

NOTES:

Assumed Live Load = H20-S16-44

For other design data and general note see sheet S-N.

No test piles are required. Order length shall be 39 Ft. for End Bent No. 1, 45 Ft. for Bent No. 1, 50 Ft. for Bent No. 2 and No. 3, 45 Ft. for Bent No. 4, and 39 Ft. for End Bent No. 2.

Piles for End Bent No. 1 and End Bent No. 2 to be driven to a minimum bearing capacity of 25 tons each. Piles for Bent No. 1, No. 2, No. 3 and No. 4 to be driven to a minimum bearing capacity of 27 tons each.

The contractor will be required to excavate completely through the fill at End Bent No. 1, Bent No. 3, Bent No. 4, and End Bent No. 2 before driving piles. See sheet S-N.

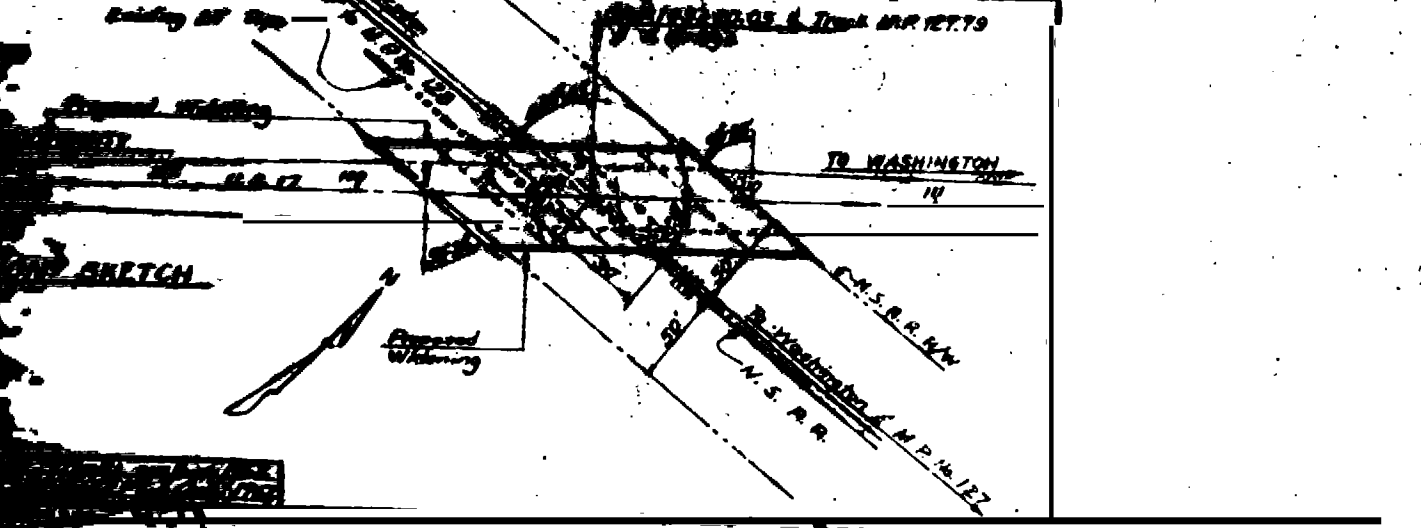
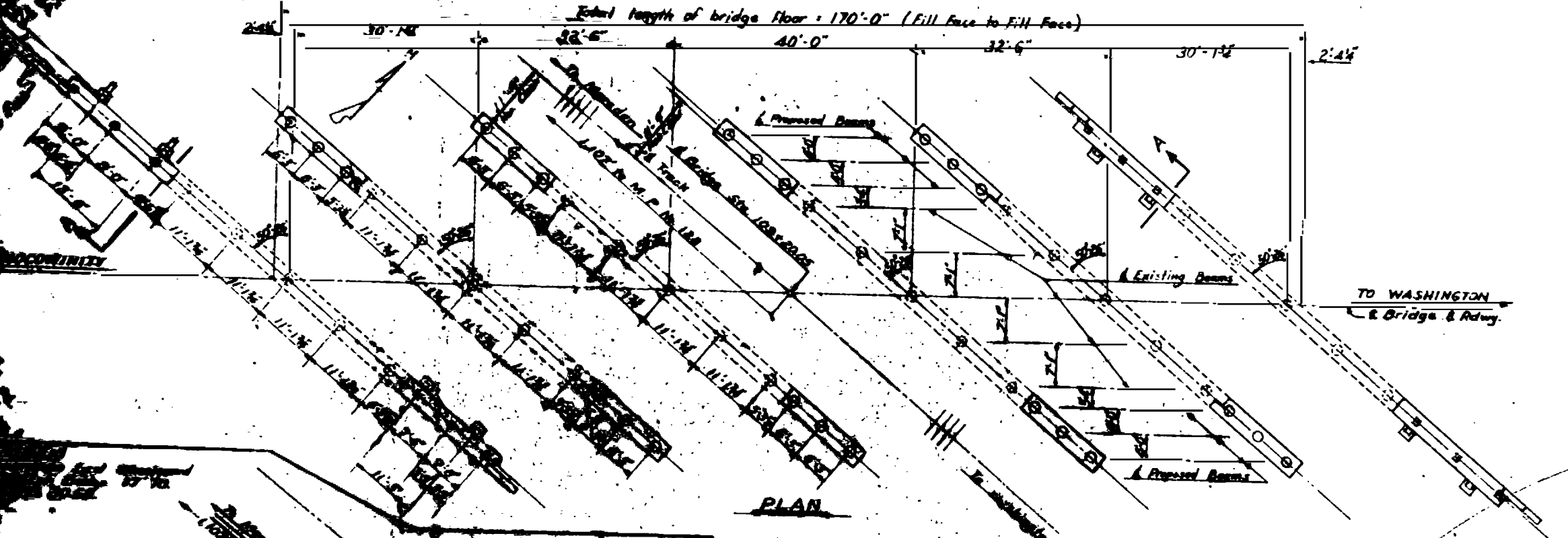
Traffic to be maintained, see Special Provisions.

Red Soundings taken from original survey made in 1930.

I-Beam lengths are subject to change to fit actual span lengths. Span lengths to be checked by the contractor's Engineer as early as is practical in order to determine whether I-beam lengths as shown on plans are correct.

Removal of Existing Structure At Station 50+00: Remove existing R.C. D.G. bridge over abandoned R.R. and consisting of 7 spans (5 @ 52'-0" & 2 @ 29'-0") 32" Reinforced on precast concrete pile bents. Remove superstructure completely, substructure down to 2' below subgrade or natural ground in accordance with the Specifications and the Special Provisions.

Proposed 1 1/2" wearing surface to be placed by Roadway Contractor.



	TOTAL BILL OF MATERIAL						
	Class A Concrete Cu. Yds.	Reinf. Steel Lbs.	Structural Steel Approx. Lbs.	12" Sp. Arch. Cast. Piles No.	20" Oct. Arch. Cast. Piles No.	20" Lin. Piles No.	Removal of Existing Structure Cu. Yds.
Superstructure and Bent No. 1	144.0	26,758	23,700	10	390		LS
Bent No. 2	9.2	1,619			6	270	
Bent No. 3	9.9	1,619			6	300	
Bent No. 4	9.9	1,619			6	300	
End Bent No. 2	9.2	1,619			6	270	
Approach Corps	15.5	3,070		10	390		
Totals	216.4	39,490	23,700	20	780	24	1140

Bridge No. 17-15-10

PROJECT NO. 1111

BEAUFORT

STATION 105+00

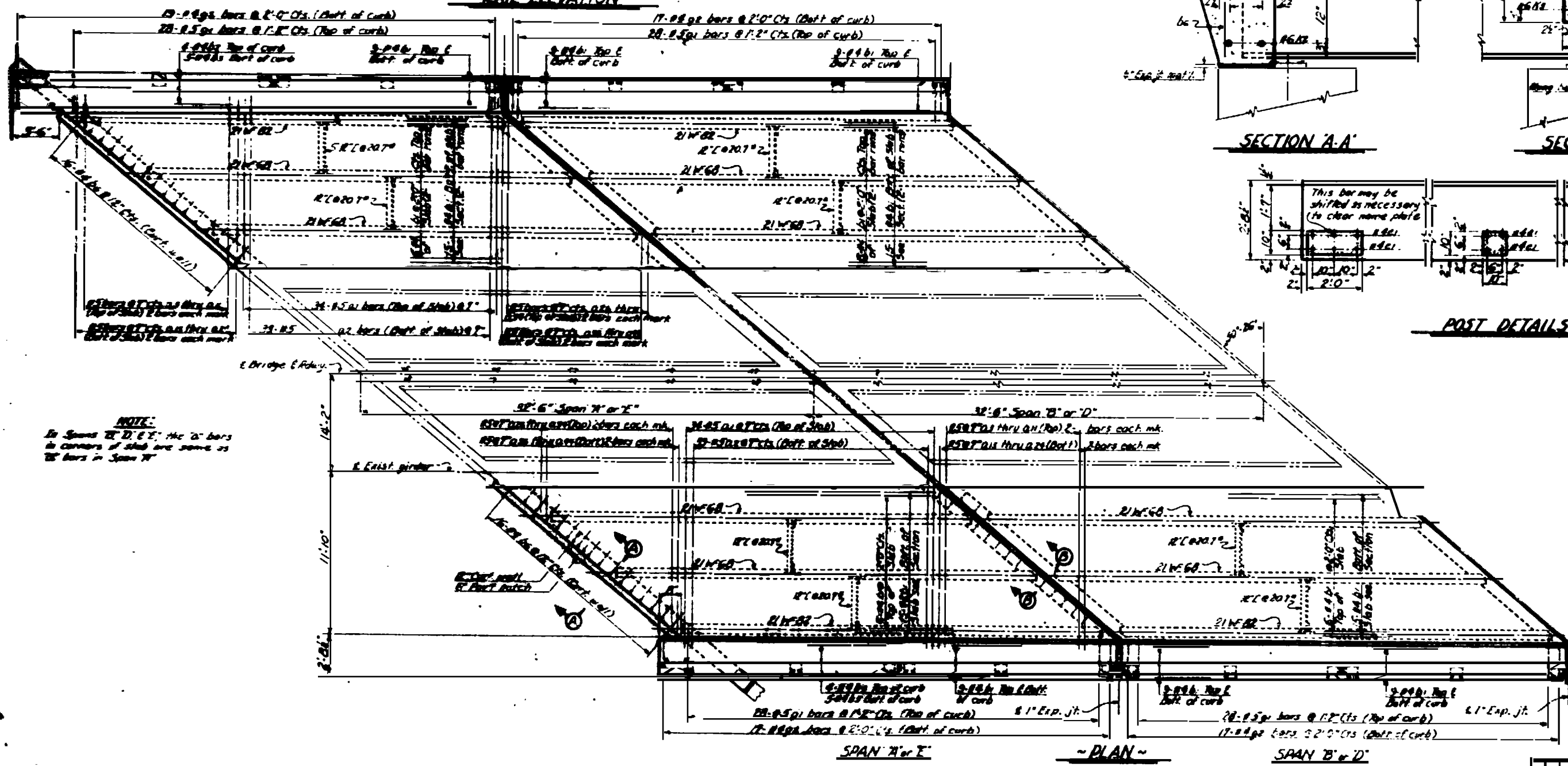
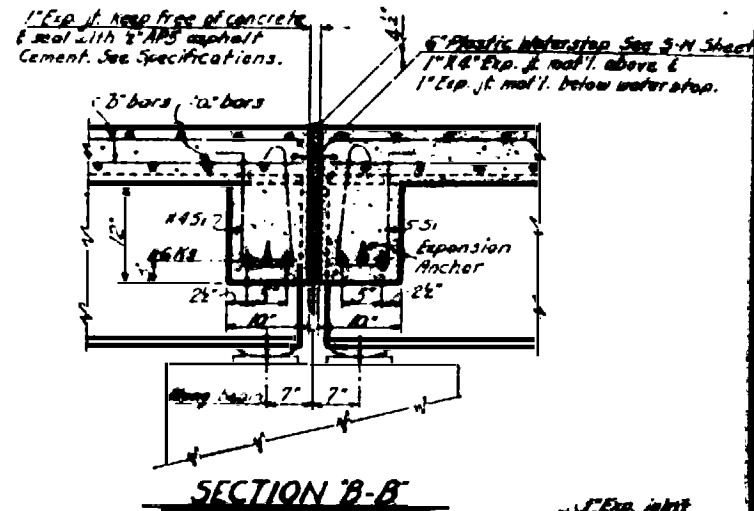
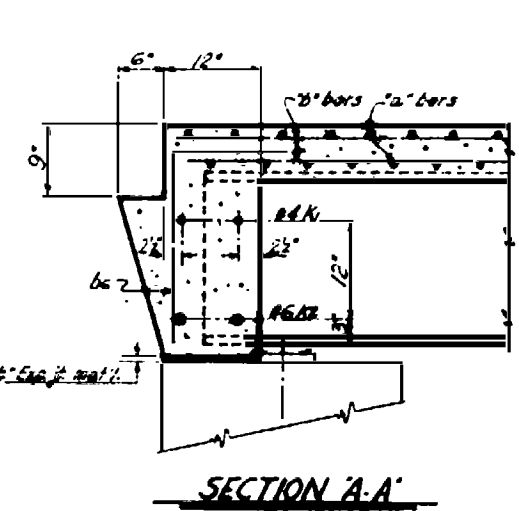
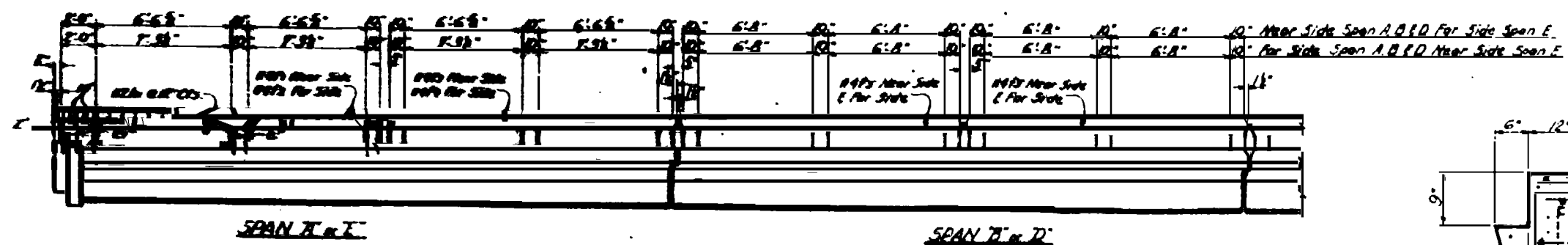
STATE HIGHWAY

GENERAL DRAWING FOR BRIDGE

NORFOLK SOUTHERN RAILROAD

ON U.S. 17

CHOCOWINY AND WASHINGTON



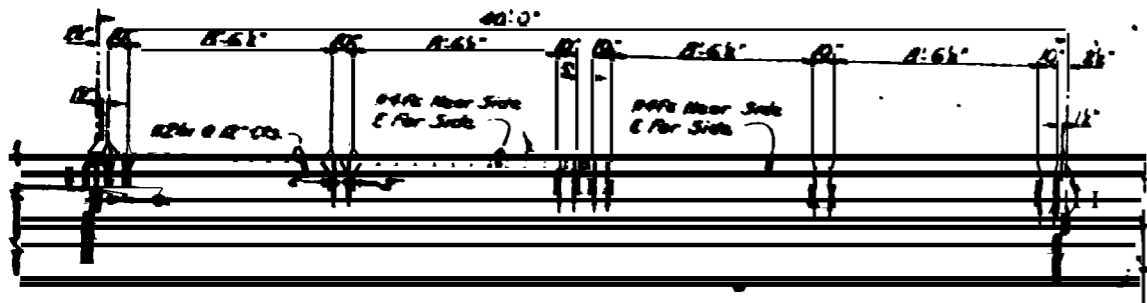
NOTE:
In Spans B, D, E, the 2" bars in corners of slab are same as 2" bars in Span A.

PROJECT No. 81702
BEAUFORT COUNTY
STATION: 109+7.70

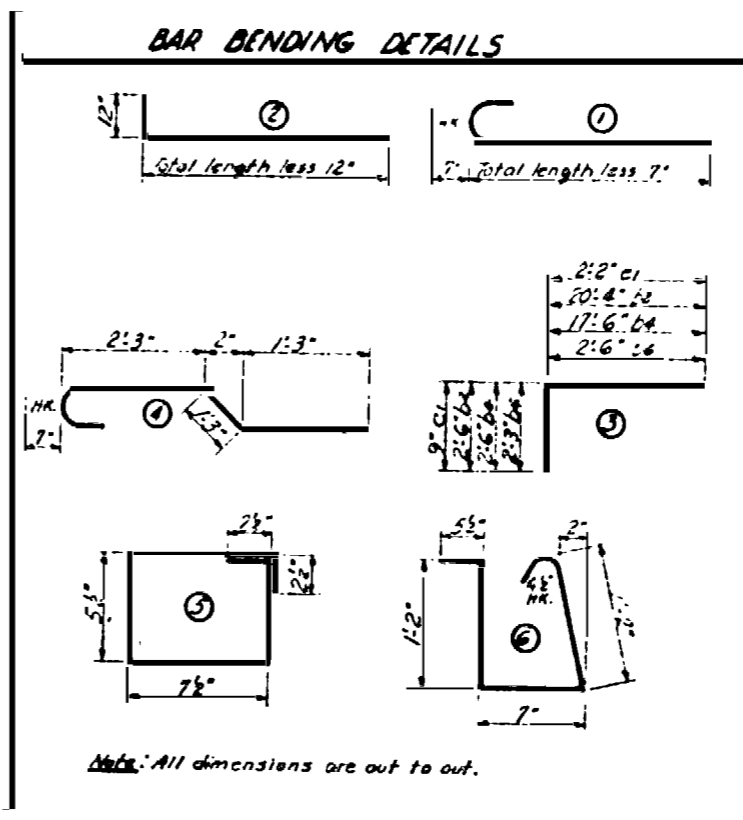
STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SPANS A & B

JUNE 1973

WID. CONC. FL. NOT CONT.



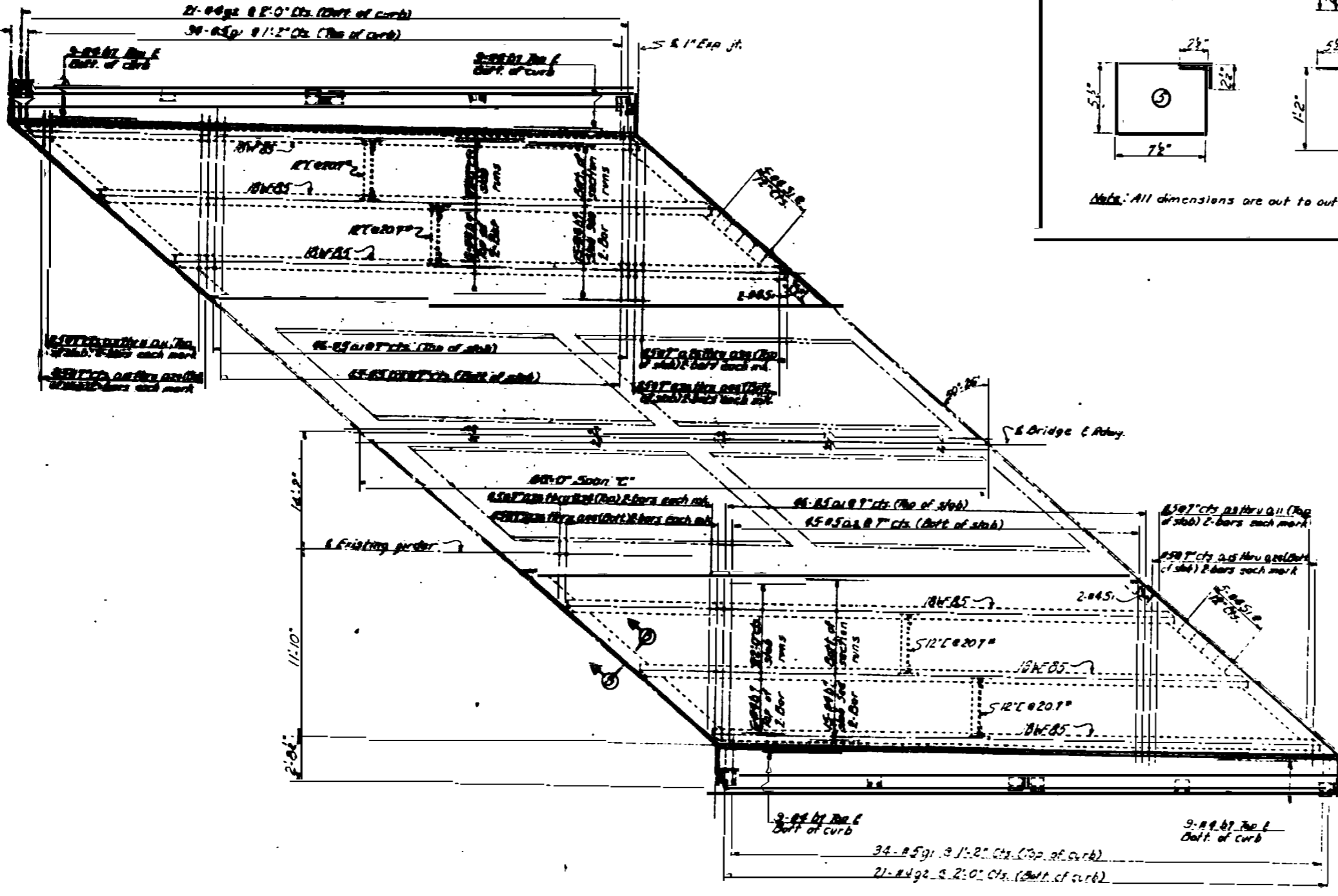
RAIL ELEVATION - SPAN C



TOTAL BILL OF MATERIAL FOR SPANS A'B'C'D'E'E'

BAR NO.	QTY	SIZE	TYPE	LENGTH	WEIGHT	BAR NO.	QTY	SIZE	TYPE	LENGTH	WEIGHT
Q1	364	#3	1	12'-2"	4,619	F1	8	#4	Sfr	16'-9"	90
Q2	354	#5	2	12'-5"	4,585	F2	8	#4	Sfr	18'-2"	97
Q3	20	#5	1	11'-3"	235	F3	8	#4	Sfr	15'-7"	83
Q4	20	#5	1	10'-3"	214	F4	8	#4	Sfr	17'-0"	91
Q5	20	#5	1	9'-4"	185	F5	32	#4	Sfr	15'-10"	338
Q6	20	#5	1	8'-4"	170	F6	16	#4	Sfr	19'-7"	209
Q7	20	#5	1	7'-5"	155						
Q8	20	#5	1	6'-5"	134						
Q9	20	#5	1	5'-6"	115	91	292	#5	4	5'-4"	1624
Q10	20	#5	1	4'-6"	94	92	182	#4	Sfr	2'-4"	284
Q11	20	#5	1	3'-6"	73						
Q12	20	#5	2	11'-9"	245						
Q13	20	#5	2	10'-9"	224	h1	348	#2	5	2'-9"	150
Q14	20	#5	2	9'-10"	205						
Q15	20	#5	2	8'-10"	186						
Q16	20	#5	2	7'-11"	165						
Q17	20	#5	2	6'-11"	144	K1	8	#4	Sfr	20'-11"	112
Q18	20	#5	2	5'-11"	123	K2	8	#6	Sfr	20'-11"	251
Q19	20	#5	2	5'-0"	104	K3	32	#6	Sfr	15'-11"	765
Q20	20	#5	2	4'-0"	83						
Q21	20	#5	2	3'-1"	64						
Q22	20	#5	Sfr	9'-9"	203	J1	192	#4	6	3'-9"	281
Q23	20	#5	Sfr	8'-10"	184						
Q24	20	#5	Sfr	7'-10"	163						
Q25	20	#5	Sfr	6'-11"	144						
Q26	20	#5	Sfr	5'-11"	123						
Q27	20	#5	Sfr	5'-0"	104						
Q28	20	#5	Sfr	4'-0"	83						
Q29	20	#5	Sfr	3'-1"	64						
Q30	20	#5	Sfr	2'-1"	45						
Q31	20	#5	Sfr	9'-10"	205						
Q32	20	#5	Sfr	8'-11"	186						
Q33	20	#5	Sfr	7'-11"	165						
Q34	20	#5	Sfr	7'-0"	146						
Q35	20	#5	Sfr	6'-0"	125						
Q36	20	#5	Sfr	5'-1"	106						
Q37	20	#5	Sfr	4'-1"	85						
Q38	20	#5	Sfr	3'-1"	64						
Q39	20	#5	Sfr	2'-2"	45						
Q40	20	#5	Sfr	9'-10"	205						
Q41	20	#5	Sfr	8'-11"	186						
Q42	20	#5	Sfr	7'-11"	165						
Q43	20	#5	Sfr	7'-0"	146						
Q44	20	#5	Sfr	6'-0"	125						
Q45	20	#5	Sfr	5'-1"	106						
Q46	20	#5	Sfr	4'-1"	85						
Q47	20	#5	Sfr	3'-1"	64						
Q48	20	#5	Sfr	2'-2"	45						
Q49	44	#4	Sfr	16'-10"	4,993						
Q50	8	#4	3	22'-10"	122						
Q51	10	#4	Sfr	20'-8"	136						
Q52	8	#4	3	20'-0"	107						
Q53	10	#4	Sfr	17'-6"	117						
Q54	64	#4	3	4'-9"	203						
Q55	10	#4	Sfr	20'-7"	1,690						
Q56	243	#4	3	2'-11"	403						

Reinforcing Steel Lab
Structural Steel Approx. 11/15/65
Class 74 Conc. To Top

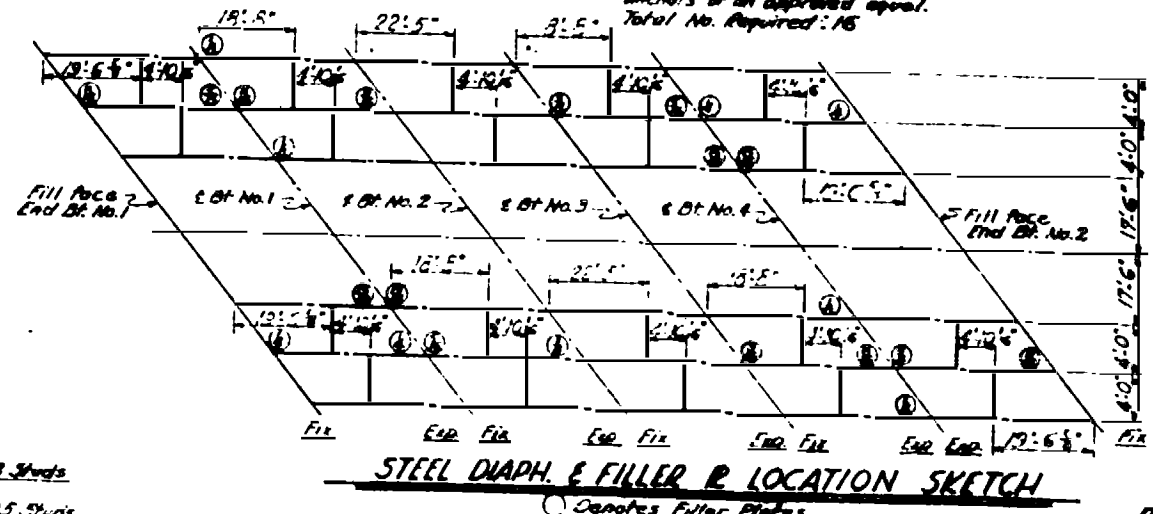
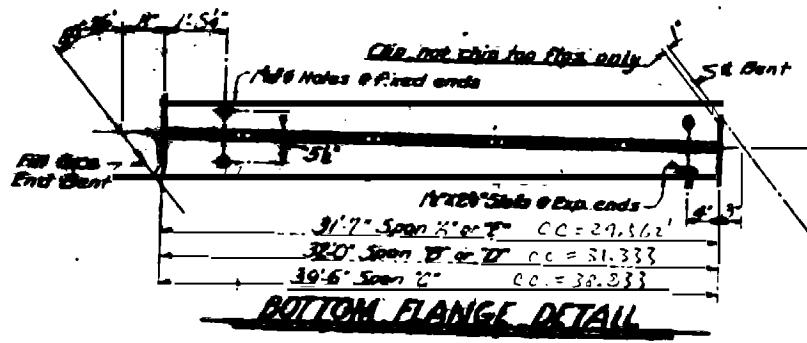
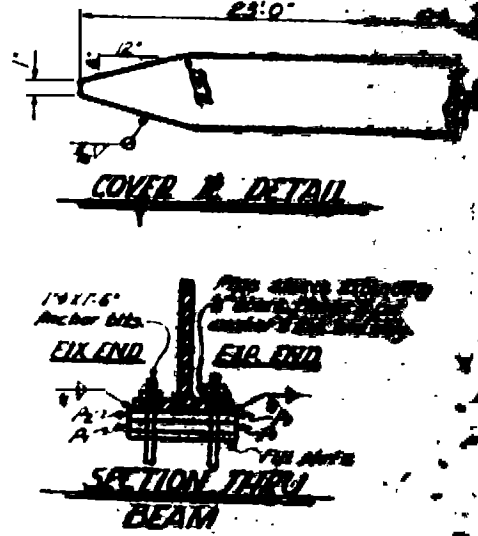
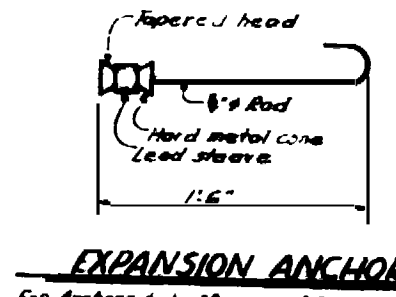
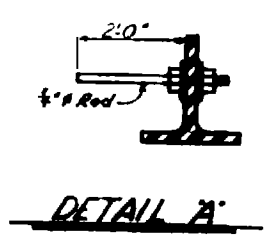
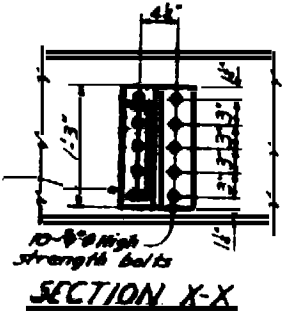
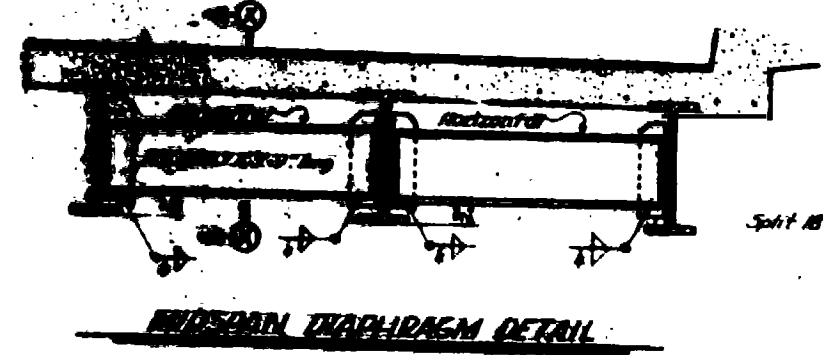
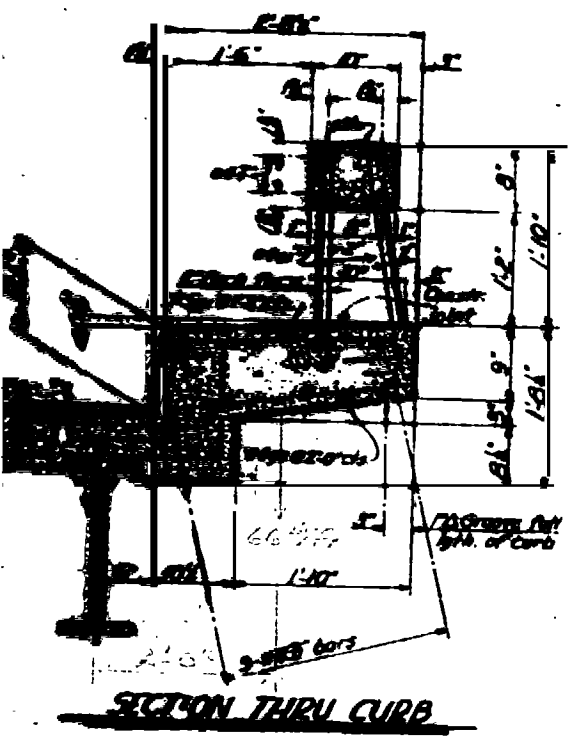
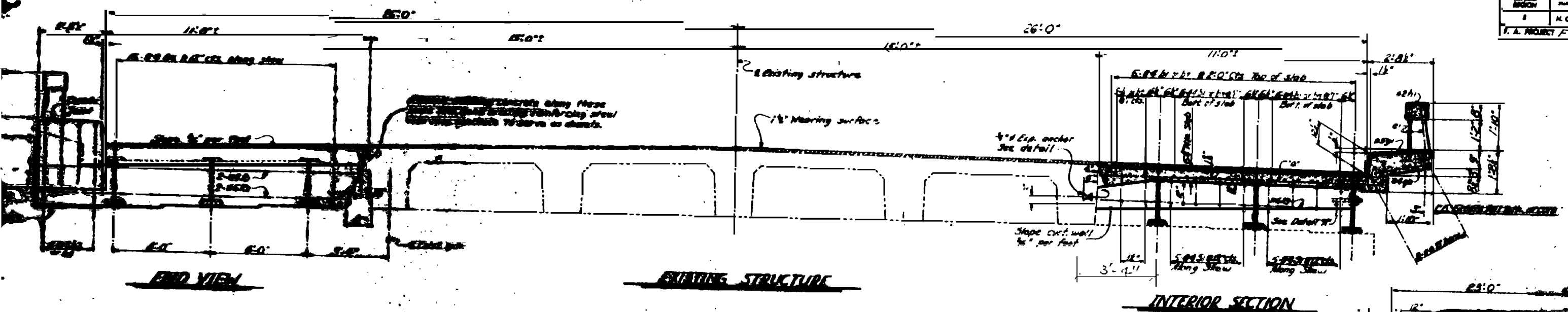


PLAN - SPAN C

PROJECT NO. 811028
BEAUFORT COUNTY
STATION: 109+20.05

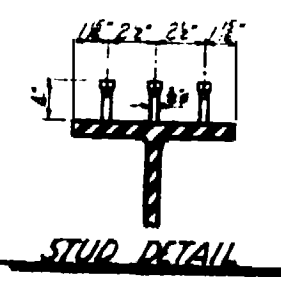
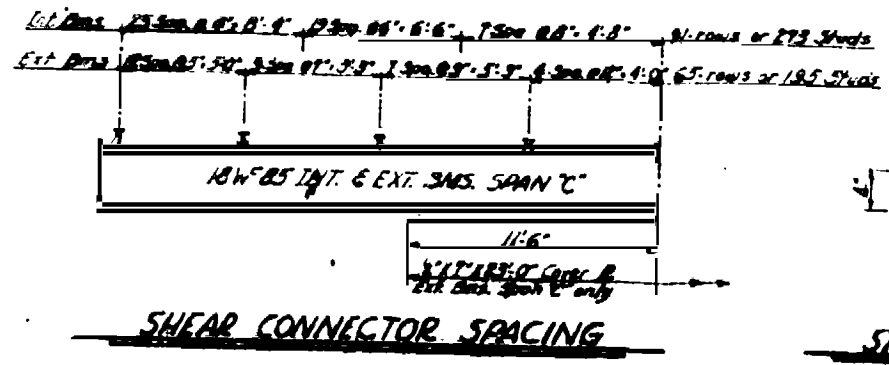
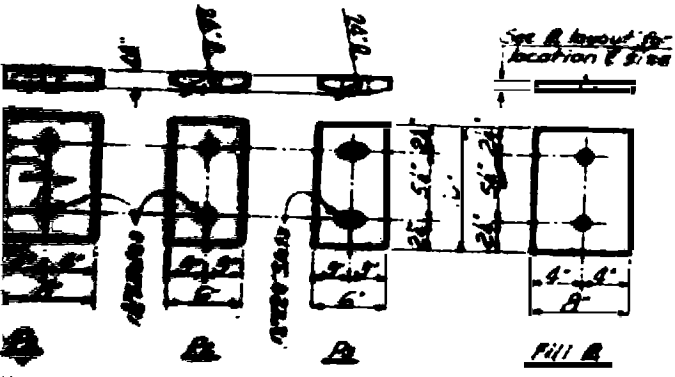
STATE OF NORTH CAROLINA
STATE HIGHWAY COMMISSION
SUPERSTRUCTURE
SPAN C

JUNE 1965



NOTE: At the Contractor's option, 3" x 6" channels may be used in lieu of the steel shear connectors. See sheet 5-N.

All beams, cover plates and channel shear connectors shall be A36 grade structural steel, unless otherwise specified.



	DEFLECTION TABLE			
	SPAN A-B		SPAN C	
	INT.	EXT.	INT.	EXT.
Dead Load Deflection	1/4"	1/4"	1/4"	1/4"
Deflection due to Im. wt.	1/4"	1/4"	1/4"	1/4"
TOTAL DEFLECTION	1/2"	1/2"	1/2"	1/2"
Vertical Curve Ordinate	1/4"	1/4"	1/4"	1/4"
LANE BEAMS	0"	0"	0"	0"

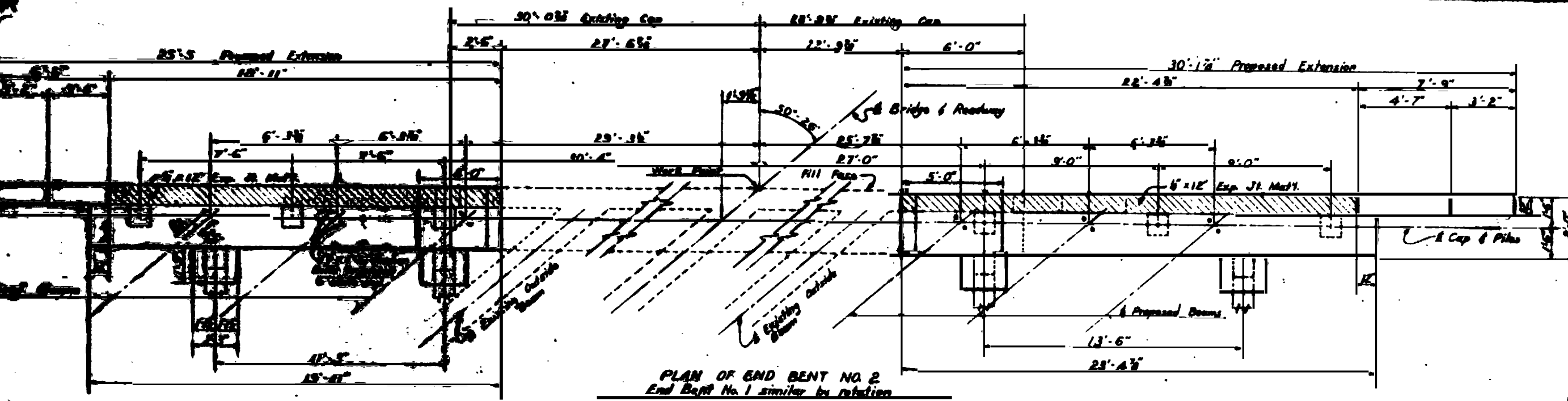
No lumber other than natural mill lumber is required in Spans A, B, C, D, & E.

PROJECT NO. 811028
 BEAUFORT COUNTY
 STATION: 129+76.05

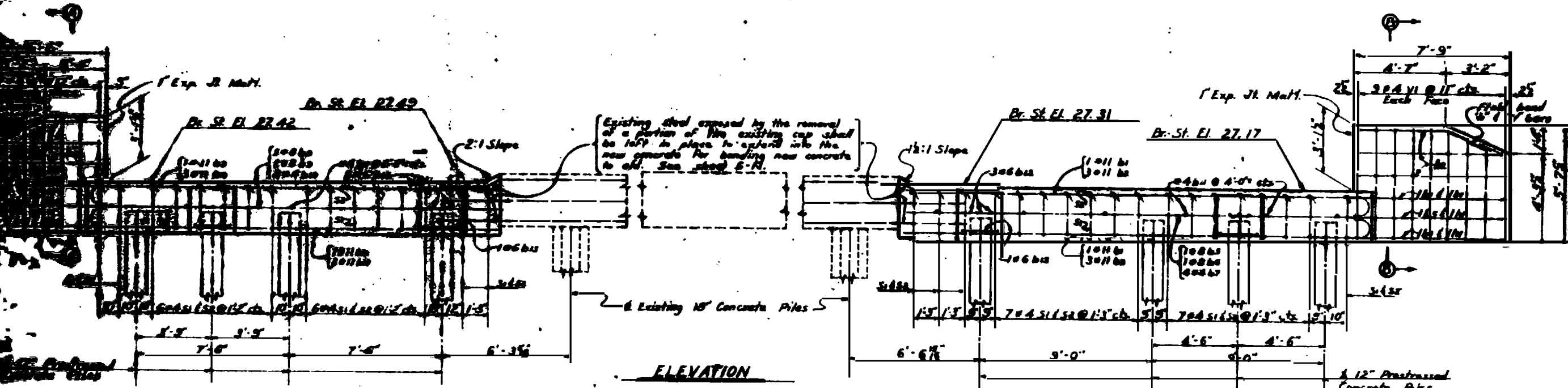
STATE OF NORTH CAROLINA
 STATE HIGHWAY COMMISSION
 SUPERSTRUCTURE SECTIONS
 AND
 STRUCTURAL STEEL DETAILS

P1A. 7 6

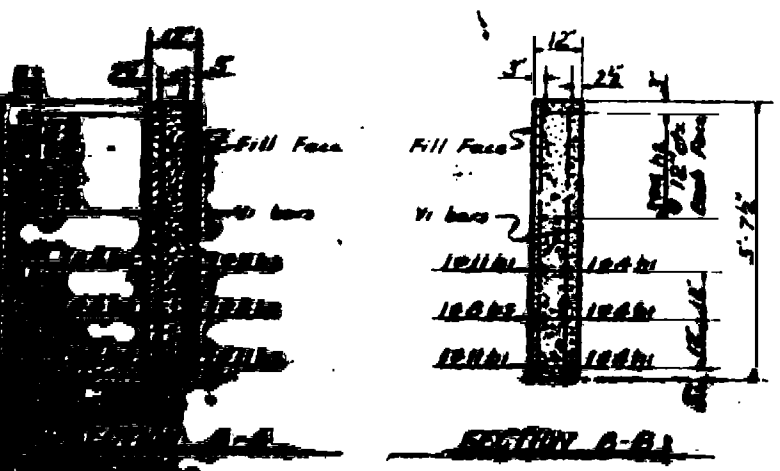
NO.	DESCRIPTION
1	REVISION
2	REVISION
3	REVISION
4	REVISION
5	REVISION
6	REVISION
7	REVISION
8	REVISION
9	REVISION
10	REVISION
11	REVISION
12	REVISION
13	REVISION
14	REVISION
15	REVISION
16	REVISION
17	REVISION
18	REVISION
19	REVISION
20	REVISION
21	REVISION
22	REVISION
23	REVISION
24	REVISION
25	REVISION
26	REVISION
27	REVISION
28	REVISION
29	REVISION
30	REVISION
31	REVISION
32	REVISION
33	REVISION
34	REVISION
35	REVISION
36	REVISION
37	REVISION
38	REVISION
39	REVISION
40	REVISION
41	REVISION
42	REVISION
43	REVISION
44	REVISION
45	REVISION
46	REVISION
47	REVISION
48	REVISION
49	REVISION
50	REVISION



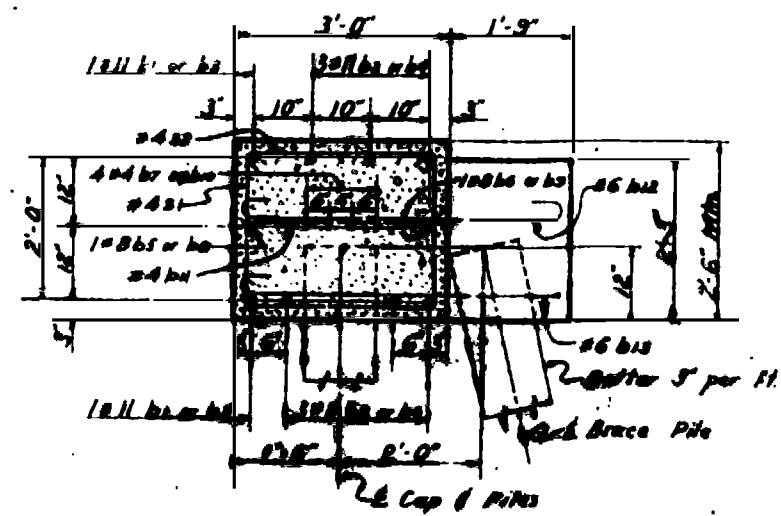
PLAN OF END BENT NO 2
 End Bent No. 1 similar by rotation



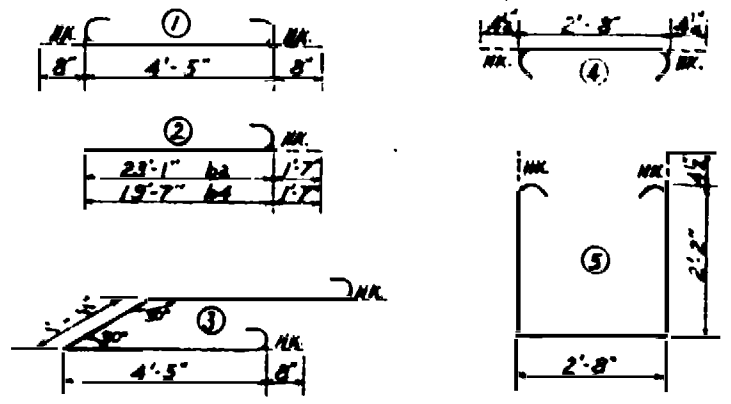
ELEVATION



SECTION C-C



SECTION THRU PILE



BAR DETAILS
 All bar dimensions are out to out

PROJECT NO. 24
 BEAUFORT
 STATION 108+00
 STATE HIGHWAY
 SUBSTRUCTURE
 END BENT NO. 2