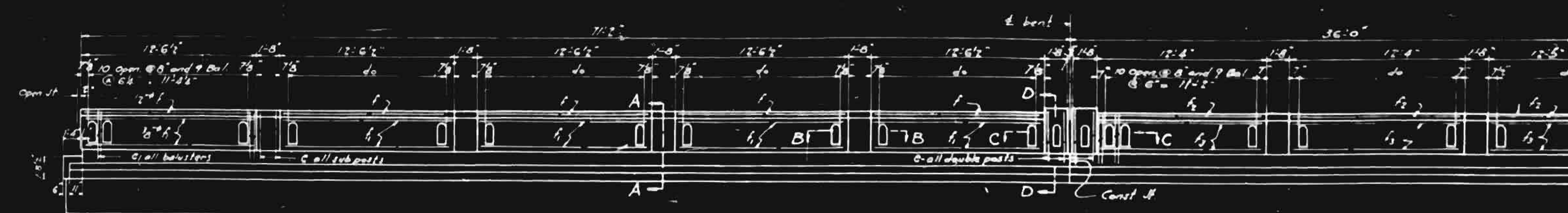
PROJECT NO. 7424  
 FOR THE ROAD DISTRICT OF  
 STATION: 356+30

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION  
 GENERAL DRAWING  
 FOR BRIDGE NO. 7424  
 BETWEEN THE STATES OF  
 NORTH CAROLINA AND  
 VIRGINIA  
 JULY 1950

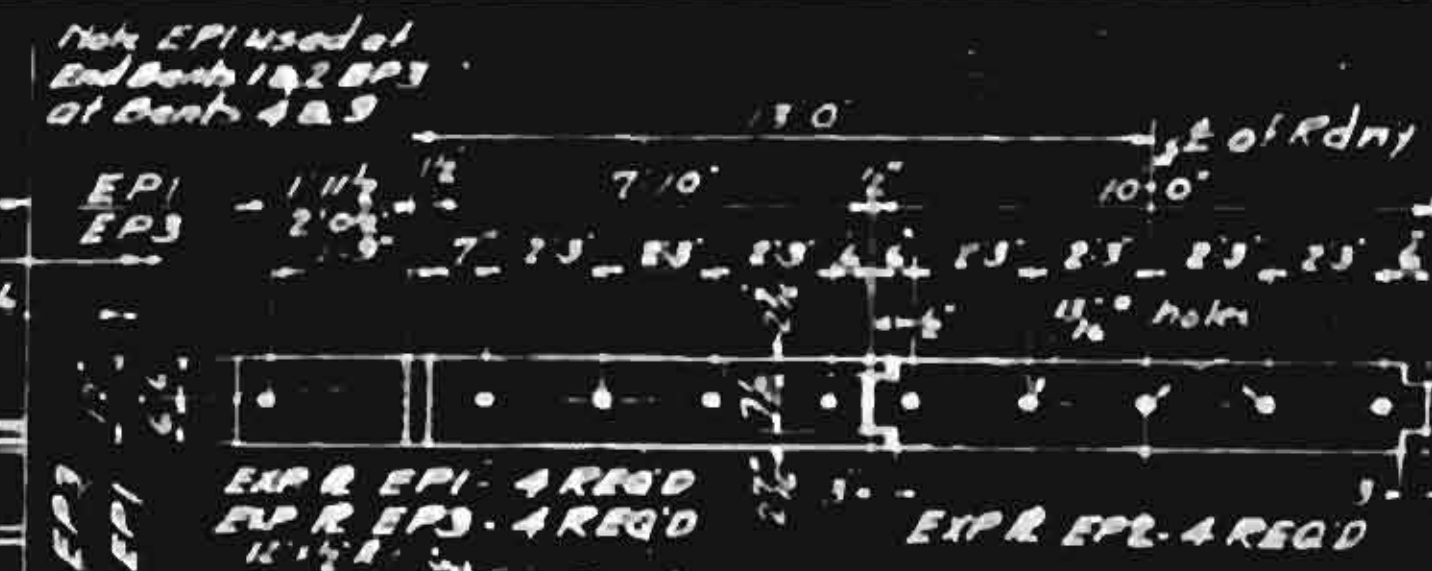
DESIGNED BY J.B. Furr  
 CHECKED BY W.H. Rogers

7424

PROJECT NO.	STATE	PROJECT NO.	SHEET NO.
7424	N.C.	7424	1



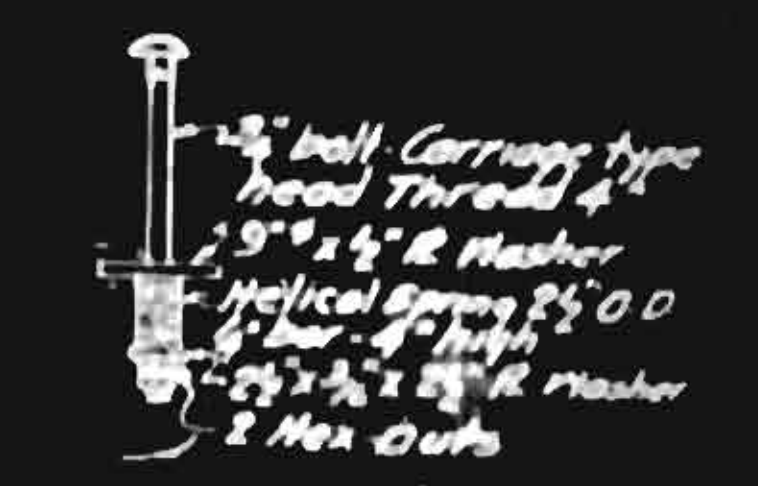
ELEVATION



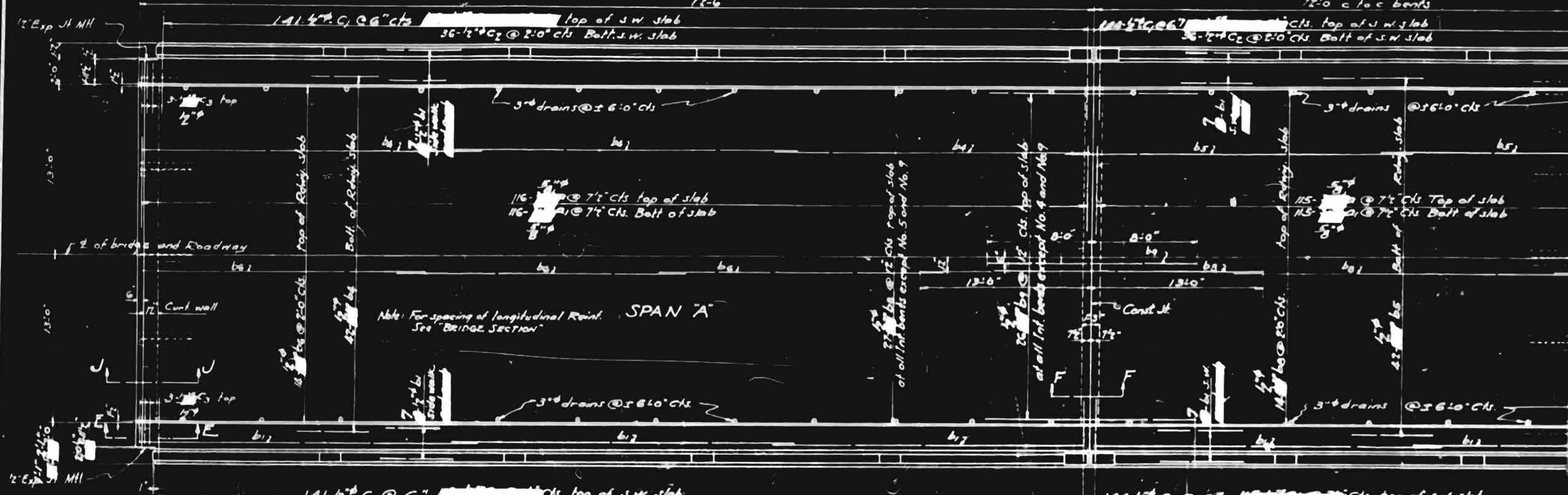
EXPANSION PLATE DETAIL

Note: Checkered portion of plate which comes in contact with ball heads shall be removed.

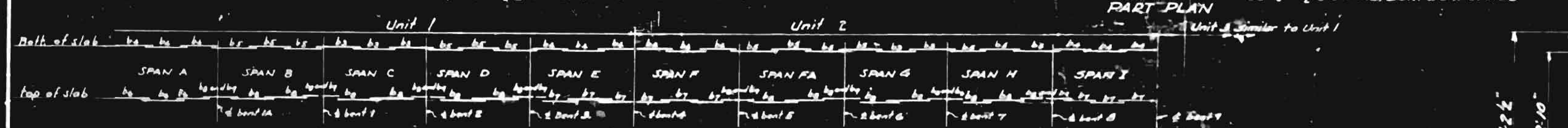
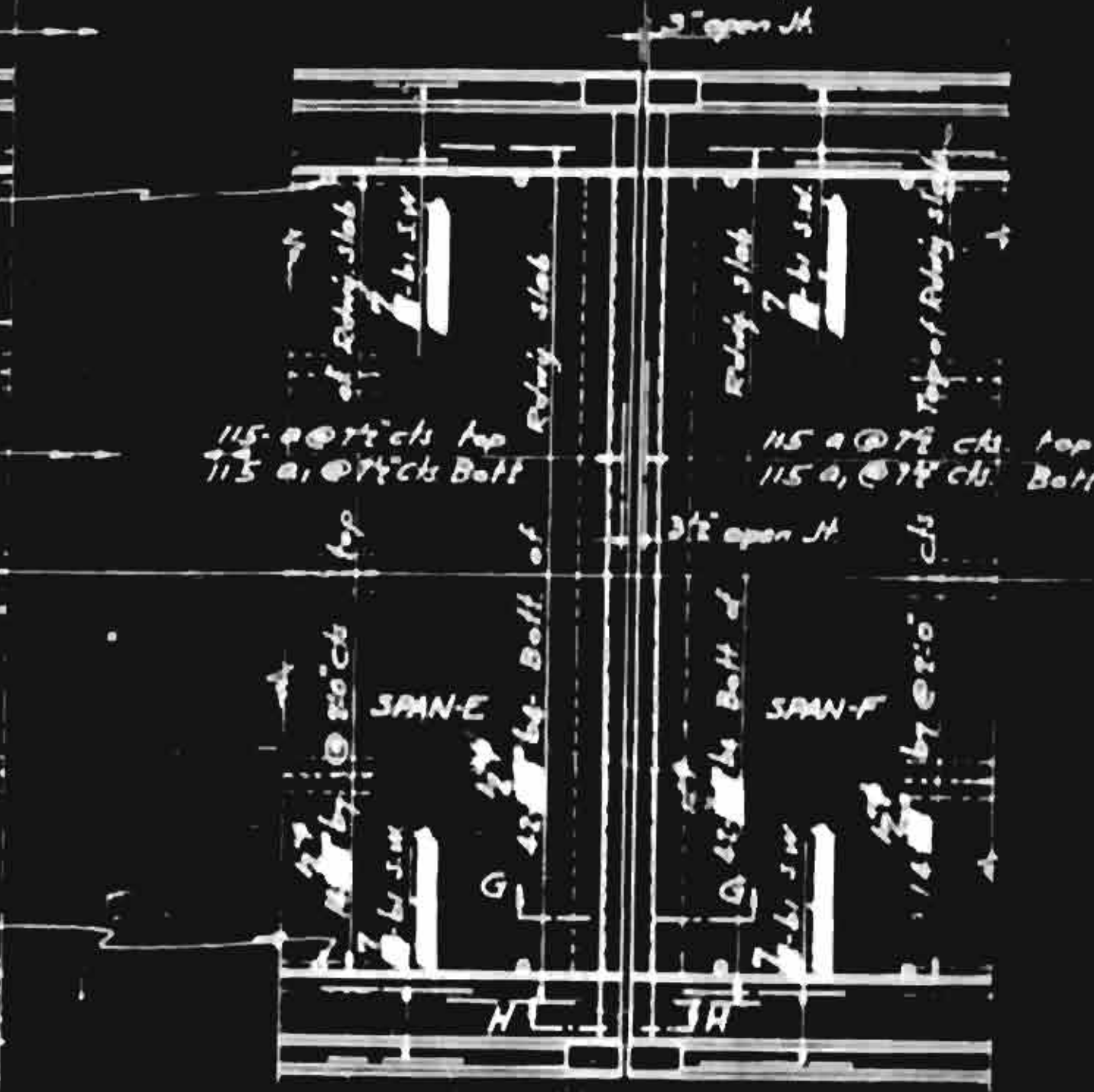
Note: Expansion Plates to be U.S. Steel Multi Grip, A.M. Super Diamond Fiber Plate or approved equal.



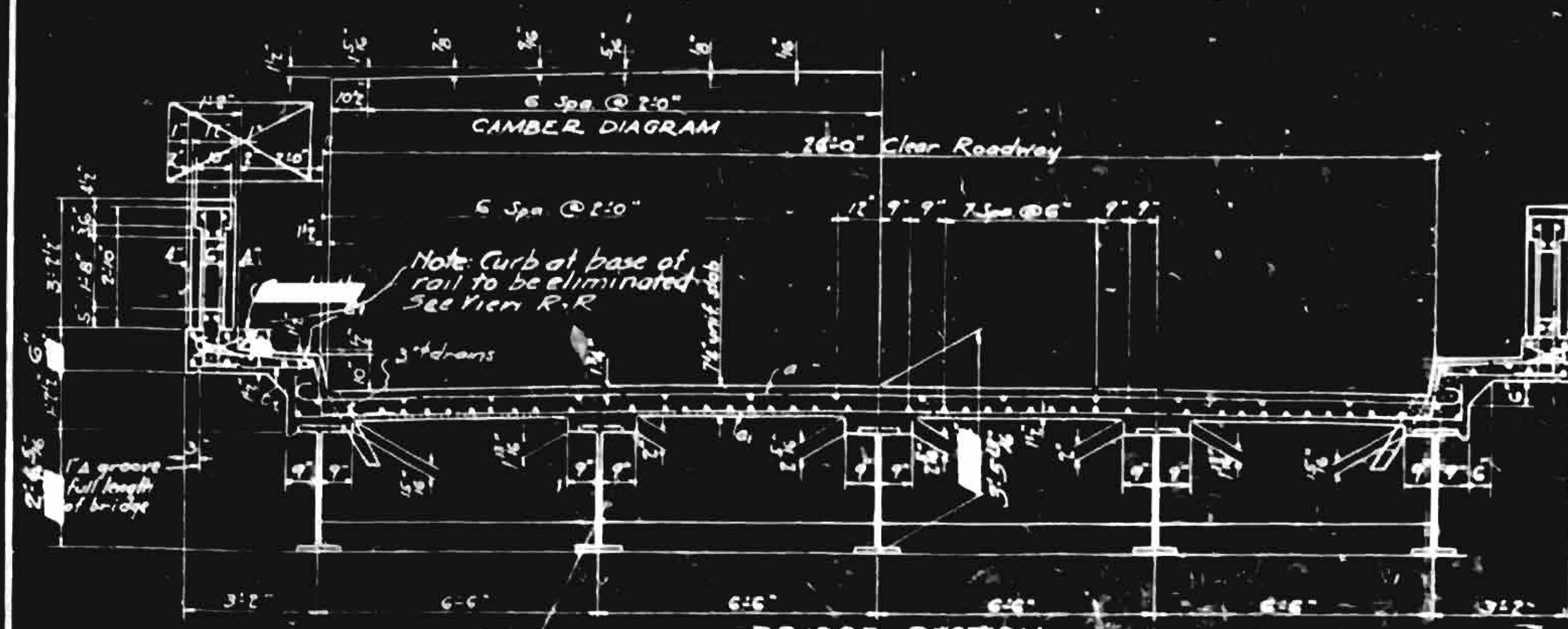
EXPANSION PLATE BOLT ASSEMBLY



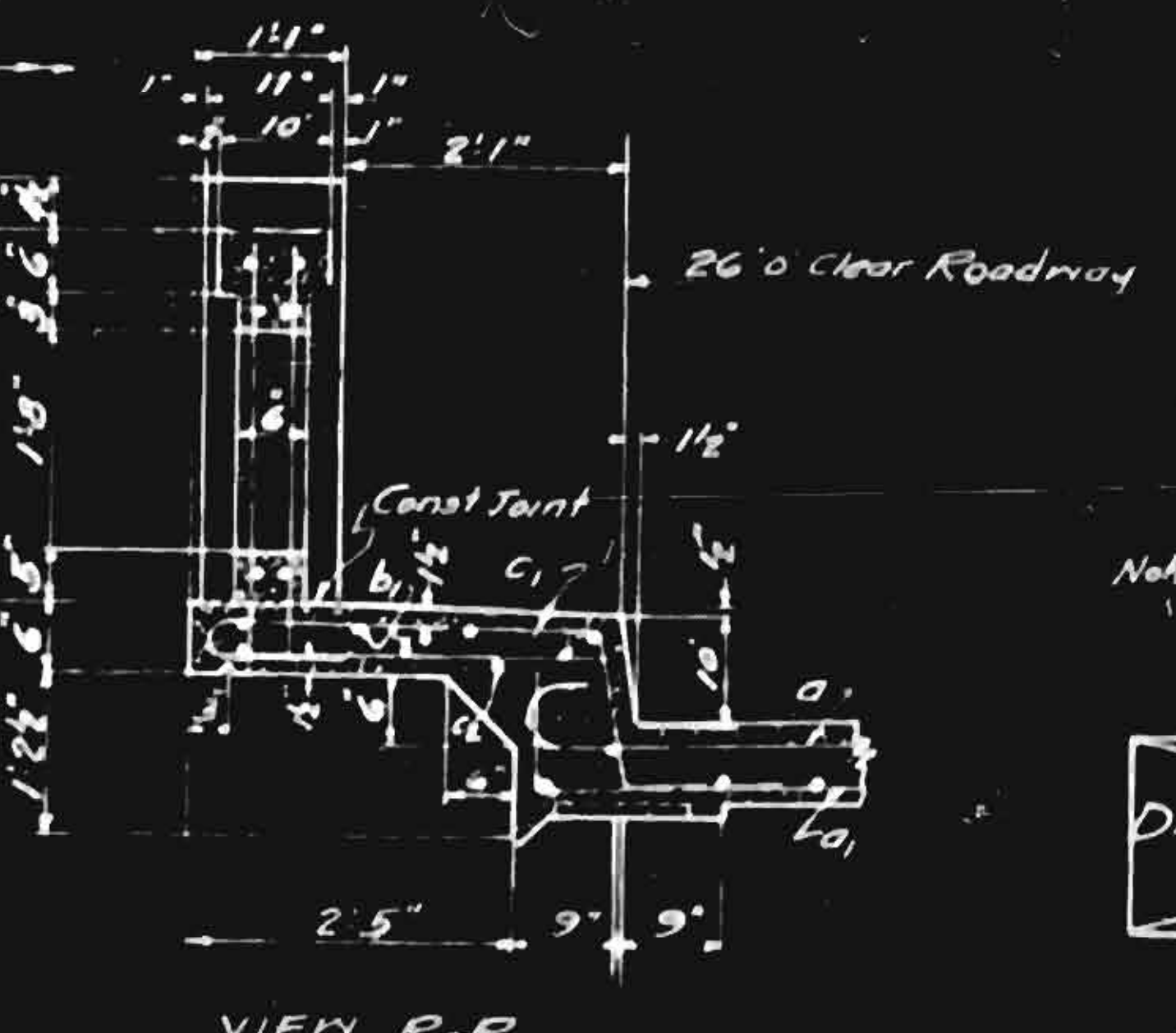
PART PLAN



CAMBER DIAGRAM



BRIDGE SECTION



VIEW R-R

Note: For Design Data and Gen. Note see Gen. Plan. For superstructure details not shown on this sheet see following sheet.

PROJECT NO. 7311  
DAVIE AND FORSYTH COUNTY  
STA. 431+64

PROJECT NO. 7424  
FORSYTH AND YADKIN COUNTIES  
STATION 356+80

STATE OF NORTH CAROLINA  
STATE HIGHWAY AND  
PUBLIC WORKS COMMISSION  
SUPERSTRUCTURE DETAILS  
FOR  
BRIDGE OVER YADKIN RIVER

FEBRUARY 1940

Assem. by R.S.M. checked by J.A. July 13, 1940

Note: All vertical dimensions indicated on this sheet are from the top of the roadway surface unless otherwise noted.

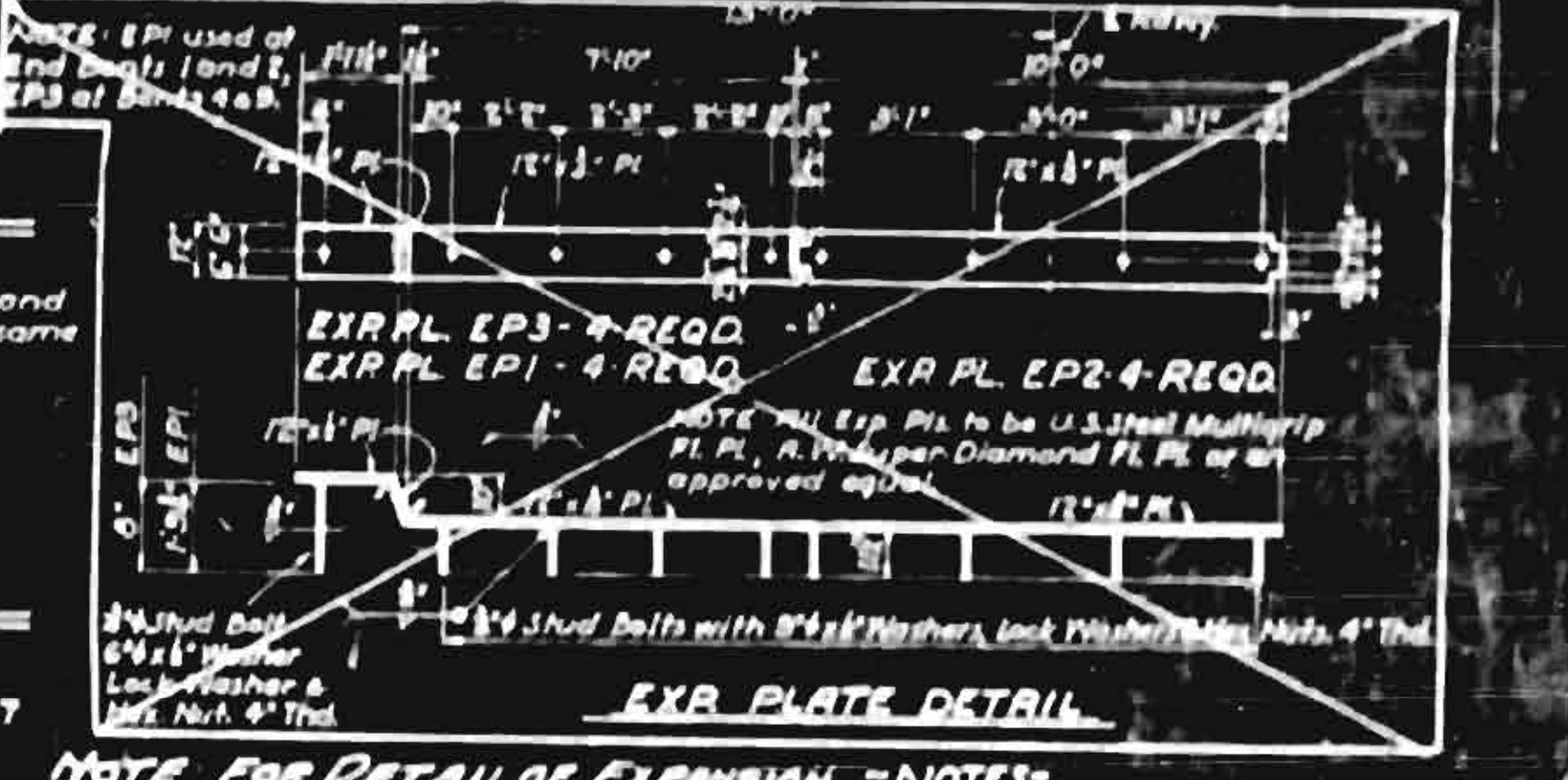
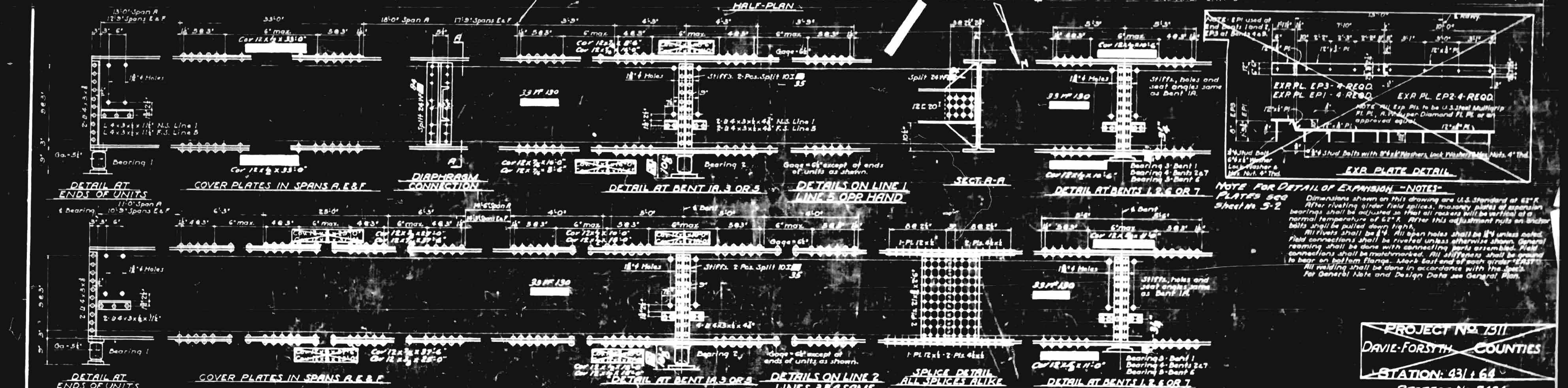
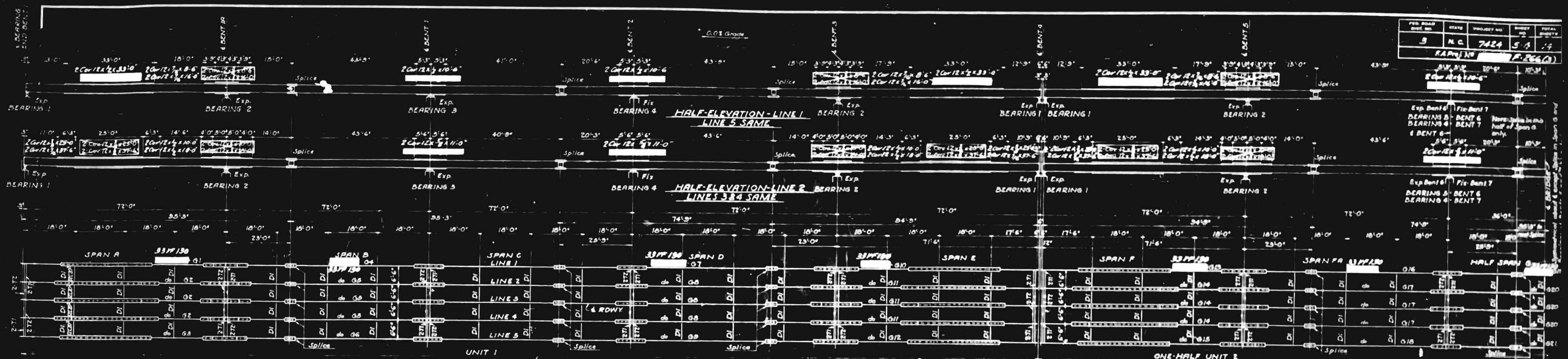
SPECIAL	DESIGNED BY	DATE
	BY	
	CHECKED BY	DATE
	BY	

PART SECTION THRU EXTERIOR GOR. BM

Edward T. DeLoe, C.E., P.E., Eng. Div.



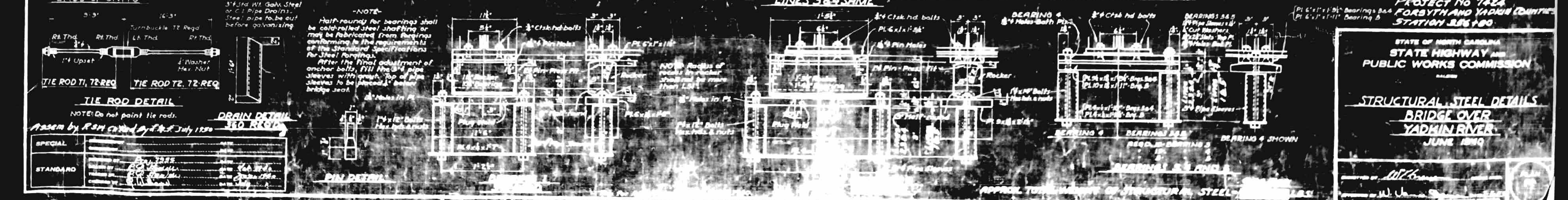
Proj. No.	Sheet No.	Total Sheets
7424	54	172



**NOTE FOR DETAIL OF EXPANSION - NOTES**

Dimensions shown on this drawing are U.S. Standard at 62°F. Bearings shall be adjusted so that all rollers will be vertical at a normal temperature of 62°F. After this adjustment rolls on under balls shall be pulled down 1/8".

All rivets shall be 1/4" unless noted. Field connections shall be riveted unless otherwise shown. General riveting shall be done with connecting parts assembled. Field connections shall be matchmarked. All stiffeners shall be placed to bear on bottom flanges. Work last end of each girder shall be done in accordance with the design. For General Note and Design Data see General Plan.



PROJECT NO. 1511  
DAVE FORSTH COUNTIES  
STATION 431 + 64

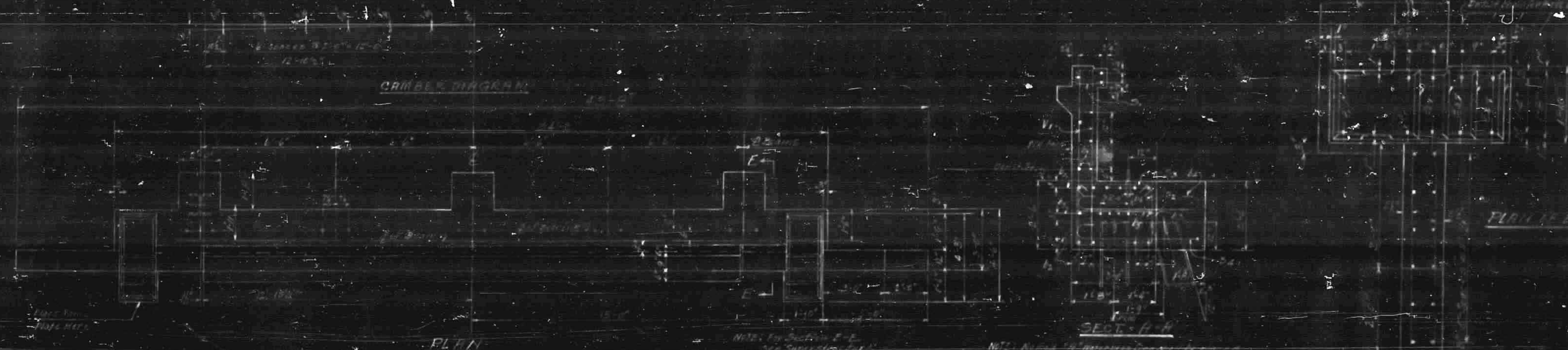
PROJECT NO. 7424  
FORSTH AND YADKIN COUNTIES  
STATION 386 + 80

STATE OF NORTH CAROLINA  
STATE HIGHWAY AND  
PUBLIC WORKS COMMISSION

STRUCTURAL STEEL DETAILS  
BRIDGE OVER  
YADKIN RIVER  
JUNE 1970

W. J. ...  
Prof. 250 Sheet 2

FED. ROAD DIST. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	N. C.	7424	25	25



PLAN OF WHITES				PLAN OF ENLARGED			
F.S.	VE.	PAV.	SP.	B.	HE.	ILL.	TOP.
1	1	1	1	1	1	1	1
2	2	2	2	2	2	2	2
3	3	3	3	3	3	3	3
4	4	4	4	4	4	4	4
5	5	5	5	5	5	5	5
6	6	6	6	6	6	6	6
7	7	7	7	7	7	7	7
8	8	8	8	8	8	8	8
9	9	9	9	9	9	9	9
10	10	10	10	10	10	10	10

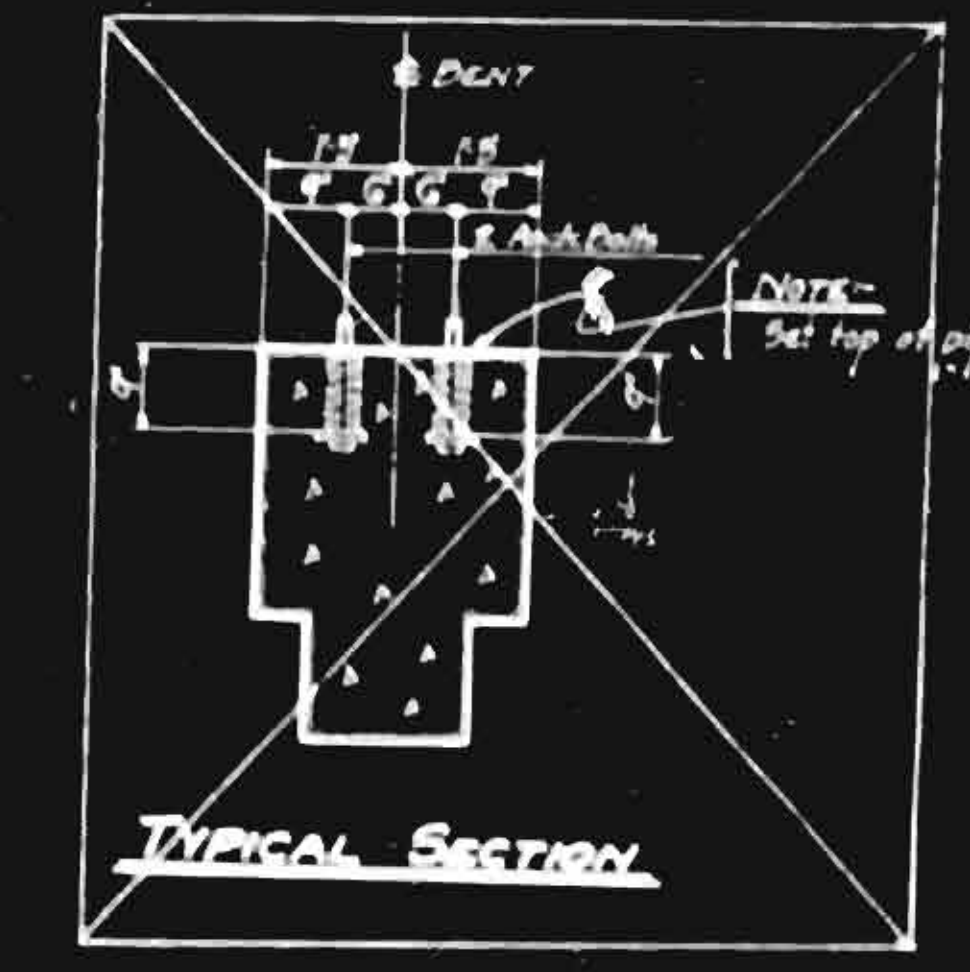
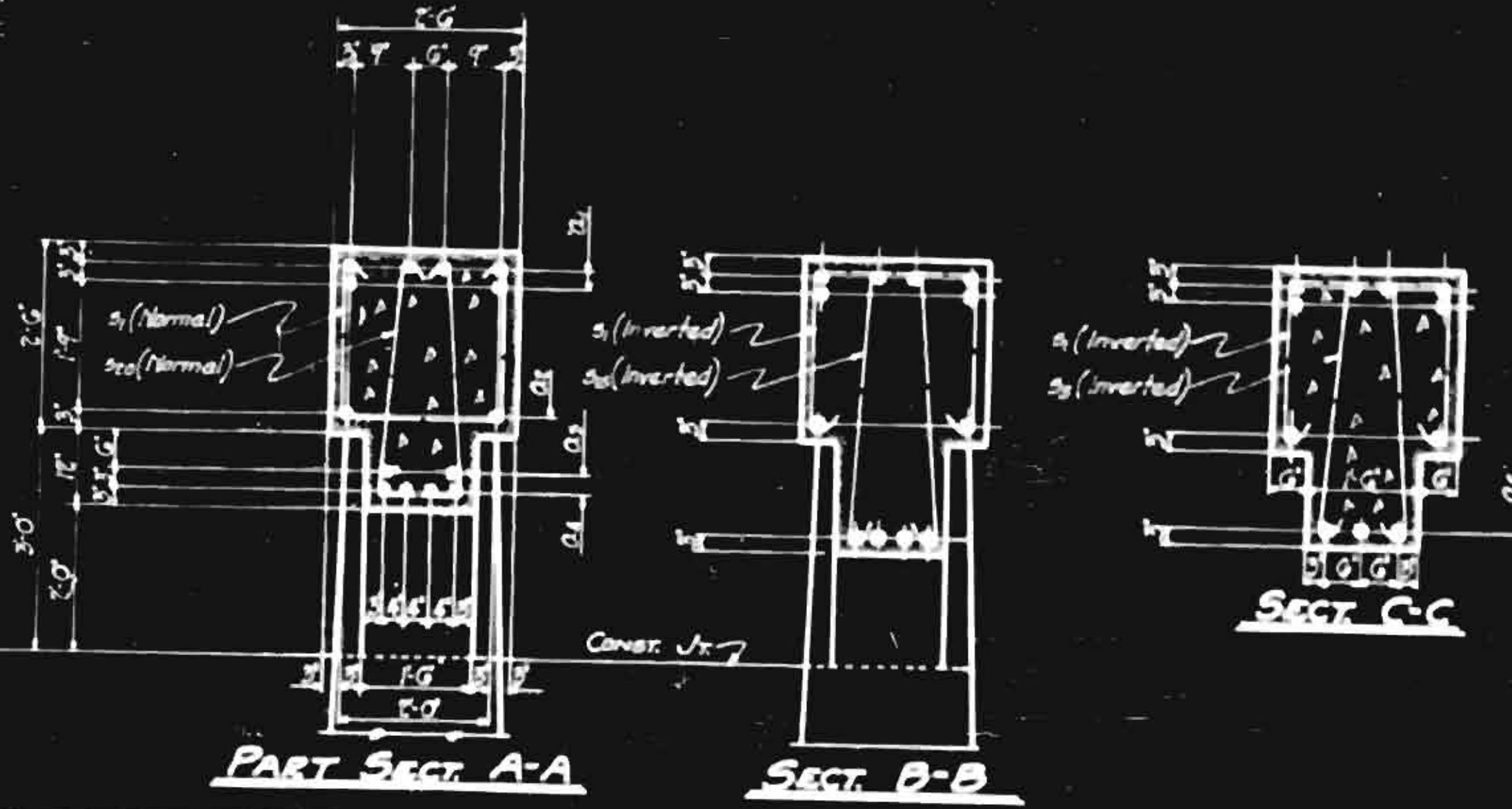
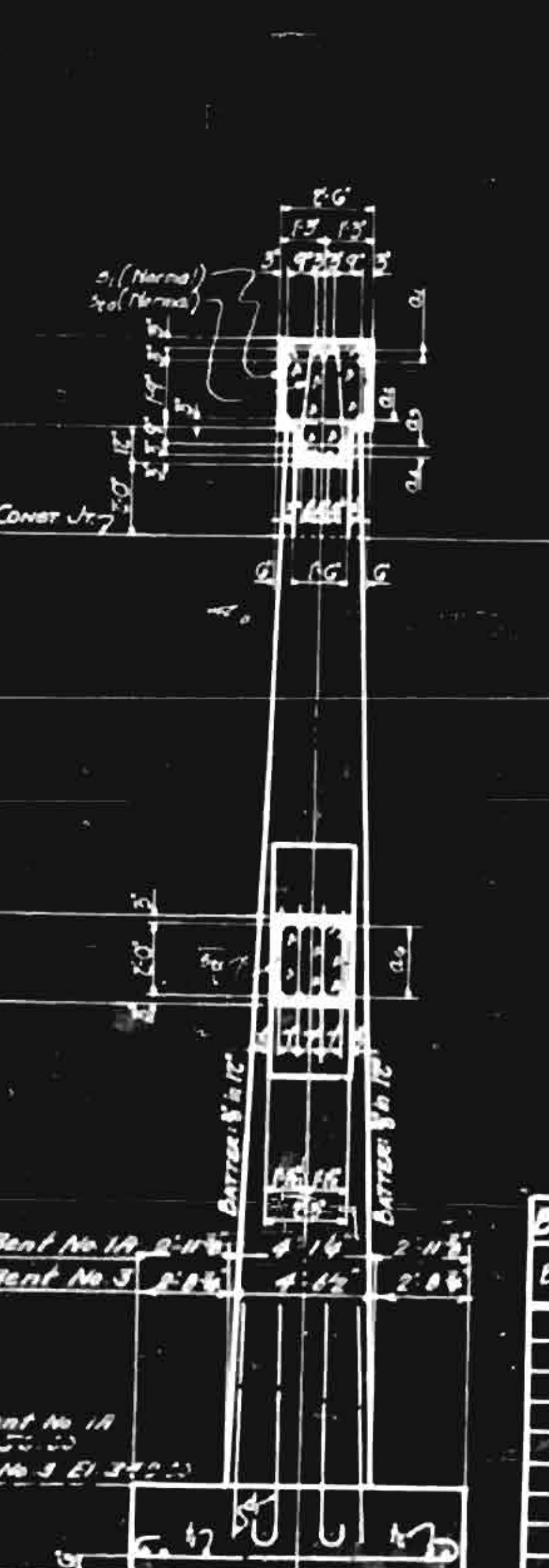
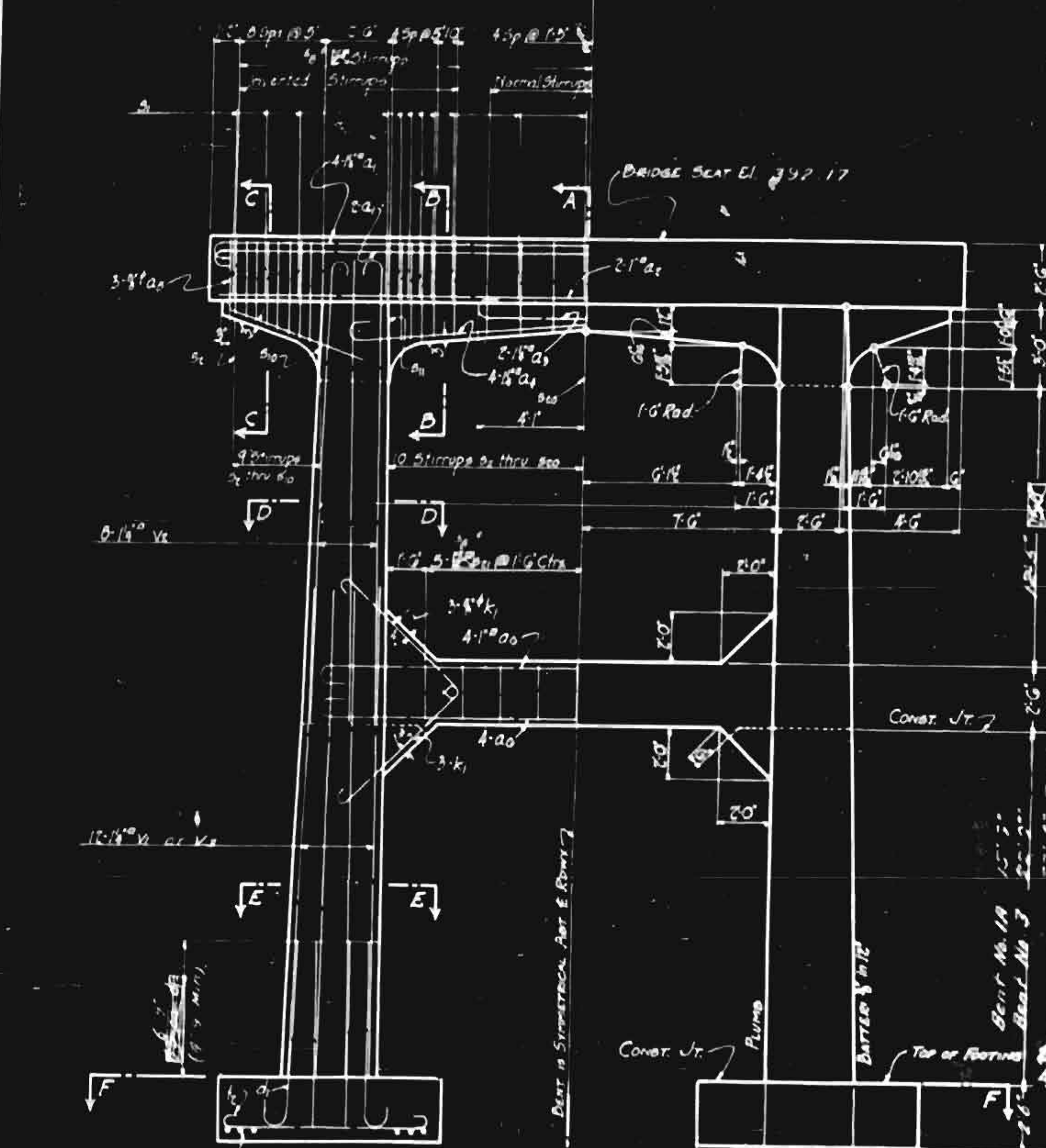
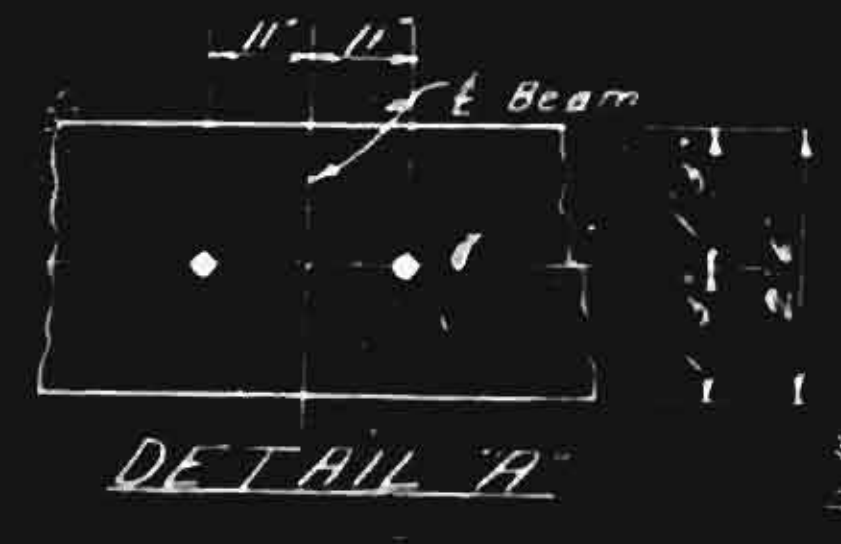
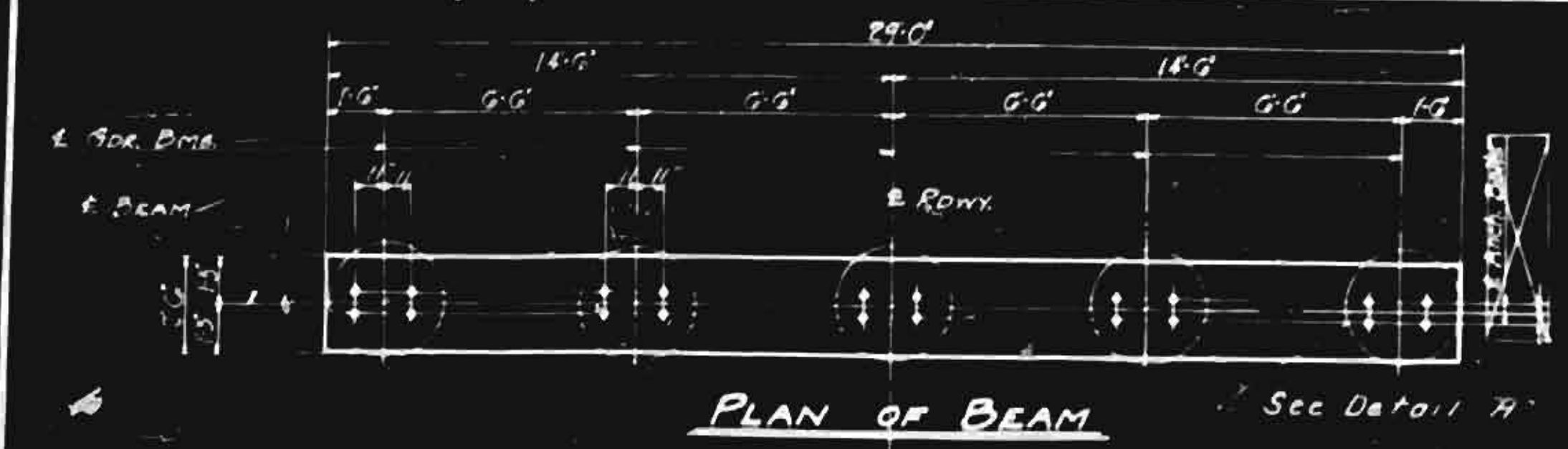
PROJECT NO. 7424  
 COUNTY COUNTRIES  
 STATION:

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

J. B. Smith  
 W. B. Smith

SPECIAL	DESIGNED BY	DATE
STANDARD	CHECKED BY	DATE

NO. DRAW.	DATE	PROJECT NO.	POST	SCALE
3	M.C.	7424	56	1/4"
FA Proj. F-266(C)				



**BILL OF MATERIAL FOR ONE BENT (BENT 1A OR 3)**

BAR	No.	SIZE	TYPE	Dimensions			LENGTH	WEIGHT
				a	b	c		
a <sub>1</sub>	0	#4	S	27'-10"	1'-6"	0"	30'-10"	485
a <sub>2</sub>	1	#4	S	27'-10"	1'-6"	0"	28'-6"	475
a <sub>3</sub>	2	#4	S	7'-6"	1'-6"	0"	10'-6"	175
a <sub>4</sub>	1	#4	S	7'-6"	1'-6"	0"	10'-6"	175
a <sub>5</sub>	6	#4	S	7'-6"	1'-6"	0"	10'-6"	1050
a <sub>6</sub>	0	#4	S	17'-9"	1'-6"	0"	18'-0"	300
a <sub>7</sub>	0	#4	S	17'-9"	1'-6"	0"	18'-0"	300
a <sub>8</sub>	12	#4	S	2'-6"	1'-6"	0"	7'-0"	264
a <sub>9</sub>	0	#4	S	2'-6"	1'-6"	0"	7'-0"	264
a <sub>10</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>11</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>12</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>13</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>14</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>15</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>16</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>17</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>18</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>19</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>20</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>21</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>22</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>23</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>24</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>25</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>26</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>27</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>28</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>29</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>30</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>31</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>32</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>33</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>34</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>35</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>36</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>37</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>38</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>39</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528
a <sub>40</sub>	2	#4	S	2'-6"	1'-6"	0"	7'-0"	528

**NOTE:**  
 For General Note and Design Data see General Drawing.  
 For detail of Arch Bent see Typical Drawing for Arch Bent.  
 Bent shall be carried down at least 4' into rock and shall have the minimum thickness of that shown in this drawing.

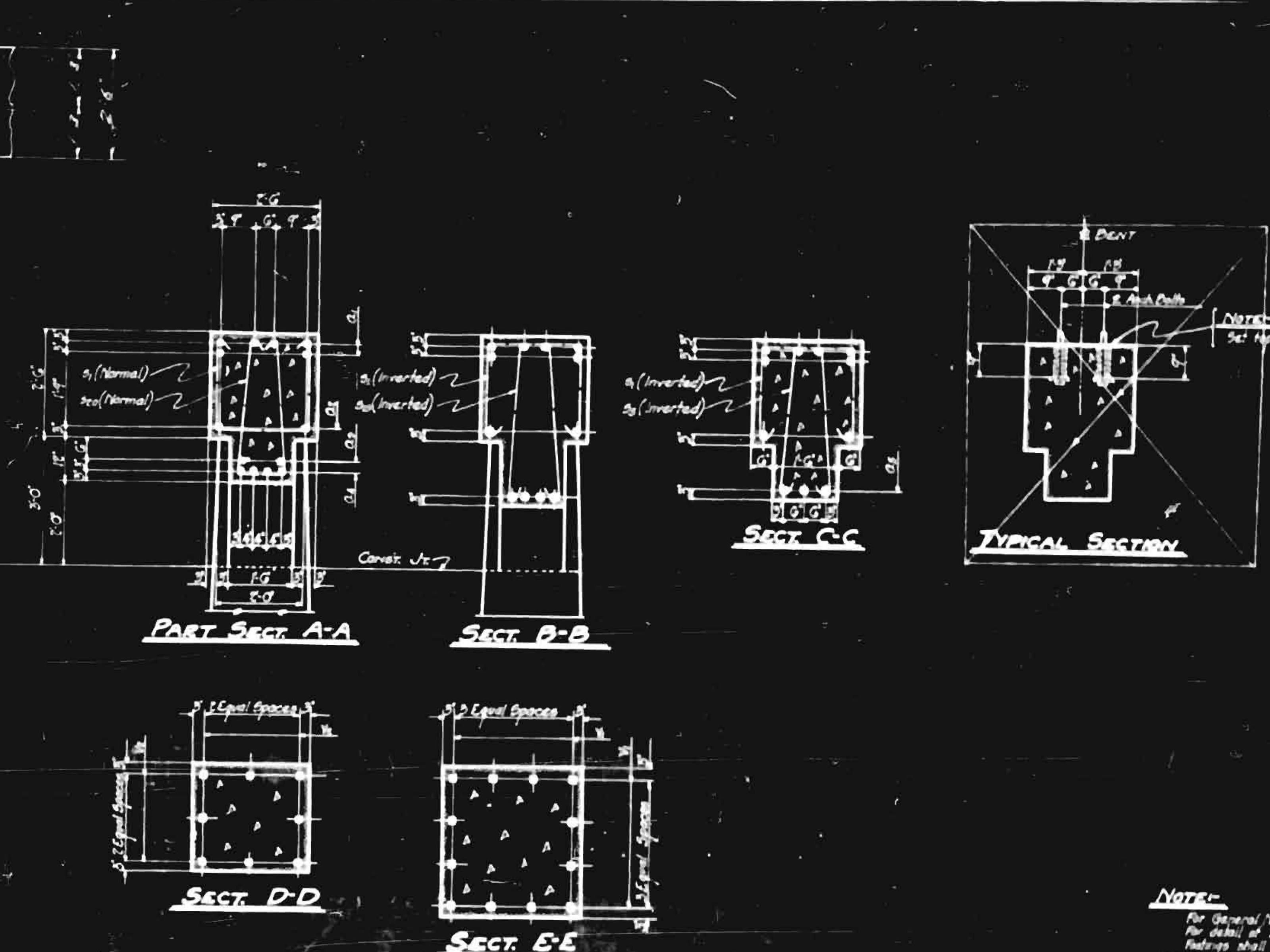
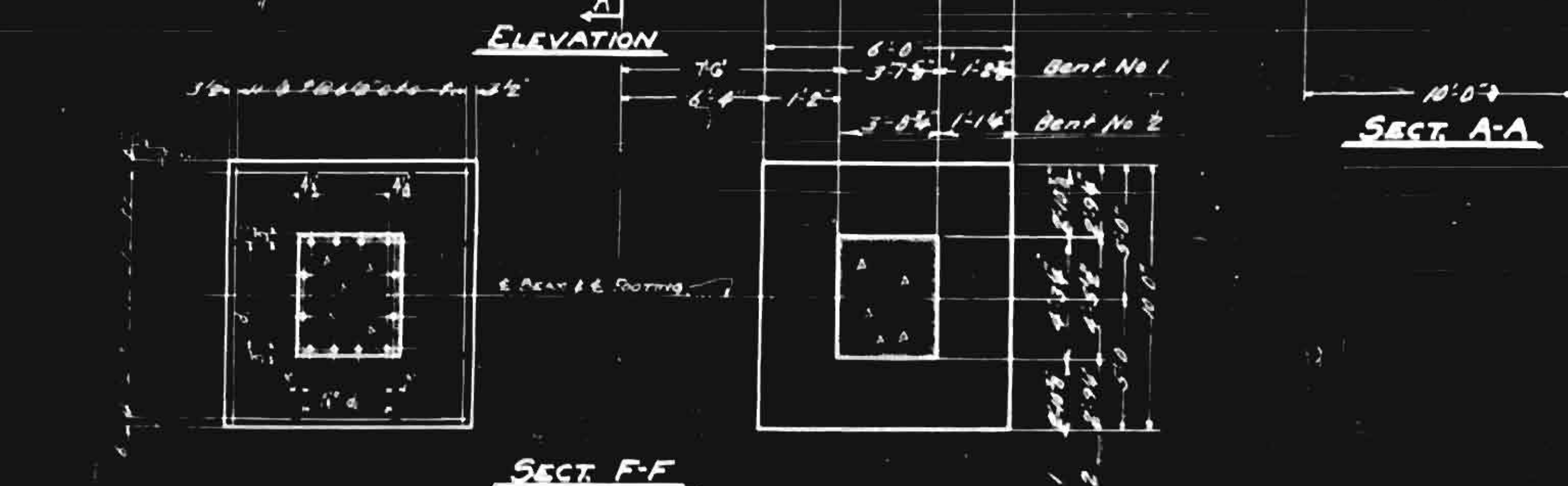
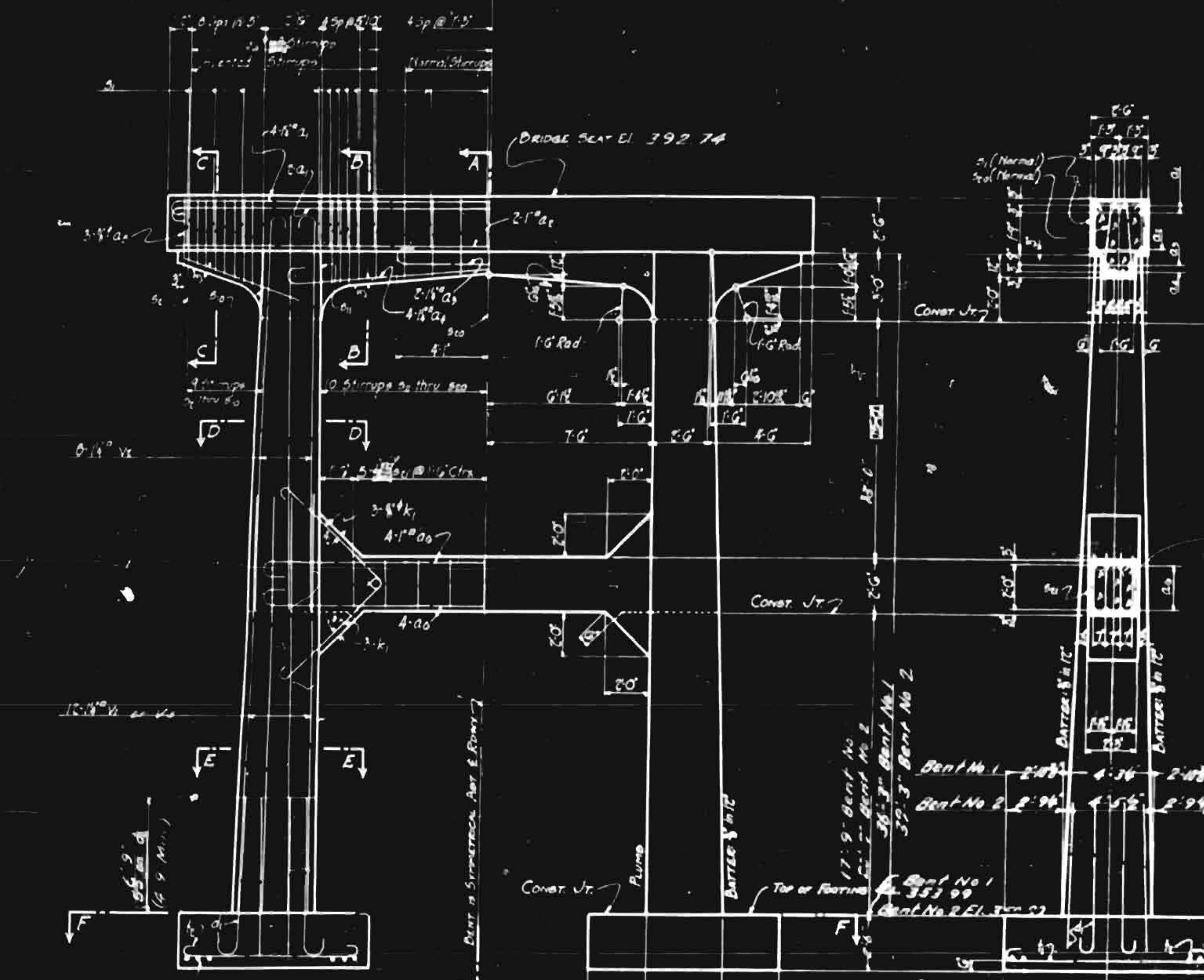
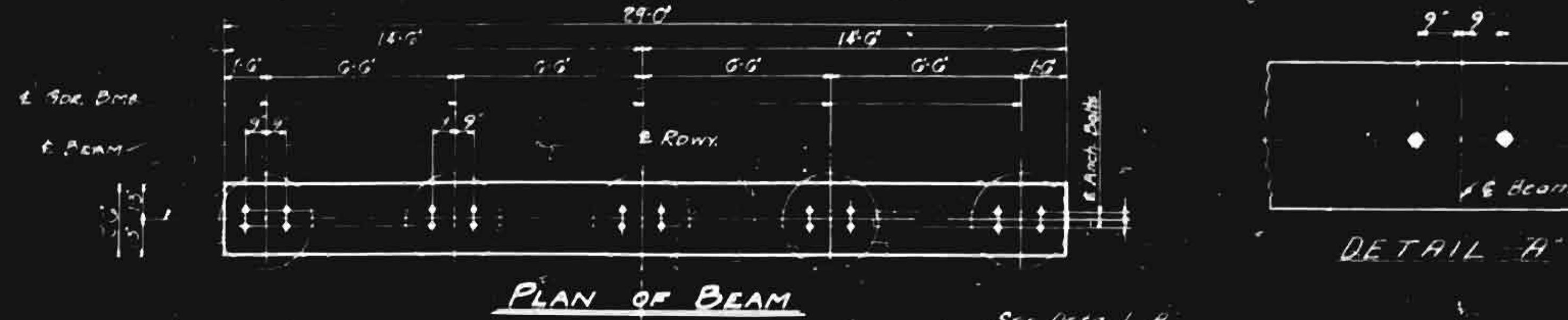
PROJECT NO. 7424  
 FORSYTH-YADIN COUNTY  
 STA. 356+80  
 BENTS No. 1A & 3

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

J.P. [Signature]  
 W.H. Rogers Jr. [Signature]

SPECIAL	ASSEMBLED BY	[Signature]	DATE	7-1-55
	CHECKED BY	[Signature]	DATE	7-1-55
STANDARD	DESIGNED BY	[Signature]	DATE	7-1-55
	DRAWN BY	[Signature]	DATE	7-1-55
	TRACED BY	[Signature]	DATE	7-1-55
	CHECKED BY	[Signature]	DATE	7-1-55

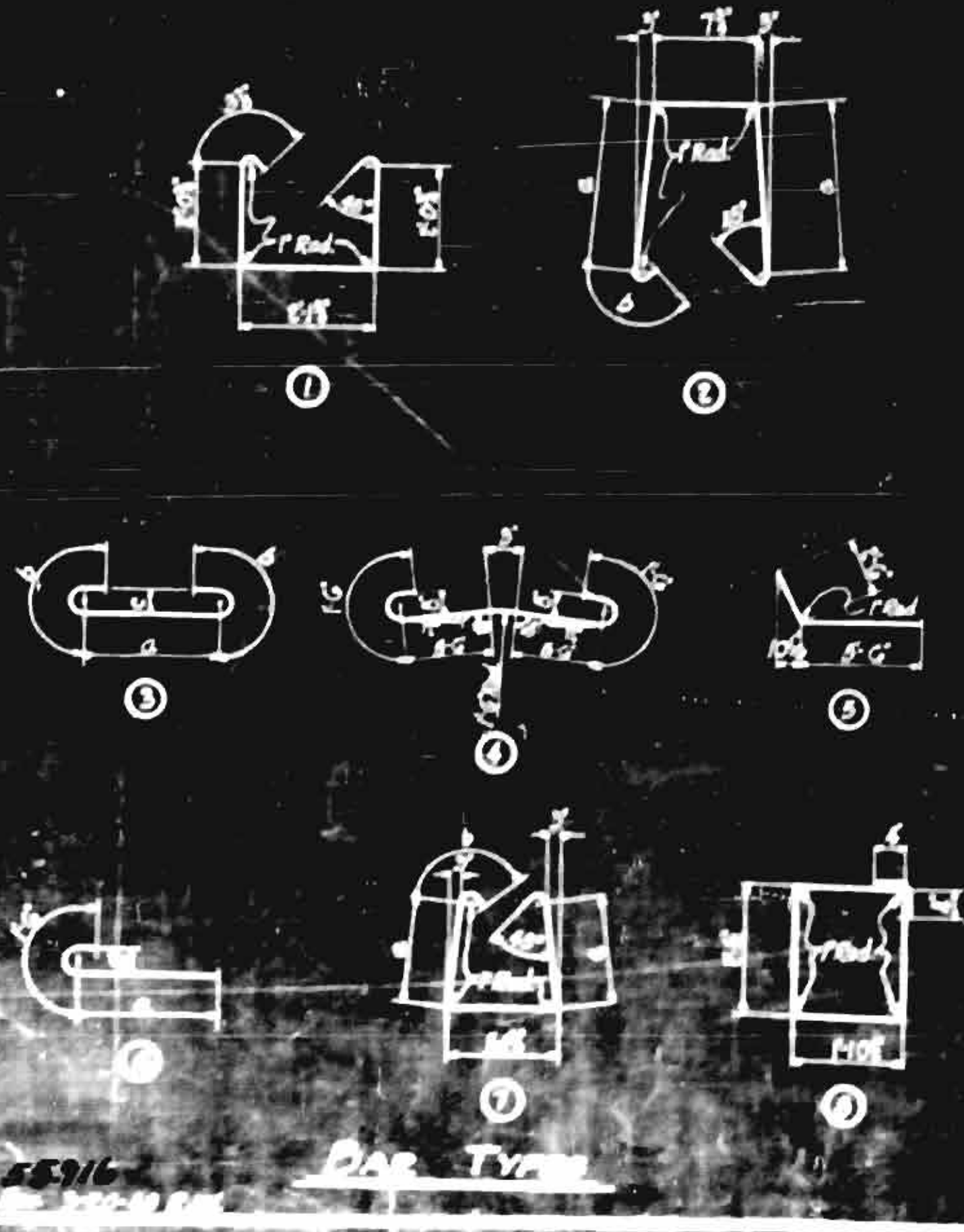
DESIGN NO.	PROJECT NO.	DATE
3	7424	57 16
1/4 Proj. F-246(3)		



BILL OF MATERIAL FOR ONE BENT (BENT 1 or 2)

Bar No.	Size	Type	Dimensions			Length	Weight
			a	b	c		
a1	6"	3	27-10"	1'-0"	0"	30'-10"	985
a2	6"	3	27-10"	1'-0"	0"	20'-0"	744
a3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
a12	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b1	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b2	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
b12	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c1	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c2	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
c12	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d1	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d2	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
d12	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e1	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e2	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
e12	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f1	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f2	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f3	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f4	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f5	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f6	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f7	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f8	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f9	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f10	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f11	6"	3	7'-0"	1'-0"	0"	10'-0"	310
f12	6"	3	7'-0"	1'-0"	0"	10'-0"	310

NOTE: For General Note and Design Data see General Drawing. For details of Mesh Detail see (Type) Draw for Structural Steel. Rebar shall be spaced down at least 1" into rock and shall have the minimum thickness of 1" above in this Draw.



PROJECT NO. 7424  
 FORSYTH-YADKIN COUNTY  
 STA. 358 + 00  
 BENTS No. 1 & 2

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

J.P. [Signature]  
 W.H. [Signature]

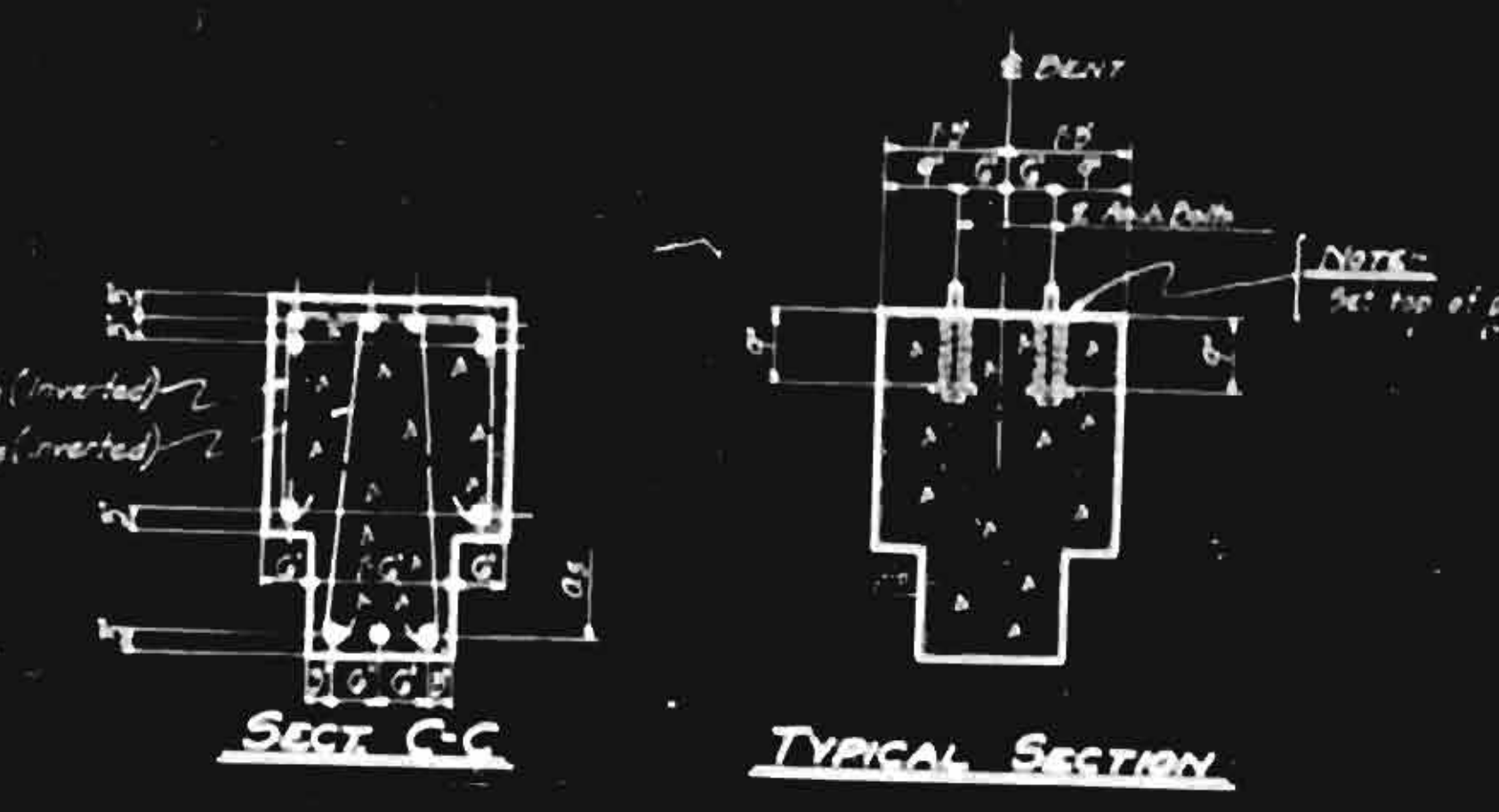
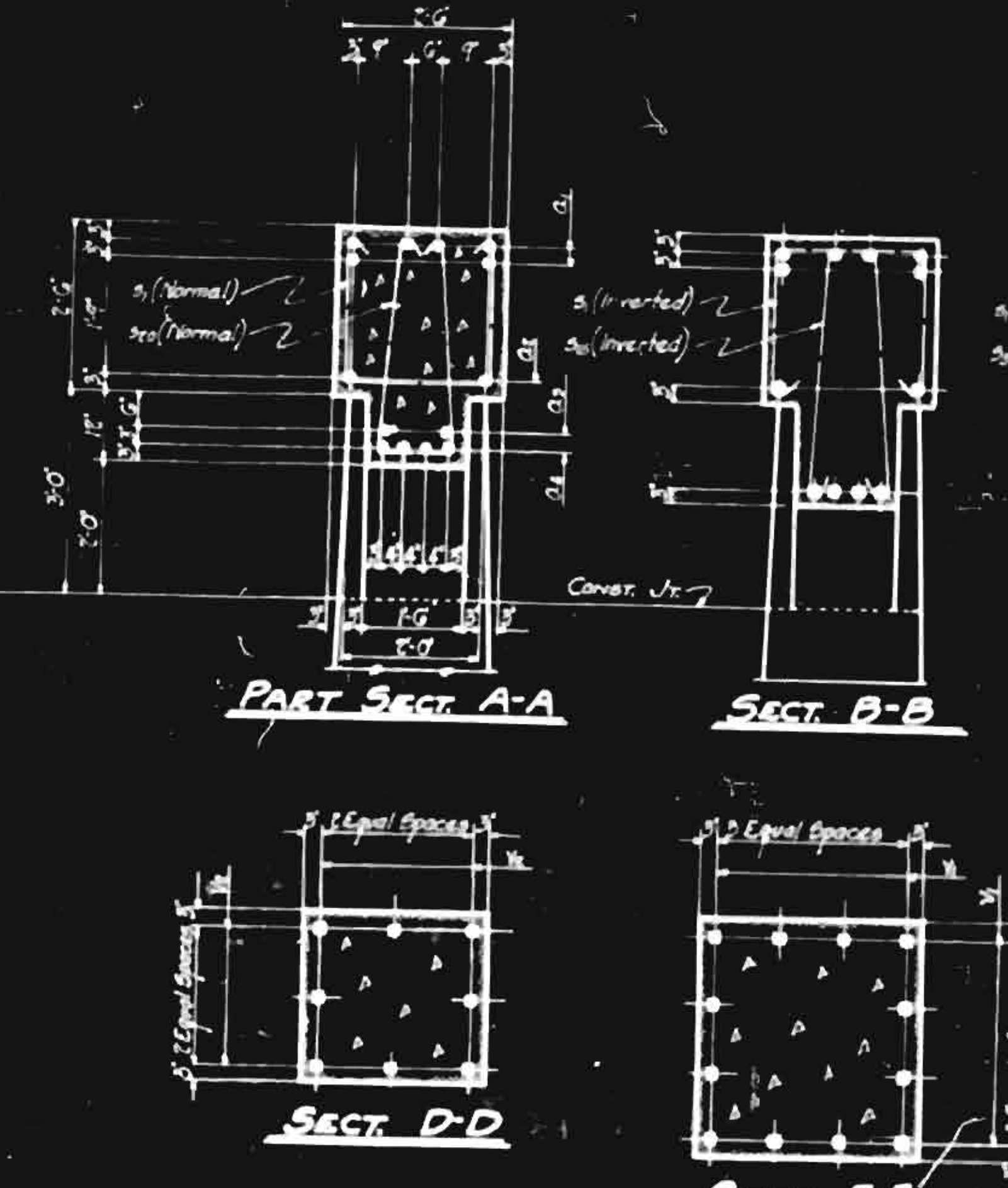
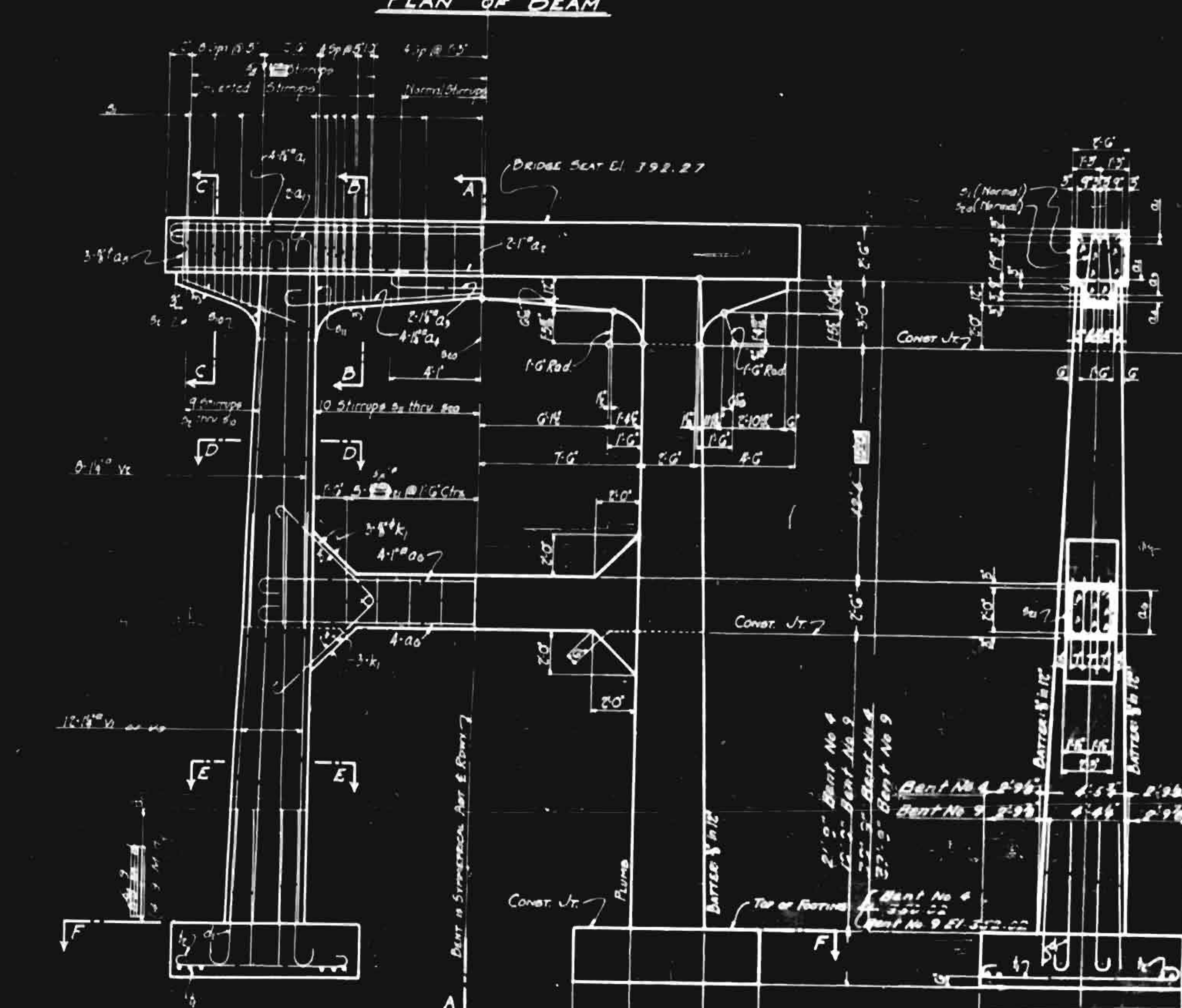
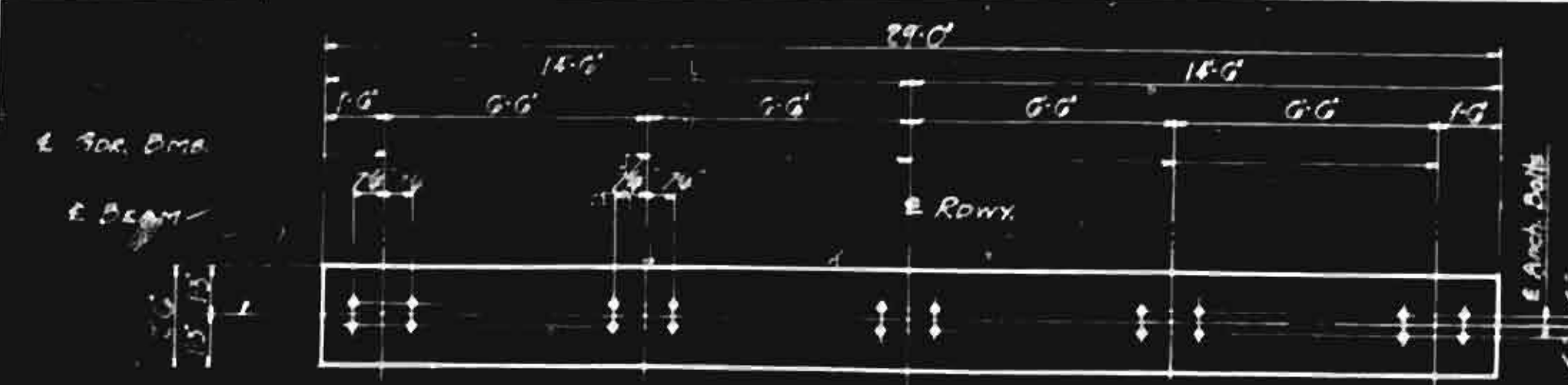
SPECIAL	ASSEMBLED BY	CHECKED BY	DATE
STANDARD	DRAWN BY	TRACED BY	DATE

Original No. 82-301  
 Original No. 82-301



PROJ. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	N.C.	7424	58	17

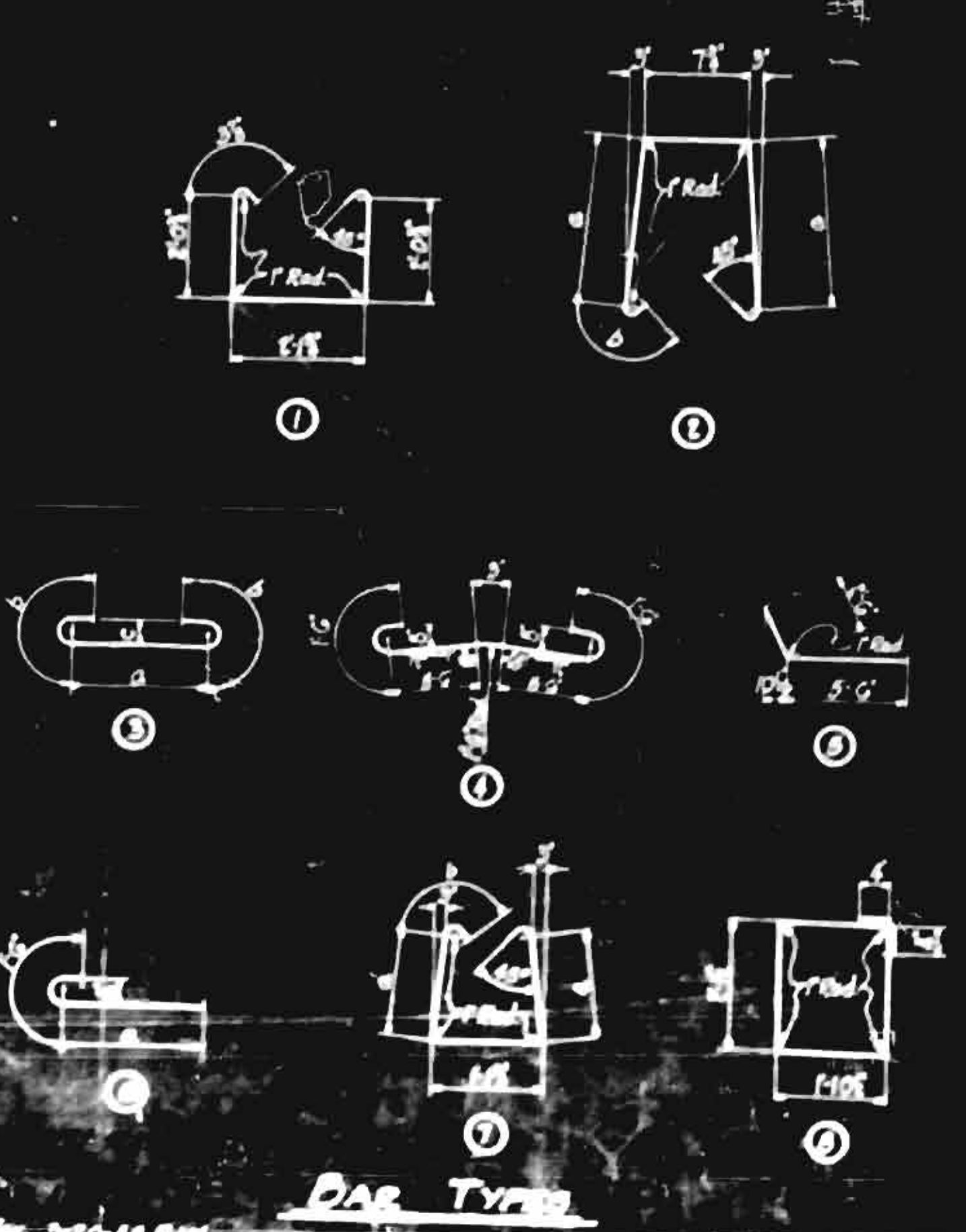
F.A. Proj. F-266(3)



**NOTE:**  
 For General Note and Design Data see General Drawing.  
 For data of Arch Daily see 1971 Draw for Structural Steel.  
 Reinforcement shall be spaced down at least 1/4" to rock and at 1" in the  
 remaining thickness of that layer in this bridge.

**BILL OF MATERIAL FOR ONE BENT (BENT 4 of 9)**

BAR	No.	SIZE	TYPE	DIMENSIONS			LENGTH	WEIGHT
				a	b	c		
a1	6	#4	S	17'-10"	1'-6"	8"	30'-10"	485
a2	7	#4	S	17'-10"	1'-6"	8"	12'-6"	174
a3	2	#4	S	7'-6"	1'-6"	8"	10'-6"	116
a4	4	#4	S	7'-6"	1'-6"	8"	10'-6"	150
a5	6	#4	S	7'-6"	1'-6"	8"	10'-6"	605
a6	8	#4	S	7'-6"	1'-6"	8"	10'-6"	605
a7	12	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a8	12	#4	S	7'-6"	1'-6"	8"	10'-6"	49
a9	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a10	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a11	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a12	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a13	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a14	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a15	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a16	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a17	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a18	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a19	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a20	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a21	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a22	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a23	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a24	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a25	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a26	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a27	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a28	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a29	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a30	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a31	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a32	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a33	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a34	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a35	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a36	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a37	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a38	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a39	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a40	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a41	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a42	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a43	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a44	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a45	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a46	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a47	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a48	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a49	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a50	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a51	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a52	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a53	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a54	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a55	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a56	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a57	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a58	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a59	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a60	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a61	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a62	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a63	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a64	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a65	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a66	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a67	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a68	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a69	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a70	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a71	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a72	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a73	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a74	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a75	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a76	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a77	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a78	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a79	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a80	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a81	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a82	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a83	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a84	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a85	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a86	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a87	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a88	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a89	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a90	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a91	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a92	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a93	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a94	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a95	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a96	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a97	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a98	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a99	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247
a100	2	#4	S	7'-6"	1'-6"	8"	10'-6"	1247



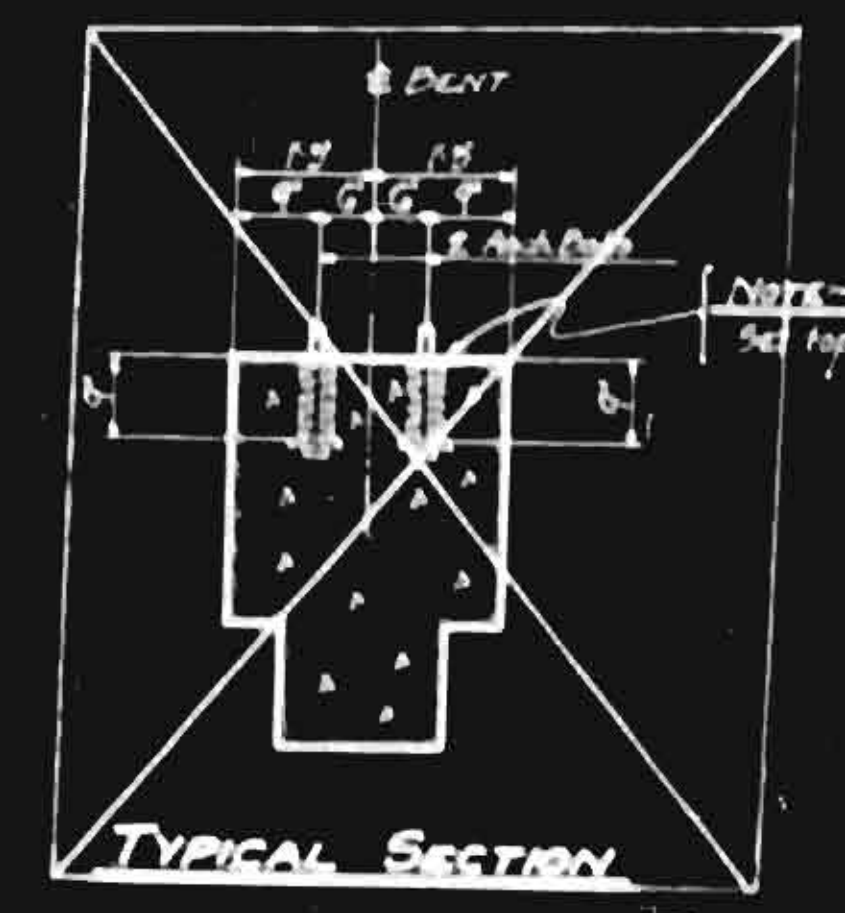
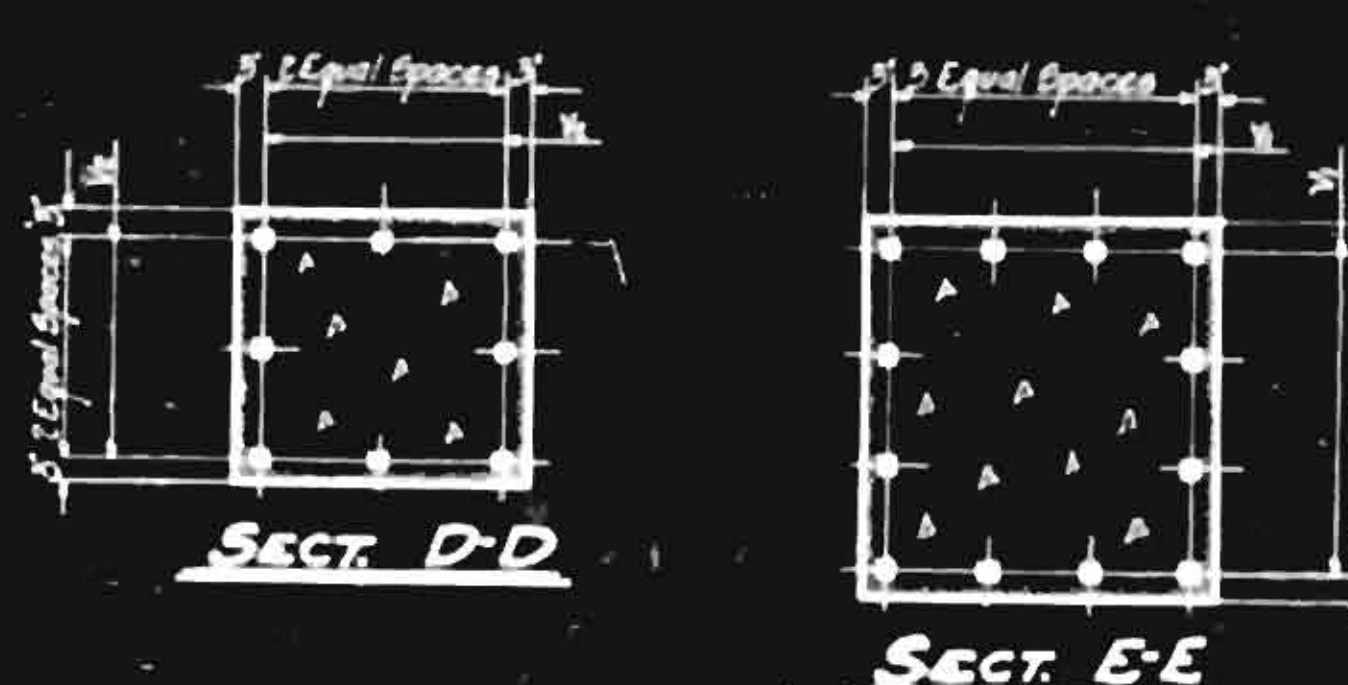
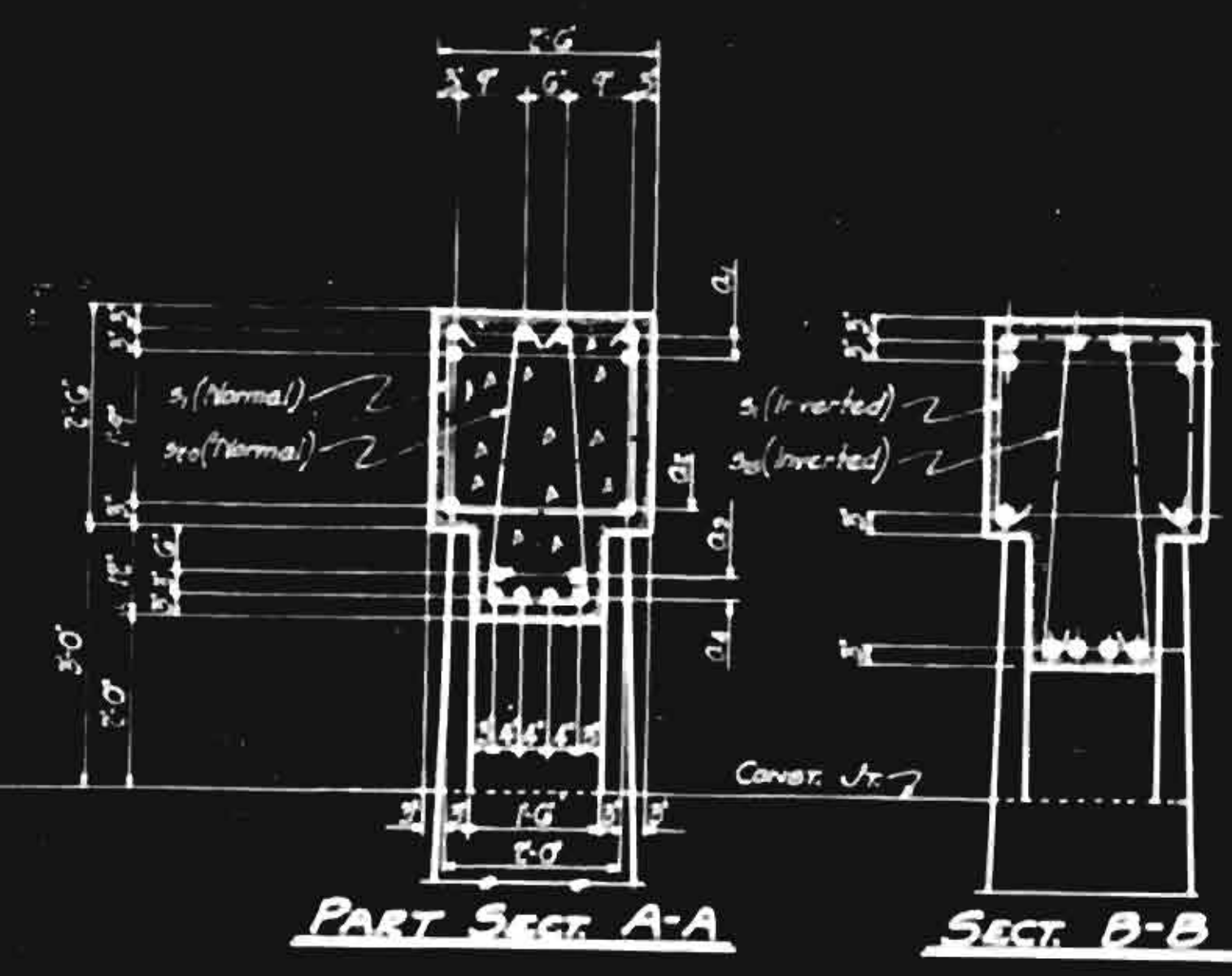
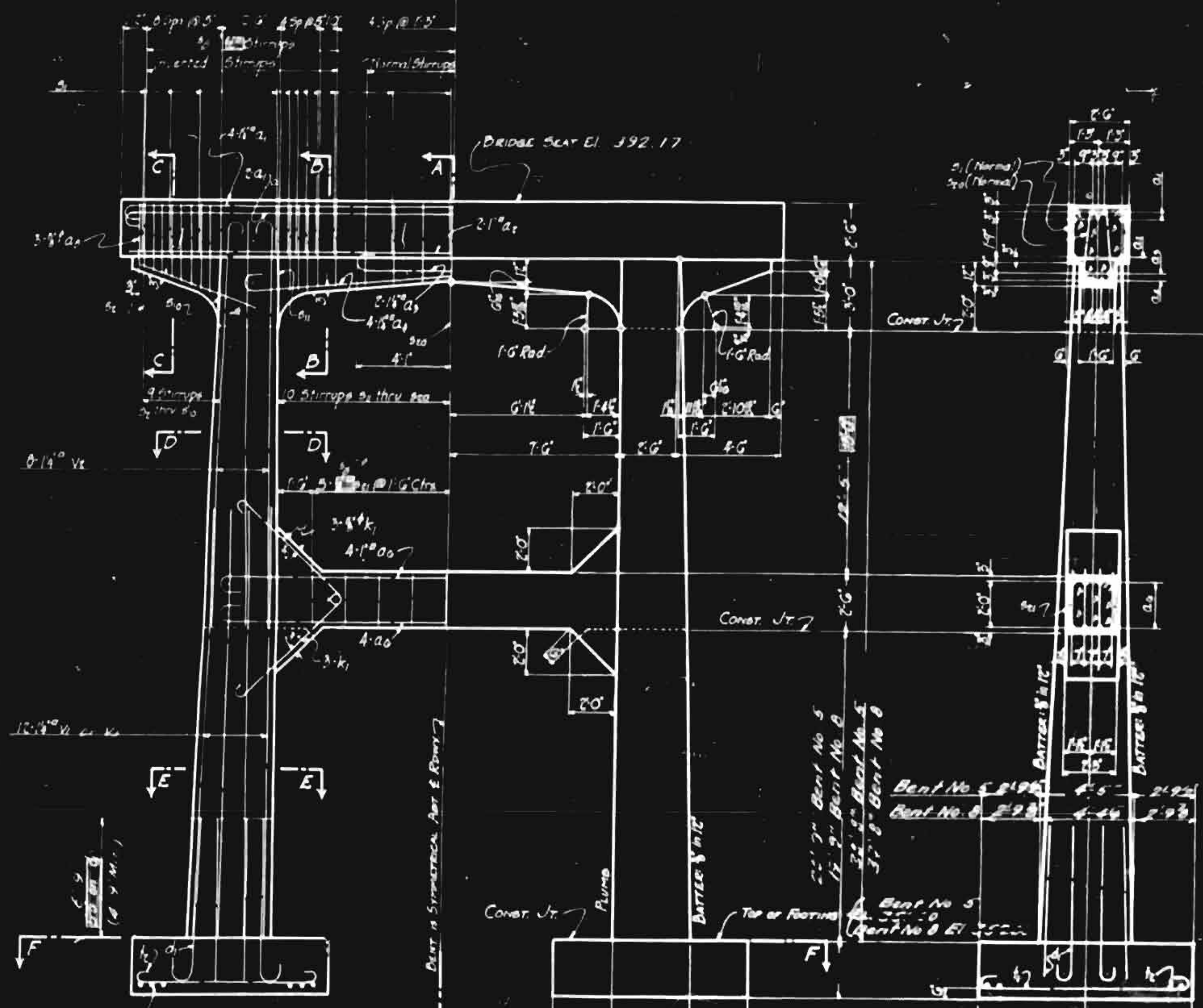
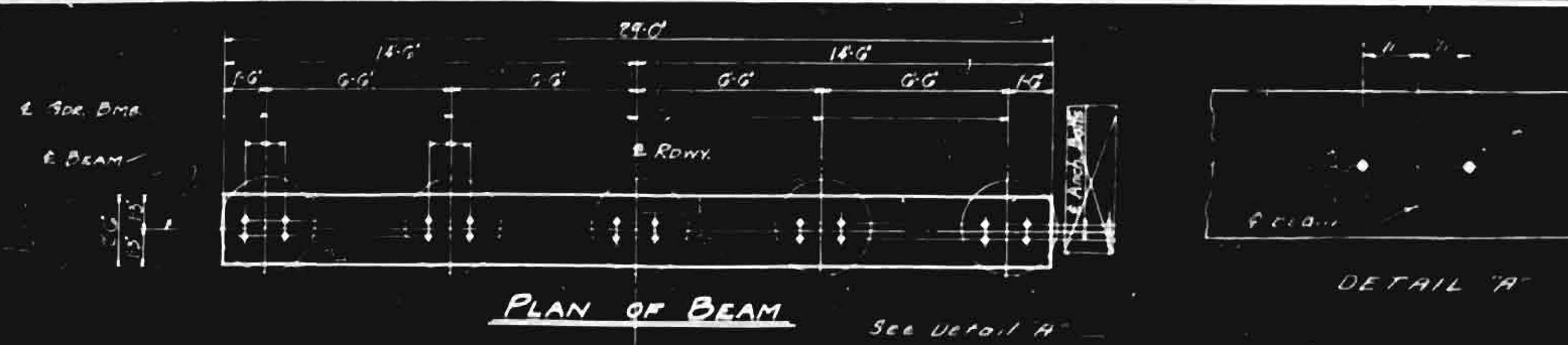
**PROJECT NO. 7424**  
**FORSYTH-YADKIN COUNTY**  
**STA. 1356+80**  
**BENTS No. 4 & No. 9**

STATE OF NORTH CAROLINA  
**STATE HIGHWAY AND PUBLIC WORKS COMMISSION**

J.P. [Signature]  
 W.B. [Signature]

SPECIAL	ASSEMBLED BY	DATE
STANDARD	CHECKED BY	DATE
	DESIGNED BY	DATE
	TRACED BY	DATE
	CHECKED BY	DATE

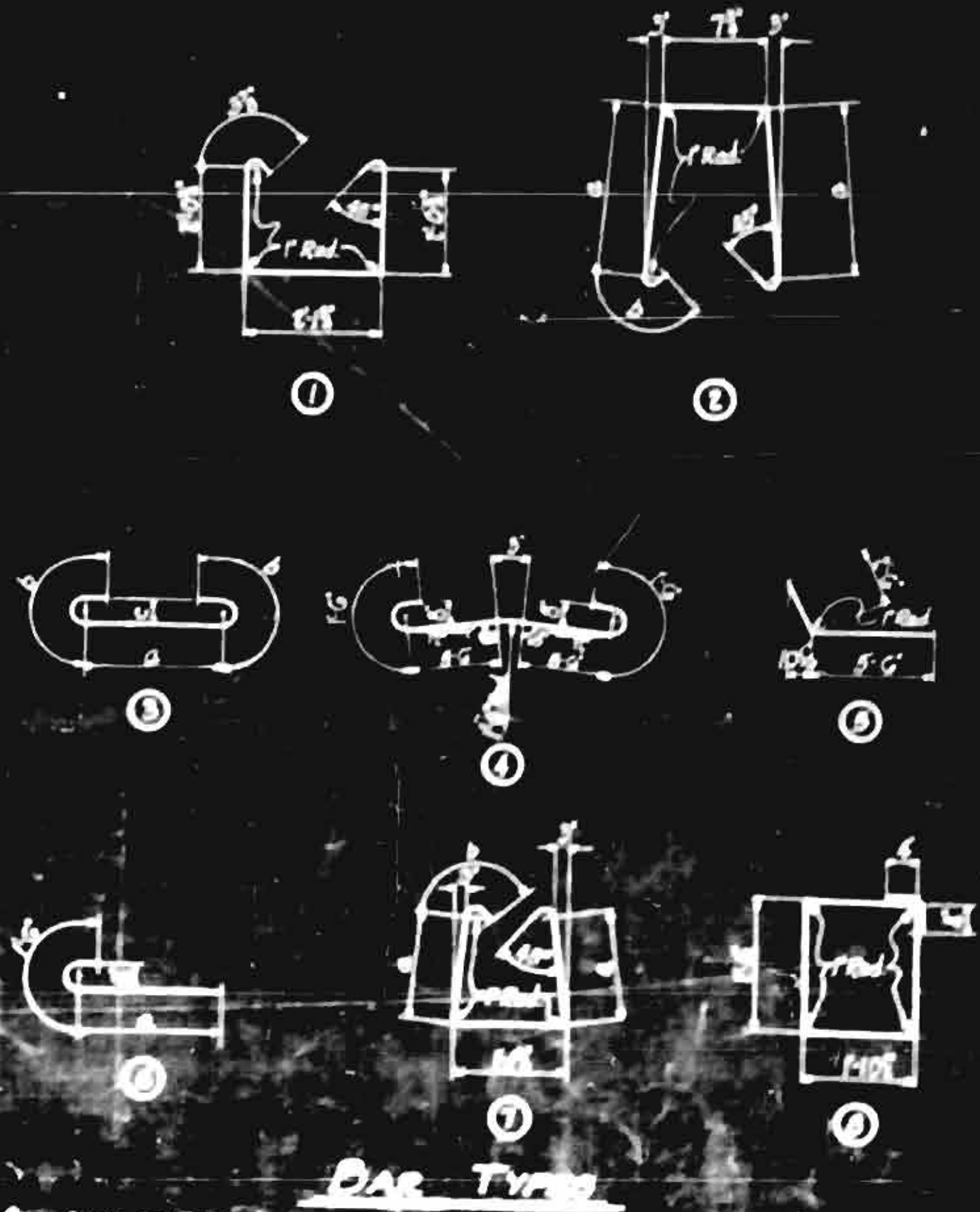
PROJ. NO.	DATE	REVISED BY	DATE
3	N.C.	7484	50 14
T.A.P. 1-866(S)			



NOTE:  
For General Notes and Design Data, see General Drawing.  
For details of Anch. Detail, see "Anch. Detail" for Structural Steel.  
Reinforcing steel shall be lap spliced at least 40 diam. into rock and shall have the minimum thickness of that shown on this sheet.

BILL OF MATERIAL FOR ONE BENT (BENT 5 OF 8)

Qty	No.	Size	Type	Dimensions	Length	Weight
1	1	14"	3	17'-10"	1'-0"	900
1	2	14"	3	17'-10"	1'-0"	174
1	3	14"	3	17'-10"	1'-0"	112
1	4	14"	3	17'-10"	1'-0"	450
1	5	14"	3	17'-10"	1'-0"	50
1	6	14"	3	17'-10"	1'-0"	605
1	7	14"	3	17'-10"	1'-0"	1843
1	8	14"	3	17'-10"	1'-0"	44
1	9	14"	3	17'-10"	1'-0"	1621
1	10	14"	3	17'-10"	1'-0"	16
1	11	14"	3	17'-10"	1'-0"	16
1	12	14"	3	17'-10"	1'-0"	16
1	13	14"	3	17'-10"	1'-0"	16
1	14	14"	3	17'-10"	1'-0"	16
1	15	14"	3	17'-10"	1'-0"	16
1	16	14"	3	17'-10"	1'-0"	16
1	17	14"	3	17'-10"	1'-0"	16
1	18	14"	3	17'-10"	1'-0"	16
1	19	14"	3	17'-10"	1'-0"	16
1	20	14"	3	17'-10"	1'-0"	16
1	21	14"	3	17'-10"	1'-0"	16
1	22	14"	3	17'-10"	1'-0"	16
1	23	14"	3	17'-10"	1'-0"	16
1	24	14"	3	17'-10"	1'-0"	16
1	25	14"	3	17'-10"	1'-0"	16
1	26	14"	3	17'-10"	1'-0"	16
1	27	14"	3	17'-10"	1'-0"	16
1	28	14"	3	17'-10"	1'-0"	16
1	29	14"	3	17'-10"	1'-0"	16
1	30	14"	3	17'-10"	1'-0"	16
1	31	14"	3	17'-10"	1'-0"	16
1	32	14"	3	17'-10"	1'-0"	16
1	33	14"	3	17'-10"	1'-0"	16
1	34	14"	3	17'-10"	1'-0"	16
1	35	14"	3	17'-10"	1'-0"	16
1	36	14"	3	17'-10"	1'-0"	16
1	37	14"	3	17'-10"	1'-0"	16
1	38	14"	3	17'-10"	1'-0"	16
1	39	14"	3	17'-10"	1'-0"	16
1	40	14"	3	17'-10"	1'-0"	16
1	41	14"	3	17'-10"	1'-0"	16
1	42	14"	3	17'-10"	1'-0"	16
1	43	14"	3	17'-10"	1'-0"	16
1	44	14"	3	17'-10"	1'-0"	16
1	45	14"	3	17'-10"	1'-0"	16
1	46	14"	3	17'-10"	1'-0"	16
1	47	14"	3	17'-10"	1'-0"	16
1	48	14"	3	17'-10"	1'-0"	16
1	49	14"	3	17'-10"	1'-0"	16
1	50	14"	3	17'-10"	1'-0"	16
1	51	14"	3	17'-10"	1'-0"	16
1	52	14"	3	17'-10"	1'-0"	16
1	53	14"	3	17'-10"	1'-0"	16
1	54	14"	3	17'-10"	1'-0"	16
1	55	14"	3	17'-10"	1'-0"	16
1	56	14"	3	17'-10"	1'-0"	16
1	57	14"	3	17'-10"	1'-0"	16
1	58	14"	3	17'-10"	1'-0"	16
1	59	14"	3	17'-10"	1'-0"	16
1	60	14"	3	17'-10"	1'-0"	16
1	61	14"	3	17'-10"	1'-0"	16
1	62	14"	3	17'-10"	1'-0"	16
1	63	14"	3	17'-10"	1'-0"	16
1	64	14"	3	17'-10"	1'-0"	16
1	65	14"	3	17'-10"	1'-0"	16
1	66	14"	3	17'-10"	1'-0"	16
1	67	14"	3	17'-10"	1'-0"	16
1	68	14"	3	17'-10"	1'-0"	16
1	69	14"	3	17'-10"	1'-0"	16
1	70	14"	3	17'-10"	1'-0"	16
1	71	14"	3	17'-10"	1'-0"	16
1	72	14"	3	17'-10"	1'-0"	16
1	73	14"	3	17'-10"	1'-0"	16
1	74	14"	3	17'-10"	1'-0"	16
1	75	14"	3	17'-10"	1'-0"	16
1	76	14"	3	17'-10"	1'-0"	16
1	77	14"	3	17'-10"	1'-0"	16
1	78	14"	3	17'-10"	1'-0"	16
1	79	14"	3	17'-10"	1'-0"	16
1	80	14"	3	17'-10"	1'-0"	16
1	81	14"	3	17'-10"	1'-0"	16
1	82	14"	3	17'-10"	1'-0"	16
1	83	14"	3	17'-10"	1'-0"	16
1	84	14"	3	17'-10"	1'-0"	16
1	85	14"	3	17'-10"	1'-0"	16
1	86	14"	3	17'-10"	1'-0"	16
1	87	14"	3	17'-10"	1'-0"	16
1	88	14"	3	17'-10"	1'-0"	16
1	89	14"	3	17'-10"	1'-0"	16
1	90	14"	3	17'-10"	1'-0"	16
1	91	14"	3	17'-10"	1'-0"	16
1	92	14"	3	17'-10"	1'-0"	16
1	93	14"	3	17'-10"	1'-0"	16
1	94	14"	3	17'-10"	1'-0"	16
1	95	14"	3	17'-10"	1'-0"	16
1	96	14"	3	17'-10"	1'-0"	16
1	97	14"	3	17'-10"	1'-0"	16
1	98	14"	3	17'-10"	1'-0"	16
1	99	14"	3	17'-10"	1'-0"	16
1	100	14"	3	17'-10"	1'-0"	16

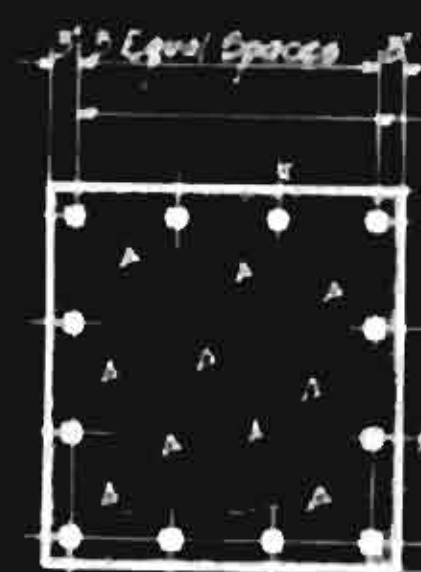
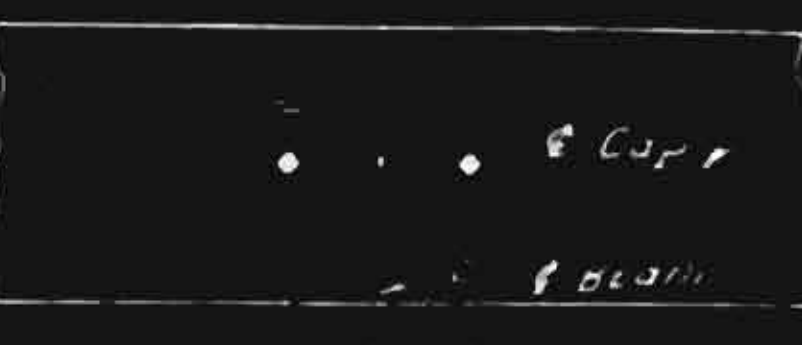
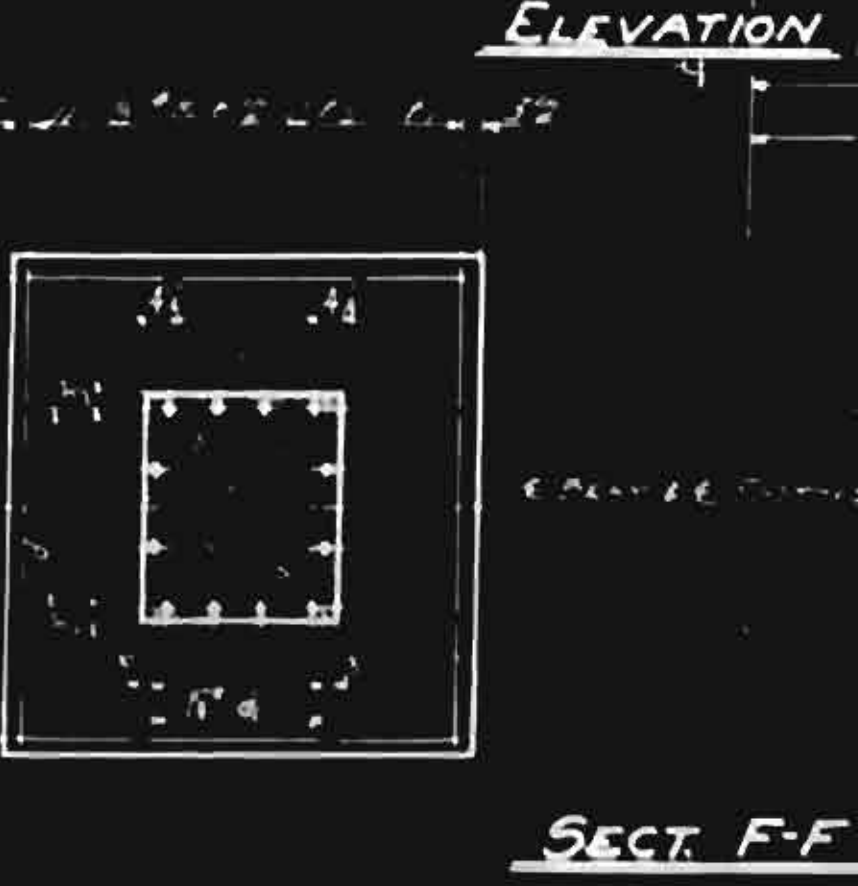
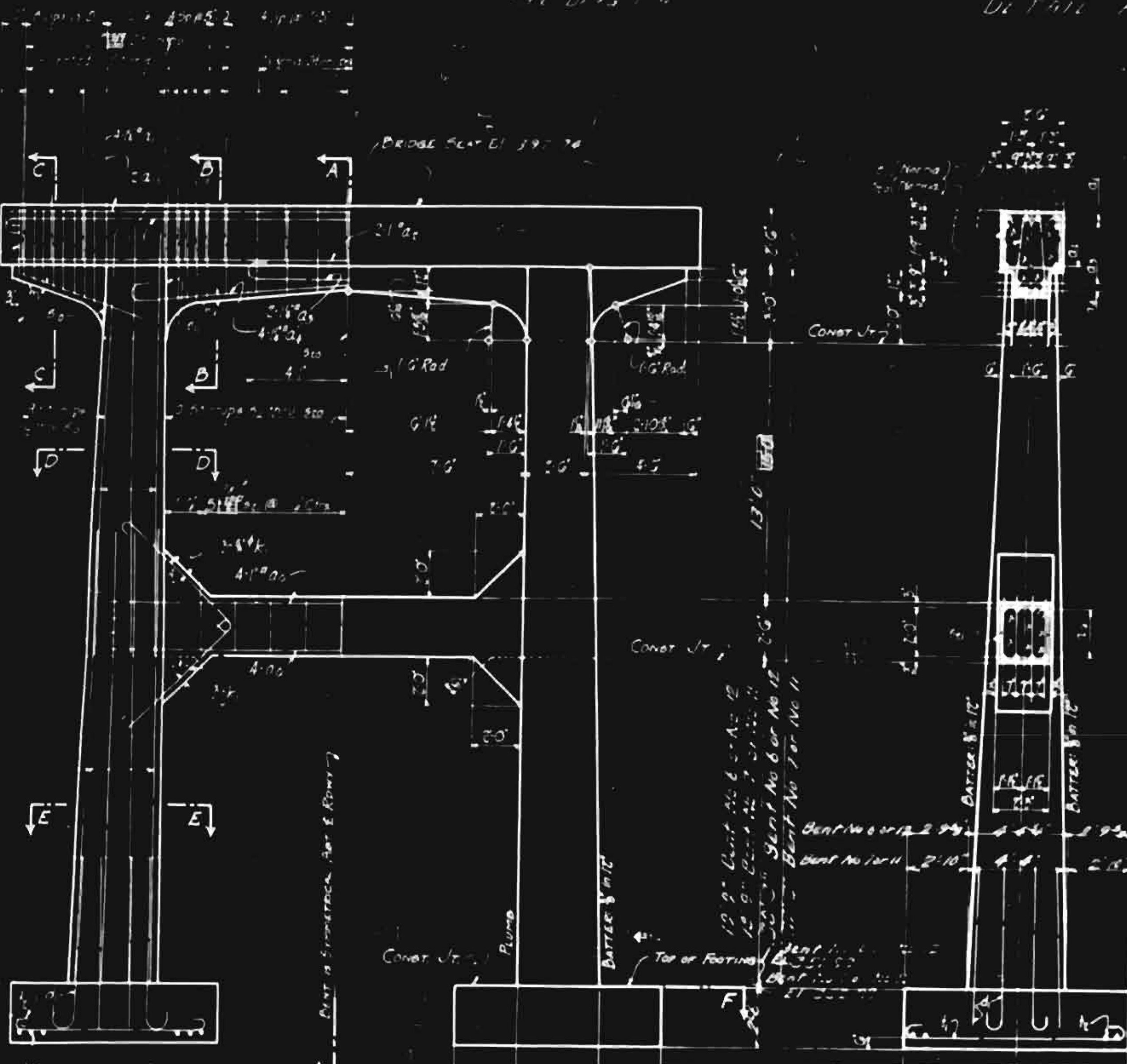


PROJECT NO. 7424  
 FORKTH-YADKIN COUNTY  
 STA. 356 + 80  
 BENTS No 5 & No 8

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

J.P. [Signature]  
 W.H. [Signature]

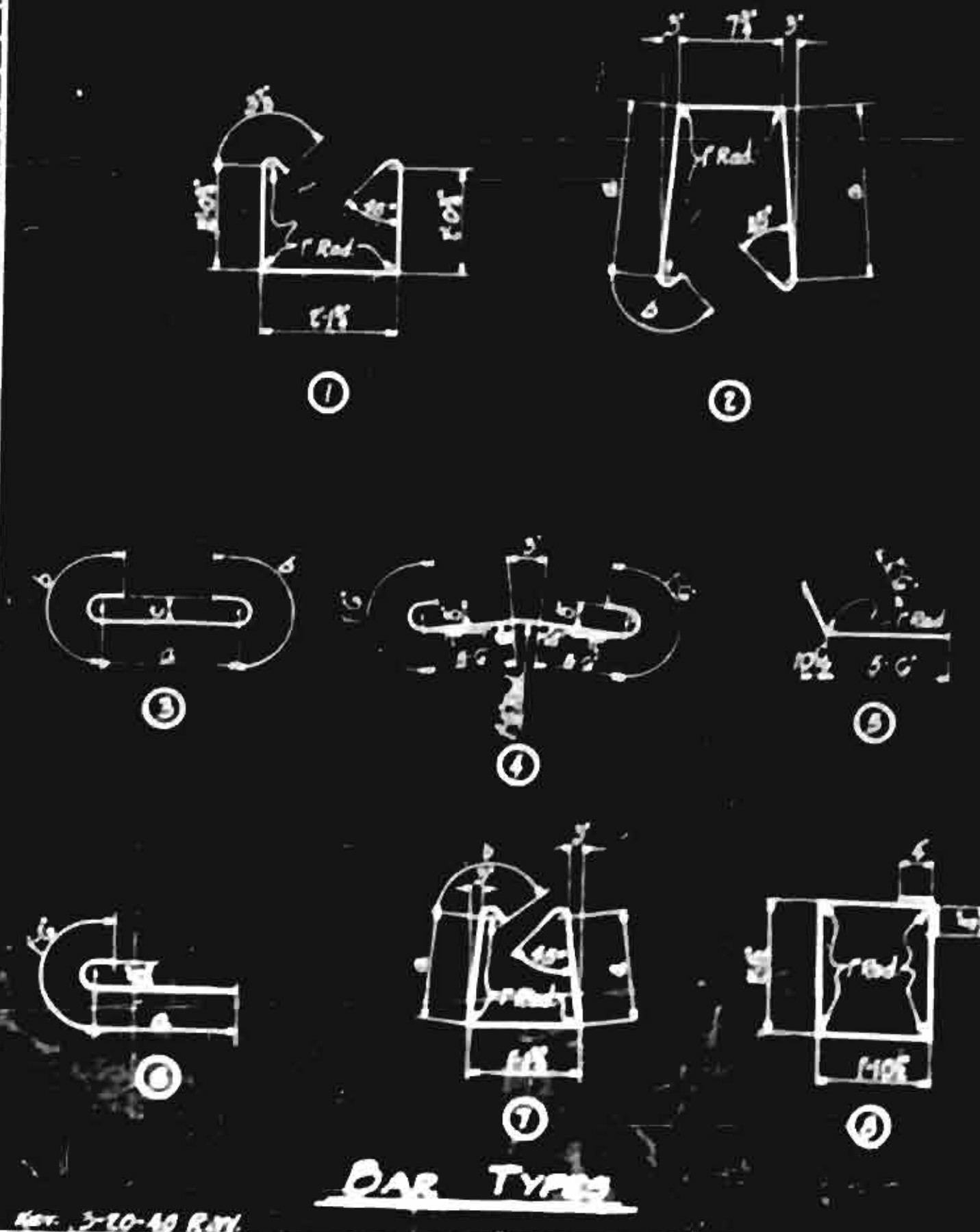
SPECIAL	ASSEMBLED BY	DATE
STANDARD	CHECKED BY	DATE
	DESIGNED BY	DATE
	DRAWN BY	DATE
	TRACED BY	DATE
	CHECKED BY	DATE



NOTE: For the purpose of design, the bent shall be considered as a rigid frame. The bent shall be designed for the maximum live load plus a dead load of 10 lb/sq ft. The bent shall be designed for the maximum wind load plus a dead load of 10 lb/sq ft. The bent shall be designed for the maximum seismic load plus a dead load of 10 lb/sq ft.

BILL OF MATERIAL FOR ONE BENT (BENTS 6-7-11 & 12)

QTY	No.	SIZE	TYPE	DIMENSIONS			LENGTH	WEIGHT
				A	B	C		
4	0	14"	3	27'-10"	1'-6"	0'	30'-10"	485
4	1	14"	5/8"				30'-0"	734
4	2	14"	3	7'-0"	1'-6"	0'	10'-0"	118
4	4	14"	4				20'-0"	430
4	6	14"	5				5'-0"	50
4	8	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	9	14"	3	4'-0"	0'	0'	9'-0"	124
4	11	14"	3	5'-0"	0'	0'	7'-0"	124
4	13	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	14	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	15	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	16	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	17	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	18	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	19	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	20	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	21	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	22	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	23	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	24	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	25	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	26	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	27	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	28	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	29	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	30	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	31	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	32	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	33	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	34	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	35	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	36	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	37	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	38	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	39	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	40	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	41	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	42	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	43	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	44	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	45	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	46	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	47	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	48	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	49	14"	3	17'-9"	1'-9"	0'	22'-0"	485
4	50	14"	3	17'-9"	1'-9"	0'	22'-0"	485



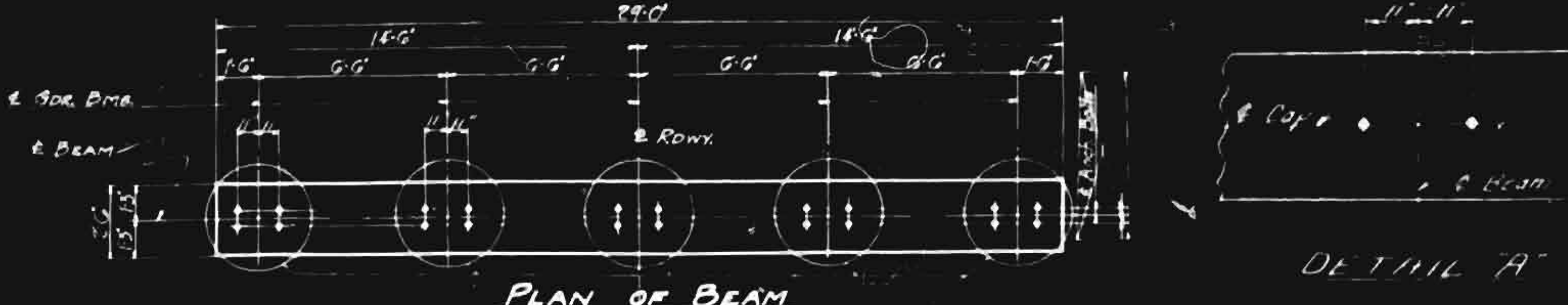
PROJECT NO. 7424  
 FORSYTH-YADKIN COUNTY  
 STA. 356+80  
 BENTS No. 6-7-11 & 12

STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

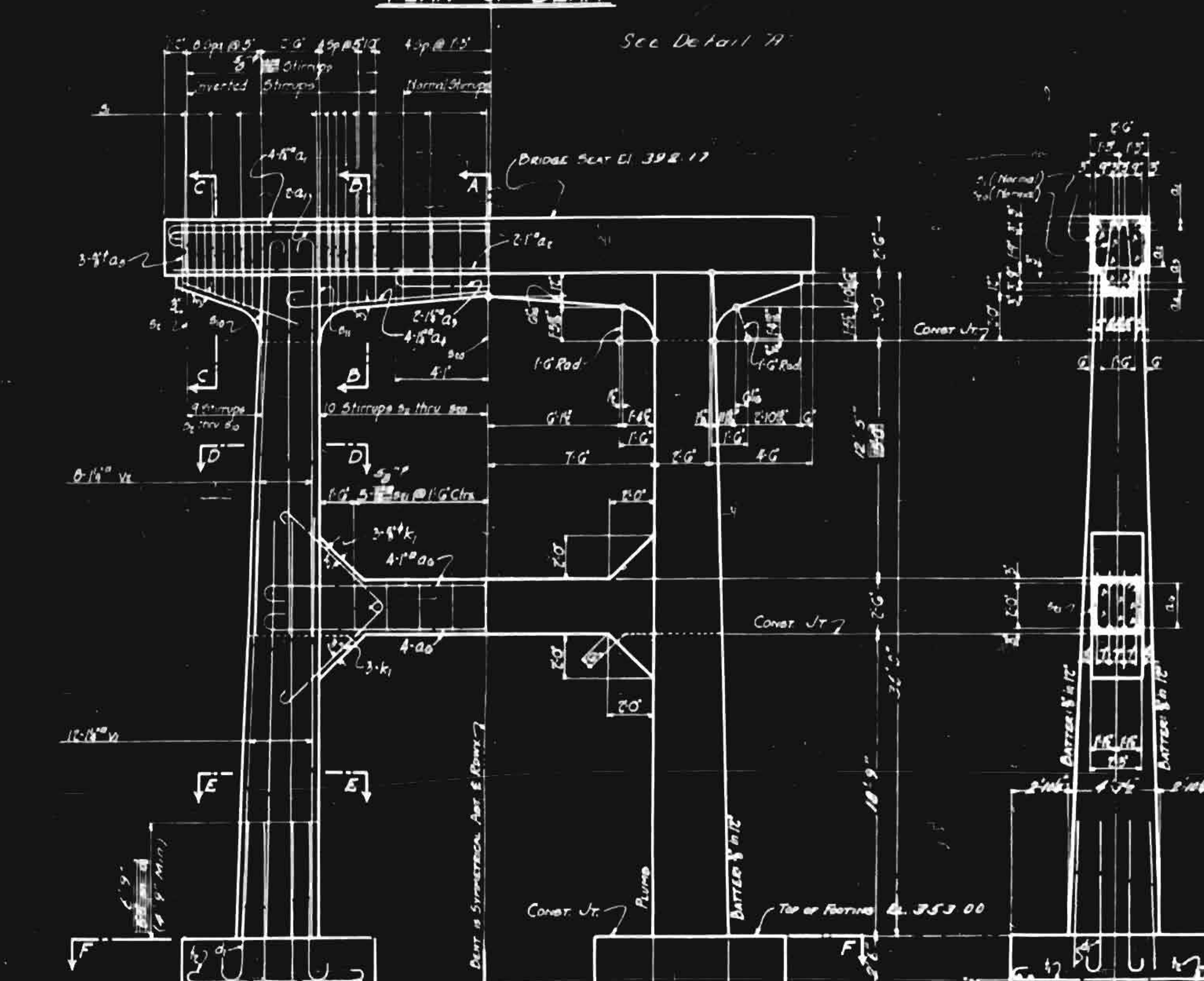
J.P. [Signature]  
 W.H. Rogers, Jr. [Signature]

SPECIAL	DATE	BY
STANDARD	DATE	BY

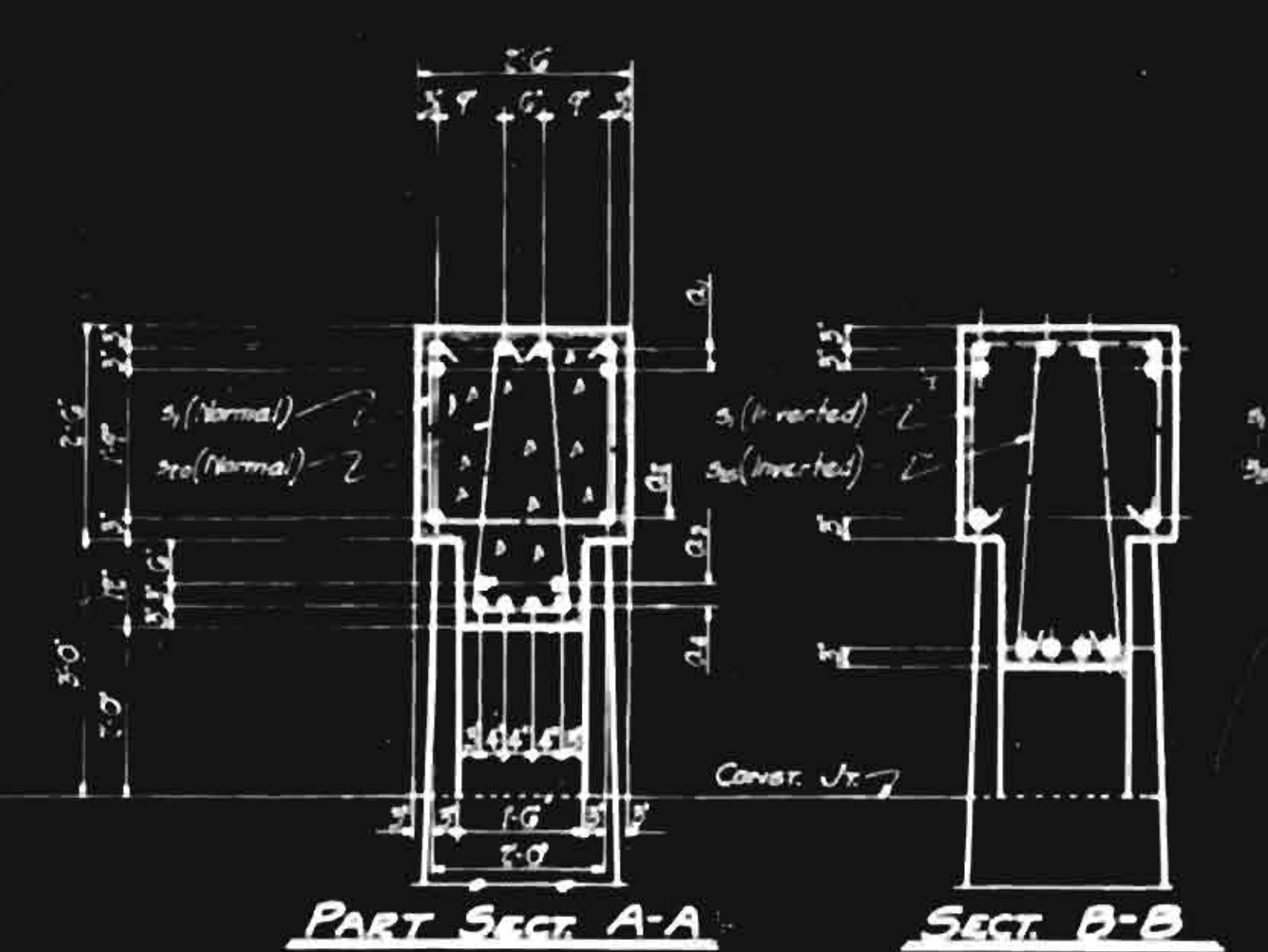
REL. ROAD DIST. NO.	STATION	PROJECT NO.	DATE	SCALE
3	N.C.	7424	5/11	1/2"
F.A. Proj. F-266(3)				



DETAIL A

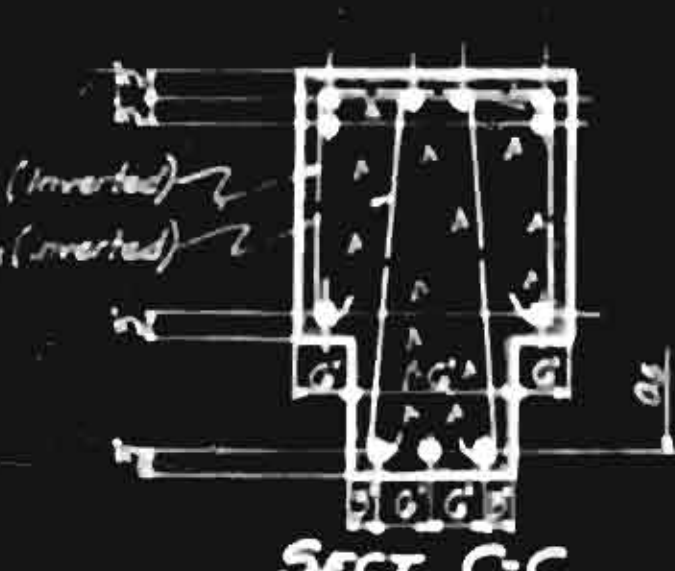


ELEVATION

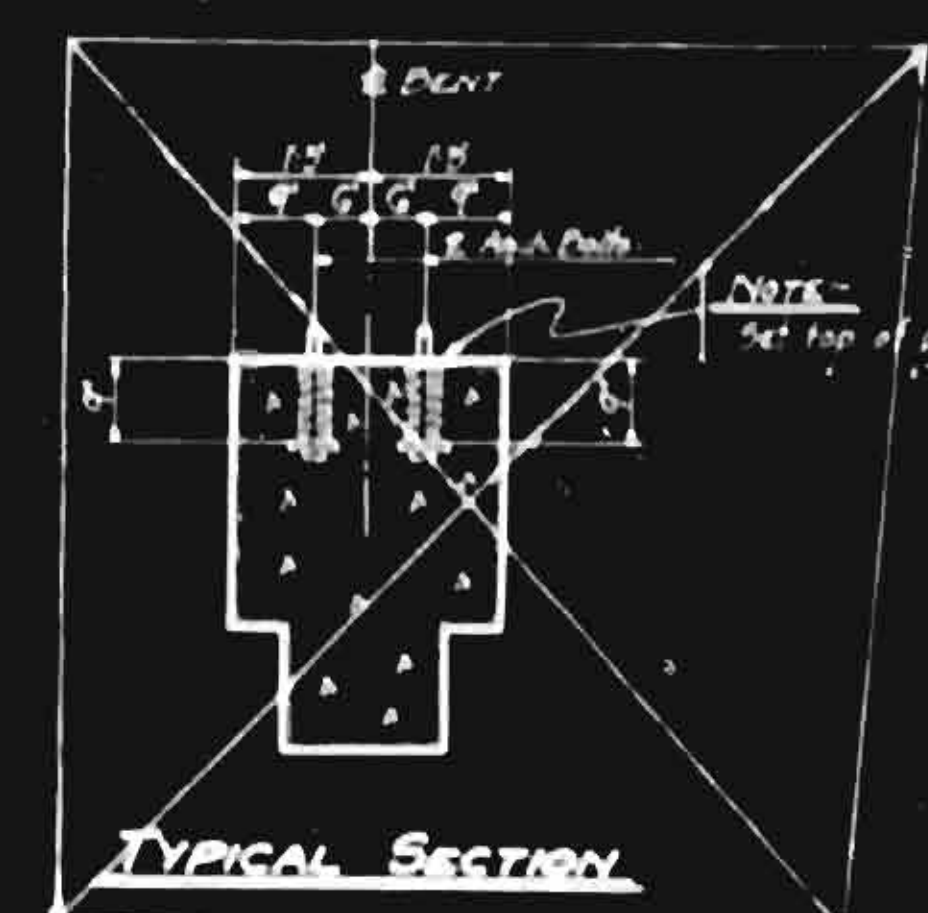


PART SECT A-A

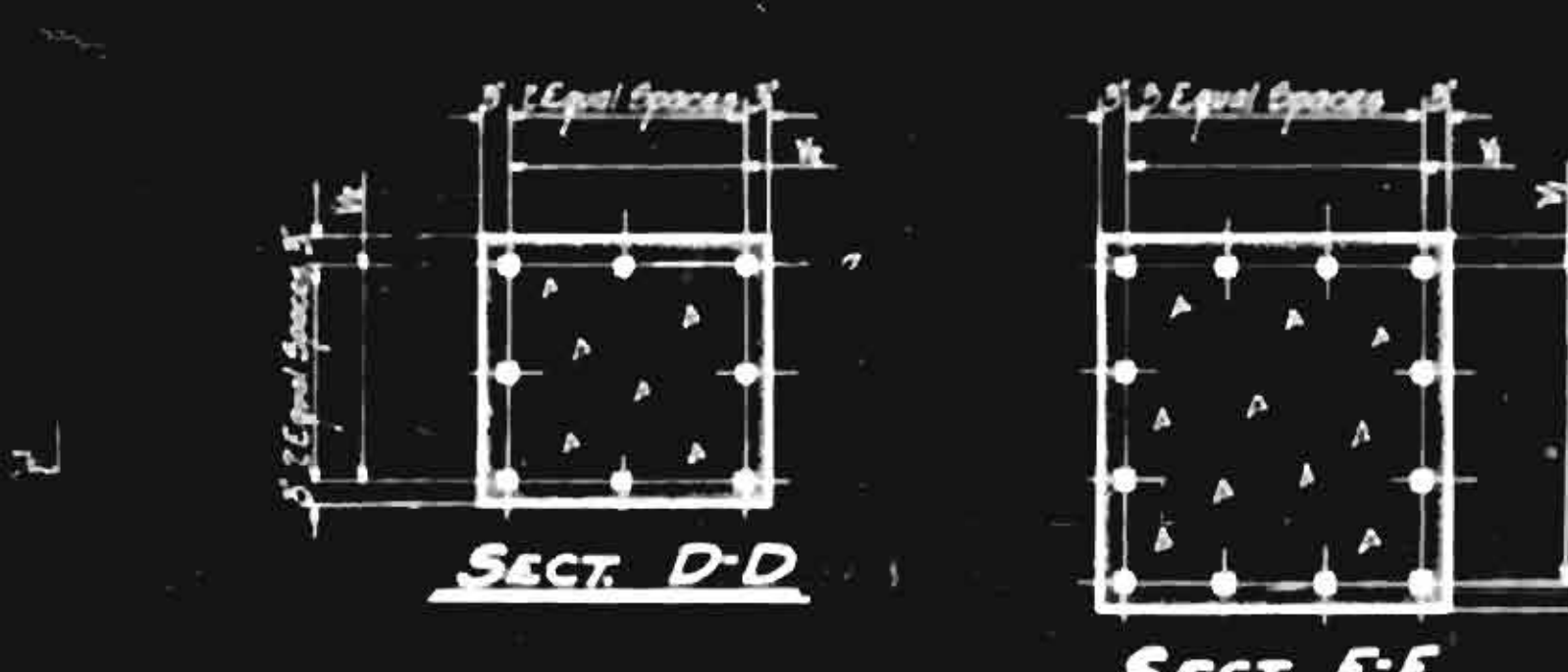
SECT B-B



SECT C-C



TYPICAL SECTION



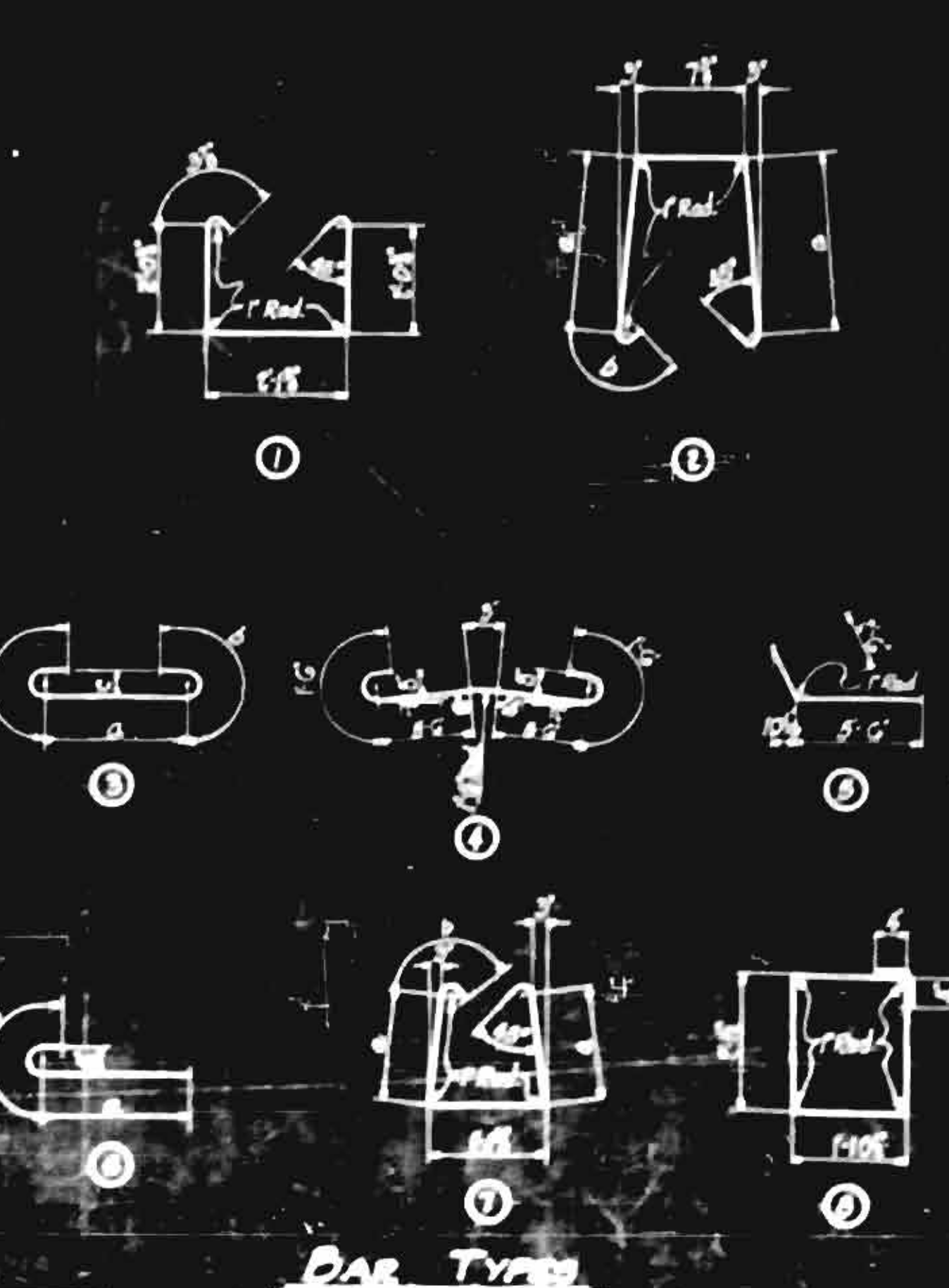
SECT D-D

SECT E-E

BILL OF MATERIAL FOR ONE BENT (BENT 10 OF 13)

BAR	No.	SIZE	TYPE	DIMENSIONS			LENGTH	WEIGHT
				a	b	c		
a <sub>1</sub>	0	#4	Str.	27'-10"	1'-0"	8"	30'-10"	485
a <sub>2</sub>	7	#4	Str.				20'-0"	174
a <sub>3</sub>	2	#4	Str.	7'-0"	1'-0"	8"	10'-0"	112
a <sub>4</sub>	4	#4	Str.				10'-0"	450
a <sub>5</sub>	6	#4	Str.				8'-0"	50
a <sub>6</sub>	8	#4	Str.	14'-0"	1'-0"	8"	22'-0"	605
b	14	#4	Str.	8'-0"			9'-0"	1843
b <sub>1</sub>	12	#4	Str.	5'-0"	10"	4"	7'-0"	94
b <sub>2</sub>	11	#4	Str.				6'-11"	122
b <sub>3</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>4</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>5</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>6</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>7</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>8</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>9</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>10</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>11</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>12</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>13</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>14</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>15</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
b <sub>16</sub>	2	#4	Str.	2'-10"	4"	4"	7'-0"	15
c	9	#4	Str.				8'-0"	225
d	20	#4	Str.	2'-10"	1'-0"	4"	11'-6"	325
e	20	#4	Str.	2'-10"	1'-0"	4"	7'-0"	96
f	20	#4	Str.				27'-0"	2997
g	16	#4	Str.				21'-0"	1785
							2990	
							501	

REINFORCEMENT SYMBOLS  
CLASS 'A' CONCRETE  
REV. 7-10-48 R.W.



VAR TYPES

NOTE:  
For General Note and Design Data see General Drawing  
For detail of Bent, see detail of Bent for Structural Steel  
Reinforcement shall be spaced down all length into rock and shall have the minimum thickness of that shown in this Drawing.

PROJECT NO. 7424  
FORBOTH-YADKIN COUNTY  
STA. 356+80  
BENTS No. 10 & No. 13

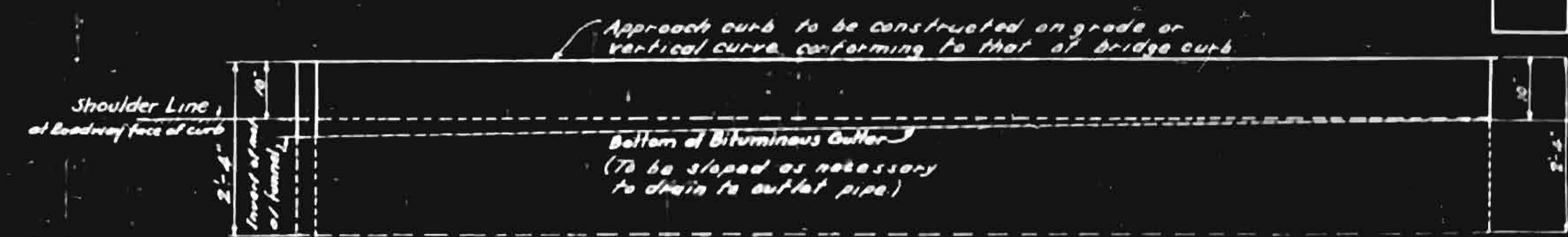
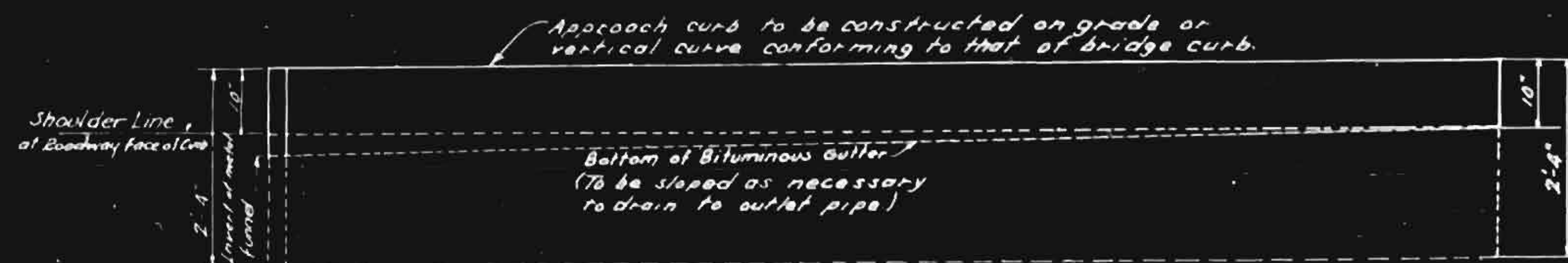
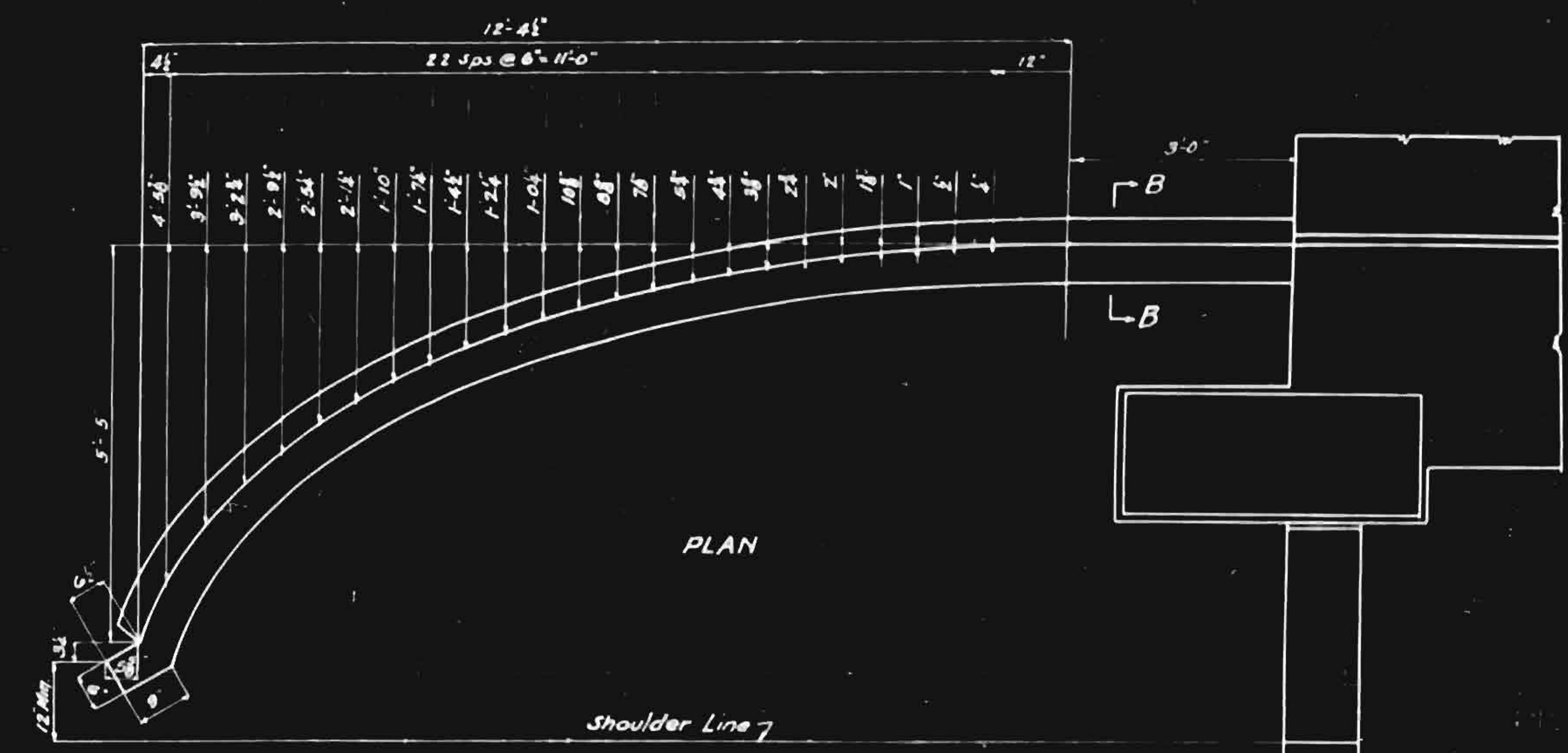
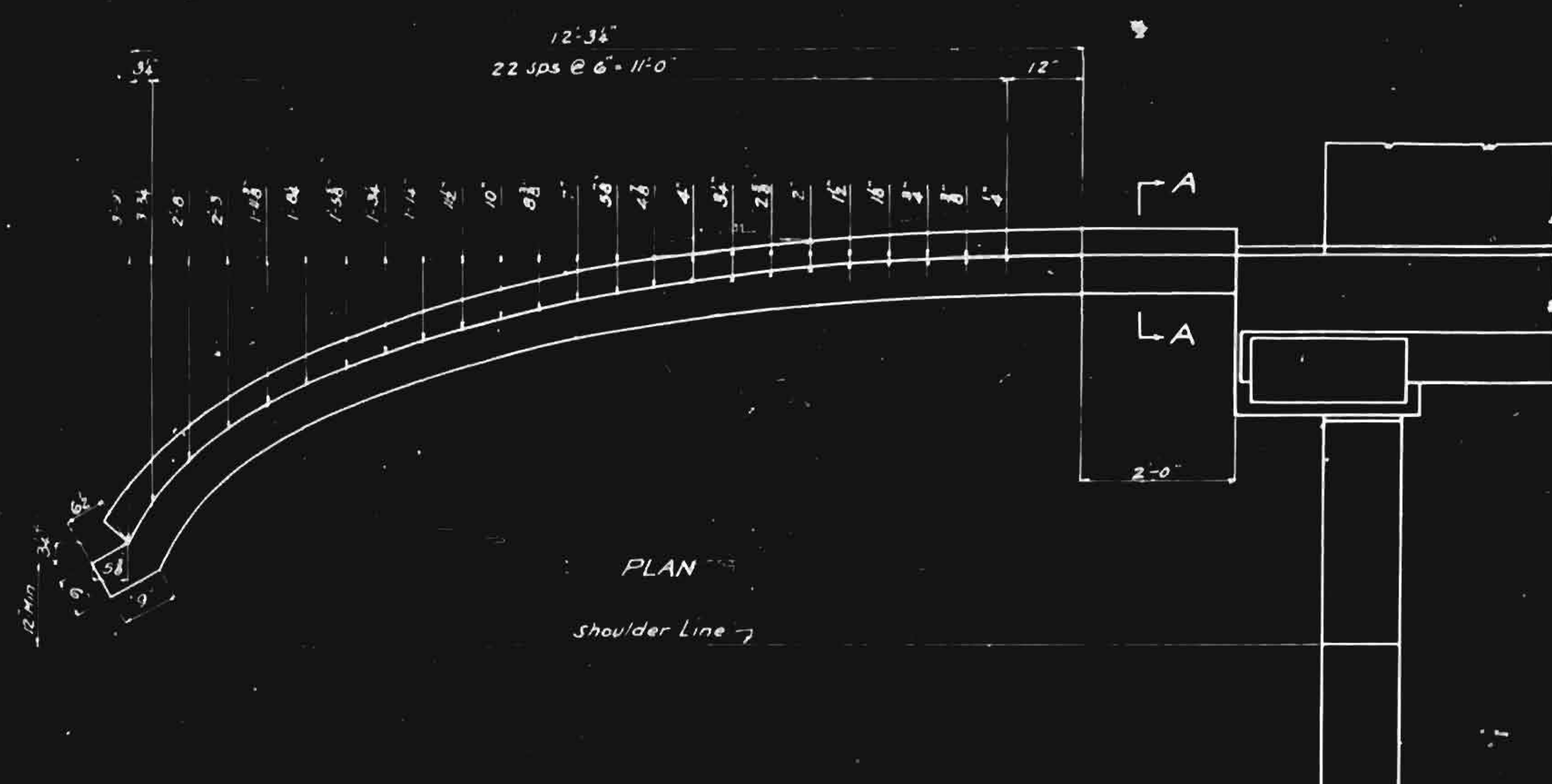
STATE OF NORTH CAROLINA  
STATE HIGHWAY AND  
PUBLIC WORKS COMMISSION

J.P. [Signature]  
W.H. [Signature]

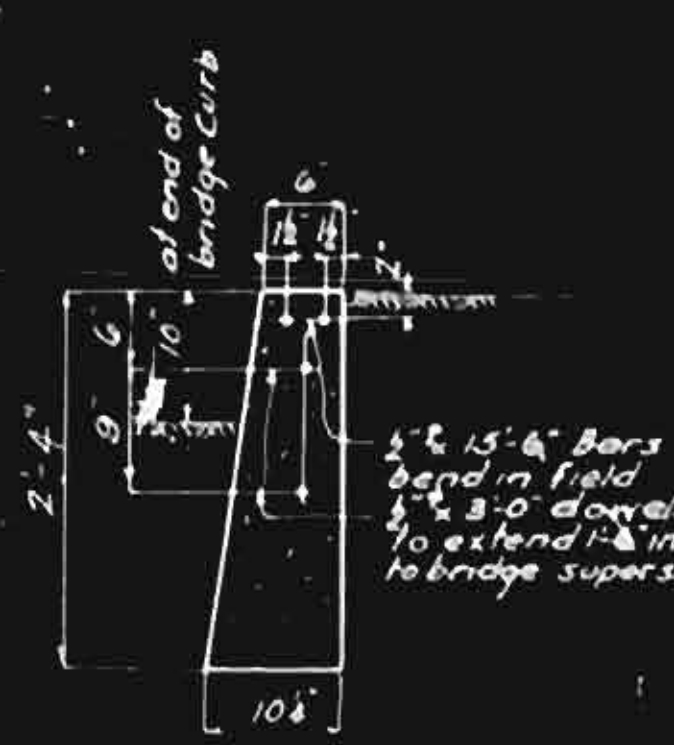
SPECIAL	DESIGNED BY	DATE
STANDARD	CHECKED BY	DATE
	DESIGNED BY	DATE
	CHECKED BY	DATE

FED. ROAD DIST. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	N. C.	7424	5/2	12

F.P. 99, F-266(C)



Note: The excavation for curbs will not be measured and paid for as separate item. Their true cost of same will be included in the unit price bid for Class A Concrete. Drains and bituminous surfacing of ends of bridge to be furnished and placed by the Roadway Contractor.



BILL OF MATERIAL FOR 4 CURBS	
0.5' x 18'-6"	85 Lbs Total Reinforcing
8'-1" x 3'-0"	16 Lbs steel 99 lbs
Class A Concrete	37 Cu Yds

TO BE USED WITH 12' & 1'-6" CURBS.



BILL OF MATERIAL FOR 4 CURBS	
0.5' x 17'-8"	82 Lbs Total Reinforcing
8'-1" x 3'-0"	16 Lbs steel 100 lbs
Class A Concrete	4.1 Cu Yds

TO BE USED WITH 2'-0" WALKS.

SPECIAL	DESIGNED BY: C. M. MULL	DATE: 11-15-1950
	CHECKED BY: C. M. MULL	DATE: 11-15-1950
STANDARD	DESIGNED BY: J. B. GUNTER	DATE: 10-1-1946
	CHECKED BY: J. B. GUNTER	DATE: Nov. 1946
	CHECKED BY: J. M. WELLS	DATE: 11-15-1950

Revised for slope of Approach Curb - Nov. 16, 1948.

PROJECT NO. 7424  
 FORSYTHE & YADKIN COUNTIES  
 STATION: 356 ± 80

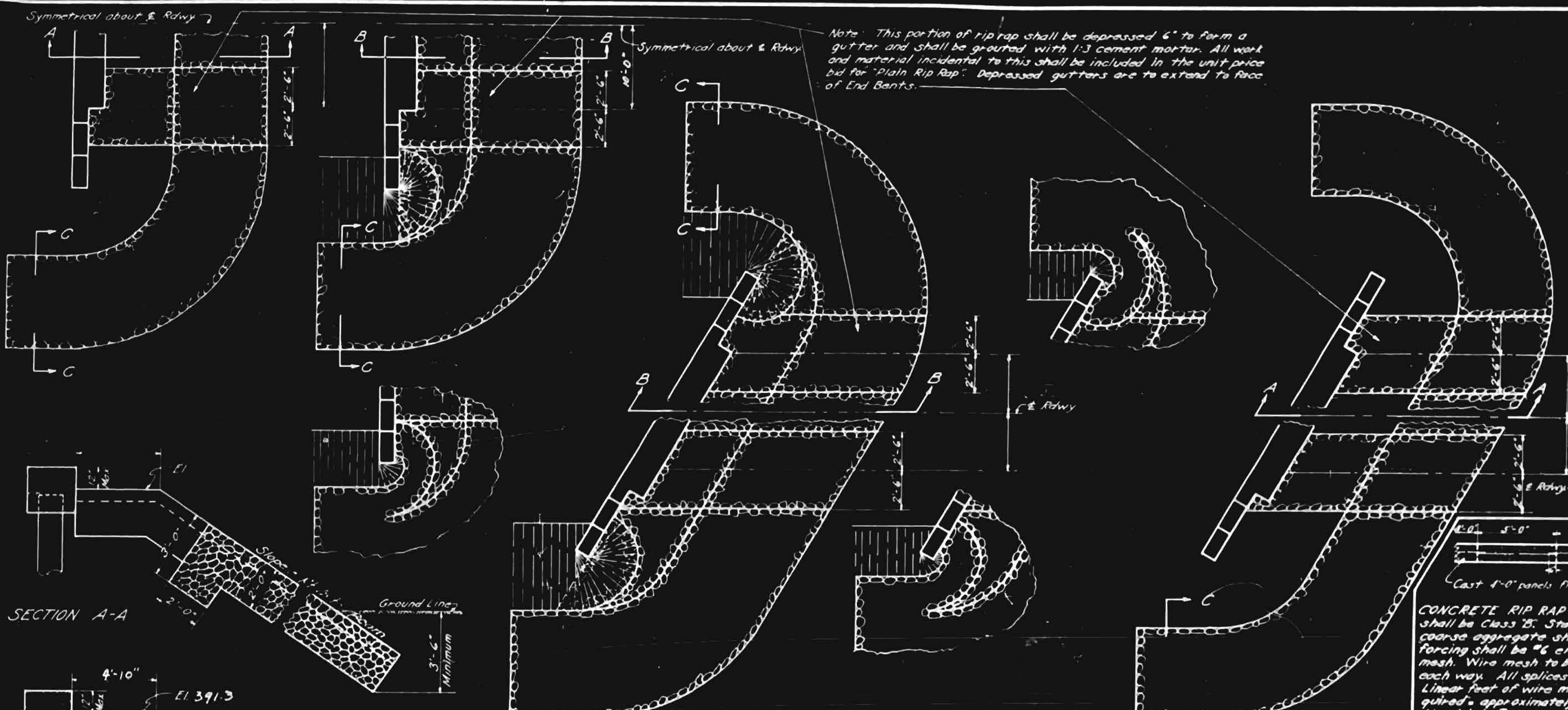
STATE OF NORTH CAROLINA  
 STATE HIGHWAY 100  
 PUBLIC WORKS COMMISSION

SPECIAL  
 DETAILS  
 FOR  
 BRIDGE APPROACH CURBS  
 NOVEMBER, 1950

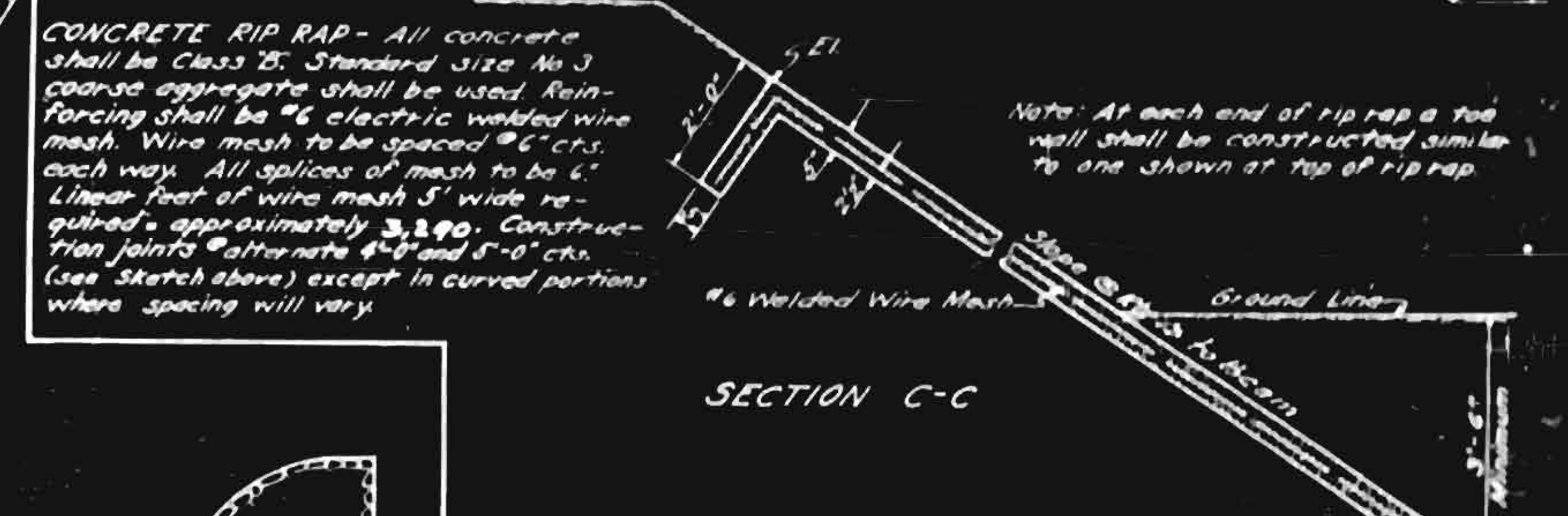
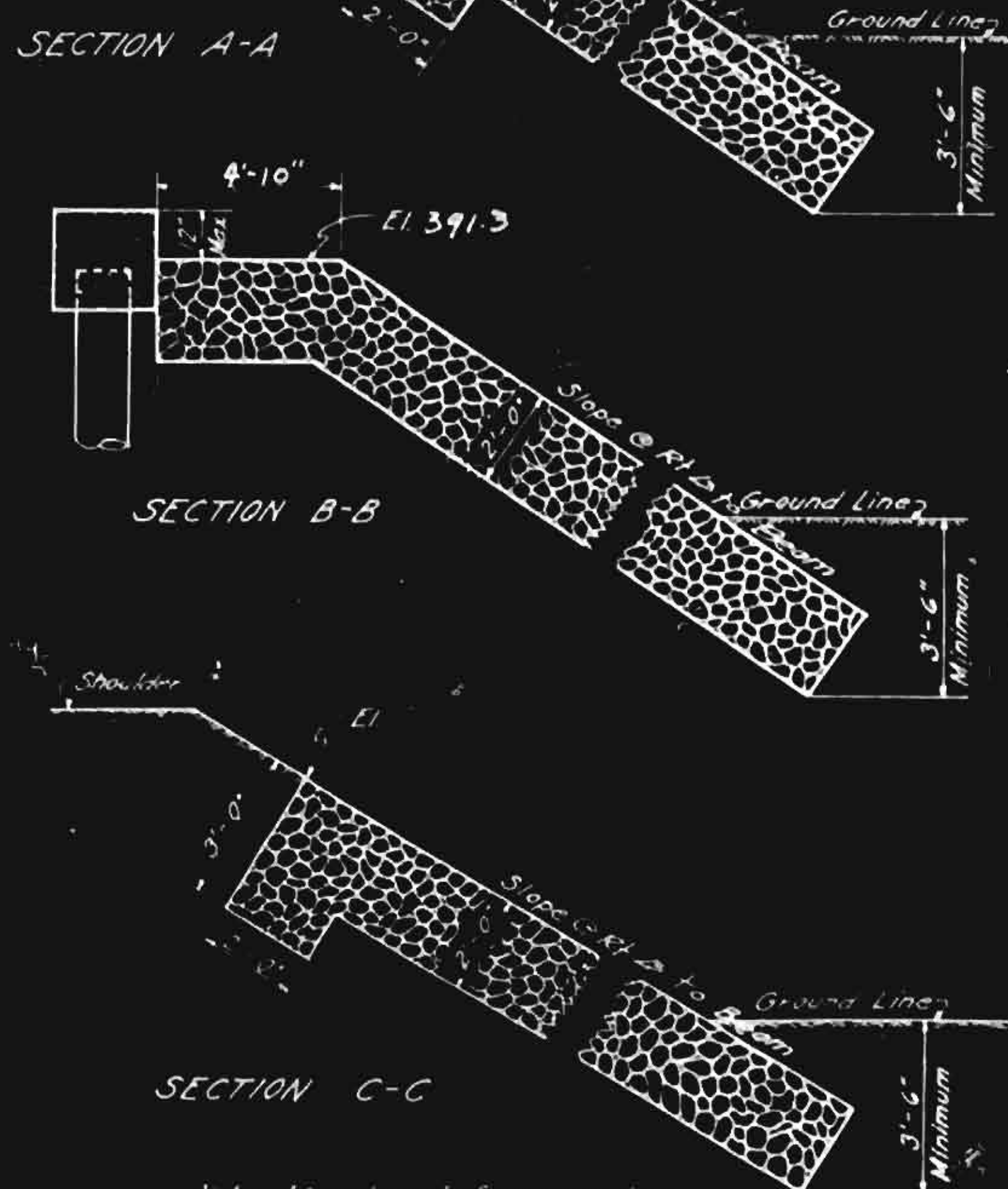
J. B. Gunter  
 W. Vance Thomas

FED. ROAD DIST. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	N.C.	7424	53	14

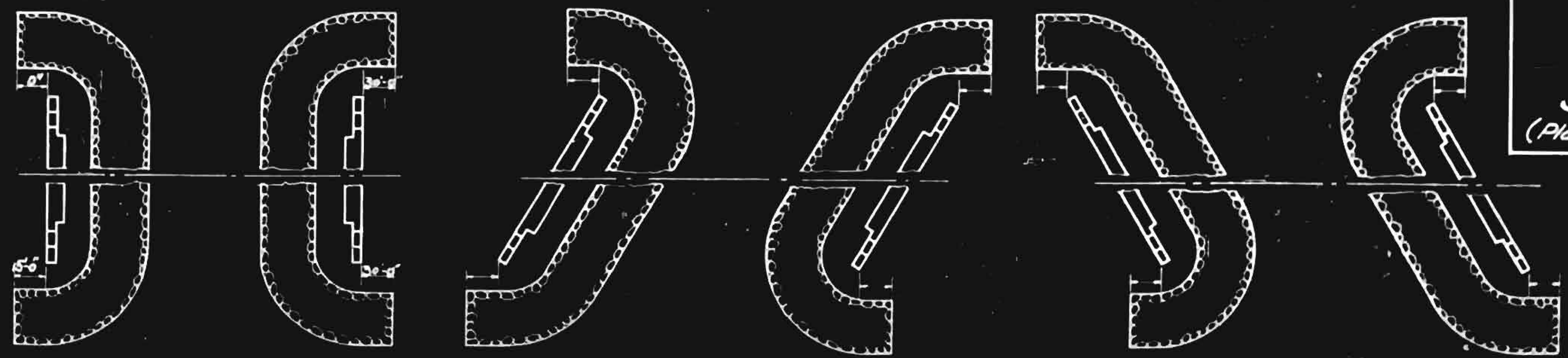
PLAN F-366(3)



TYPICAL PLANS



SECTIONS FOR CONCRETE RIP RAP  
(Plan of concrete rip rap similar to that shown for plain rip rap)



TYPICAL PLANS

PLAIN RIP RAP DETAILS

CONCRETE RIP RAP - All concrete shall be Class 25. Standard size No 3 coarse aggregate shall be used. Reinforcing shall be #6 electric welded wire mesh. Wire mesh to be spaced #6 cfs. each way. All splices of mesh to be 6" linear feet of wire mesh 5' wide required, approximately 3,300. Construction joints alternate 4'-0" and 5'-0" cks. (See sketch above) except in curved portions where spacing will vary.

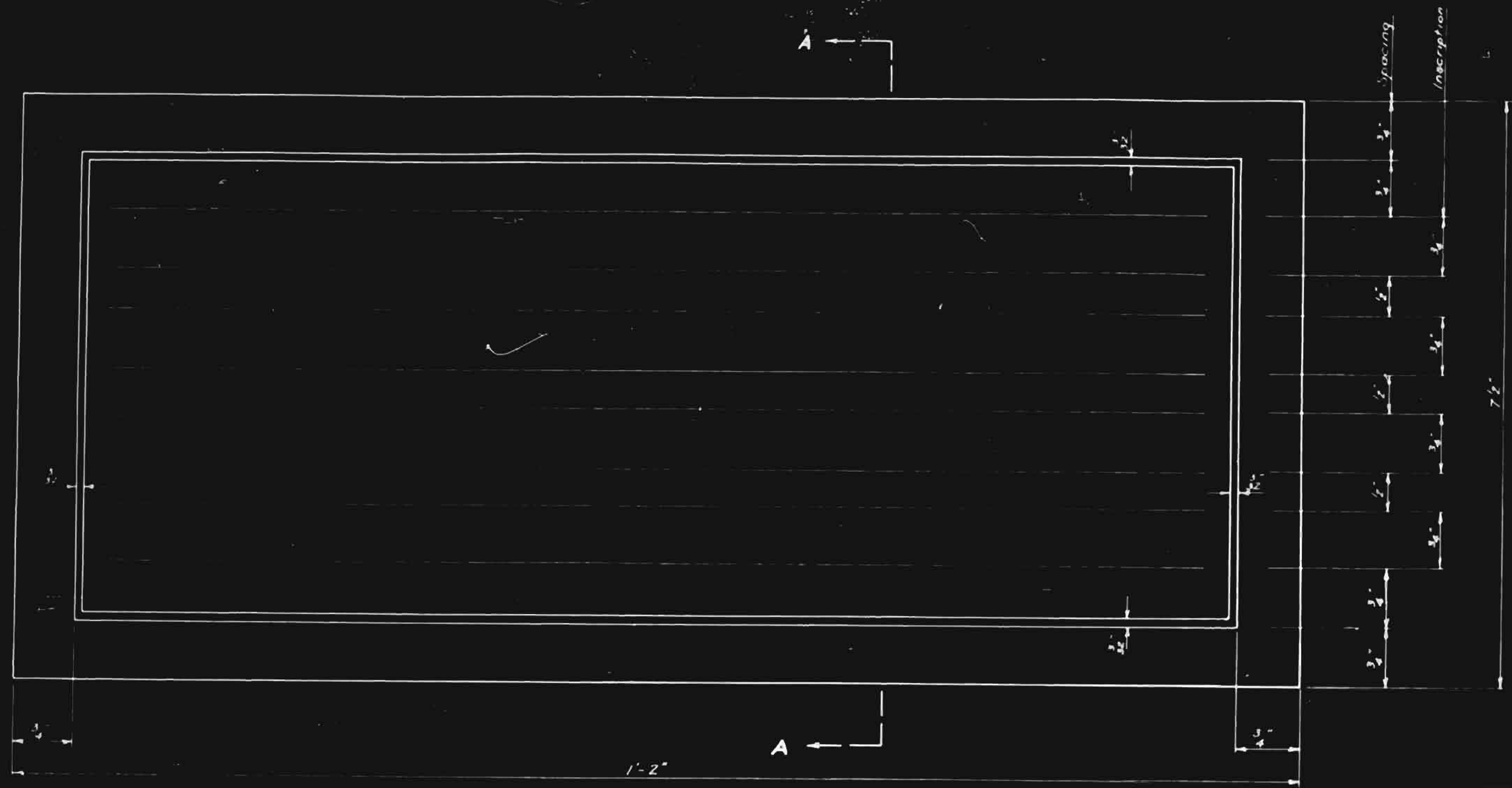
PROJECT NO. 7424  
FORSYTH-YADKIN COUNTIES  
STATION: 356+80

STATE OF NORTH CAROLINA  
STATE HIGHWAY AND  
PUBLIC WORKS COMMISSION  
STANDARD  
RIP RAP  
DETAILS  
OCT. 1947  
J.P. ...  
W. ...

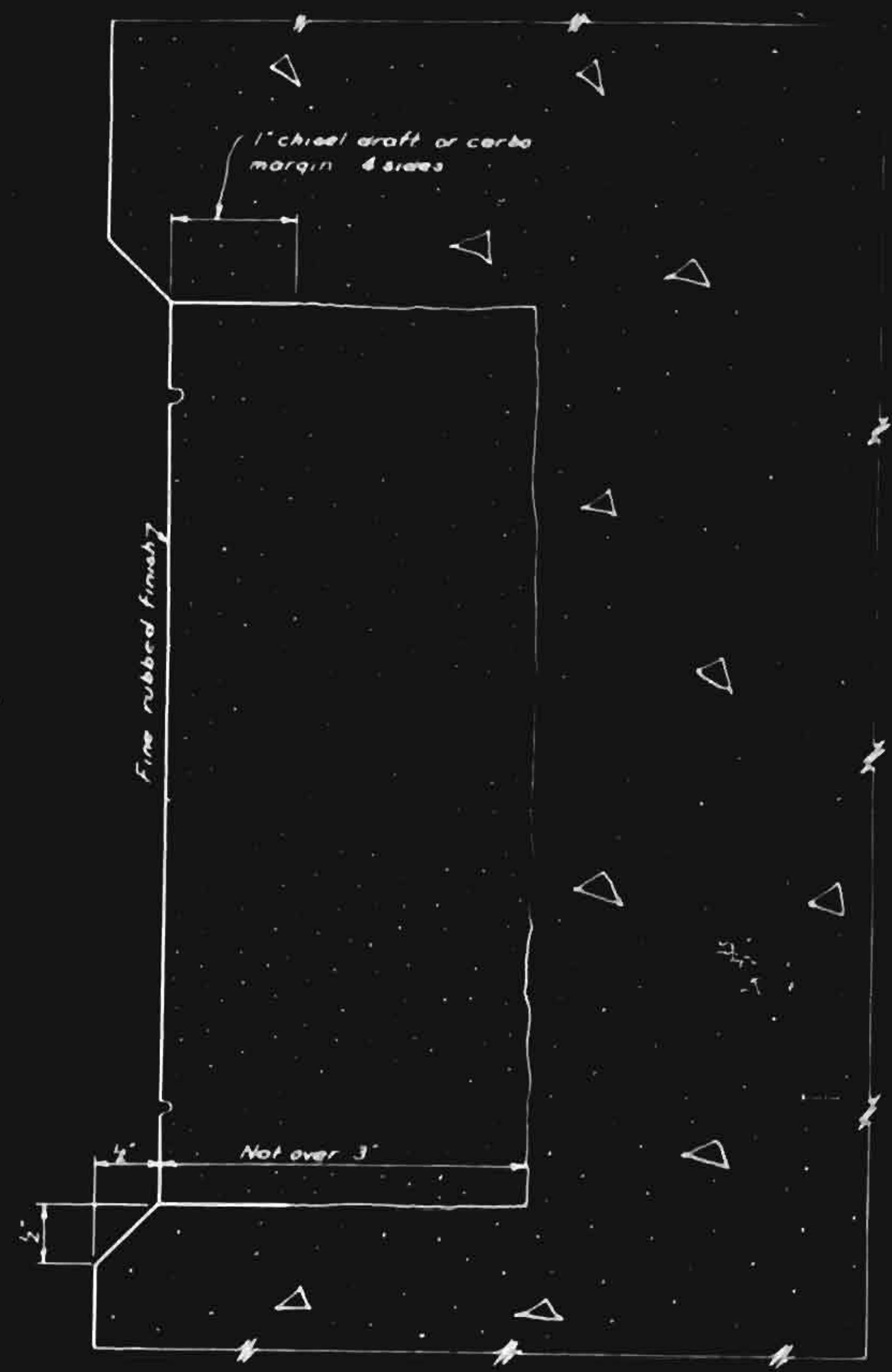
APPROVED	DATE
REVISION	DATE
REVISION	DATE
REVISION	DATE

Revised for constr. jts @ 4' and 5' cts in conc. rip rap 12-20-48

FED. ROAD DIST. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	N.C.	7424	7	7
K. A. F. L. & S. I.				



**ELEVATION**



**SECTION A-A**

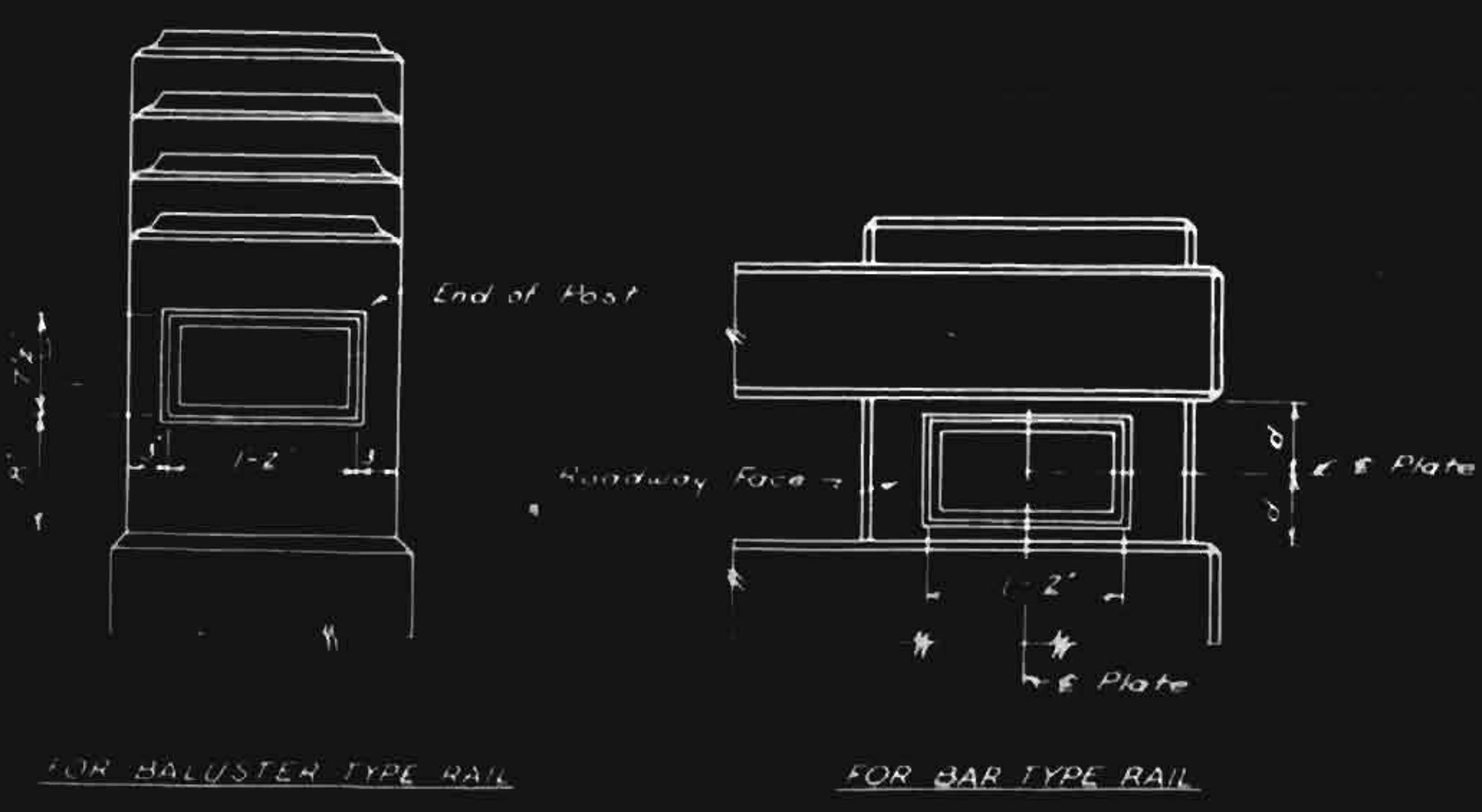
**GENERAL NOTE**

Two name plates are required for each bridge, one on each right hand end post approaching the bridge. See location detail.

The name plates are to be made of granite. Granite shall be light gray, fine or medium grained, sound in quality and free from defects that would mar its appearance. Exposed face of plates to have a fine rubbed finish. Lettering shall be sandblast sunk 3/8" high vertical Modern Roman style. The wording shall be as shown in the detail showing the correct wording.

The entire cost of the name plates complete in place shall be included in the contract price bid for Class A concrete.

**PROJECT NO. 7424**  
**FORSYTH-YADKIN COUNTIES**  
**STATION: 356+80**



**LOCATION DETAIL FOR NAME PLATE**

**FORSYTH COUNTY**  
**STATE PROJECT 7424**  
**FEDERAL AID**  
**1950**

DETAIL SHOWING CORRECT WORDING  
 ONE ONLY REQUIRED

**YADKIN COUNTY**  
**STATE PROJECT 7424**  
**FEDERAL AID**  
**1950**

DETAIL SHOWING CORRECT WORDING  
 ONE ONLY REQUIRED

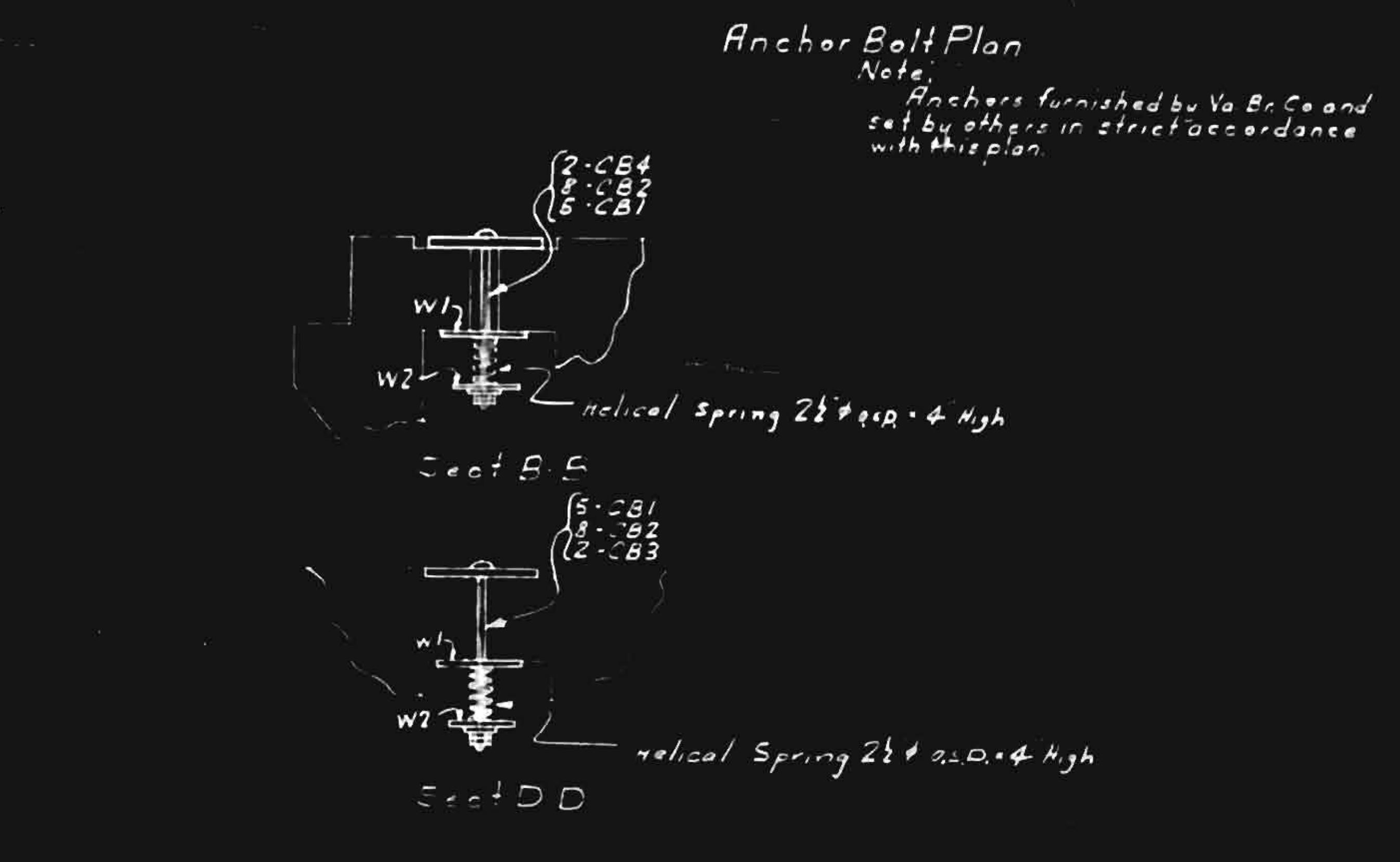
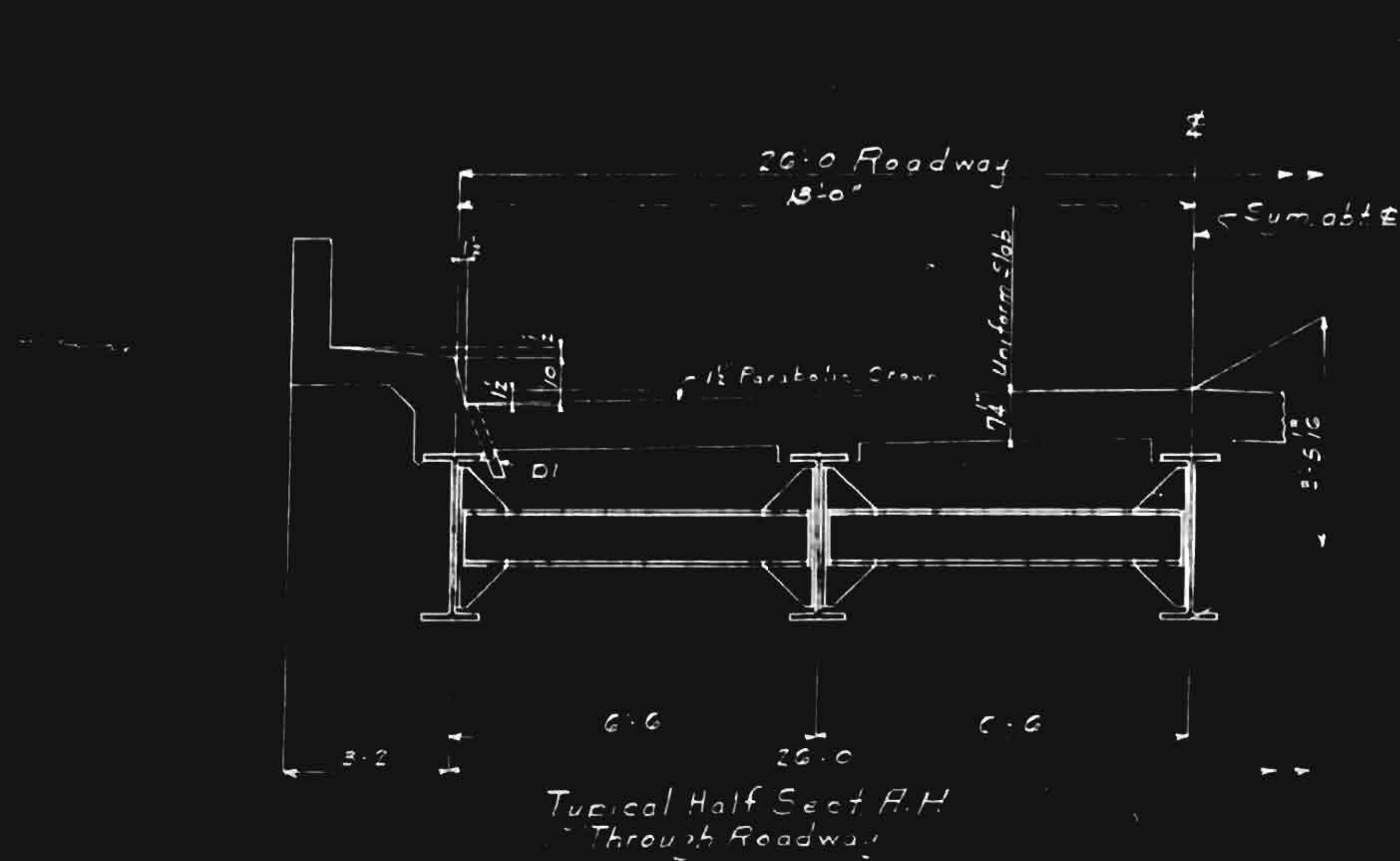
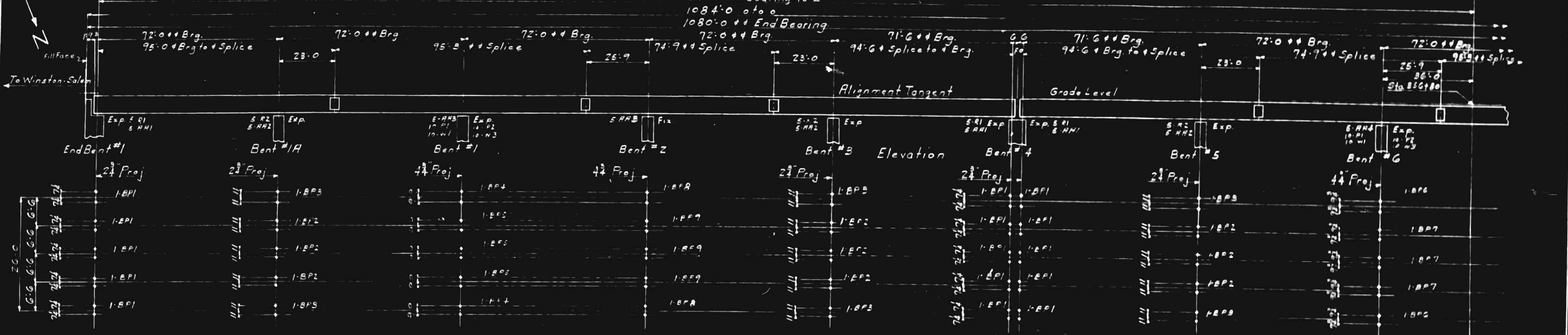
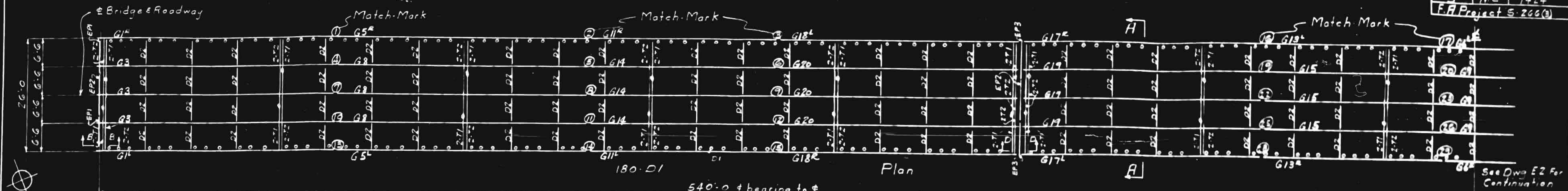
STATE OF NORTH CAROLINA  
 STATE HIGHWAY AND  
 PUBLIC WORKS COMMISSION

**STANDARD  
 NAME PLATE  
 FOR  
 CONCRETE BRIDGES  
 MAY 1944**

*J.P. ...*  
*W. ...*

SPECIAL	27 MAY 1944	1944
STANDARD	W.S.S. W.A. OFFZ	May 1944 June 1949 July 1949

Fed. Road Dist. No.	State	Project No.
3	N.C.	7424
F.A. Project S-266(3)		



**Reaming Note:**  
 General reaming is not required. All holes for stirrups field splices shall be sub-punched and reamed to 1 1/8" and match-marked while a complete line of stirrups is assembled in the shop, except that holes in material more than 1/2" in thickness shall be sub-drilled and reamed or drilled from the solid.

**Note:**  
 Reinforcing steel, H-piles or accessories, field splices or splicing material, recall or other material not furnished by Va. Br. Co.

**Shop Part:**  
 All finished surfaces to receive 12" white lead & flake.

REVISION	DATE

**CONTRACT V 7125**  
 Details of Erection of Anchor Bolt Plan  
 for Yadkin River Bridge  
 Location: Forsyth & Yadkin Cos., N.C.  
 Owner: Harvey H. Stewart (Cont.)

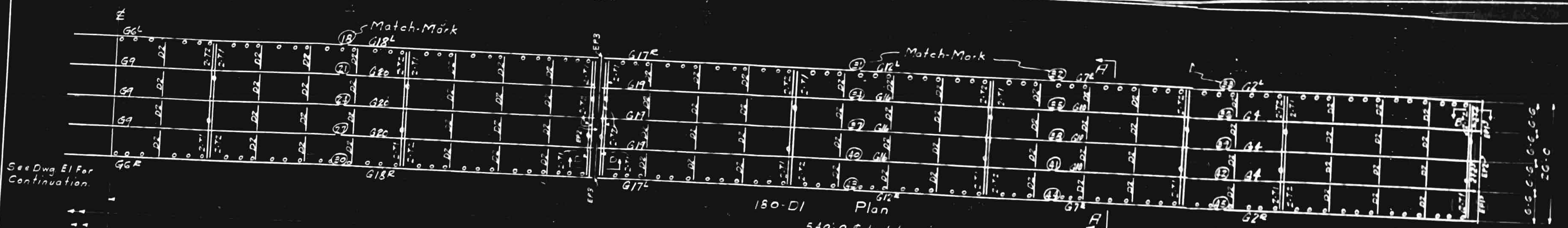
**VIRGINIA BRIDGE COMPANY**  
 ROANOKE, VIRGINIA

To be fabricated at **Roanoke**  
 Made by **E.W. Scott** 10-7-50  
 Erected by **Scott** 11-1-50  
 Checked by **Scott** 11-1-50

Shop Part: 1 of 9 L 40.1 (Part of 11.2 Sta. Hwy. Spec.)  
 Shop Contact: Frutkin & Robertson  
 Reaming - See Note  
 Erection - See Note  
 Field Contact - Frutkin & Robertson

Scale: 1/4" = 1'-0"





**Anchor Bolt Plan**  
 Note:  
 Anchors furnished by Va. Br. Co.  
 and set by others in strict accordance  
 with this plan.

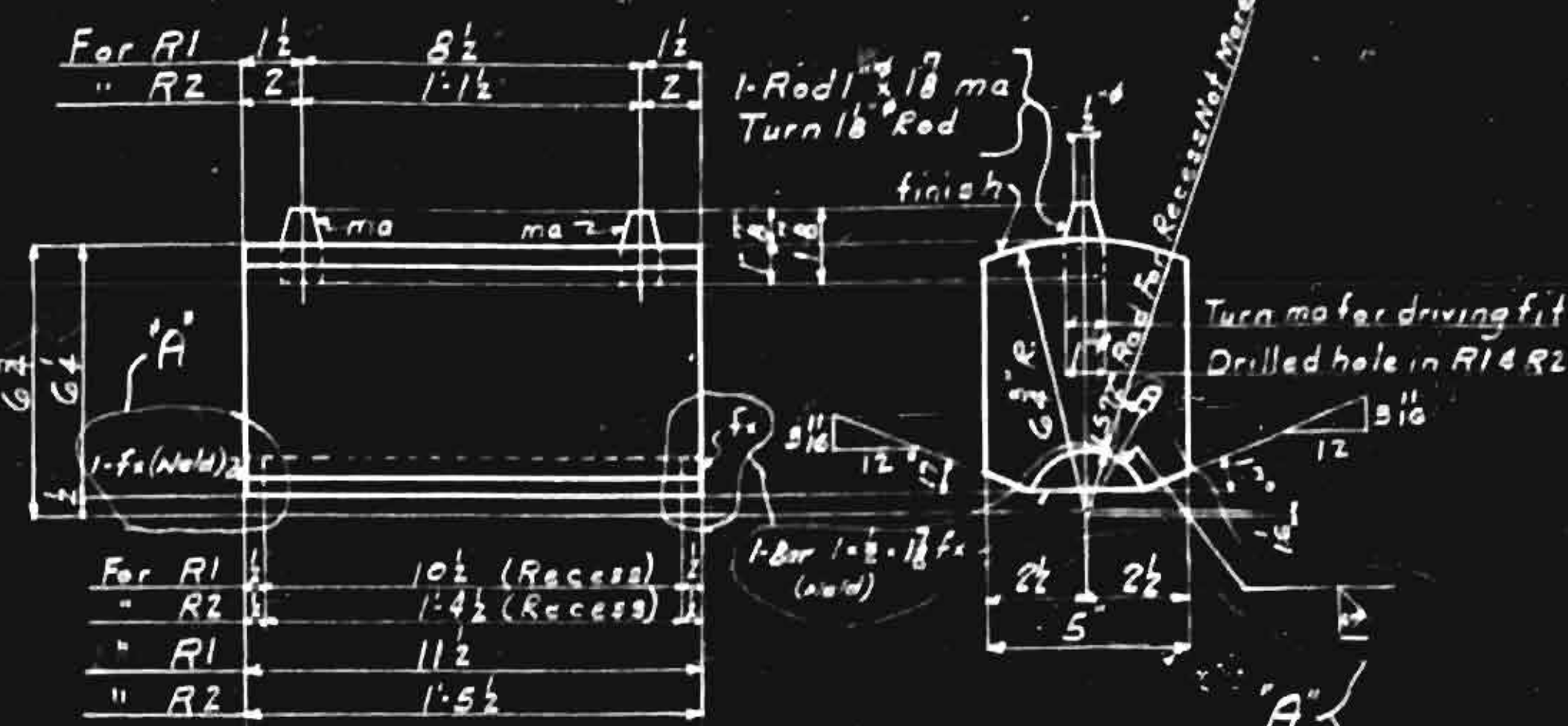
1-BF7 x 1 1/2 x 2 1/2 ~ BP2 (Hot Straightened)  
 1-BF9 x 1 1/2 x 2 1/2 ~ BP3 ( " " )

REVISION	DATE

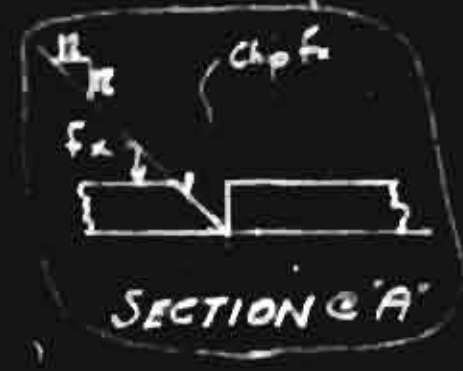
Shop Fabricate  
 Shop Contact Surfaces and Grouting Inspection  
 Shop  
 Notes  
 Special Handling  
 See Dwg. E1  
 Mark on Shop Bill

**CONTRACT V 7625**  
 Details of Erection of Anchor Bolt Plan  
 For York River Bridge  
 Location: Foreth, York River, N.C.  
 Owner: Harlow H. Stewart (Contractor)  
**VIRGINIA BRIDGE COMPANY**  
 ROANOKE, VIRGINIA  
 To be fabricated at: Roanoke  
 Made by: E.H.S. 10-10-50  
 In charge of: Scott  
 Checked by: [Signature]  
 Date: 1/15/50  
 Scale: 1/4" = 1'-0"  
 Sheet No. E2 of E2

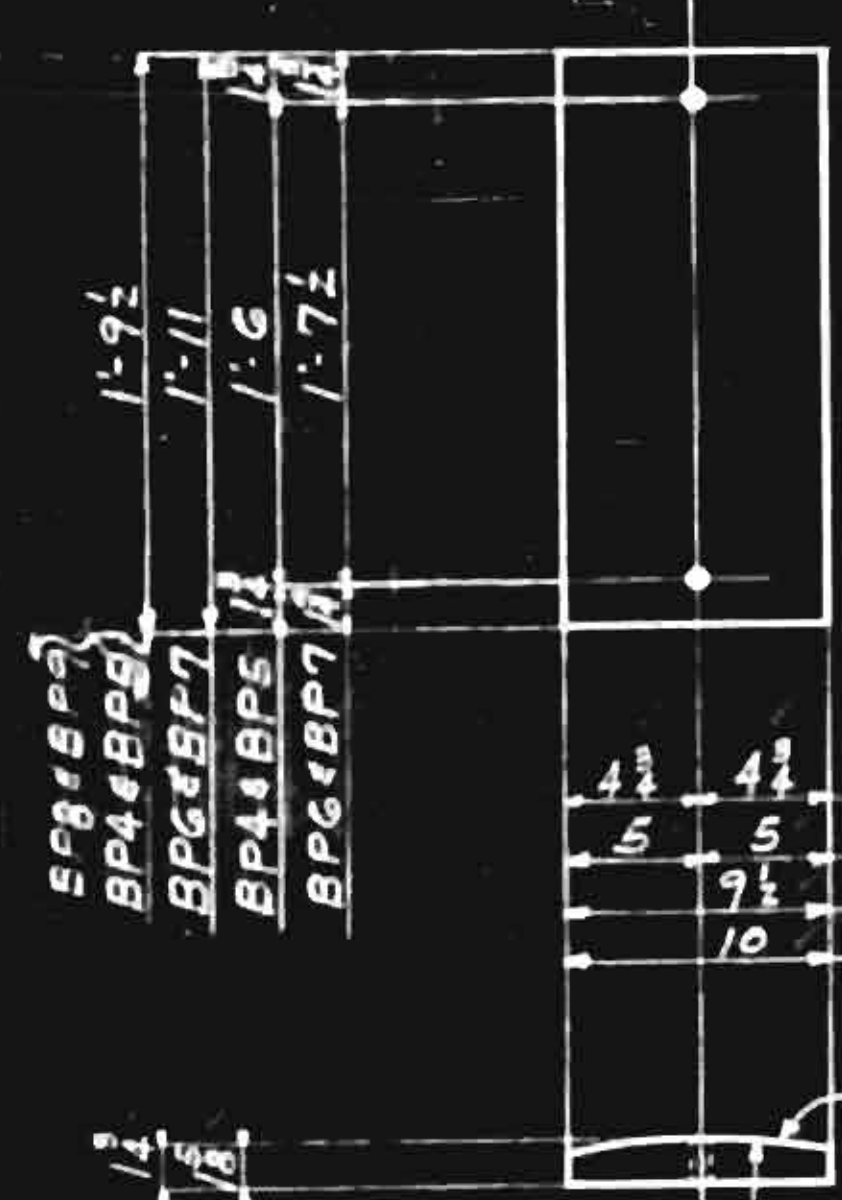
NY  
 5



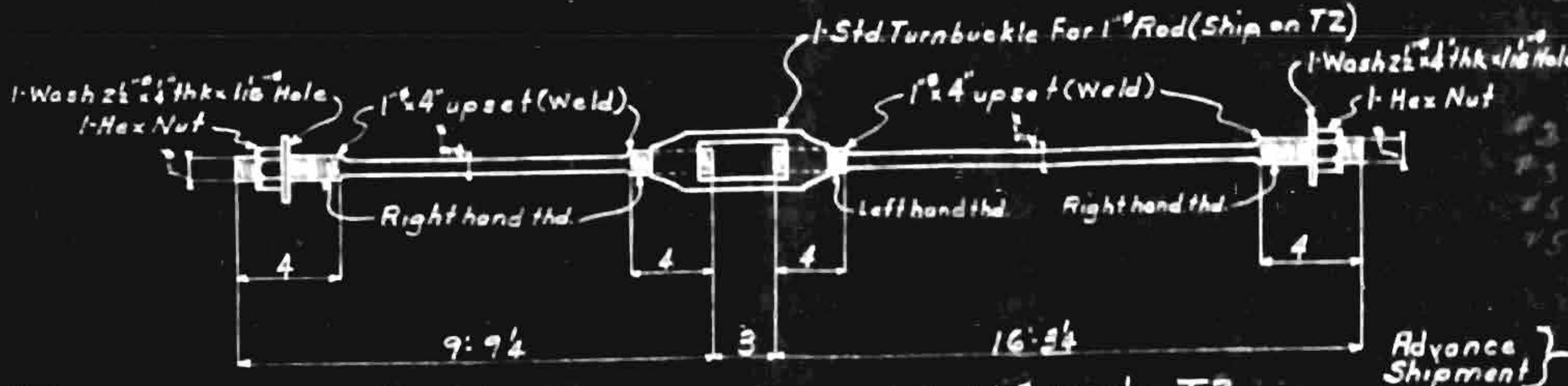
1-bar 5x6 1/2 x 11 1/2 ~ R1  
 1-bar 5x6 1/2 x 15 1/2 ~ R2



1 1/2\"/>



- 1-R 9 1/2 x 1 1/2 ~ BP4 (Finish 1 1/2\"/>
- 1-R 10 x 1 1/2 x 1-11 ~ BP6 ( do )
- 1-R 9 1/2 x 1 1/2 ~ BP5 (Finish 1 1/2\"/>
- 1-R 10 x 1 1/2 x 1-11 ~ BP7 ( do )
- 1-R 9 1/2 x 1 1/2 ~ BP8 (Finish 1 1/2\"/>
- 1-R 9 1/2 x 1 1/2 ~ BP9 ( do 1 1/2\"/>

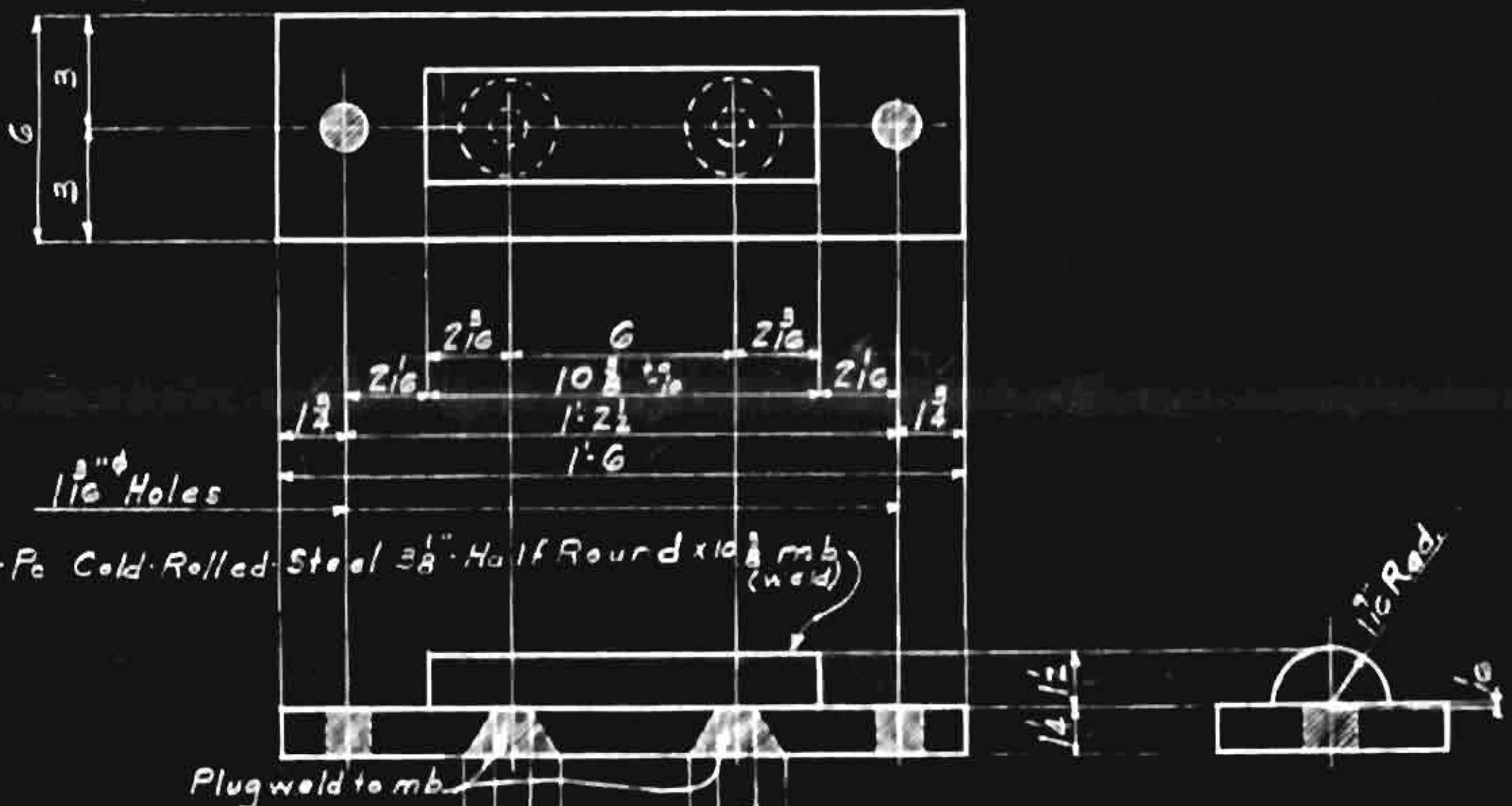


1-Rod 3\"/>

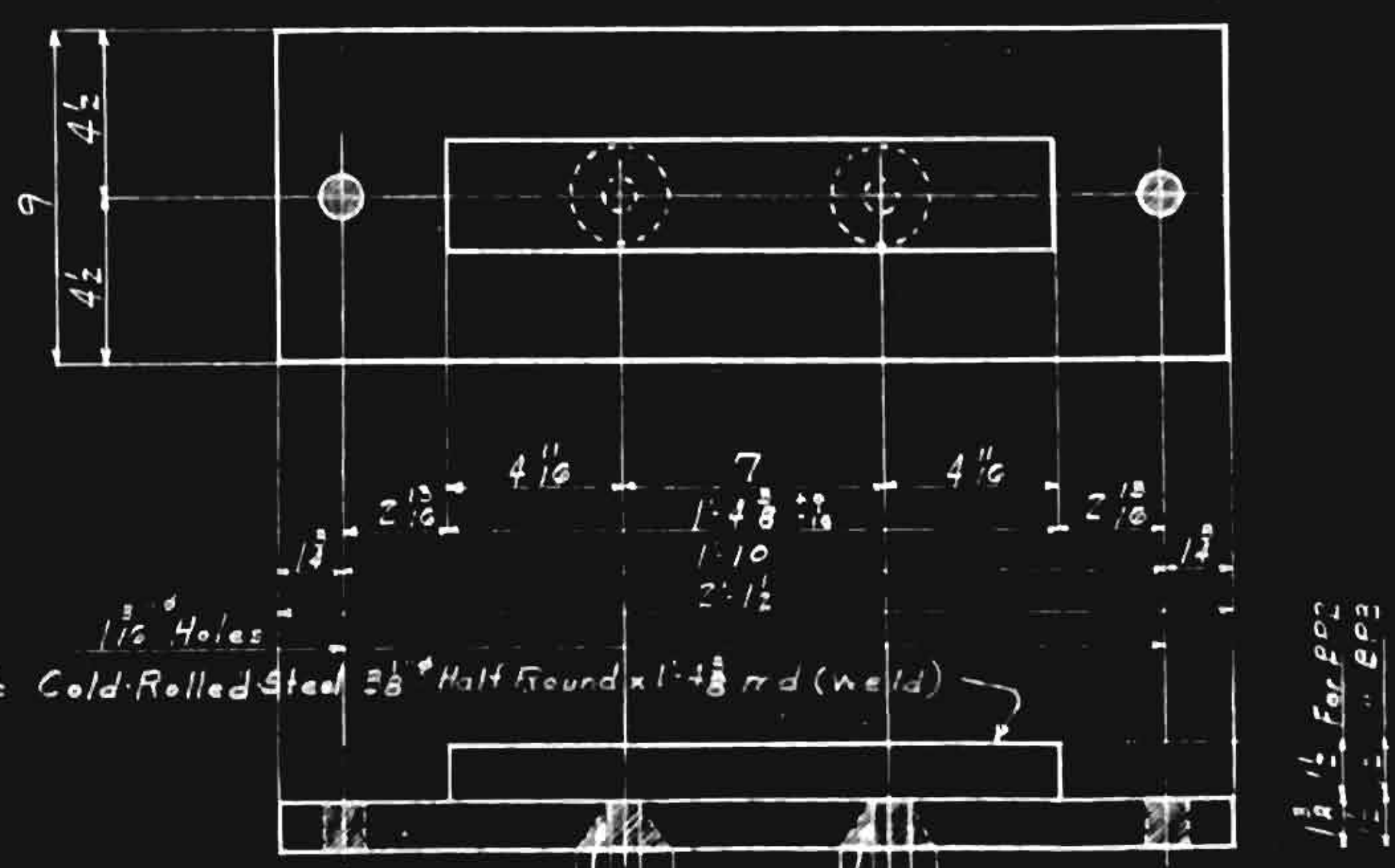
Advance Shipment

**REQUIRED LIST**

MAKE	DESCRIPTION	MARK
30	Rockers	R1
30	do	R2
30	Base Plates	BP1
18	do	BP2
12	do	BP3
4	do	BP4
6	do	BP5
2	do	BP6
3	do	BP7
30	Anchor Assemblies	AA1
30	do	AA2
25	do	AA3
5	do	AA4
6	Base Plates	BP8
30	Pre-Straked Roadways	PI
30	Washers	W
72	Tie Rods	TR
72	do	TR2
360	Roadway Drains	DI
9	Base Plates	BP9

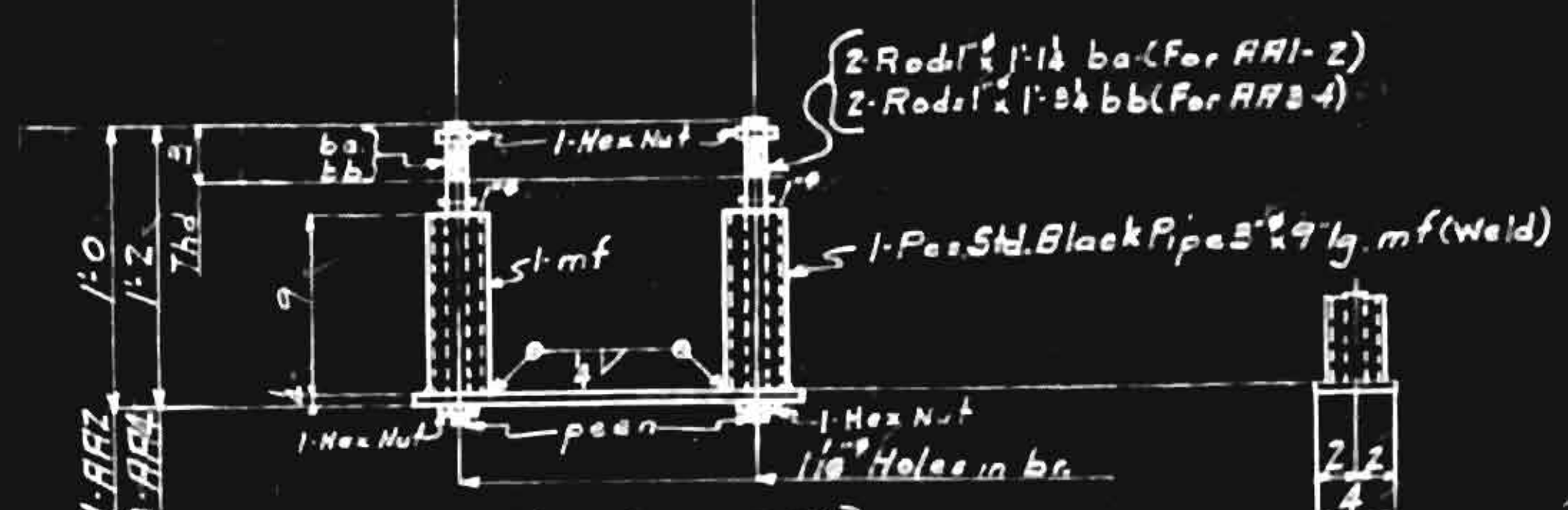


1-R 6 x 14 x 1-6 ~ BP1 (Hot Straightened)



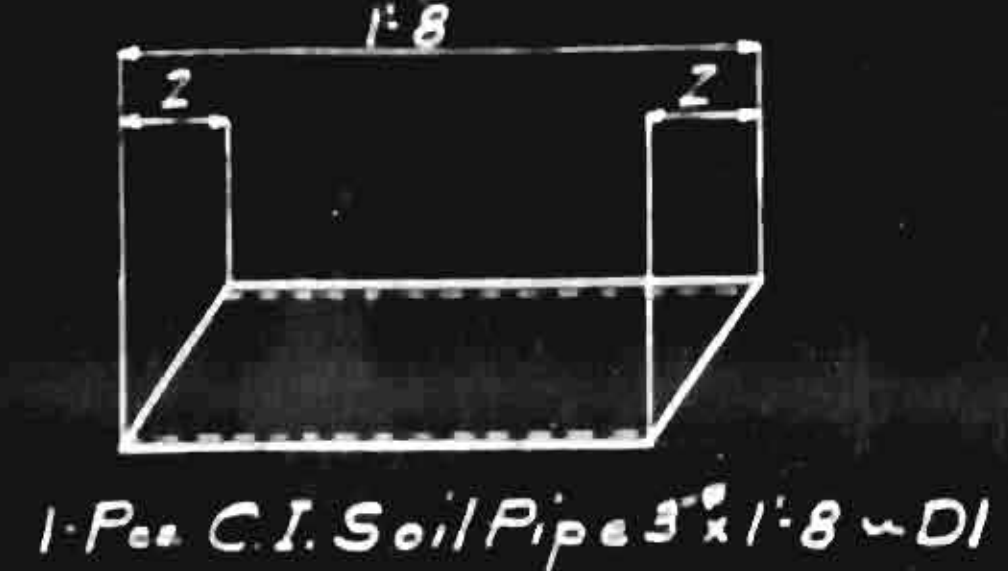
1-R 9 x 18 x 2-1/2 ~ BP2 (Hot Straightened)  
 1-R 9 x 12 x 2-1/2 ~ BP3 ( do )

- 1-2 1/2\"/>
- 1-10\"/>
- 1-6\"/>
- 1-9 1/2\"/>



- 1-br. 4 x 4 x 1-7 ~ AA1
- 1-br. 4 x 4 x 2-2 1/2 ~ AA2
- 1-br. 4 x 4 x 1-10 ~ AA3
- 1-br. 4 x 4 x 1-11 1/2 ~ AA4

No Point



1-Pce C.I. Soil Pipe 5\"/>

Point Note:  
 All finished surfaces to receive  
 1-qt. White Lead & Tallow.

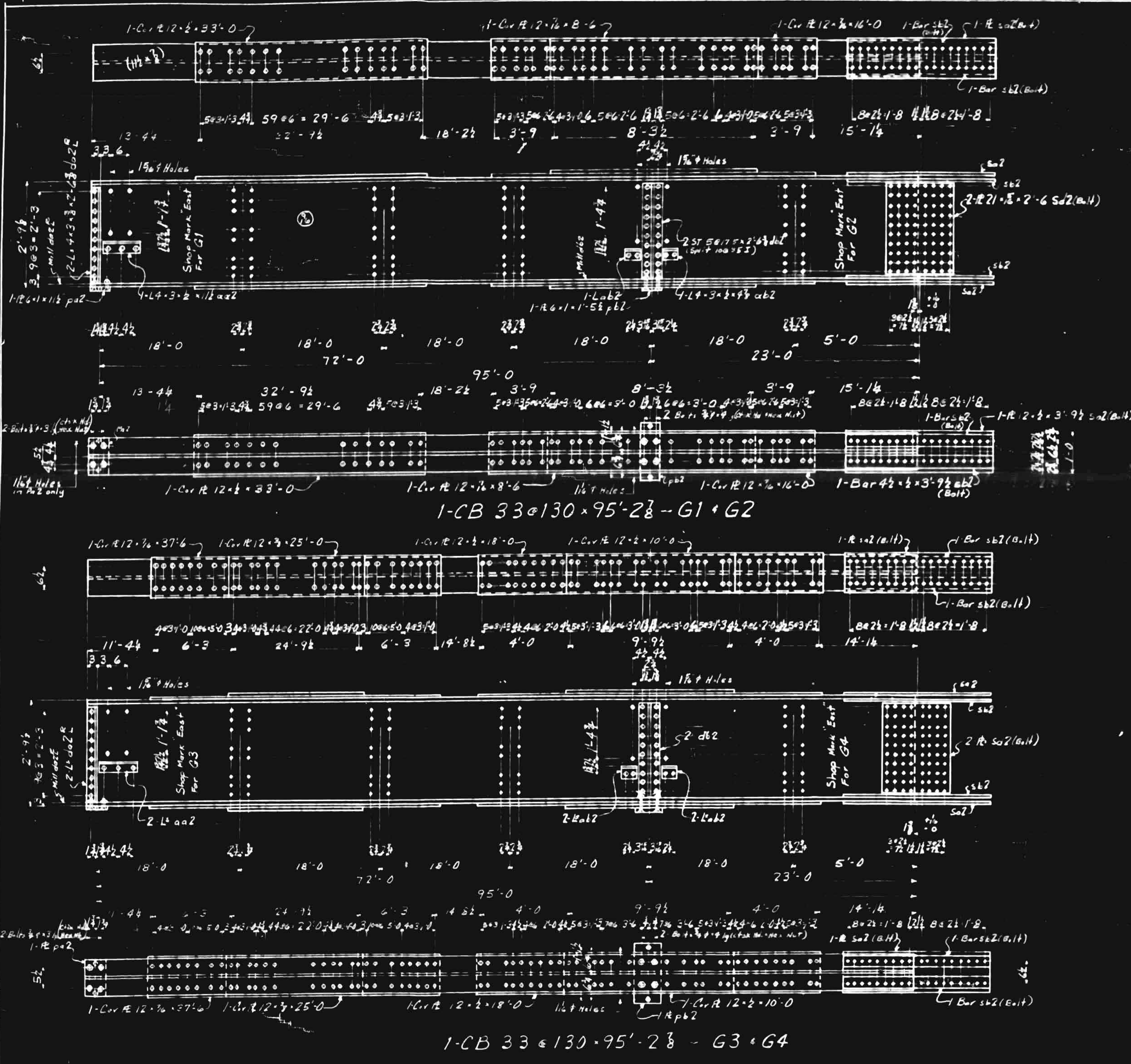
REVISION	DATE

**CONTRACT V 76 25**  
 Design of Rockers, Base Plates, Tie Rods, Drains, Anchor Assemblies and Misc. for York in River Bridge.  
 Location: For a York & York in River, N.C.  
 Owner: Harry H. Stewart (Contn.)

**VIRGINIA BRIDGE COMPANY**  
 ROANOKE, VIRGINIA

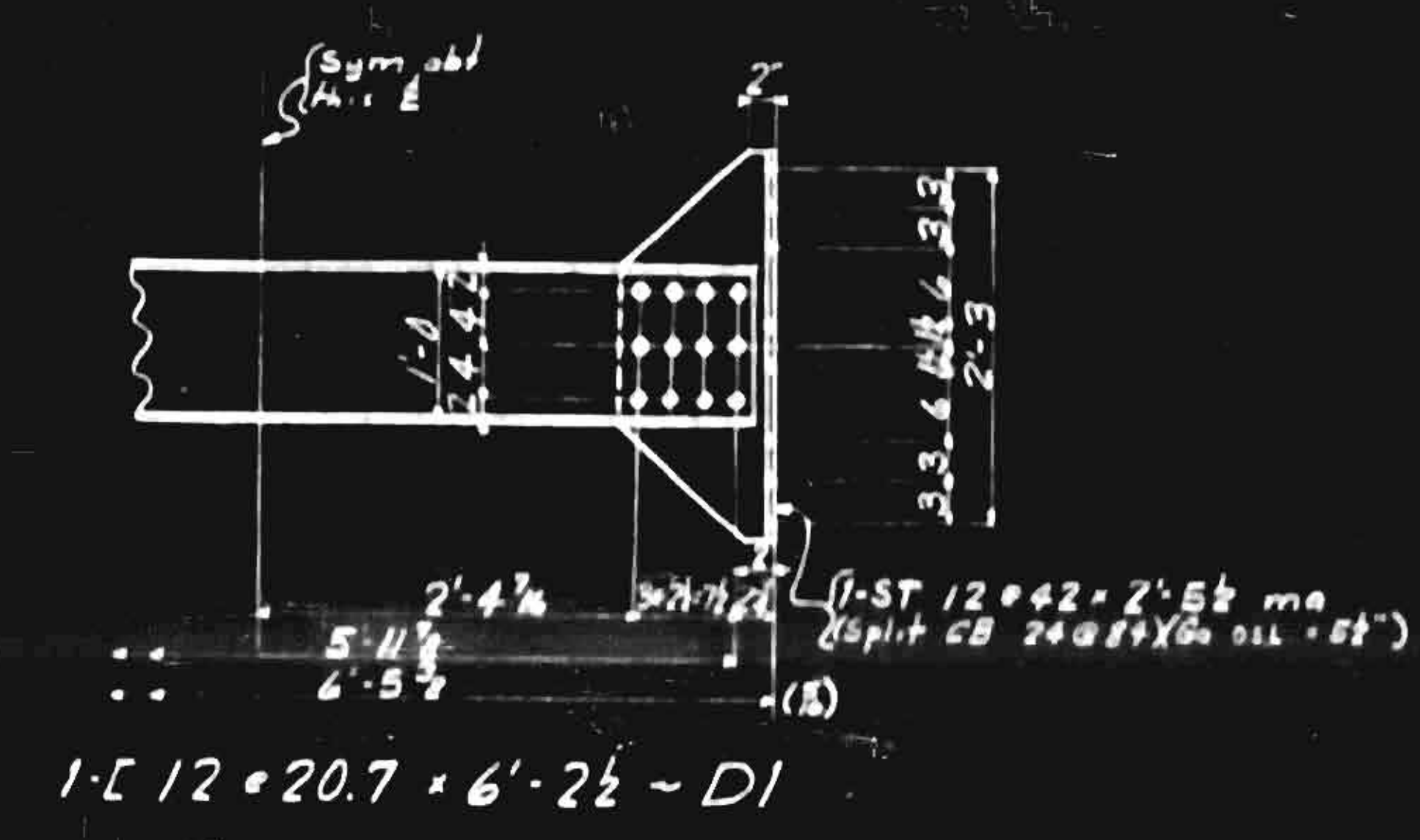
To be fabricated at Roanoke, Va.  
 Made by L.W.S. 10-16-46  
 In charge of Scott

Scale: 1/4" = 1'-0"  
 Date: 1-2-5

**REQUIRED LIST**

MAKE	DESCRIPTION	MARK
1	Girders (See) G1	
1	do (do) G2	
1	do (do) G3	
3	do (do) G4	
3	do (do) G5	
180	Diaphragms D2	
30	Push or Pull P1-P2	
30	St. Cols. W/ing G1-Pull W3	

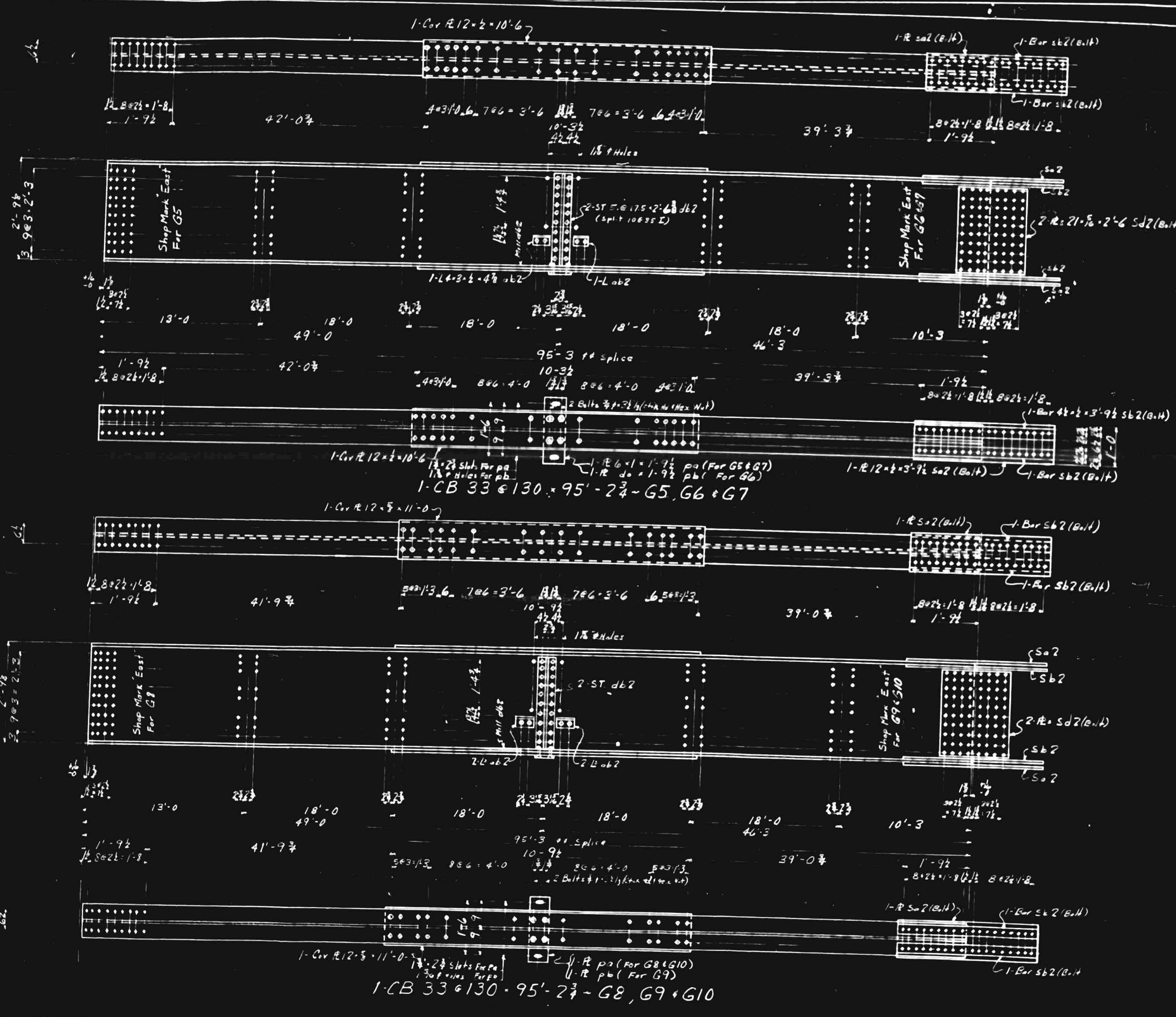


**Beaming Note:**  
 General beaming is not required.  
 All holes for stringer field splices shall be sub-punched 1/8" and reamed to 1/8" and match-marked while a complete line of stringers is assembled in the shop, except that holes in material more than 1/2" in thickness shall be sub-drilled and reamed or drilled from the solid.

REVISION	
NO.	DATE

**CONTRACT V 7625**  
 Details of Girder Beams  
 For Tuckahoe River Bridge  
 Location: Raleigh, Virginia, N.C.  
 Owner: Harvey H. Stewart (Contractor)  
**VIRGINIA BRIDGE COMPANY**  
 ROANOKE, VIRGINIA  
 To be fabricated at: **Roanoke**  
 Made by: **BM** 11-6-50 Checked by: **EM** 11-27-50  
 In charge of: **Scott** Date: **11-50**  
 Scale: **1/8" = 1'-0"** Sheet No. **2**

NO.	DATE	BY	DESCRIPTION



**REQUIRED LIST**

MAKE	DESCRIPTION	MARK
1	Girders (two)	G5 <sup>R</sup>
1	do	(do) G7 <sup>R</sup>
1	do	(do) G5 <sup>L</sup>
1	do	(do) G7 <sup>L</sup>
3	do	(two) G8
3	do	(do) G10
1	do	(do) G6 <sup>L</sup>
3	do	(two) G6 <sup>R</sup>

**Examining Note:**  
 General reaming is not required.  
 All holes for stringer field splices shall be sub-punched 1/8" and reamed to 1/8" and match-marked while a complete line of stringers is assembled in the shop, except that holes in material more than 1/2" in thickness shall be sub-drilled and reamed or drilled from the field.

**REVISION**

NO.	DATE	DESCRIPTION

Shop Plans  
 1 of R.L.10  
 (Per 1946 NC St. Hwy. Spec.)  
 Shop Contact Surfaces not Painted  
 Inspection:  
 Freshling & Robertson  
 Date: 11-5-50  
 Status: Final

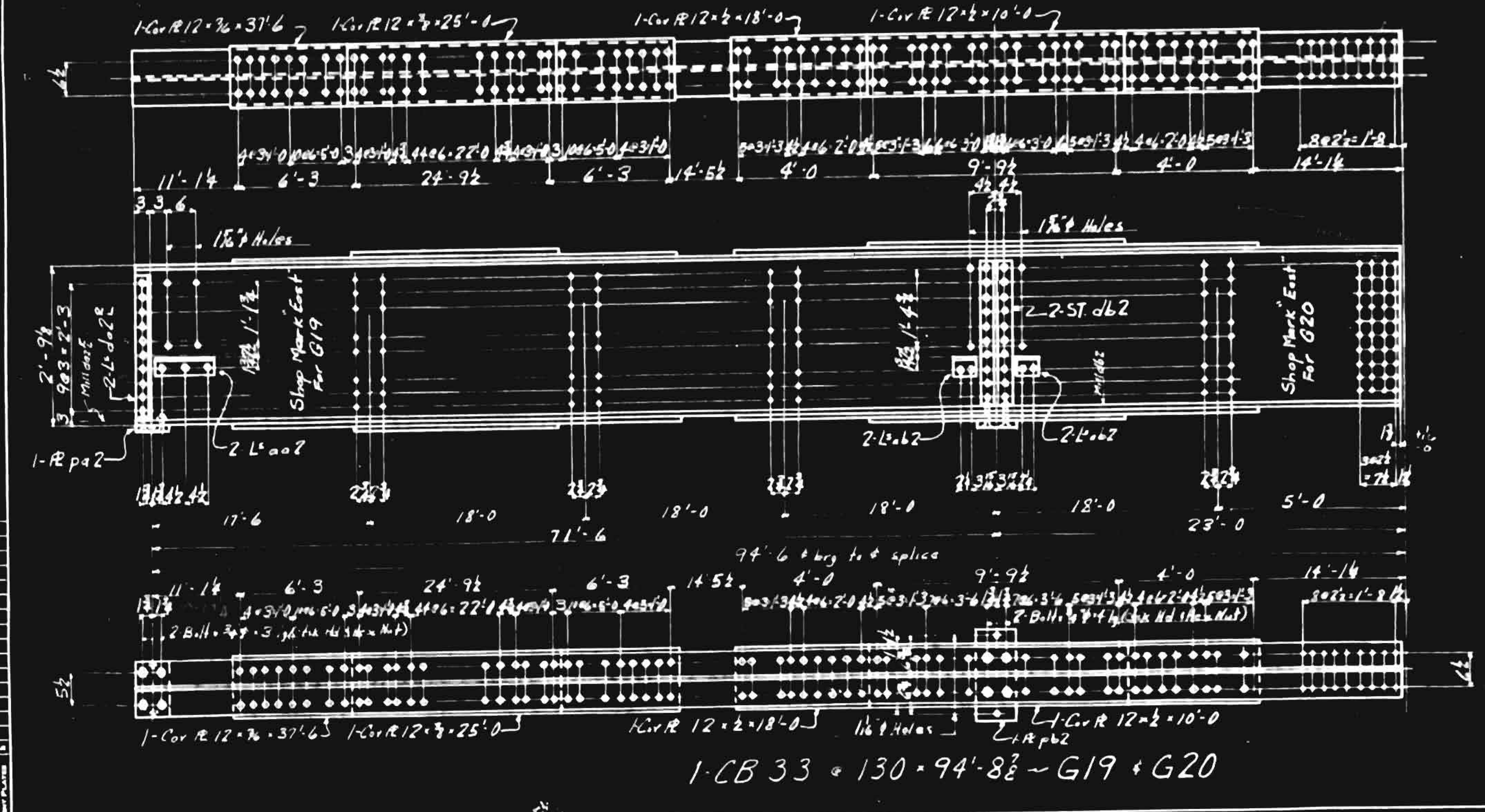
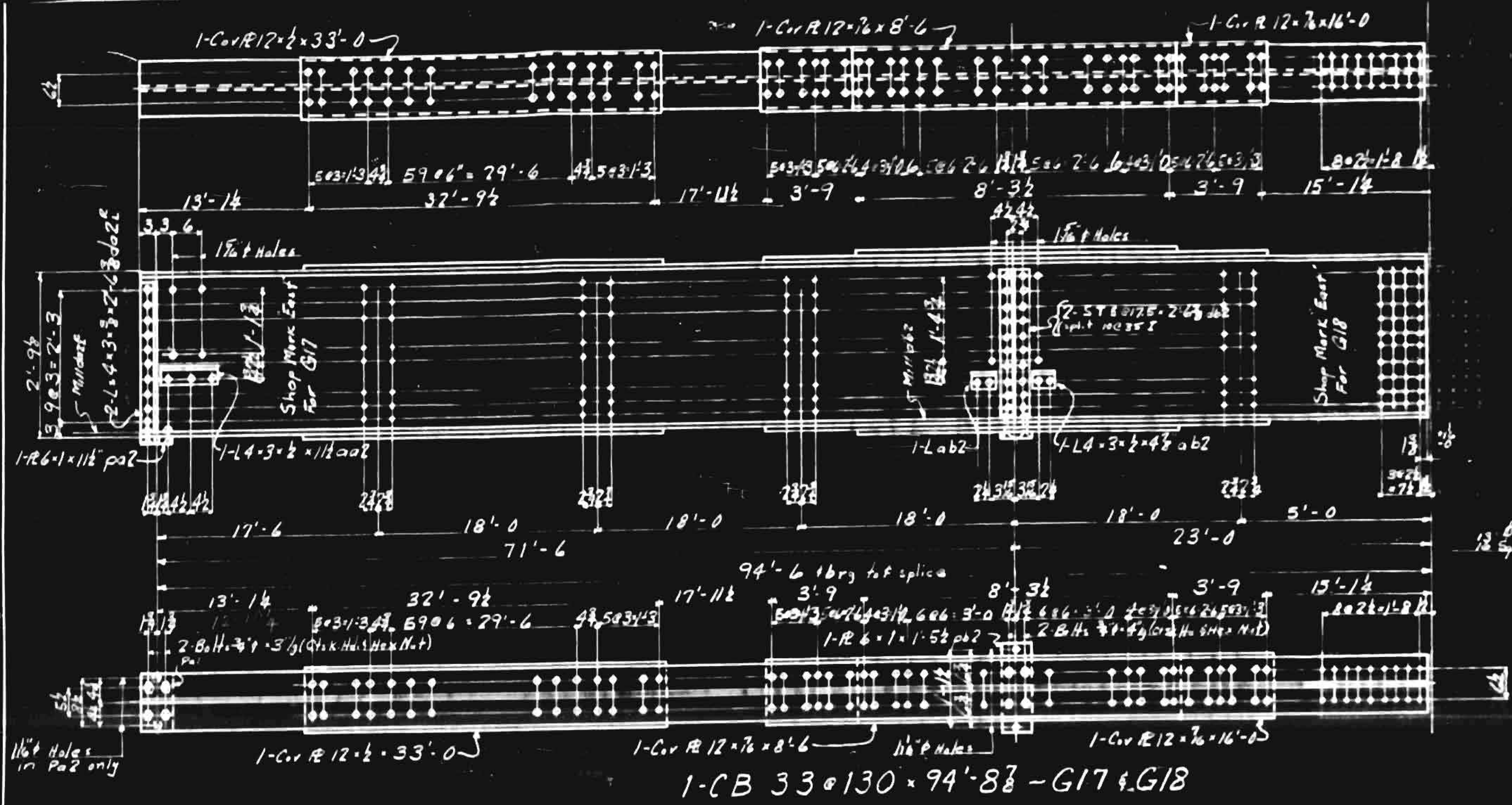
**CONTRACT V 7625**  
 Details of Girder Beams  
 on Yadkin River Bridge  
 Location: Foreyth, Yadkin Co., NC  
 Owner: Harvey M. Stewart - (Int'l.)  
**VIRGINIA BRIDGE COMPANY**  
 ROANOKE, VIRGINIA

To be fabricated at: **Roanoke**  
 Made by: **BM** 11-5-50  
 In charge of: **Suff**  
 Checked by: **SM** 11-27-50  
 Date: **1950**  
 Sheet No. **3**

APPROVED BY:

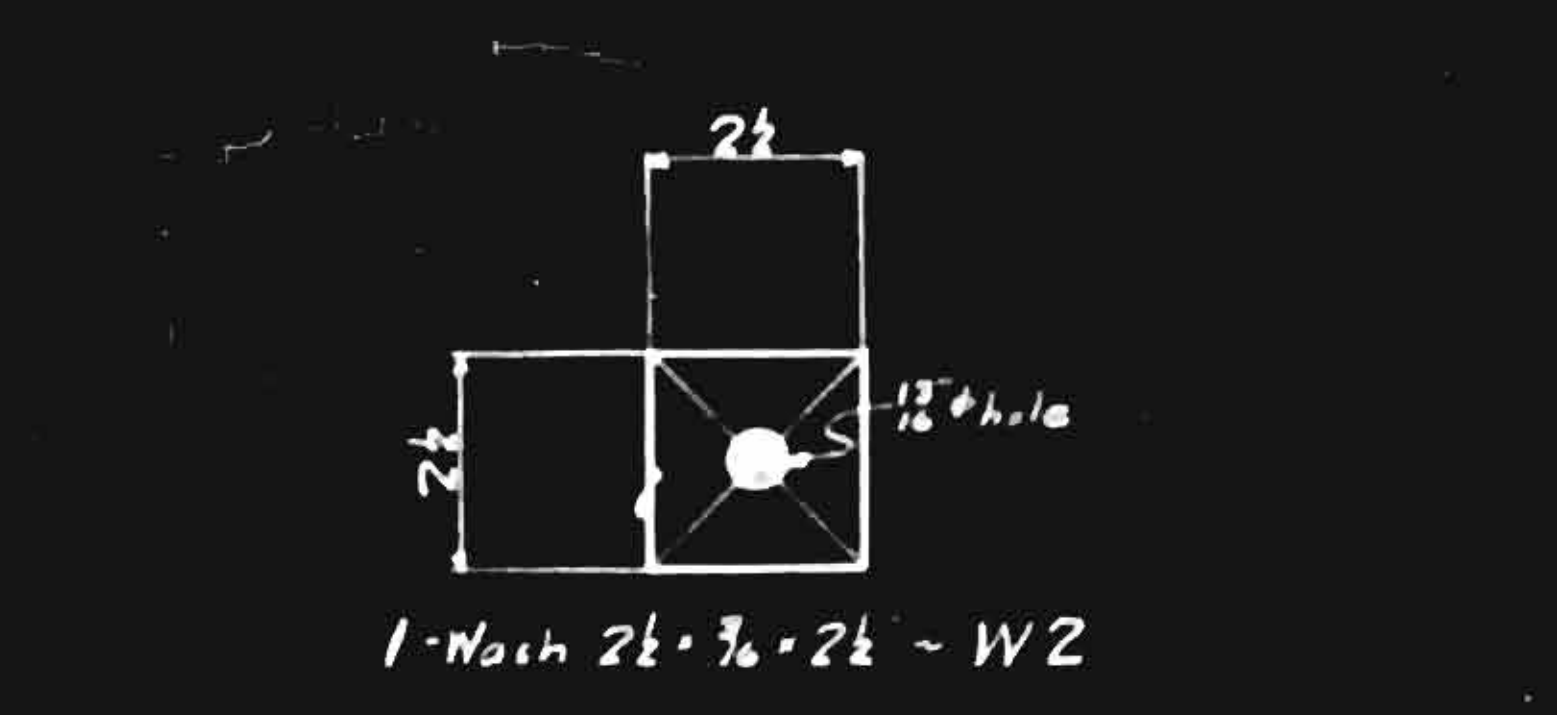
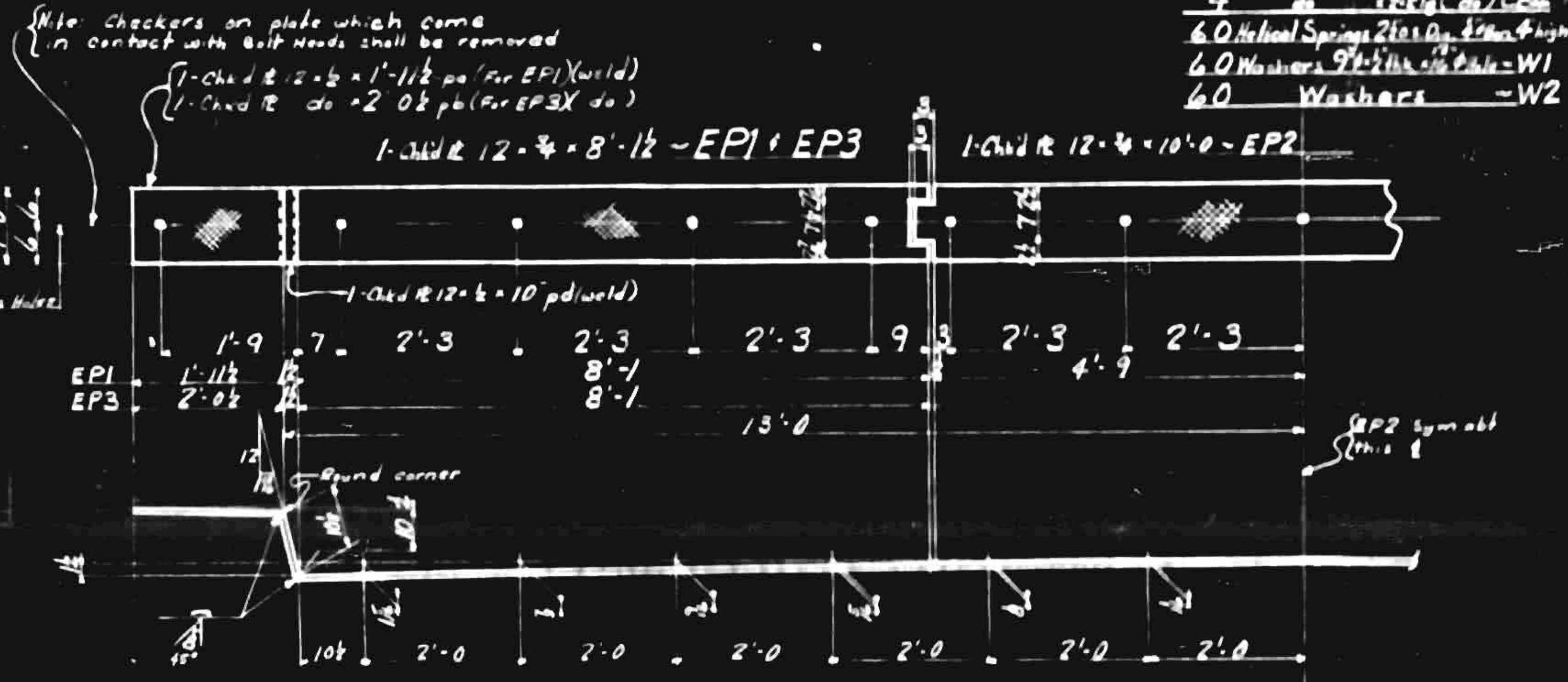

DATE: 11-5-50





**REQUIRED LIST**

MAKE	DESCRIPTION	MARK
2	Girders (Shop)	G17+G18
2	do	G19+G20
4	Expansion Plates	EP1
4	do	EP2
4	do	EP3
20	Coverage Plates 8" x 12" (20' x 12')	CP1
32	do 12" x 12" (16' x 16')	CP2
4	do 12" x 12" (16' x 16')	CP3
4	do 12" x 12" (16' x 16')	CP4
60	Helical Springs 2 1/2" Dia. 4" High	HS
60	Washers 9/16" Dia. 5/16" Thick	W1
60	Washers	W2



**Reaming Note:**  
 General reaming is not required.  
 All holes for Stringer field splices shall be sub-punched 1/8" and reamed to 1/8" and match-marked while a complete line of stringers is assembled in the shop, except that holes in material more than 3/4" in thickness shall be sub-drilled and reamed or drilled from the solid.

REVISION	
DATE	
Shop Date: 1/25/50 (Per 1946 AISC Aug. Spec)	
Shop Contact: Surface not Painted	
Inspector: Freshling & Robertson	
Maker:	
Special Order:	

<b>CONTRACT V 7625</b>	
Details of Girder Details, Expansion Plates	
For Yorklin Bridge	
Location: Frying Pan Creek, N.C.	
Owner: Harry H. Stewart (Contractor)	
<b>VIRGINIA BRIDGE COMPANY</b>	
ROANOKE, VIRGINIA	
To be fabricated at: <b>Roanoke</b> Made by: <b>BM</b> 11-15 In charge of: <b>Scott</b> Date: 1950	Checked by: <b>SM</b> 11-27-50 Date: 1950 Scale: <b>1" = 1'</b> Sheet No. <b>5</b> of <b>5</b>
