
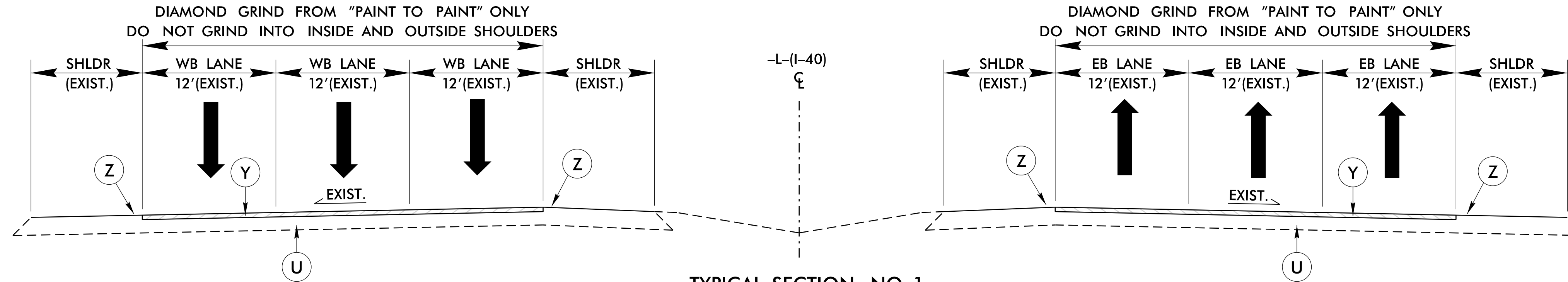


# PAVEMENT SCHEDULE

<b>B</b>	PROP. APPROX. 3/4" OPEN-GRADED ASPHALT FRICTION COURSE, TYPE FC-1 MODIFIED, AT AN AVERAGE RATE OF 90 LBS. PER SQ. YD.	<b>Q</b>	8" INCIDENTAL STONE BASE
<b>C1</b>	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.	<b>R</b>	VAR. DEPTH REPAIR OF JOINTED CONCRETE PAVEMENT SLABS (MATCH DEPTH TO ADJACENT CONCRETE PAVING)
<b>D1</b>	PROP. APPROX. 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.	<b>T</b>	EXISTING EARTH MATERIAL
<b>E1</b>	PROP. APPROX. 5.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.	<b>U</b>	EXISTING PAVEMENT
<b>N</b>	GEOTEXTILE FOR SOIL STABILIZATION	<b>Y</b>	DIAMOND GRINDING
		<b>Z</b>	MILLED CONCRETE RUMBLE STRIPS (FOR SHOULDER LOCATIONS WITH PCCP SLAB REPAIR)

NOTE: PAV. EDGES ARE 1:1 UNLESS SHOWN OTHERWISE.

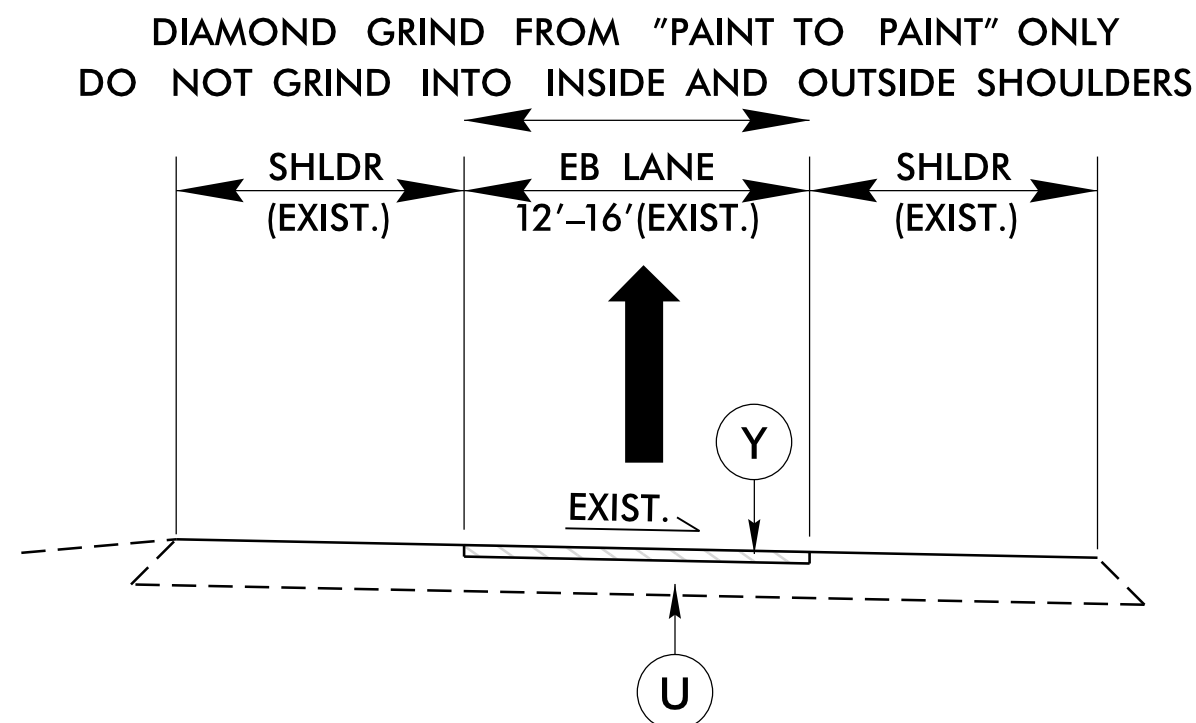
PROJECT REFERENCE NO. 1-5795	SHEET NO. 2A-1
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	
	
Documented by: <u>Scott Jones</u> 8/11/2020 DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	



**TYPICAL SECTION NO. 1**

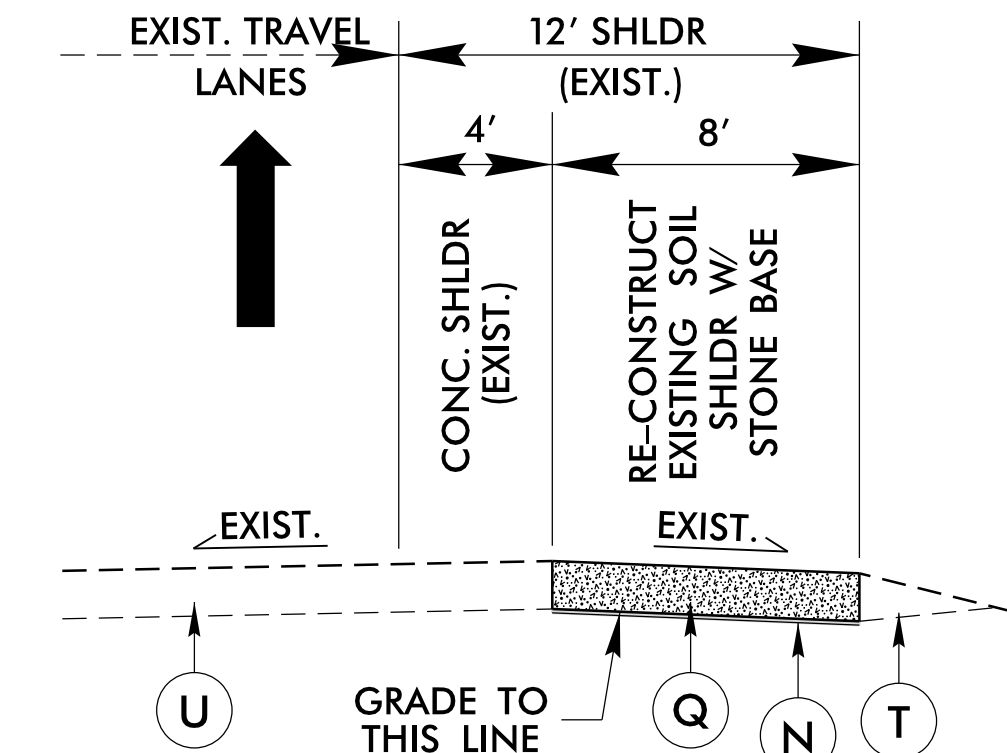
- L- M.M. 188.74 TO M.M. 192.79
- L- M.M. 195.90 TO M.M. 197.59
- L- M.M. 200.66 TO 204.77

NOTE: DO NOT DISTURB EXISTING RUMBLE STRIPS, FEATHER DIAMOND GRINDING ALONG EDGE LINES TO DRAIN



**TYPICAL SECTION NO. 2**

NOTE:  
-DO NOT DISTURB EXISTING RUMBLE STRIPS



**TYPICAL SECTION NO. 3**

NOTES:  
-USE IN CONJUNCTION WITH TYPICAL SECTION #2  
-DO NOT DISTURB EXISTING SHOULDER DRAINS

CONCRETE RAMPS TO AND FROM:  
 STRATFORD ROAD (2 RAMPS + 2 LOOPS, INCLUDES -RPB1- & -RPC1-)  
 HANES MALL BLVD (2 RAMPS, INCLUDES -RPA1- & -RPD1-)  
 PETERS CREEK PARKWAY (3 RAMPS, 1 LOOP, INCLUDES -RPA2-, -RPB2-, & -RPC2)  
 SILAS CREEK PARKWAY (1 RAMP)  
 UNION CROSS ROAD (2 RAMPS, INCLUDES -RPA5- & -RPD5)  
 NC-66 (4 RAMPS, INCLUDES -RPB6-, -RPC6-, & -RPD6-)  
 US 74311 NB ramp to I-40 EB  
 I-40 WB to US 74311 SB ramp

-RPB1- (WB RAMP FROM STRATFORD RD)  
1+34 +/- to 6+62 +/- (RT) -PSH5

-RPC1- (EB RAMP TO STRATFORD RD)  
8+50 +/- to 13+24 +/- (RT) -PSH5

-RPD1- (EB RAMP FROM HANES MALL BLVD)  
1+14 +/- (RT) to 5+11 +/- (RT) -PSH6

-L12- (I-40)- 605+00 +/- (RT) to RPC6- (EB RAMP TO NC HWY66) 4+90 +/- (RT) -PSH9

-RPC6- (EB RAMP TO NC HWY66)  
7+40 +/- (RT) to 14+15 +/- (RT) -PSH9

-RPB6- (WB RAMP FROM NC-66)  
8+42 +/- (RT) to 12+50 +/- (RT) -PSH9

-RPB6- (WB RAMP TO NC HWY66)  
0+50 +/- (LT) to 12+74 +/- (LT) -PSH9

-RPD6- (EB RAMP FROM NC HWY66)  
4+33 +/- (RT) to 14+30 +/- (RT) -PSH10

-RPD6- (EB RAMP FROM NC HWY66)  
6+44 +/- (LT) to 13+48 +/- (LT) -PSH10

-RPD5- (EB RAMP FROM UNION CROSS RD)  
5+30 +/- (RT) to 13+83 +/- (RT) -PSH8

-RPA5- (WB RAMP TO UNION CROSS RD)  
5+08 +/- (LT) to 11+00 +/- (LT) -PSH8

REVISIONS

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 5:41 PM 2021 Development\TIP\_P\Projects\_1\1-5795-40-Forsyth\Roadway\DesignFiles\1-5795\_ddc\_2A-1.dgn  
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