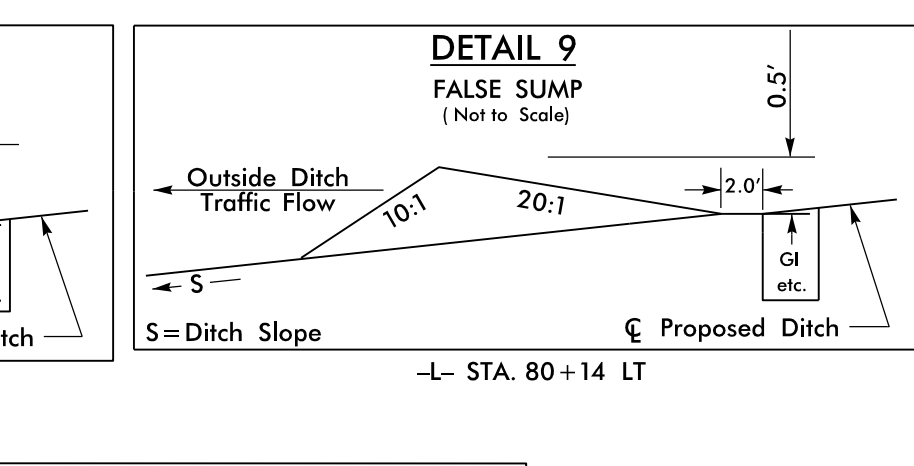
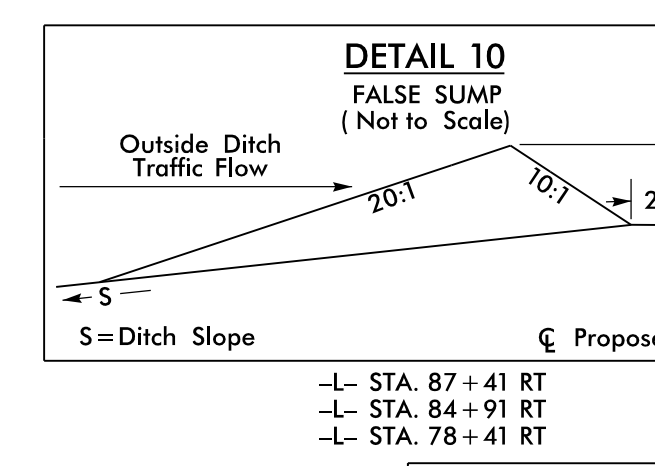


PI Sta 76+02.21  
Δ = 3° 53' 59.4" (LT)  
D = 1' 00" 00.4"  
L = 389.95'  
T = 195.05'  
R = 5,729.00'  
e = 3%  
Runoff = 162'

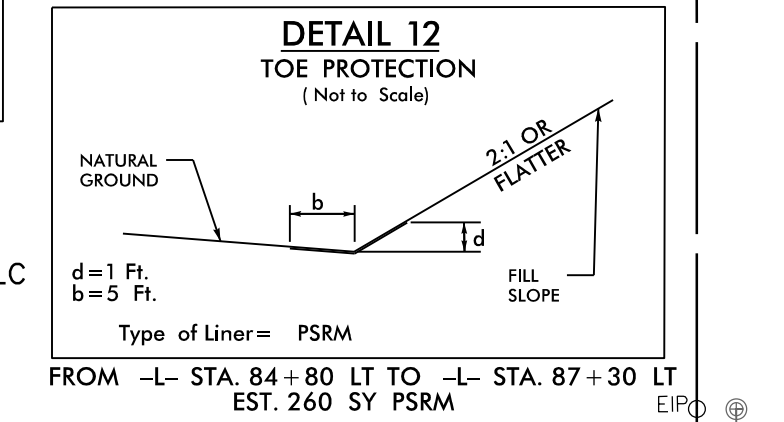
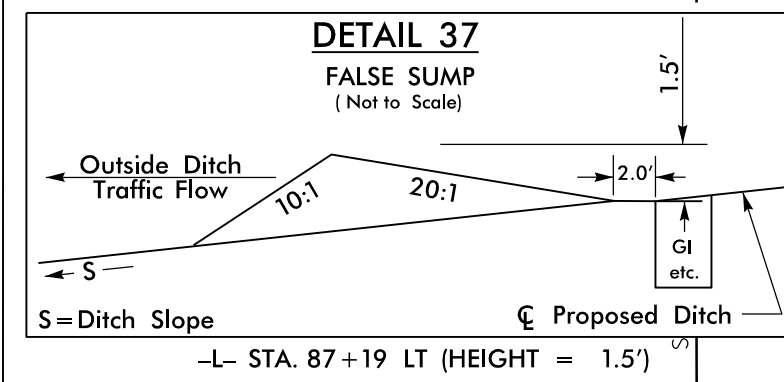
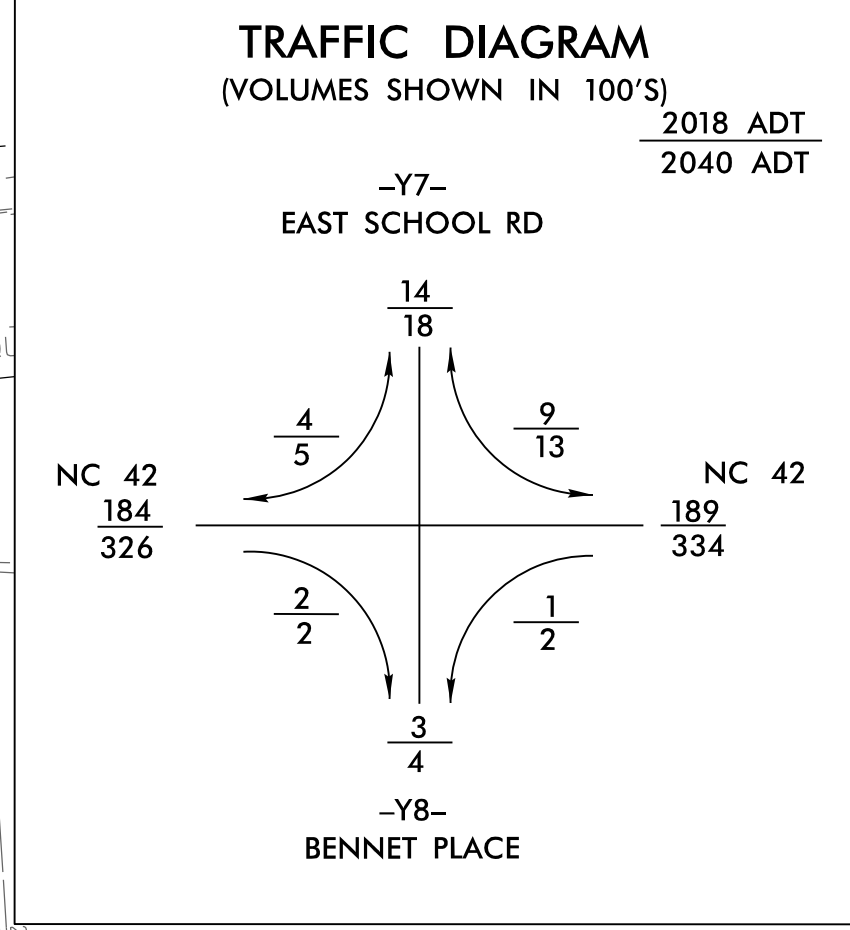
PI Sta 79+93.48  
Δ = 3° 55' 35.2" (RT)  
D = 1' 00" 00.4"  
L = 392.61'  
T = 196.38'  
R = 5,729.00'  
e = 3%  
Runoff = 162'

PI Sta 12+47.04  
Δ = 35° 17' 22.8" (RT)  
D = 7' 22' 38.3"  
L = 478.36'  
T = 247.04'  
R = 776.65'

PI Sta 11+77.40  
Δ = 13° 43' 20.5" (LT)  
D = 23' 38" 08.6"  
L = 58.06'  
T = 29.17'  
R = 242.41'

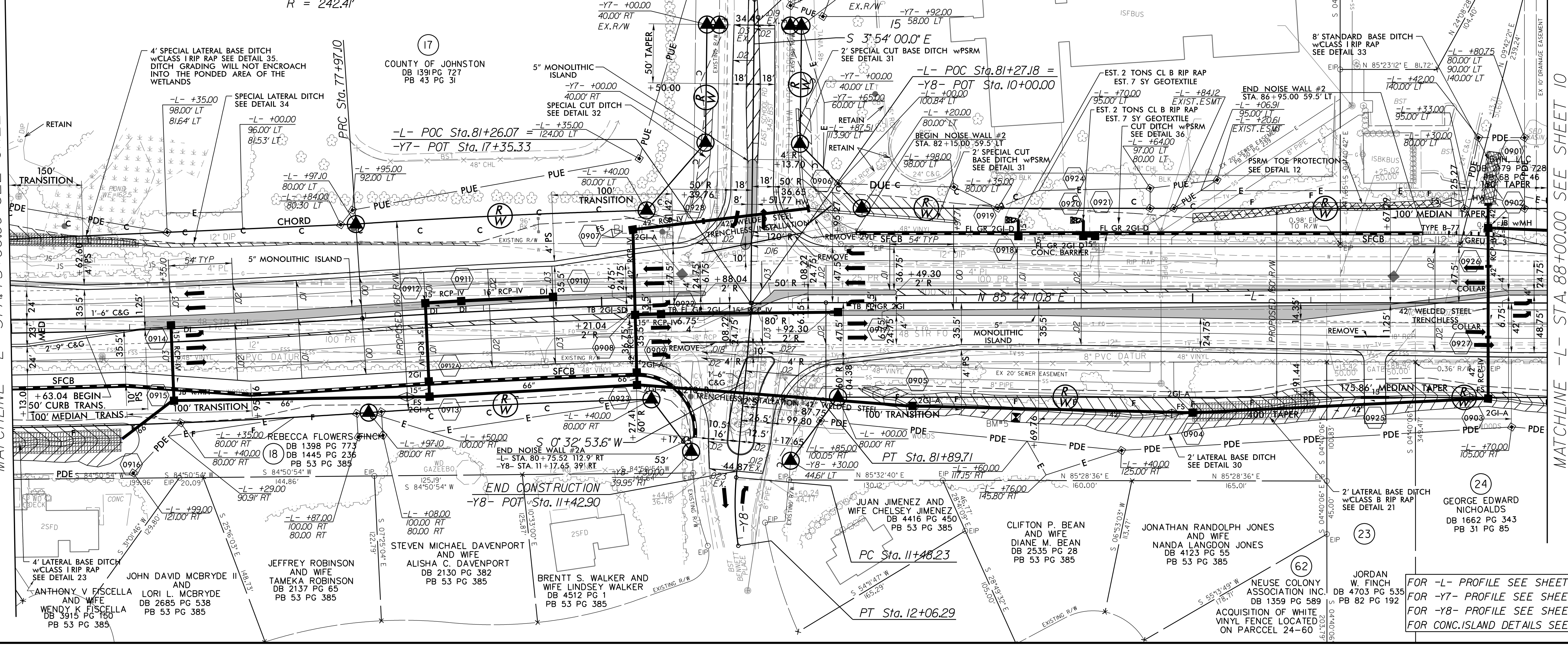


PROJECT REFERENCE NO. <b>R-3825B</b>	SHEET NO. <b>9</b>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER <b>Edward S. Jones</b> Professional Seal 18470	HYDRAULICS ENGINEER <b>D. Busch</b> Professional Seal 037863
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	



75+00

MATCHLINE -L- STA. 75+00.00 SEE SHEET 8



MATCHLINE -L- STA. 88+00.00 SEE SHEET 10

FOR -L- PROFILE SEE SHEET 26  
FOR -Y7- PROFILE SEE SHEET 34  
FOR -Y8- PROFILE SEE SHEET 34  
FOR CONC. ISLAND DETAILS SEE SHEET 2B-4

8/17/2018 10:51 AM P:\03825B\_r.dwg psh09.dgn  
TRAVELER