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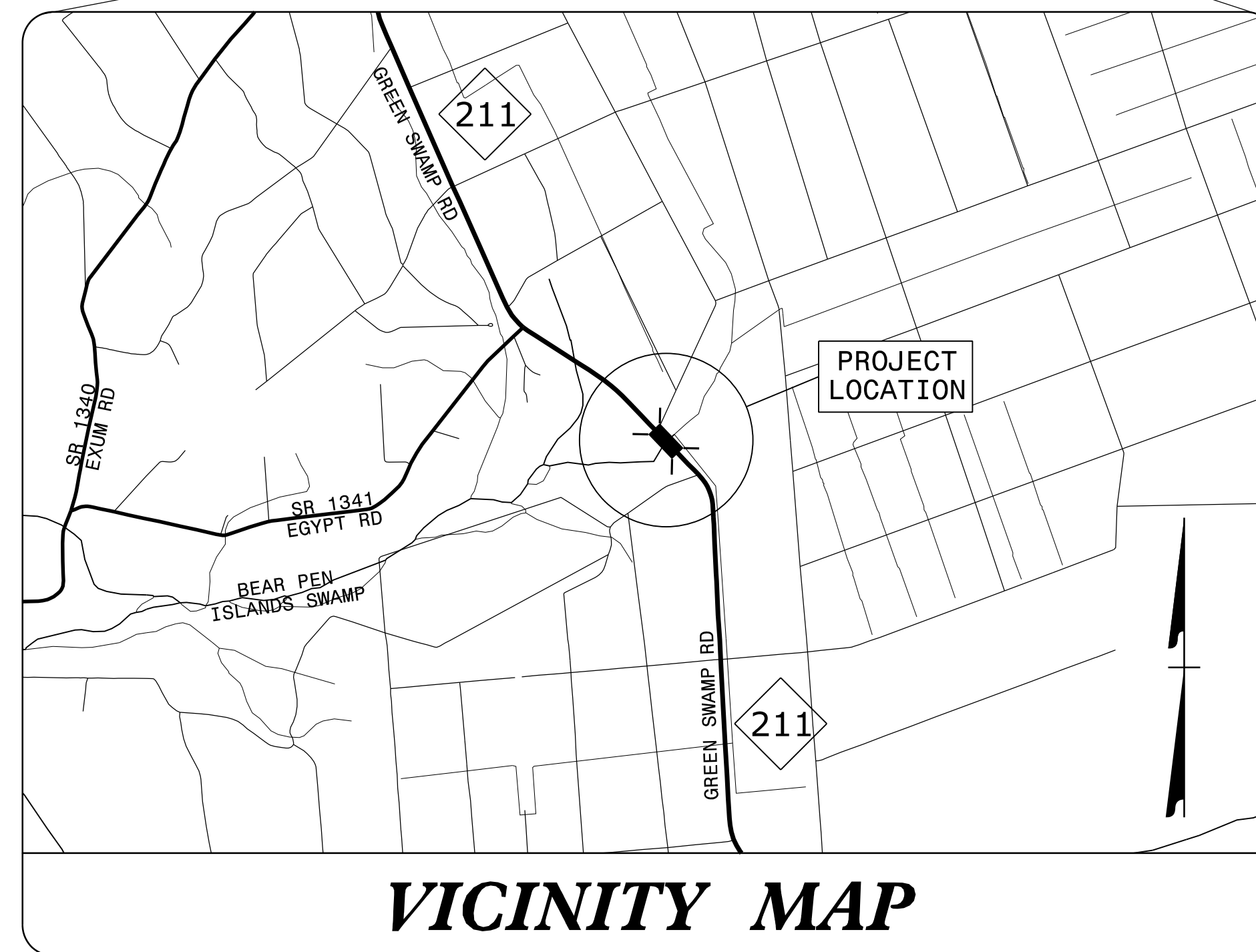
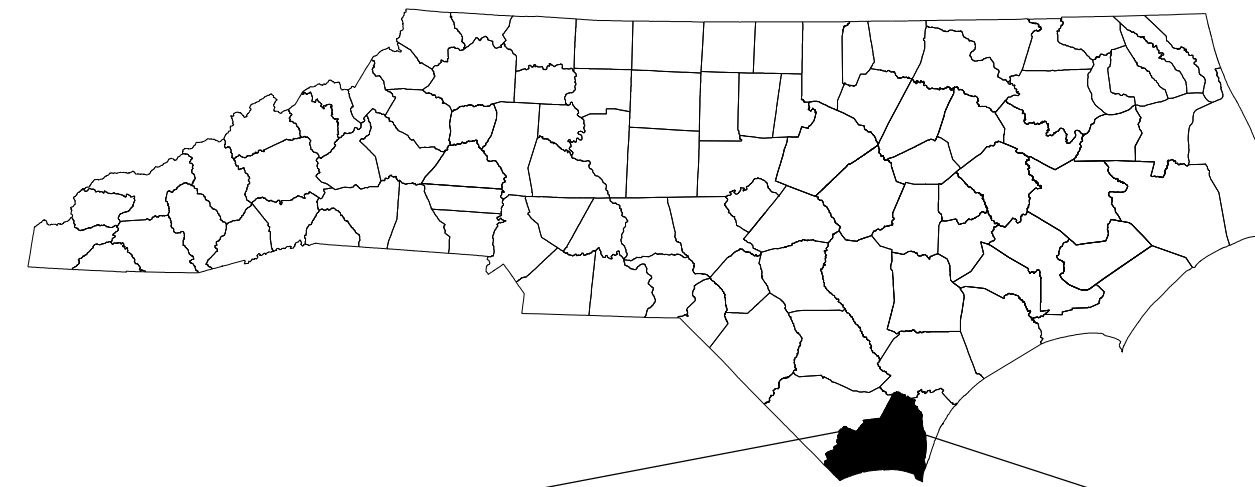
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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

BRUNSWICK COUNTY



**LOCATION: BRIDGE NO. 47 ON NC 211 (GREEN SWAMP RD)
OVER BEAR PEN ISLANDS SWAMP**

**TYPE OF WORK: GRADING, DRAINAGE, PAVING, AND
STRUCTURE**

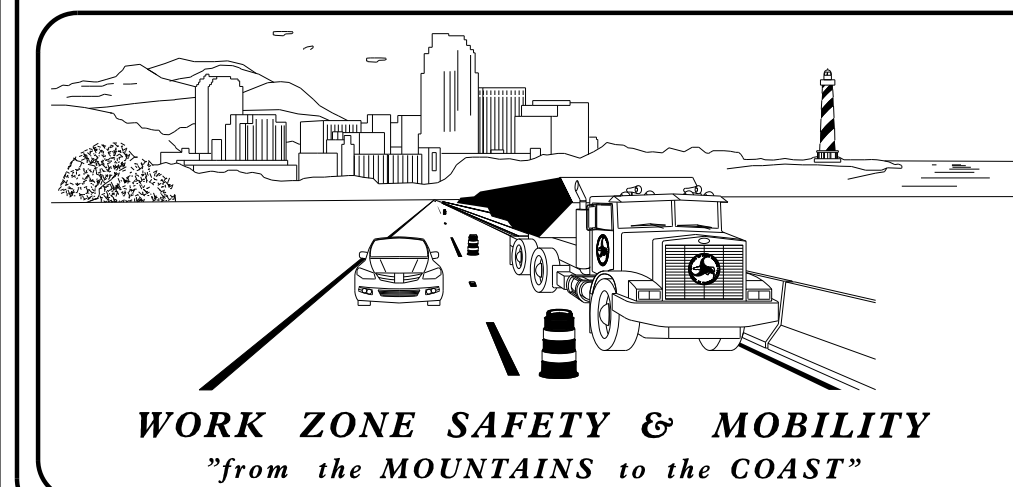
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TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-2	GENERAL NOTES, AND LOCAL NOTES
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SHEET NO.
TMP-1

B-4438

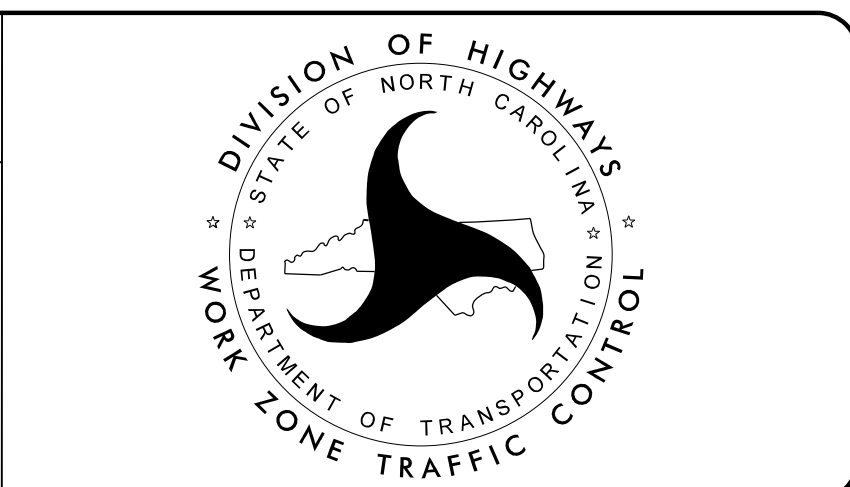
TIP PROJECT:

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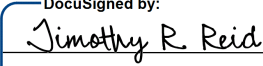


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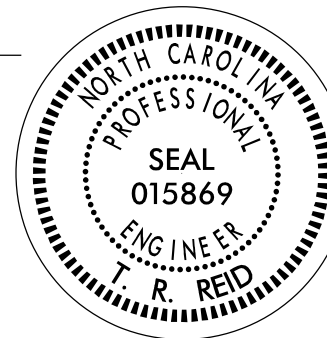
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DATE: 2/22/2019

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ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - CONTRACT STANDARDS AND DEVELOPMENT UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

<u>STD. NO.</u>	<u>TITLE</u>
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	POSITIVE PROTECTION
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTI-LANE ROADWAYS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

TRAFFIC MANAGEMENT STRATEGY

THIS PROJECT WILL UTILIZE THE EXISTING ROADWAY AND BRIDGE TO CONSTRUCT MOST OF THE NEW ROADWAY AND BRIDGE AWAY FROM TRAFFIC. FLAGGING WILL BE USED TO CONSTRUCT TIE-INS.

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.
- TEMP. SHORING (LOCATION PURPOSES ONLY)

- WORK AREA
- REMOVAL

SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM
- SKINNY DRUM
- TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

- PA WHITE EDGELINE (PAINT 4")
- PI YELLOW DOUBLE CENTER (PAINT 4")

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 DATE: 2/22/2019

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DIVISION OF HIGHWAYS
 STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 WORK ZONE TRAFFIC CONTROL

**ROADWAY STANDARD
 DRAWINGS & LEGEND**

GENERAL NOTES / LOCAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
NC 211 (-L-)	MONDAY THRU FRIDAY 7:00 A.M. - 9:00 A.M. 4:00 P.M. - 6:00 P.M.

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
NC 211 (-L-)

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S DAY BETWEEN THE HOURS OF 7:00 A.M. DECEMBER 31st AND 6:00 P.M. JANUARY 2nd. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN UNTIL 6:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 7:00 A.M. THURSDAY TO 6:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN BETWEEN THE HOURS OF 7:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 7:00 A.M. TUESDAY TO 6:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS DAY.

C) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
NC 211 (-L-)	MONDAY THRU FRIDAY 7:00 A.M. - 9:00 A.M.	15 MINUTES FOR TRAFFIC SHIFTS, PAVEMENT TIE-INS, PAVEMENT MARKING TIE-INS

LANE AND SHOULDER CLOSURE REQUIREMENTS

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

J) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

PAVEMENT MARKINGS AND MARKERS

N) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
NC 211 (-L-)	PAINT	TEMPORARY RAISED

O) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

P) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

Q) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

TEMPORARY SHORING

SHORING LOCATION NO. 1

R) FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

S) BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

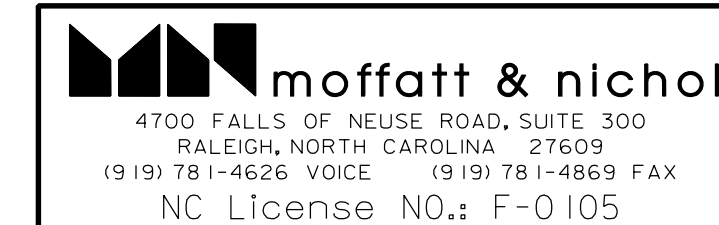
T) DESIGN TEMPORARY SHORING FROM STATION -L- 25+60 ±, 20.0 FT ± RIGHT, TO STATION -L- 31+80 ±, 12.0 FT ± LEFT, FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUND WATER ELEVATION:

UNIT WEIGHT (γ) = 120 LB/CF
FRICTION ANGLE (φ) = 30 DEGREES
COHESION (c) = 0 LB/SF
GROUNDWATER ELEVATION = 58 FT ±

U) DO NOT USE A TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION -L- 25+60 ±, 20.0 FT ± RIGHT, TO STATION -L- 31+80 ±, 12.0 FT LEFT.

V) AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION -L- 25+60 ±, 20.0 FT ±RIGHT, TO STATION -L- 31+80 ±, 12.0 FT ± LEFT. SEE STANDARD DETAIL NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

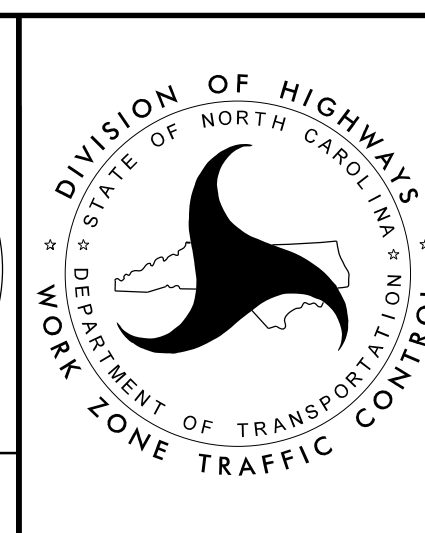
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GENERAL NOTES AND LOCAL NOTES

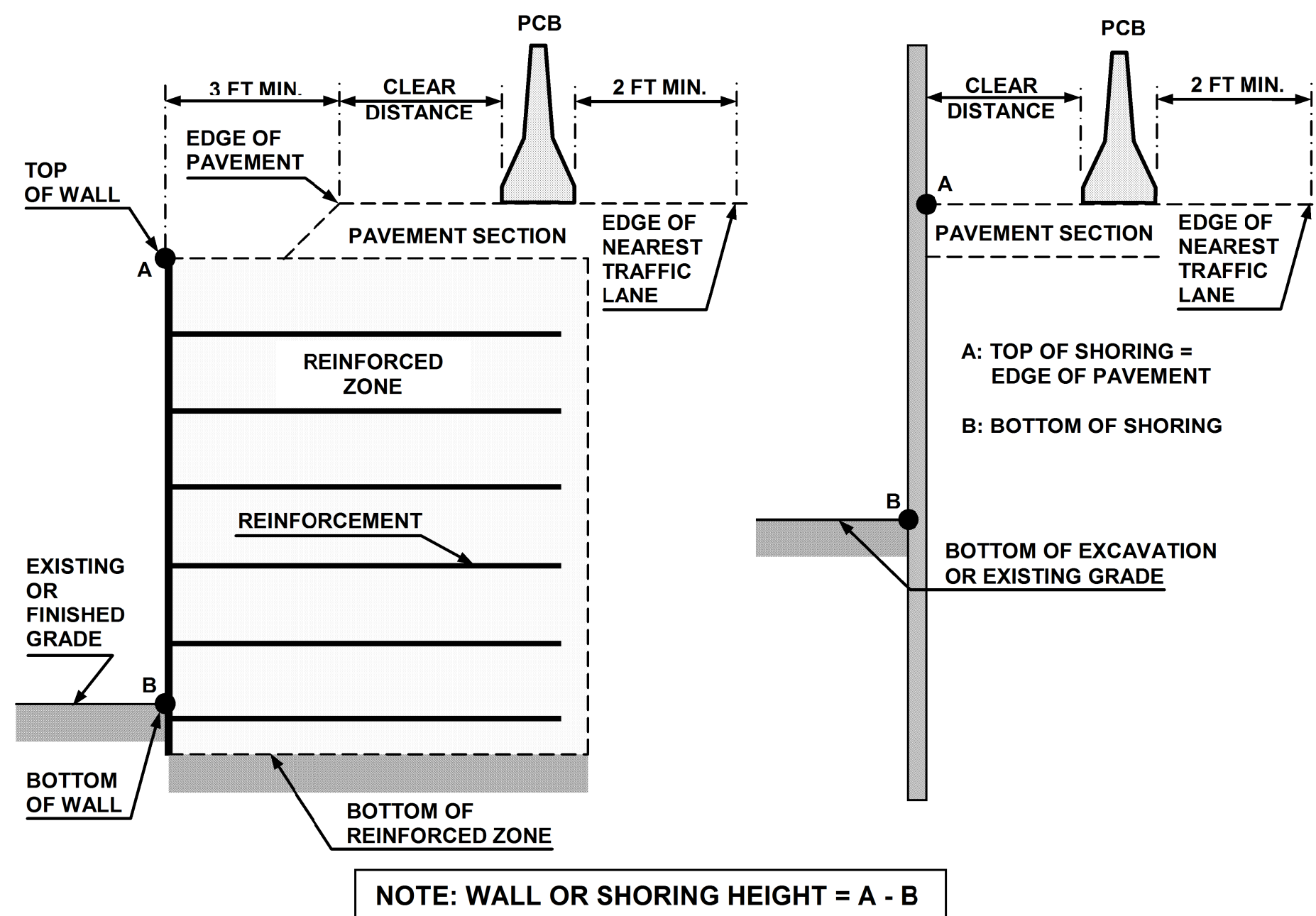


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" PROJECT SPECIAL PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT UNIT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- PCB REQUIREMENTS FOR TEMPORARY WALLS APPLY TO TEMPORARY MECHANICALLY STABILIZED EARTH (MSE) WALLS AND TEMPORARY SOIL NAIL WALLS.
- 8- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS AND OR AS APPROVED BY THE ENGINEER.
- 9- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THESE MINIMUM REQUIRED DISTANCES ARE NOT AVAILABLE, CONTACT THE ENGINEER.
- 10- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS. BARRIER DEFLECTIONS AND RESULTING MINIMUM REQUIRED CLEAR DISTANCES MIGHT VARY SIGNIFICANTLY FOR LARGER HEAVIER VEHICLES, RUNS OF BARRIER LESS THAN 200 FT IN LENGTH AND WET OR DRY PAVEMENT.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
	44-50	31	35	41	43	46	49	
	50-56	32	36	42	44	47	50	
	>56	32	36	42	45	47	51	
	Concrete	<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
26-32		24	25	27	28	32	35	
32-38		24	26	27	30	33	36	
38-44		25	26	28	30	34	37	
44-50	26	26	28	32	35	37		
50-56	26	26	28	32	35	38		
>56	26	27	29	32	36	38		
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
Anchored PCB	Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds					

* See Figure Below

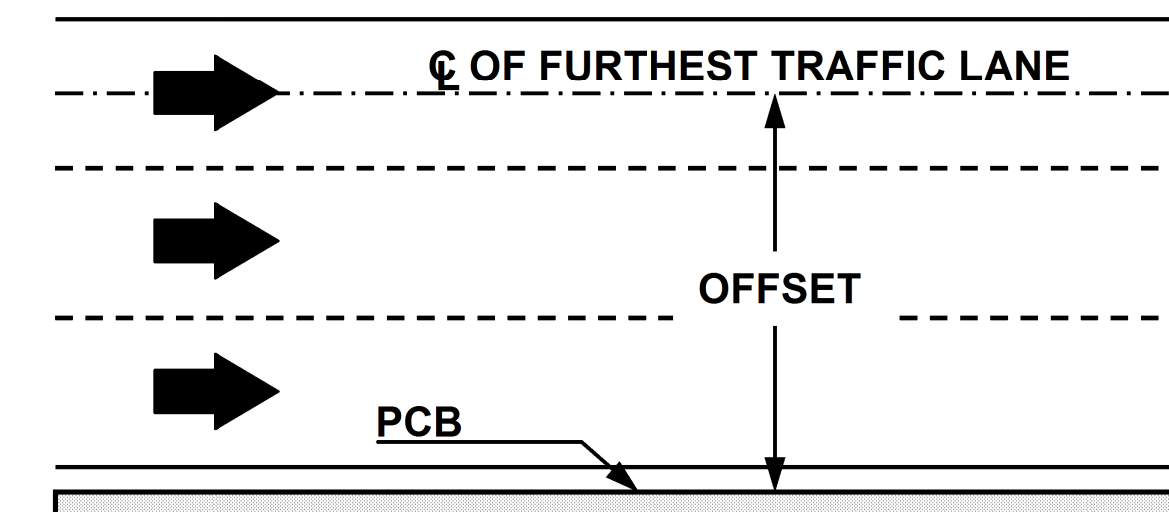
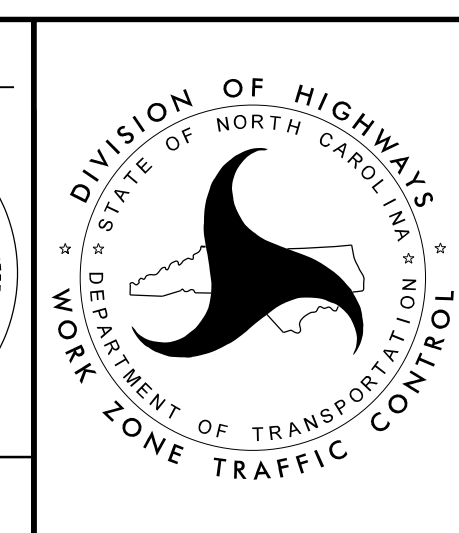


FIGURE B

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PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS

PROJ. REFERENCE NO.	SHEET NO.
B-4438	TMP-3

PHASING

PHASE I

NOTE: TRAFFIC AND EXISTING LITTLE LOOP ROAD ACCESS SHALL BE MAINTAINED ON EXISTING NC 211 FOR THIS PHASE.

STEP 1

PRIOR TO BEGINNING CONSTRUCTION, INSTALL WORK ZONE ADVANCE WARNING SIGNS USING ROADWAY STANDARD DRAWING 1101.01 SHEET 3 OF 3 ALONG EXISTING NC 211 AND LITTLE LOOP ROAD.

STEP 2

USING FLAGGING OPERATION PER ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14, INSTALL TEMPORARY PRECAST CONCRETE BARRIER AND TEMPORARY CRASH CUSHION ALONG THE EXISTING ROADWAY (SEE TMP-4)

- STA. 25+52 +/- -L- TO STA. 33+30 +/- -L-

STEP 3

INSTALL TEMPORARY SHORING AS SHOWN ON TMP-4

- STA. 25+60 +/- -L- TO STA. 31+80 +/- -L-

STEP 4

EXCAVATE UNDERCUT MATERIAL AND INSTALL ROAD EMBANKMENT UP TO SUBGRADE LINE

- STA. 23+75 +/- -L- TO STA. 24+90 +/- -L-
 - STA. 25+60 +/- -L- TP STA. 31+80 +/- -L-

PHASE II

NOTE: TRAFFIC AND EXISTING LITTLE LOOP ROAD ACCESS SHALL BE MAINTAINED ON EXISTING NC 211 FOR THIS PHASE.

STEP 1

SHIFT TEMPORARY PRECAST CONCRETE BARRIER PLACED UNDER PHASE I, INSTALL A NEW SECTION OF TEMPORARY PRECAST CONCRETE BARRIER AND ADD A TEMPORARY CRASH CUSHION

-SHIFT ENDING OF PCB AND CRASH CUSHION FROM STA. 33+30 +/- TO STA. 31+00 (SEE TMP-4 & TMP-5)

-ADD NEW SECTION OF PCB AND CRASH CUSHION STA. 22+50 +/- TO STA. 25+00 (TMP-5)

STEP 2

AWAY FROM TRAFFIC AND USING ROADWAY STANDARD DRAWINGS 1101.02(SHEET 1 OF 14)AND 1101.04 AS NEEDED, CONSTRUCT THE PROPOSED BRIDGE AND ROADWAY. (SEE TMP-5)

- STA. 23+00 +/- -L- TO STA. 28+50 +/- -L-

STEP 3

INSTALL TEMPORARY PAVEMENT MARKINGS AND MARKERS IN THE FINAL TRAFFIC PATTERN ON THE REALIGNED NC 211 AS FOLLOWS, (SEE TMP-5)

- STA. 23+00 +/- -L- TO STA. 28+50 +/- -L-

PHASE III

NOTE: TRAFFIC AND EXISTING LITTLE LOOP ROAD ACCESS SHALL BE MAINTAINED ON EXISTING NC 211 FOR THIS PHASE.

STEP 1

SHIFT TEMPORARY PRECAST CONCRETE BARRIER PLACED UNDER PHASE II AS FOLLOWS:

-SHIFT BEGINNING OF PCB AND CRASH CUSHION FROM STA. 22+50 +/- TO STA. 23+00 (SEE TMP-5 & TMP-6)

-SHIFT ENDING OF PCB AND CRASH CUSHION FROM STA. 31+00 TO STA. 28+20 +/- (SEE TMP-5 & TMP-7)

STEP 2

INSTALL TRAFFIC CONTROL DEVICES AND AWAY FROM TRAFFIC, USING ROADWAY STANDARD DRAWINGS 1101.02 (SHEET 1 OF 14) AND 1104.04 AS NEEDED, CONSTRUCT THE PROPOSED ROADWAY UP TO EXISTING EDGE OF PAVEMENT ELEVATIONS AND CONSTRUCT -Y1- (SEE TMP-6 AND TMP-7). ONLY THE BASE AND INTERMEDIATE ROADWAY COURSES ARE TO BE CONSTRUCTED.

- STA. 14+70 +/- -L- TO STA. 23+00 +/- -L-
 - STA. 28+50 +/- -L- TO STA. 36+00 +/- -L-

STEP 3

USING FLAGGING OPERATION PER ROADWAY STANDARD DRAWING 1102.02 SHEET 1 OF 14, CONSTRUCT TEMPORARY WEDGING BETWEEN NEWLY CONSTRUCTED PAVEMENT AND EXISTING PAVEMENT.

- STA. 14+70 +/- -L- TO STA. 23+00 +/- -L-
 - STA. 28+50 +/- -L- TO STA. 36+00 +/- -L-

STEP 4

INSTALL TEMPORARY PAVEMENT MARKINGS AND MARKERS IN THE FINAL TRAFFIC PATTERN ON THE REALIGNED NC 211 AS FOLLOWS, (SEE TMP-5 AND TMP-6)

- STA. 14+70 TO STA 36+00 -L-

PHASE IV

NOTE: TRAFFIC SHALL BE SHIFTED AND MAINTAINED ON THE NEWLY CONSTRUCTED NC 211 FOR THIS PHASE.

STEP 1

INSTALL TRAFFIC CONTROL DEVICES AND SHIFT TRAFFIC TO NEWLY CONSTRUCTED NC 211 AS SHOWN ON SHEETS TMP-8 AND TMP-9

- STA. 14+70 +/- -L- TO STA. 36+00 +/- -L-

STEP 2

USING FLAGGING OPERATION PER ROADWAY STANDARD DRAWING 1101.02 SHEET 1 OF 14, CONSTRUCT FINAL LAYER OF SURFACE COURSE AND FINAL PAVEMENT MARKINGS.

- STA. 14+70 +/- -L- TO STA. 36+00 +/- -L-

STEP 3

REMOVE THE EXISTING BRIDGE, ROADWAY AND GRADE THE OLD ROADBED TO DRAIN AS SHOWN ON SHEETS TMP-8 AND TMP-9.

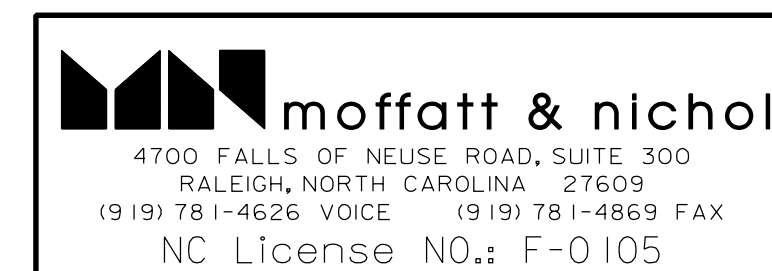
- STA. 14+70 +/- -L- TO STA. 36+00 +/- -L-

STEP 4

REMOVE ANY REMAINING TRAFFIC CONTROL SIGNS AND DEVICES.

-STA 14+70 +/- -L- TO STA. 36+00 +/- -L-

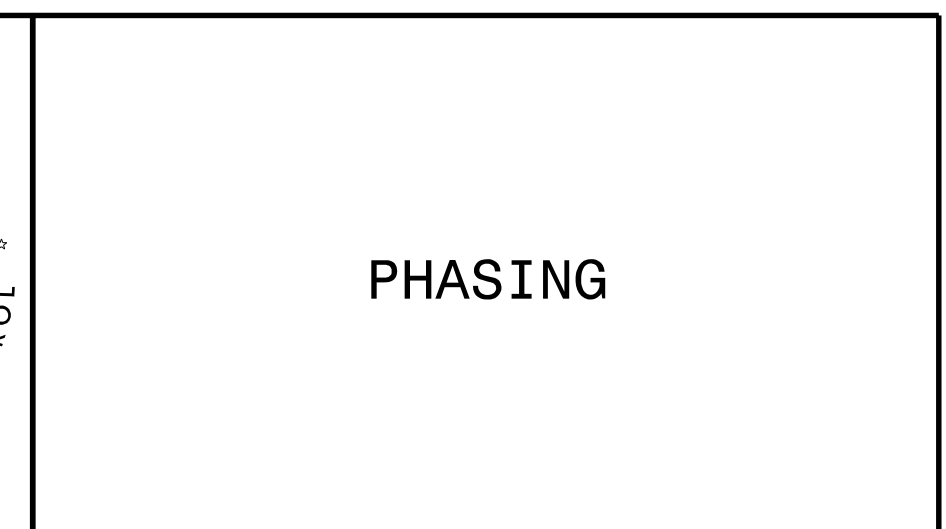
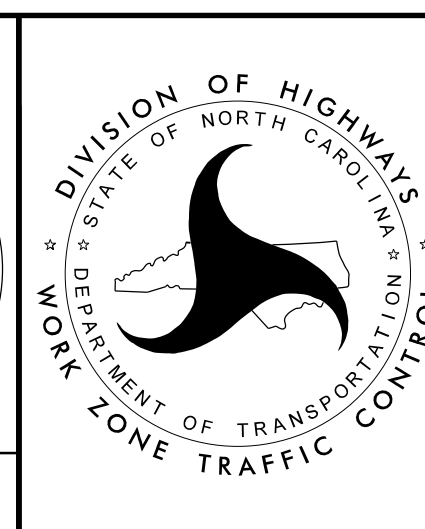
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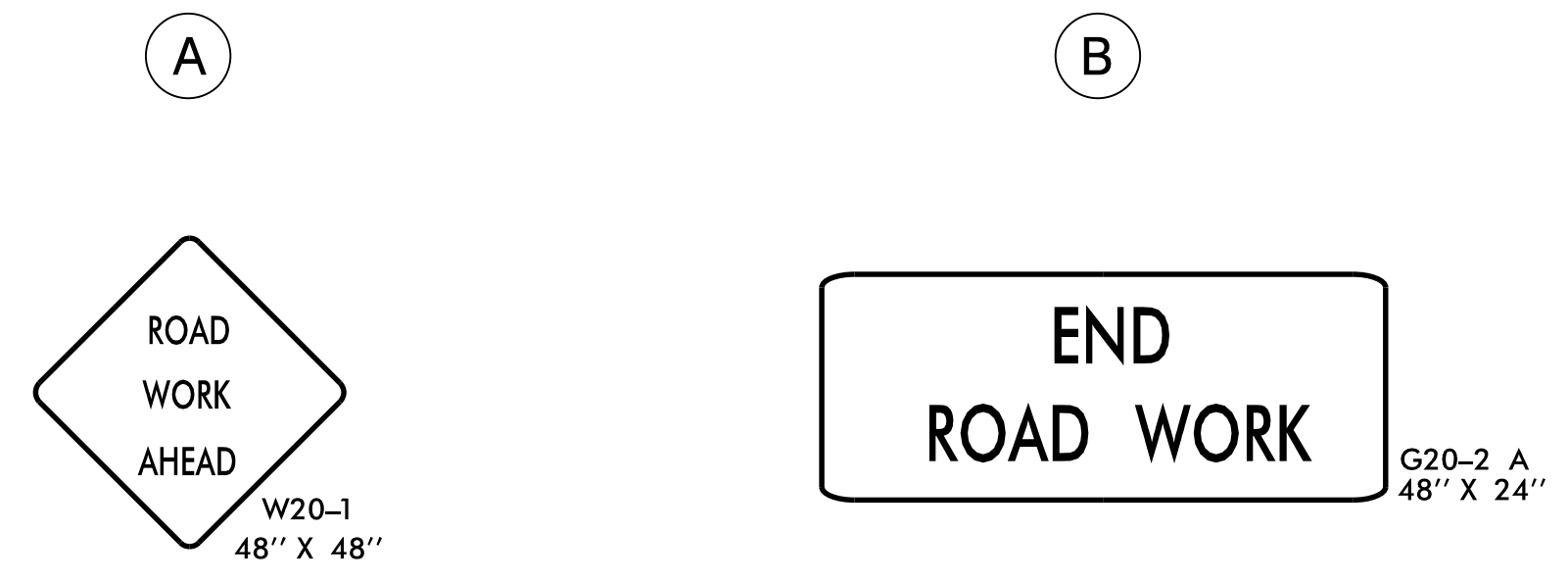


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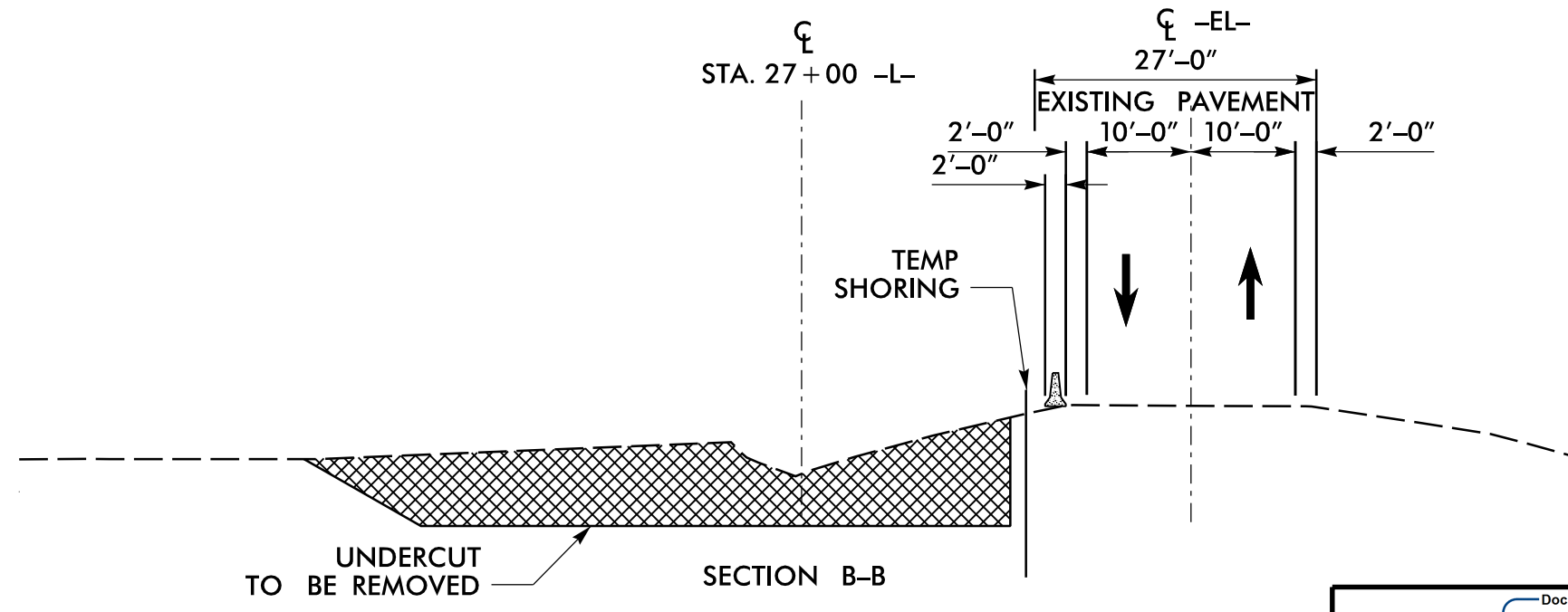
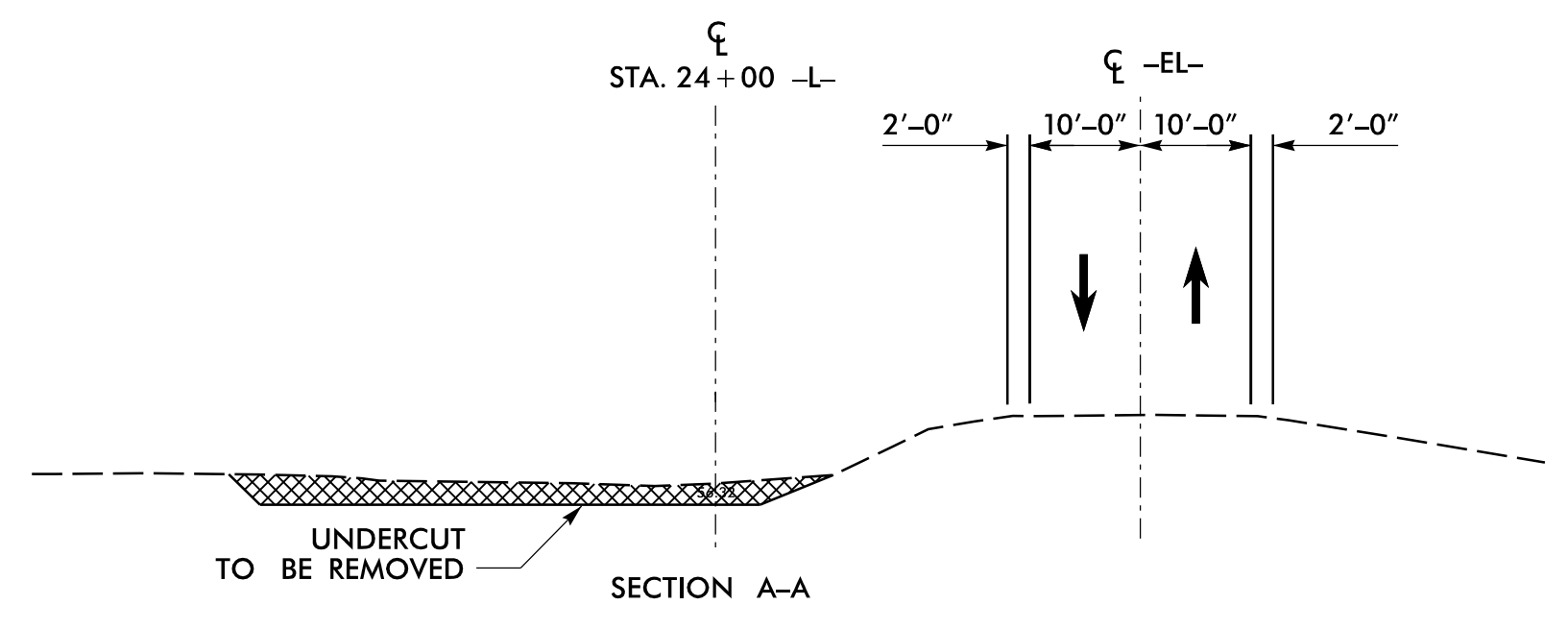
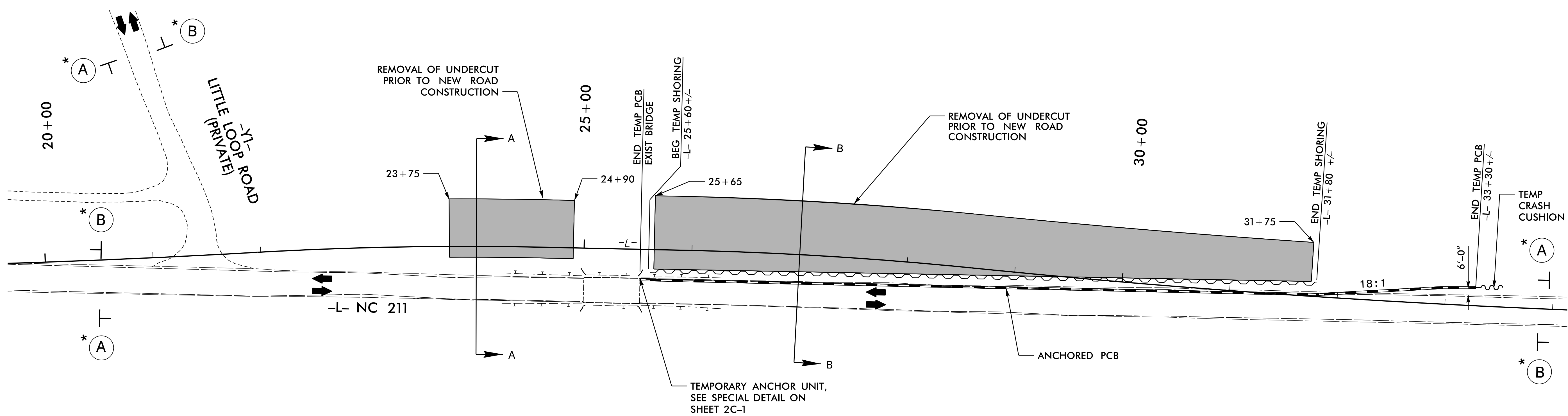
PHASE I

TEMP. SHORING NOTE:

SHORING LOCATION NO.1	BEGIN STATION & OFFSET	END STATION & OFFSET	ESTIMATED AVERAGE HEIGHT	ESTIMATED MAXIMUM HEIGHT	SHORING LOCATION TYPE	SHORING QUANTITY
NO.1	-L- 25+60 +/-, 20' +/- RT	-L- 31+80 +/-, 12' +/- LT	10.0 FT	12.0 FT	ROADWAY	6200 SF



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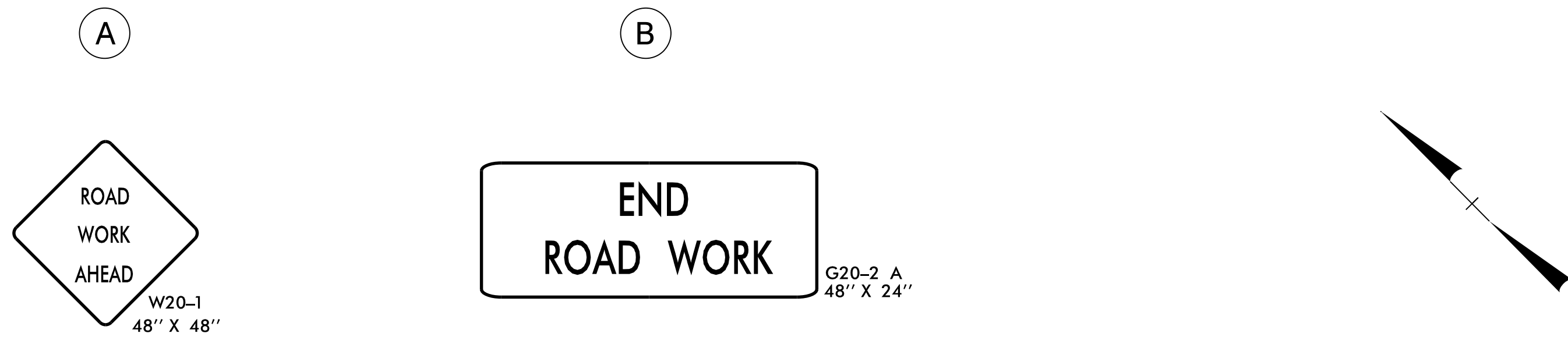
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 PROFESSIONAL ENGINEER
 T. R. REID
 015869
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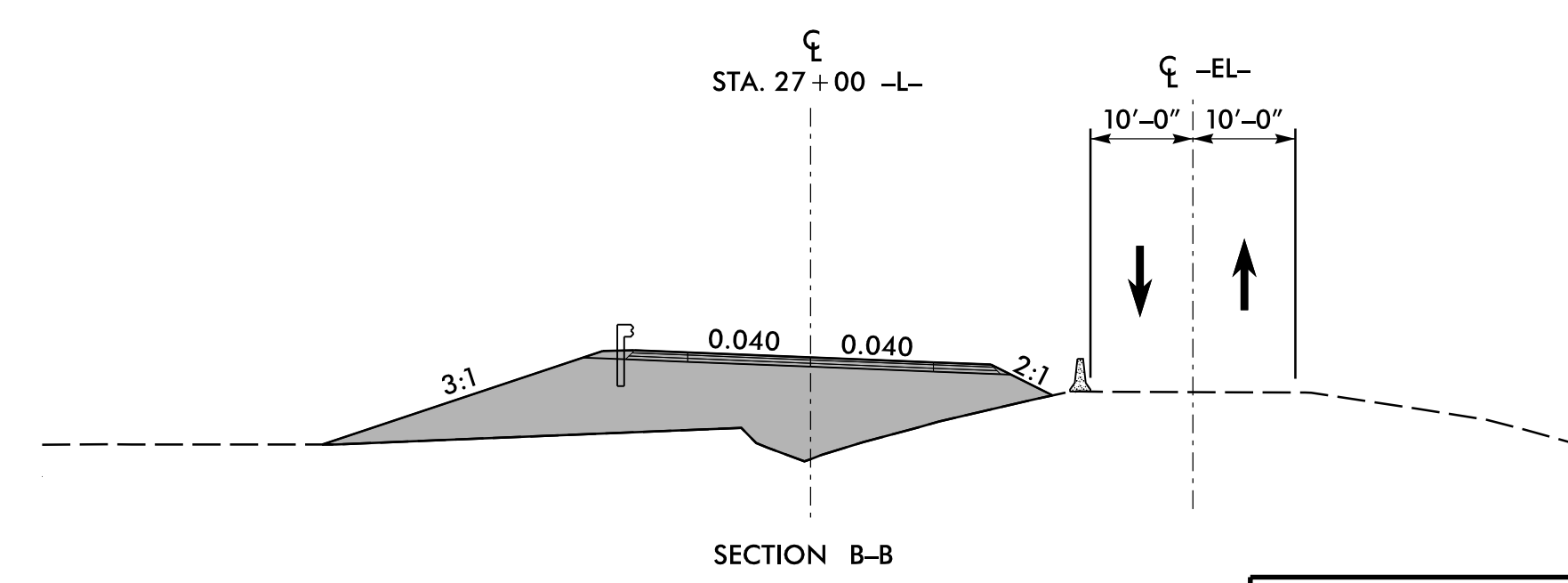
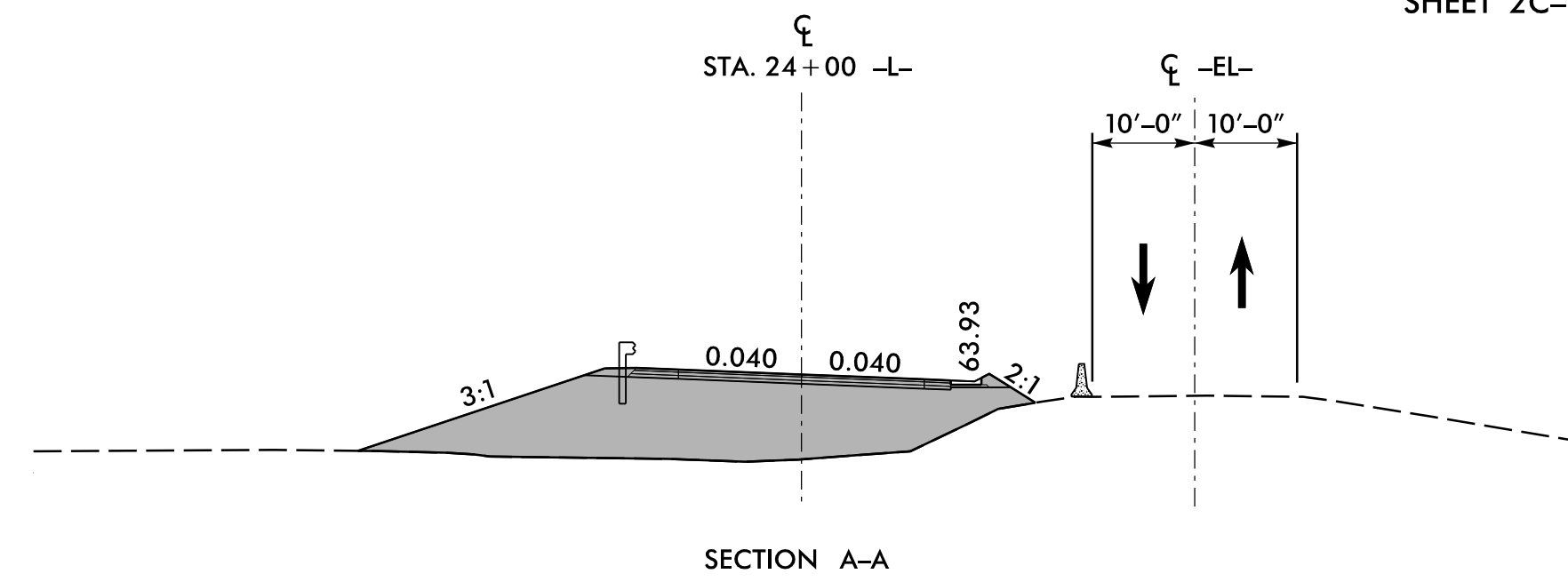
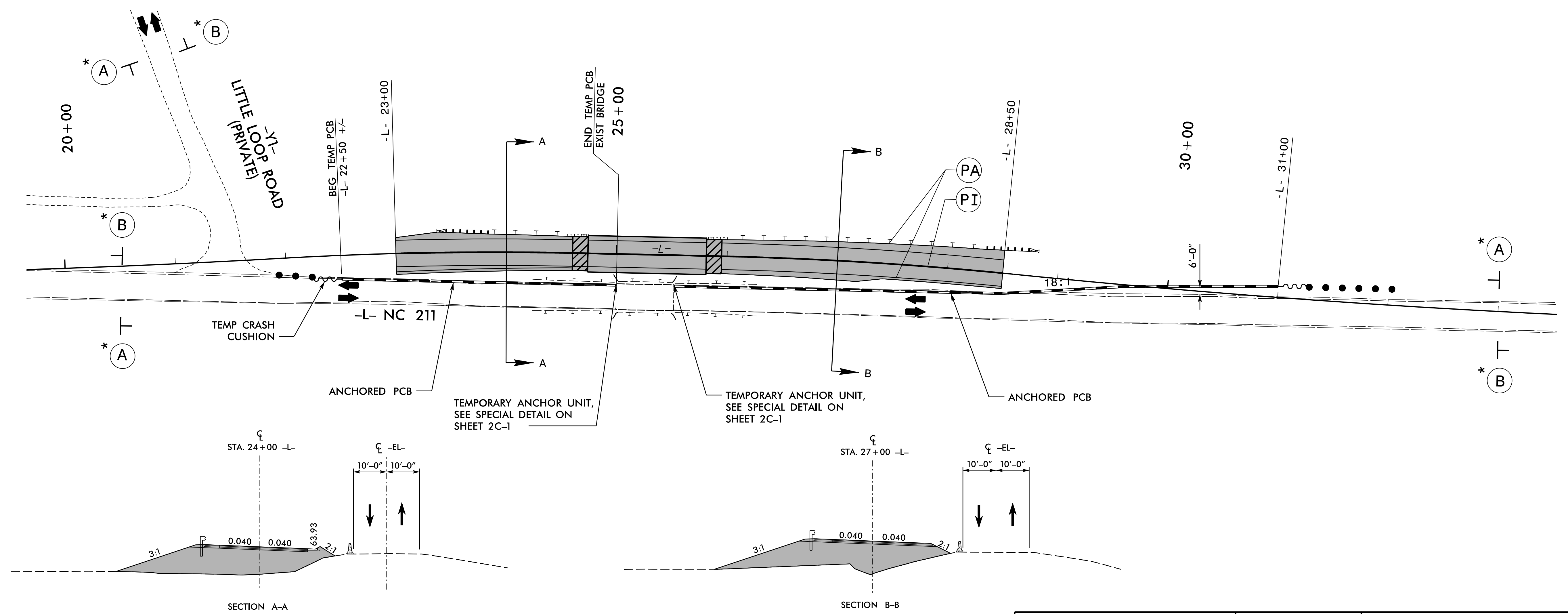
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PHASE II



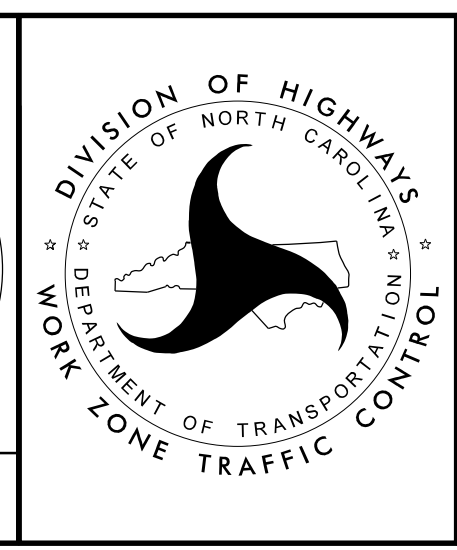
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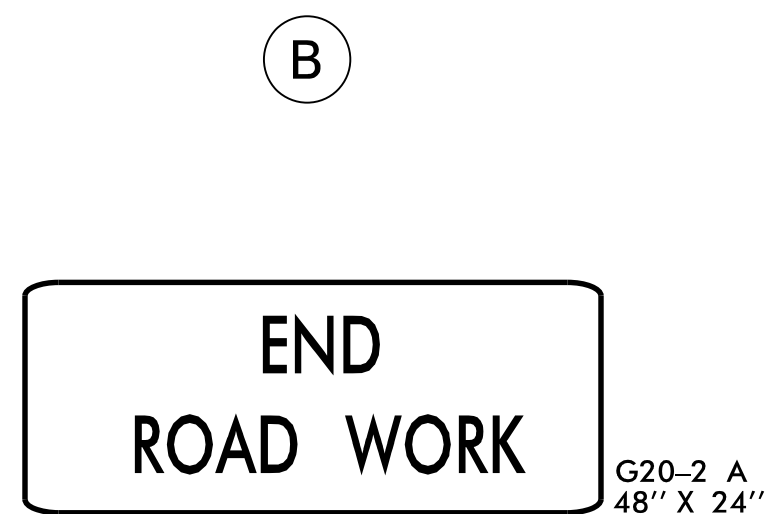
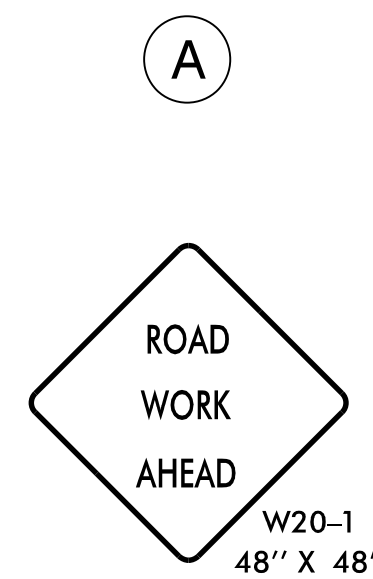
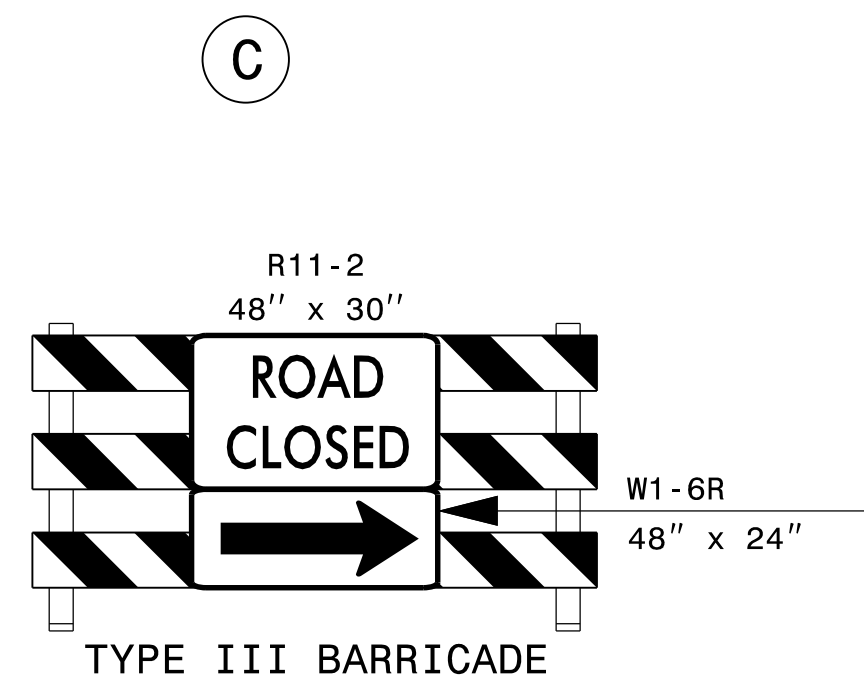
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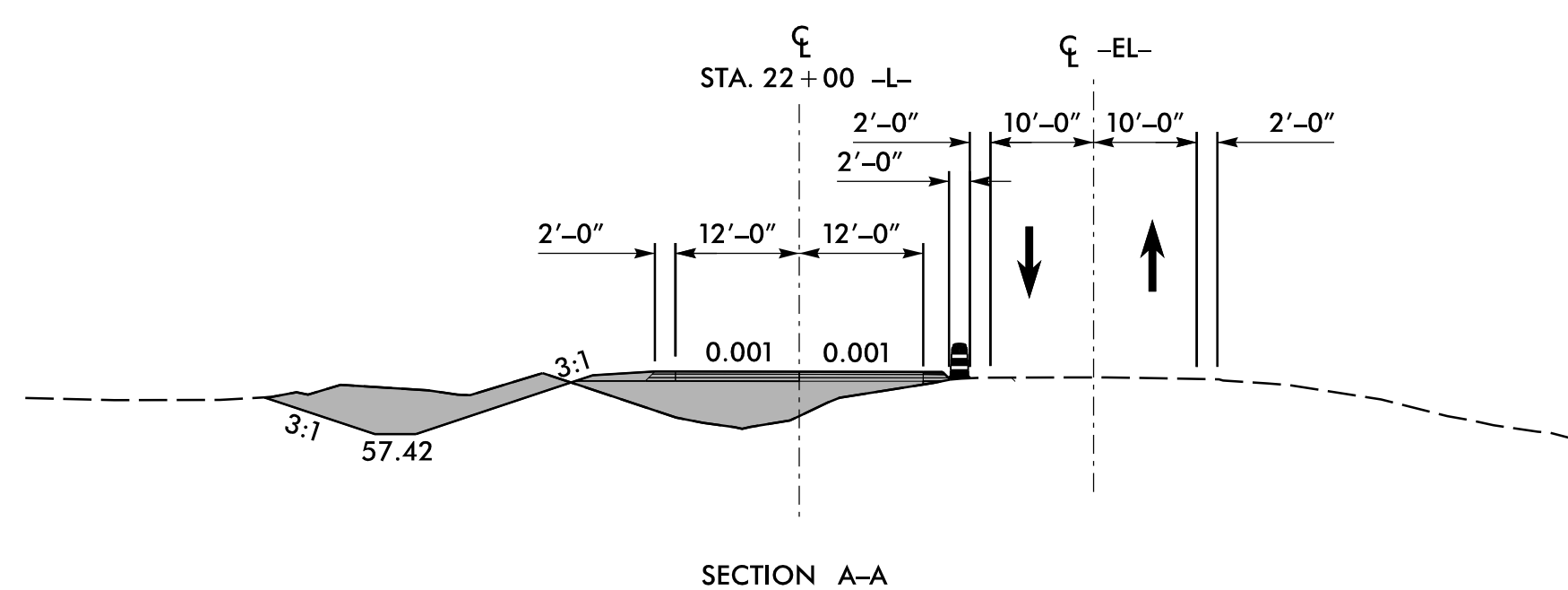
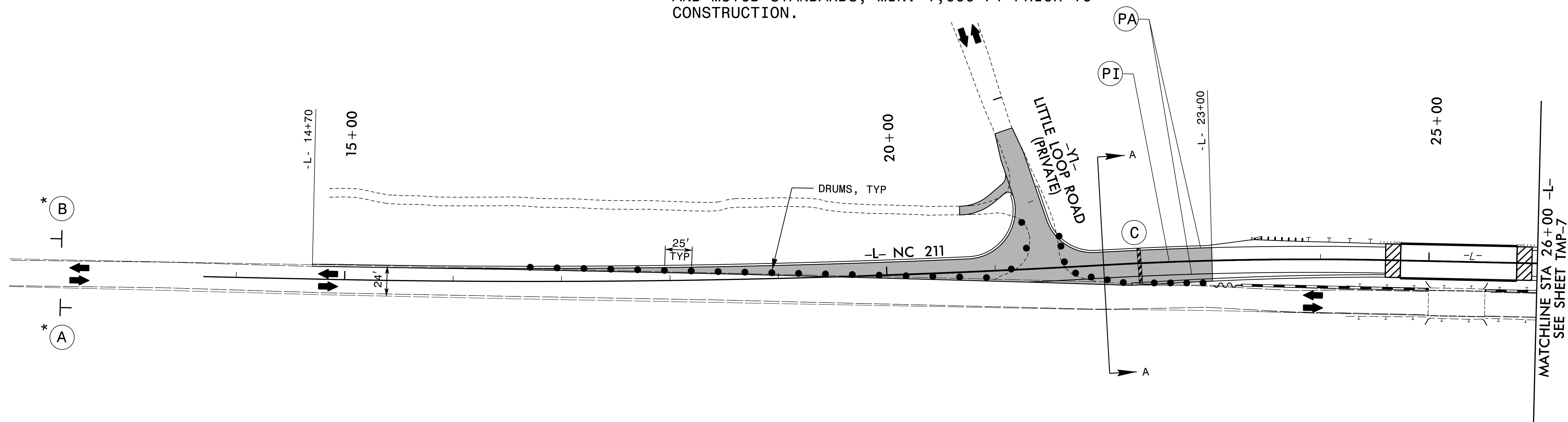
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PHASE III



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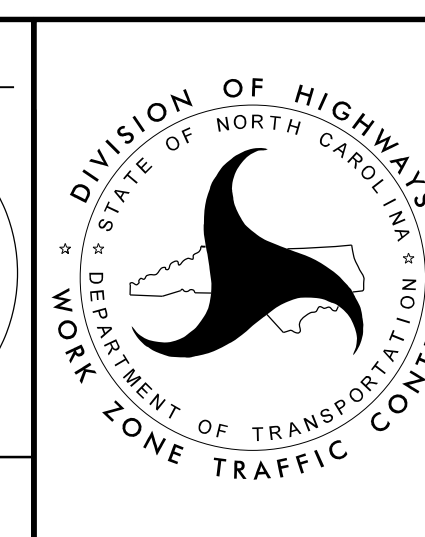


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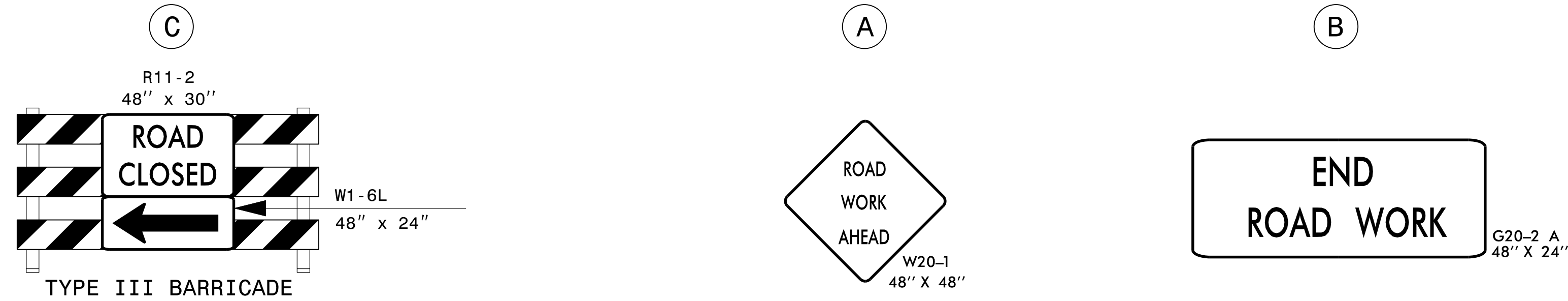
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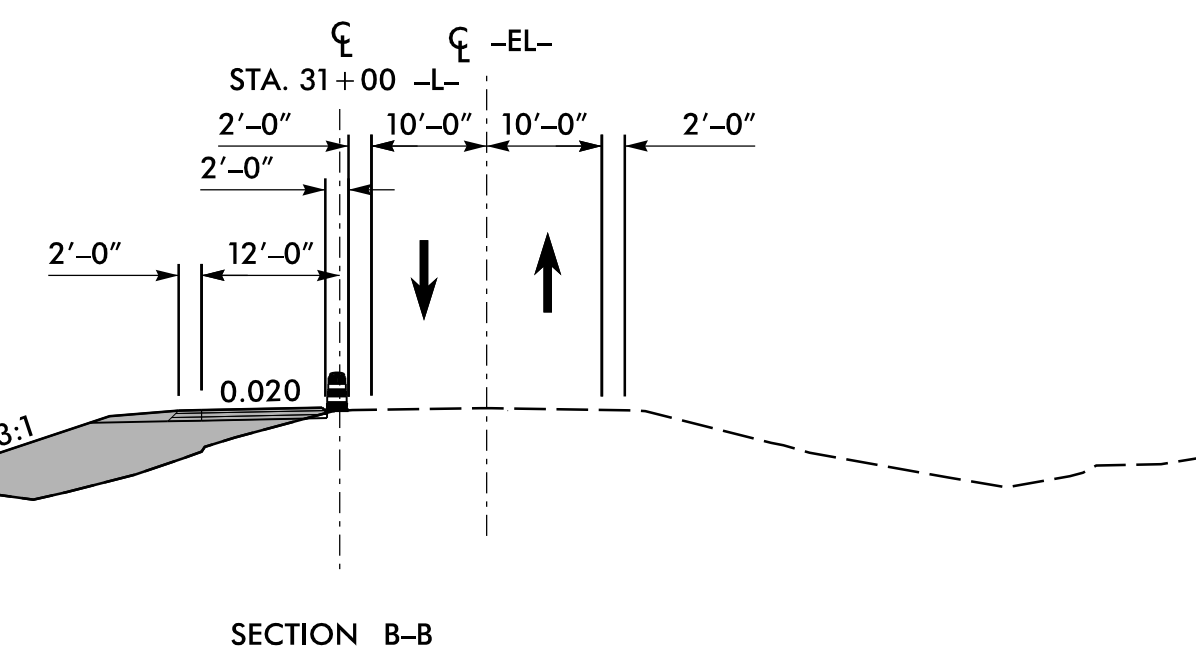
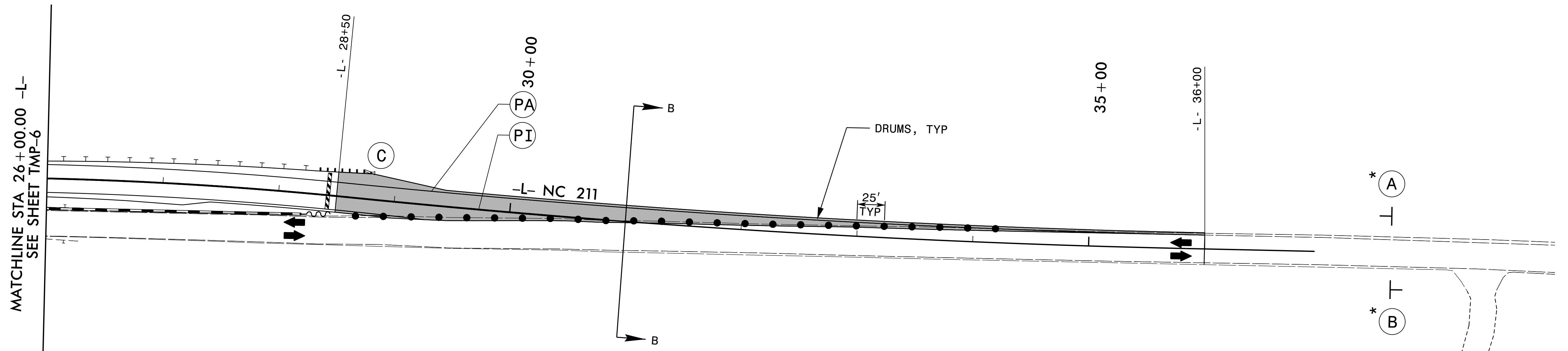


PHASE III

PROJ. REFERENCE NO.	SHEET NO.
B-4438	TMP-7



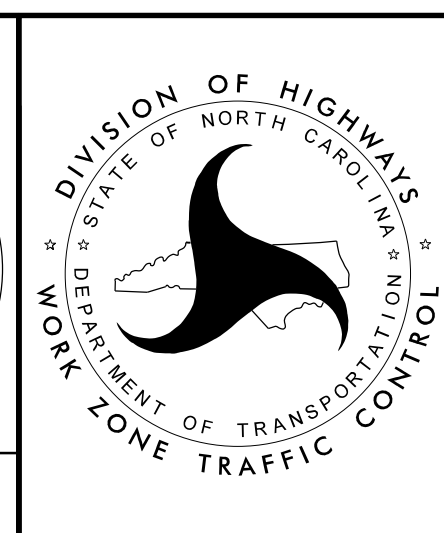
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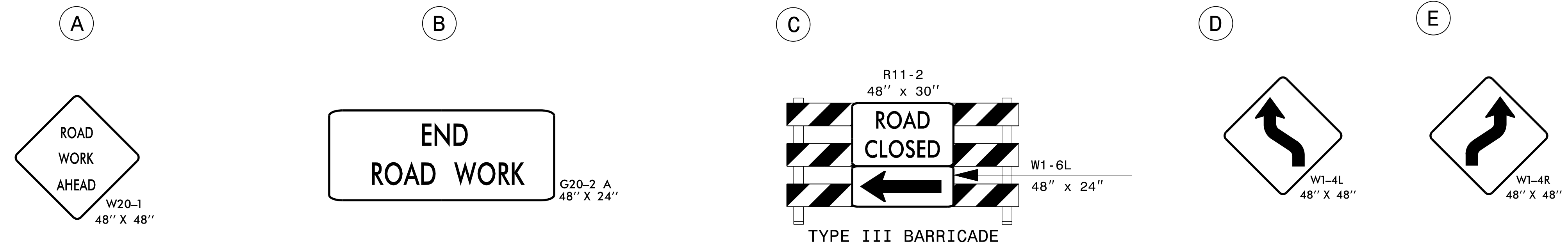
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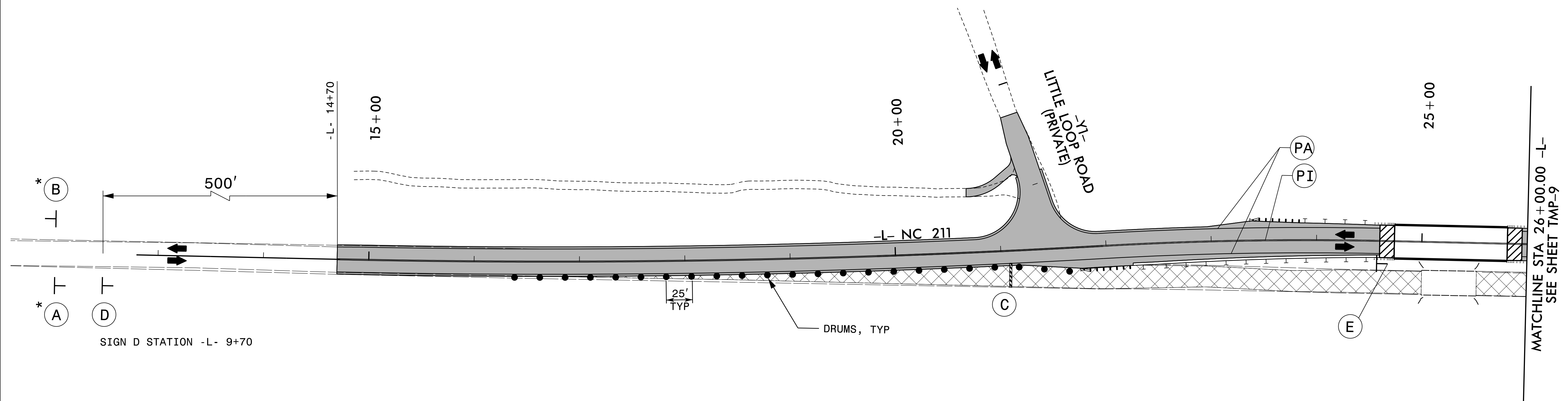
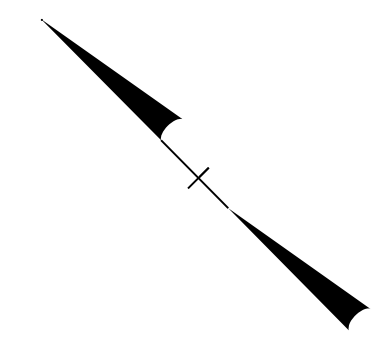


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PHASE IV



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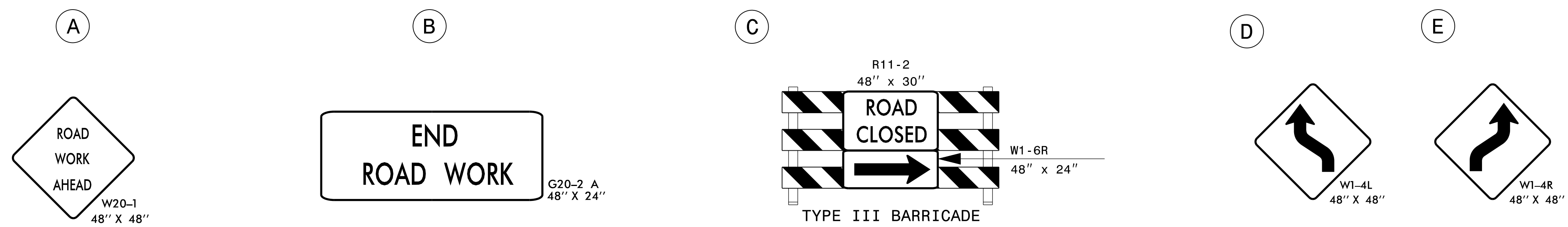
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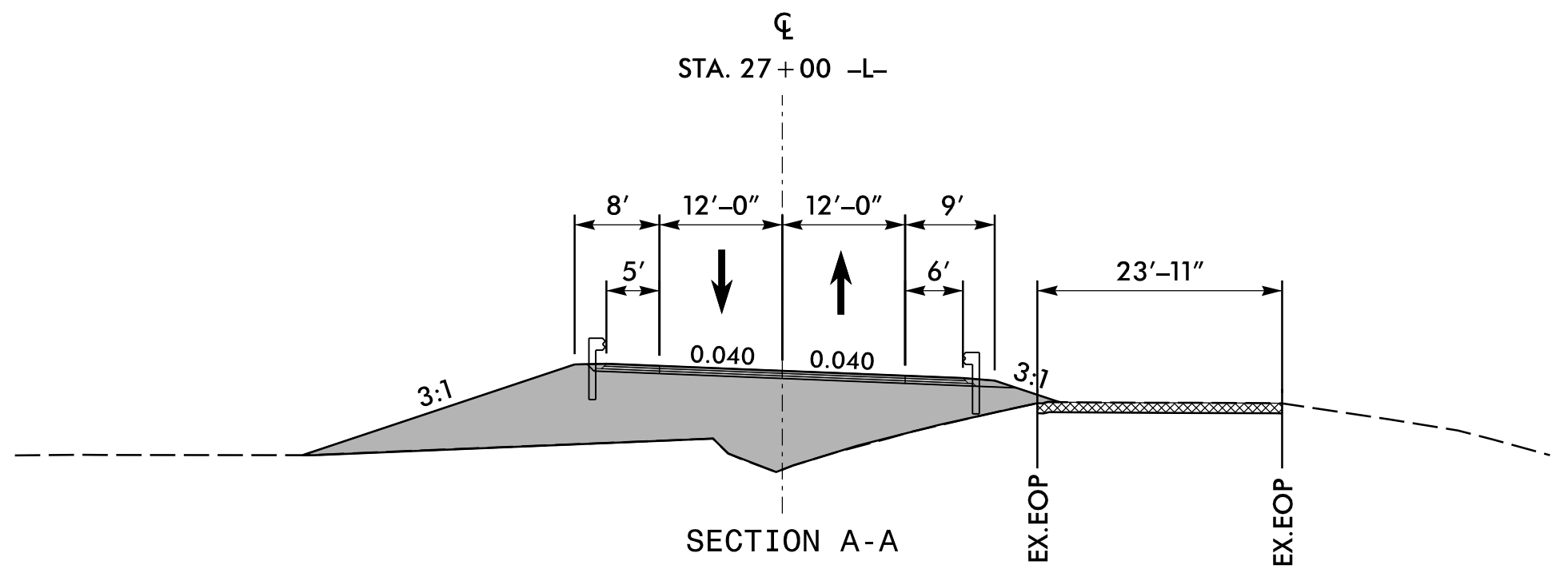
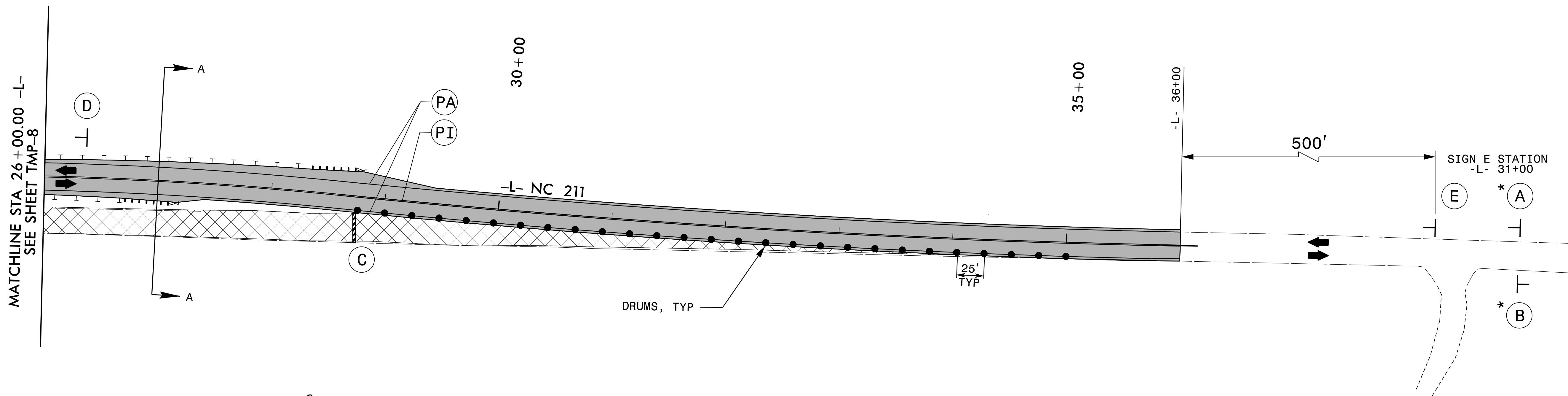
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