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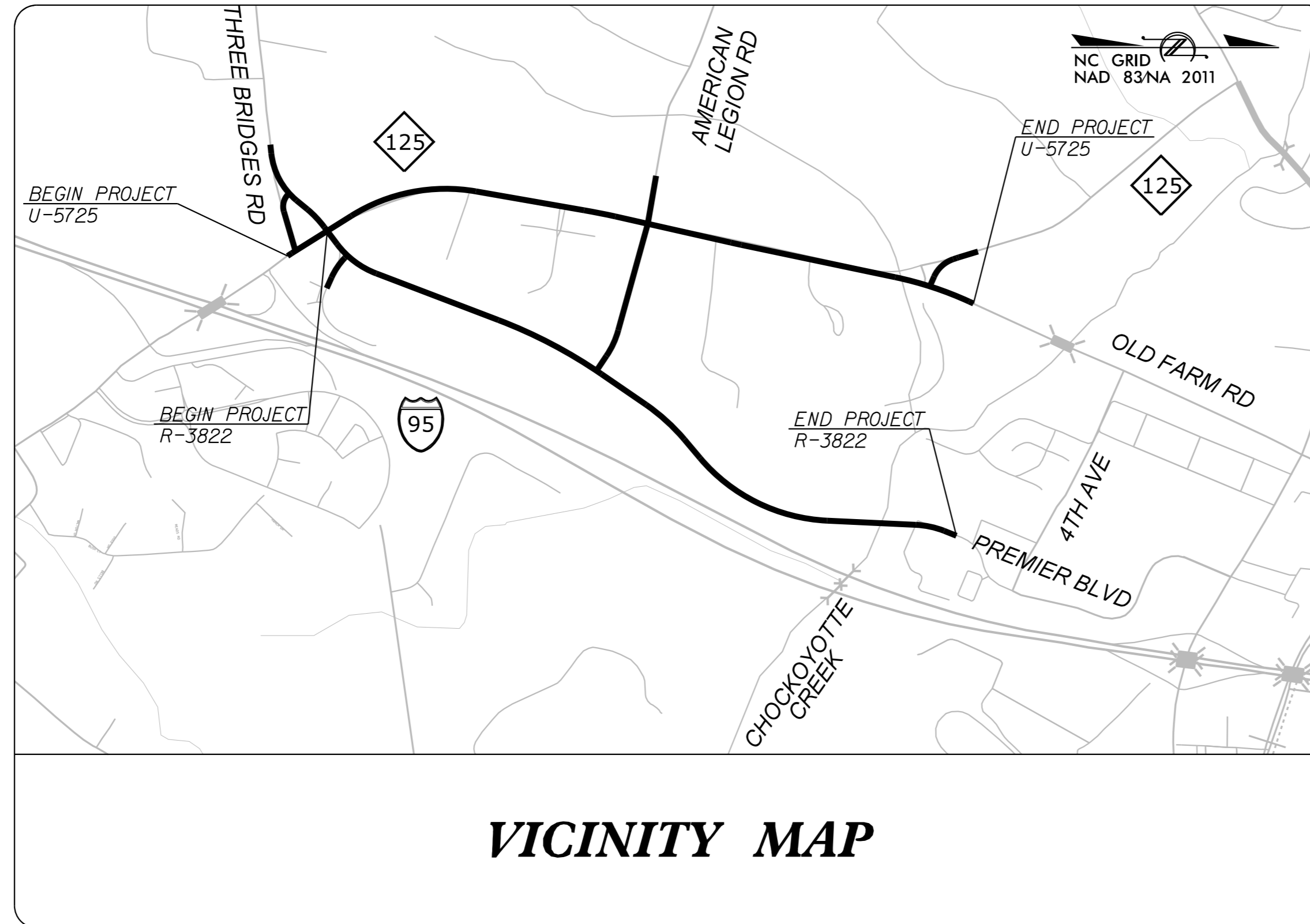
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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

HALIFAX COUNTY



VICINITY MAP

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>TITLE</u>
TMP-1	TITLE SHEET, AND INDEX OF SHEETS
TMP-1A TO TMP-1C	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, TEMPORARY PAVEMENT MARKINGS MANAGEMENT STRATEGIES, AND GENERAL NOTES
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06/05/18 DATE SUBMITTED
 SUBMITTAL:
 STAGING CONCEPT
 MIDPOINT
 PRE-FINAL
 FINAL

Kimley»Horn

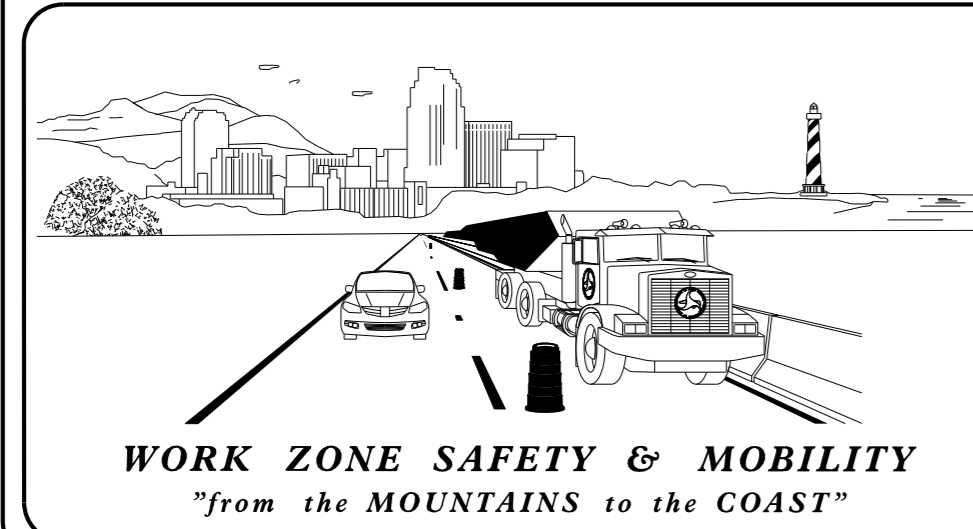
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APPROVED: David Shinbara
 DATE: 8/15/2018

SEAL



8/15/2018 \$FILEL\$



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SHEET NO.
TMP-1

U-5725/R-3822

TIP PROJECT:

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1170.01	POSITIVE PROTECTION
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS - (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS
1264.02	PLACEMENT OF OBJECT MARKERS
1267.01	FLEXIBLE DELINEATOR INSTALLATION
1267.02	FLEXIBLE DELINEATOR SPACING

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- DIRECTION OF PEDESTRIAN TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- _____ PROPOSED PVMT.

- WORK AREA
- REMOVAL

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW PANEL (TYPE C)
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

SIGNALS

- EXISTING
- PROPOSED
- TEMPORARY

PAVEMENT MARKINGS

- _____ EXISTING LINES
- _____ TEMPORARY LINES

PAVEMENT MARKERS

- CRYSTAL / CRYSTAL
- CRYSTAL / RED
- YELLOW / YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

PAVEMENT MARKING LINES

- PA - PAINT (4" WHITE, 2X) EDGELINE
- PB - PAINT (4" YELLOW, 2X) EDGELINE
- PC - PAINT (4" WHITE, 2X) 10' SKIP
- PD - PAINT (4" WHITE, 2X) 3'-9"/SP MINISKIP
- PE - PAINT (4" WHITE, 2X) SOLID LANE LINE
- PF - PAINT (4" YELLOW, 2X) 10' SKIP
- PH - PAINT (4" YELLOW, 2X) SINGLE CENTER LINE
- PI - PAINT (4" YELLOW, 2X) DOUBLE CENTER LINE
- PP - PAINT (8" YELLOW, 2X) DIAGONAL
- P2 - PAINT (24" WHITE, 2X) STOPBAR

PAVEMENT MARKING SYMBOLS

- QA - PAINT 2X (LEFT TURN ARROW)
- QB - PAINT 2X (RIGHT TURN ARROW)
- QC - PAINT 2X (STRAIGHT ARROW)
- QD - PAINT 2X (COMBINATION STRAIGHT & LEFT TURN ARROW)
- QE - PAINT 2X (COMBINATION STRAIGHT & RIGHT TURN ARROW)
- QF - PAINT 2X (COMBINATION LEFT & RIGHT TURN ARROW)
- QG - PAINT 2X (COMBINATION STRAIGHT, RIGHT, & LEFT TURN ARROW)
- QI - PAINT 2X (ALPHANUMERIC CHARACTER)

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Kimley»Horn

APPROVED: <u>David Skinner</u> DATE: 7/5/2018 		<h2>ROADWAY STANDARD DRAWINGS & LEGEND</h2>
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MANAGEMENT STRATEGIES

PROPOSED IMPROVEMENTS ALONG NC-125 AND ALL OTHER SIDE STREETS WILL BE CONSTRUCTED WHILE MAINTAINING TRAFFIC AND USING TEMPORARY TRAFFIC PATTERNS WITH TEMPORARY SIGNALS, AND TEMPORARY LANE CLOSURES. LOCAL ACCESS TO RESIDENTS AND BUSINESSES WILL BE MAINTAINED AT ALL POSSIBLE TIMES DURING CONSTRUCTION.

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
NC-125 OLD FARM RD S. (SR-1745) THREE BRIDGES RD (SR-1627)	MONDAY THRU FRIDAY 6:00 AM TO 9:00 AM AND 4:00 PM TO 7:00 PM
THREE BRIDGES RD (SR-1627) (DURING SCHOOL OPERATIONS)	MONDAY THRU FRIDAY 6:00 AM TO 9:00 AM AND 2:00 PM TO 7:00 PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

<u>ROAD NAME</u>
NC-125 OLD FARM RD S. (SR-1745)

TIME RESTRICTIONS (CONT.)

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 AM DECEMBER 31st TO 7:00 PM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 PM THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 AM THURSDAY AND 7:00 PM MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 AM FRIDAY TO 7:00 PM TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 AM THE DAY BEFORE INDEPENDENCE DAY AND 7:00 PM THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 AM THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 PM THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 AM FRIDAY AND 7:00 PM TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 AM TUESDAY TO 7:00 PM MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 PM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CLOSE ROADS AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
NC-125 OLD FARM RD S. (SR-1745)	ANYTIME

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER

LANE AND SHOULDER CLOSURE REQUIREMENTS

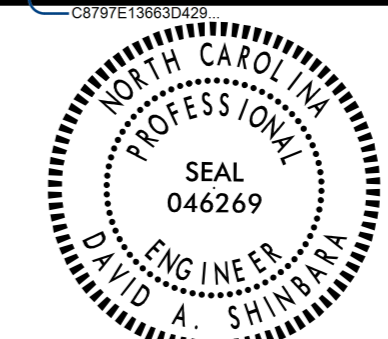
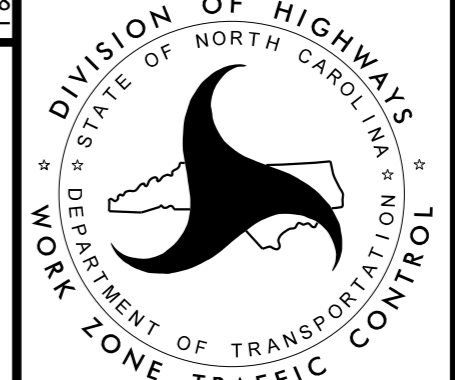
- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

Kimley»Horn

APPROVED: <i>David Shinkham</i> DATE: 6/29/2018 		<h2 style="margin: 0;">TRANSPORTATION OPERATIONS PLAN</h2>
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GENERAL NOTES (CONT.)

TRAFFIC PATTERN ALTERATIONS

- L) NOTIFY THE ENGINEER THIRTY (30) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- M) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- N) PROVIDE PERMANENT SIGNING.
- O) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- P) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

- Q) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRANSPORTATION MANAGEMENT PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE / RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRANSPORTATION MANAGEMENT PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

- R) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS: (SEE ALSO 1101.05)

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45 - 50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

- S) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS) AND 1135 (CONES) FOR ADDITIONAL REQUIREMENTS.
- T) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- U) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- V) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	PAINT	NONE

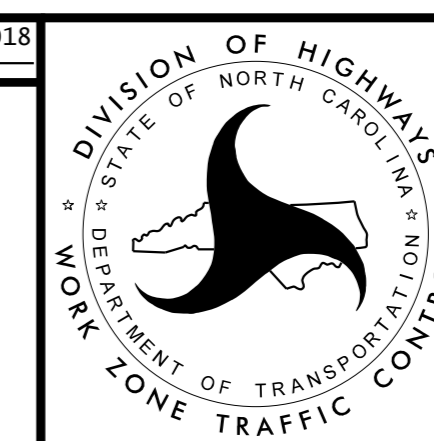
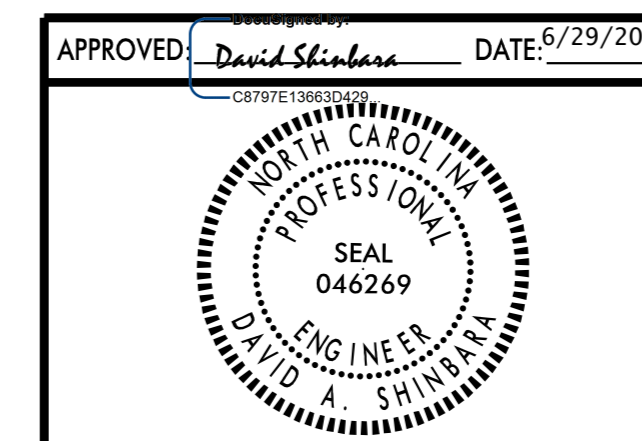
- W) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- X) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- Y) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.
- Z) TRACE THE PROPOSED MONOLITHIC ISLAND LOCATIONS WITH PROPER COLOR PAVEMENT MARKINGS PRIOR TO REMOVAL AND INSTALLATION. PLACE DRUMS TO DELINEATE ANY EXISTING AND PROPOSED MONOLITHIC ISLANDS AFTER REMOVAL AND BEFORE INSTALLATION.

MISCELLANEOUS

- AA) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

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**TRANSPORTATION
OPERATIONS
PLAN**

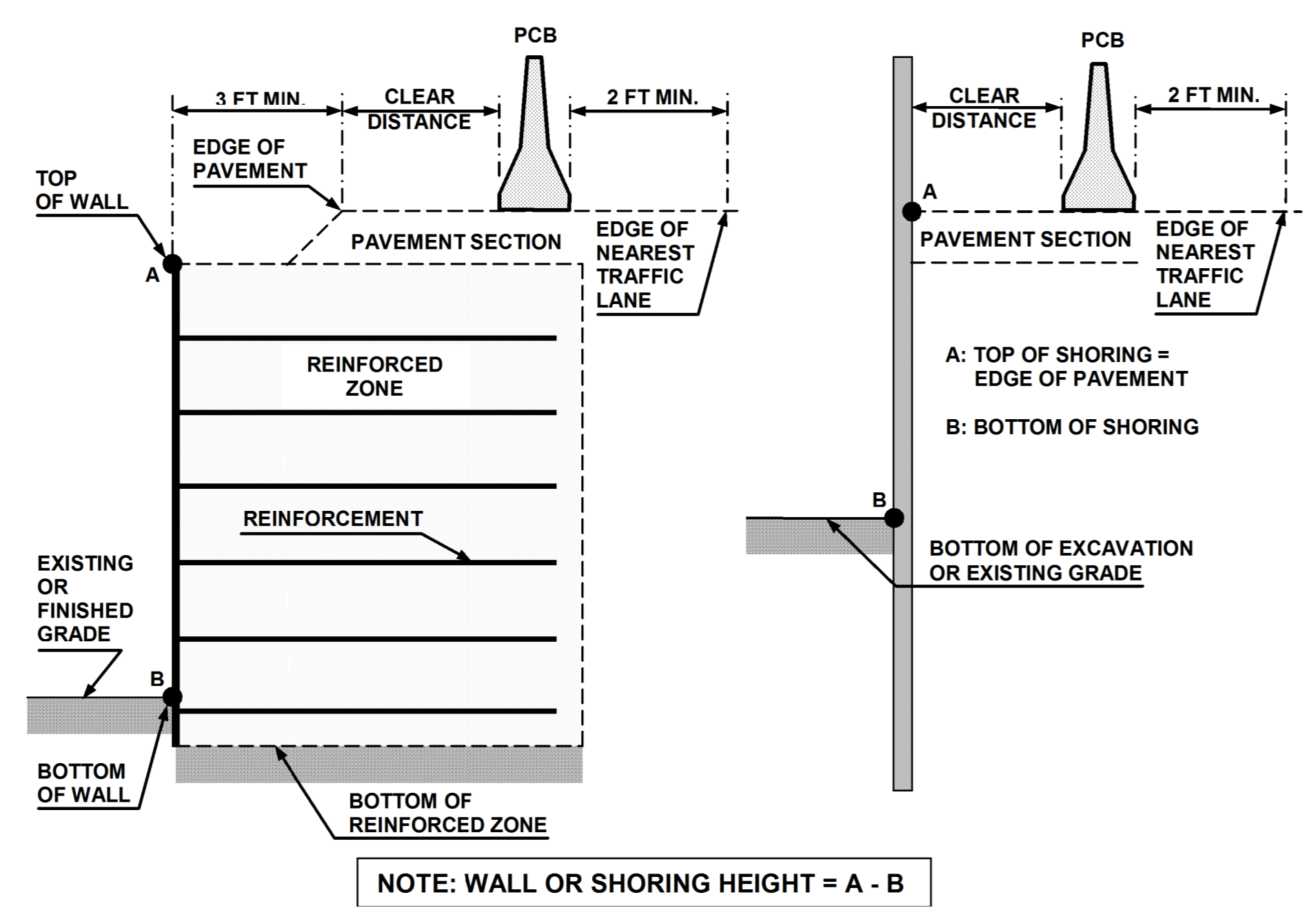


FIGURE A

NOTES

- 1- REFER TO THE TRAFFIC CONTROL PLANS FOR TEMPORARY SHORING LOCATIONS AND NOTES.
- 2- REFER TO THE "TEMPORARY SHORING" PROJECT SPECIAL PROVISION FOR INFORMATION ABOUT TEMPORARY SHORING AND PORTABLE CONCRETE BARRIER (PCB).
- 3- PCB IS REQUIRED IF TEMPORARY SHORING IS LOCATED WITHIN THE CLEAR ZONE IN ACCORDANCE WITH THE AASHTO ROADSIDE DESIGN GUIDE. DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE. (CONTACT NCDOT PAVEMENT MANAGEMENT UNIT FOR APPLICABLE PAVEMENT DESIGN).
- 4- BASED ON THE CLEAR DISTANCE, OFFSET, DESIGN SPEED AND PAVEMENT TYPE, CHOOSE AN UNANCHORED OR ANCHORED PCB FROM THE TABLE SHOWN IN FIGURE B. CLEAR DISTANCE IS DEFINED AS SHOWN IN FIGURE A AND OFFSET IS DEFINED AS SHOWN IN FIGURE B.
- 5- AT THE CONTRACTOR'S OPTION OR IF THE MINIMUM REQUIRED CLEAR DISTANCE IS NOT AVAILABLE, SET PCB NEXT TO AND UP AGAINST THE TRAFFIC SIDE OF THE TEMPORARY SHORING EXCEPT FOR BARRIER ABOVE TEMPORARY WALLS. PCB WITH THE MINIMUM REQUIRED CLEAR DISTANCE IS REQUIRED ABOVE TEMPORARY WALLS.
- 6- USE NCDOT PORTABLE CONCRETE BARRIER (PCB) IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1170.01 AND SECTION 1170 OF THE STANDARD SPECIFICATIONS.
- 7- PCB REQUIREMENTS FOR TEMPORARY WALLS APPLY TO TEMPORARY MECHANICALLY STABILIZED EARTH (MSE) WALLS AND TEMPORARY SOIL NAIL WALLS.
- 8- SET PCB WITH A MINIMUM HORIZONTAL DISTANCE OF 2 FT BETWEEN THE FRONT FACE OF THE BARRIER AND THE EDGE OF THE NEAREST TRAFFIC LANE AS SHOWN IN FIGURE A UNLESS OTHERWISE SHOWN IN THE PLANS AND OR AS APPROVED BY THE ENGINEER.
- 9- FOR PCB ABOVE AND BEHIND TEMPORARY WALLS, PROVIDE A MINIMUM DISTANCE OF 3 FT BETWEEN THE EDGE OF PAVEMENT AND THE WALL FACE AS SHOWN IN FIGURE A. IF THESE MINIMUM REQUIRED DISTANCES ARE NOT AVAILABLE, CONTACT THE ENGINEER.
- 10- TABLE SHOWN IN FIGURE B IS BASED ON NCDOT RESEARCH PROJECT NO. 2005-010 WITH VEHICLE TYPE USED FOR NCHRP 350 CRASH TESTS. BARRIER DEFLECTIONS AND RESULTING MINIMUM REQUIRED CLEAR DISTANCES MIGHT VARY SIGNIFICANTLY FOR LARGER HEAVIER VEHICLES, RUNS OF BARRIER LESS THAN 200 FT IN LENGTH AND WET OR DRY PAVEMENT.

MINIMUM REQUIRED CLEAR DISTANCE, inches

Barrier Type	Pavement Type	Offset * ft	Design Speed, mph					
			<30	31-40	41-50	51-60	61-70	71-80
Unanchored PCB	Asphalt	<8	24	26	29	32	36	40
		8-14	26	28	31	35	38	42
		14-20	27	29	34	36	39	43
		20-26	28	31	35	38	40	44
		26-32	29	32	36	39	42	45
		32-38	30	34	38	41	43	46
		38-44	31	34	41	43	45	48
		44-50	31	35	41	43	46	49
		50-56	32	36	42	44	47	50
	>56	32	36	42	45	47	51	
	Concrete	<8	17	18	21	22	25	26
		8-14	19	20	23	25	26	29
		14-20	22	22	24	26	28	31
		20-26	23	24	26	27	30	34
		26-32	24	25	27	28	32	35
		32-38	24	26	27	30	33	36
		38-44	25	26	28	30	34	37
		44-50	26	26	28	32	35	37
50-56		26	26	28	32	35	38	
>56	26	27	29	32	36	38		
Anchored PCB	Asphalt	All Offsets	24 for All Design Speeds					
Anchored PCB	Concrete (including bridge approach slabs)	All Offsets	12 for All Design Speeds					

* See Figure Below

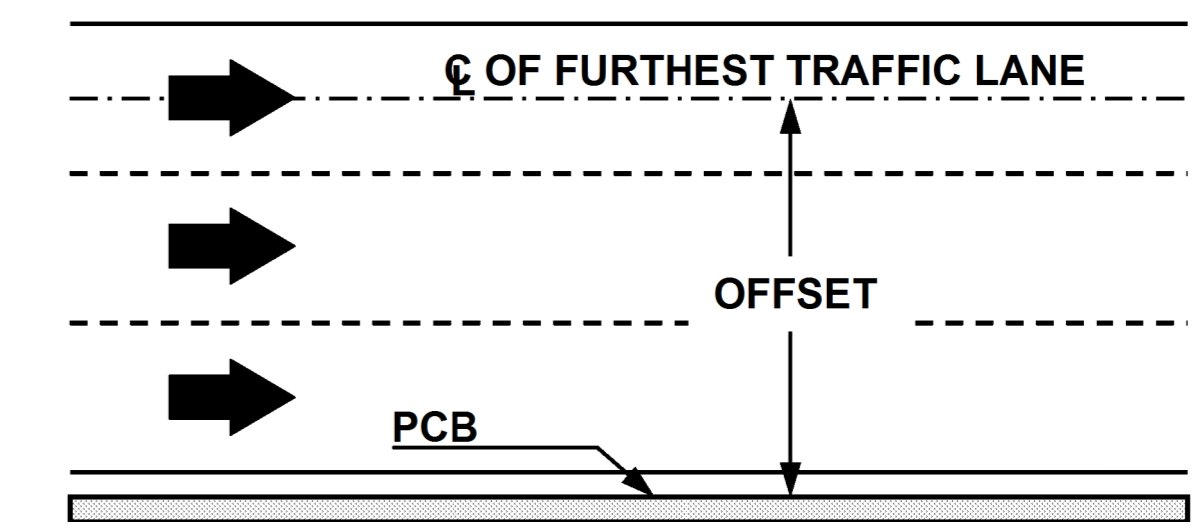


FIGURE B **Kimley»Horn**

APPROVED: <i>David Skinner</i> DATE: 8/15/2018		<p style="text-align: center;">PORTABLE CONCRETE BARRIER AT TEMPORARY SHORING LOCATIONS</p>

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TEMPORARY SHORING NOTES

TEMPORARY SHORING #1:

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION -L- 84+19.89 TO STATION 84+88.79 (1.72 FT. RT TO 1.36 FT. RT) FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

ABOVE ELEVATION 98 FT. +/-

MOIST UNIT WEIGHT = 125 PCF

SATURATED UNIT WEIGHT = 130 PCF

FRICTION ANGLE = 30 DEGREES

COHESION = 0 PSF

BELOW ELEVATION 95 FT. +/-

SATURATED UNIT WEIGHT = 125 PCF

FRICTION ANGLE = 0 DEGREES

COHESION = 1200 PSF

GROUNDWATER ELEVATION 105 FT. +/-

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM STATION -L- STA 84+19.89 TO STA 84+88.79 (1.72 FT. RT TO 1.36 FT. RT). THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED BASED ON INTERPOLATED BORING INFORMATION FROM OUTSIDE OF THE EXISTING ROADWAY EMBANKMENT IN THE VICINITY OF THE PROPOSED CULVERT AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION

DRIVEN PILING FOR TEMPORARY SHORING FROM STATION -L- STA 84+19.89 TO STA 84+88.79 (1.72 FT. RT TO 1.36 FT. RT) MAY NOT PENETRATE BELOW ELEVATION 92 FT. +/- DUE TO HARD SOIL, WEATHERED ROCK OR HARD ROCK

TEMPORARY SHORING #2:

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DESIGN TEMPORARY SHORING FROM STATION -L- STA 84+20.05 TO STA 84+89.00 (11.00 FT. RT TO 8.05 FT. RT) FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:

ABOVE ELEVATION 98 FT. +/-

MOIST UNIT WEIGHT = 125 PCF

SATURATED UNIT WEIGHT = 130 PCF

FRICTION ANGLE = 30 DEGREES

COHESION = 0 PSF

BELOW ELEVATION 95 FT. +/-

SATURATED UNIT WEIGHT = 125 PCF

FRICTION ANGLE = 0 DEGREES

COHESION = 1200 PSF



GROUNDWATER ELEVATION 105 FT. +/-

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF TEMPORARY SHORING FROM -L- STA 84+20.05 TO STA 84+89.00 (11.00 FT. RT TO 8.05 FT. RT). THE INFORMATION PROVIDED FOR TEMPORARY SHORING DESIGN WAS ASSUMED BASED ON INTERPOLATED BORING INFORMATION FROM OUTSIDE OF THE EXISTING ROADWAY EMBANKMENT IN THE VICINITY OF THE PROPOSED CULVERT AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

DRIVEN PILING FOR TEMPORARY SHORING FROM -L- STA 84+20.05 TO STA 84+89.00 (11.00 FT. RT TO 8.05 FT. RT) MAY NOT PENETRATE BELOW ELEVATION 92 FT. +/- DUE TO HARD SOIL, WEATHERED ROCK OR HARD ROCK

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Kimley»Horn

APPROVED <i>Shafiq Rahman</i> DATE 6/29/2018 		TEMPORARY SHORING NOTES
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PHASING

PHASE I

WHILE MAINTAINING TRAFFIC USING RSD 1101.04 FOR SHOULDER CLOSURES AND RSD 1101.02 FOR LANE CLOSURES AS NECESSARY, PERFORM THE FOLLOWING AS SHOWN ON SHEETS TMP-4 THRU TMP-13;

STEP 1: INSTALL ADVANCE WARNING SIGNS IN ACCORDANCE WITH RSD 1101.01

STEP 2: AWAY FROM TRAFFIC BEGIN CONSTRUCTION OF NEW LOCATION -L1- PREMIER BOULEVARD/THREE BRIDGES ROAD, -Y3- AMERICAN LEGION ROAD, -Y5- SHERATON DRIVE, NEW BRIDGE ON -L1- PREMIER BOULEVARD, AND NEW CULVERT ON -L- NC 125

STEP 3: USING THE TEMPORARY LANE CLOSURE PATTERN SHOWN ON SHEET TMP-4A:
 - REMOVE AND PATCH EXIST ISLAND ON SHERATON DRIVE
 - INSTALL TEMPORARY PAVEMENT MARKINGS ON -Y5- SHERATON DRIVE
 - SHIFT TRAFFIC ONTO TEMPORARY PATTERN
 - CONSTRUCT PROPOSED IMPROVEMENTS ON -Y5- INCLUDING BUT NOT LIMITED TO CURB AND GUTTER, NEW PAVEMENT, TEMPORARY PAVEMENT, AND ASSOCIATED UTILITIES
 - PROVIDE WEDGING AS NEEDED TO ENSURE SMOOTH TRANSITIONS AND POSITIVE DRAINAGE

STEP 4: PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AS SHOWN ON SHEETS TMP-4, TMP-9, TMP-10 AND TMP-12, REMOVE CONFLICTING MARKINGS AND SHIFT TRAFFIC ONTO NEW PATTERN

STEP 5: INSTALL PORTABLE CONCRETE BARRIER AS SHOWN ON SHEETS TMP-4, TMP-12, TMP-9 AND TMP-10. INSTALL TEMPORARY SHORING #1 AS SHOWN ON SHEET TMP-10

STEP 6: CONSTRUCT PROPOSED WIDENING UP TO BUT NOT INCLUDING THE FINAL SURFACE COURSE ALONG -L- NC 125 13+00 TO 56+00 LT (APPROX), 56+00 TO 94+00 RT (APPROX) AND ON -Y5- SHERATON DRIVE
 - INSTALL ASSOCIATED DRAINAGE, WATER LINE, AND SANITARY SEWER LINES
 - INSTALL TEMPORARY SHORING #2 AS REQUIRED AND CONTINUE CULVERT CONSTRUCTION ON -L- NC 125
 - INSTALL PERMANENT SIGNING ONLY IN LOCATIONS WHERE APPLICABLE, REFER TO SIGNING PLANS
 - PROVIDE WEDGING AS NEEDED FOR POSITIVE DRAINAGE AND TO PROMOTE A SMOOTH TRANSITION
 - MAINTAIN ACCESS TO EXISTING DRIVEWAYS AT ALL POSSIBLE TIMES

STEP 6A: USING THE TEMPORARY LANE CLOSURE PATTERN SHOWN ON SHEET TMP-4B:
 - INSTALL PART OF 30" AND 48" RCP RT OF -L- NC 125

STEP 6B: USING THE TEMPORARY LANE CLOSURE PATTERN SHOWN ON SHEET TMP-4C:
 - INSTALL PART OF 30" AND 48" RCP LT OF -L- NC 125

NOTE: STEPS 6A AND 6B TEMPORARY PATTERNS MUST BE IMPLEMENTED IN ACCORDANCE WITH GENERAL NOTES A AND B

PHASE II

NOTE: CONSTRUCTION OF NEW LOCATION -L1- PREMIER BOULEVARD, ASSOCIATED BRIDGE, AND -Y3- AMERICAN LEGION ROAD MAY CONTINUE AWAY FROM TRAFFIC

WHILE MAINTAINING TRAFFIC USING RSD 1101.02 FOR TEMPORARY LANE CLOSURES, PERFORM THE FOLLOWING AS SHOWN ON SHEETS TMP-14 THRU TMP-23:

STEP 1:
 - INSTALL TEMPORARY SIGNAL AT -L- NC-125\ -L1- PREMIER BLVD, COVER SIGNAL HEADS AND DO NOT ACTIVATE (REFER TO SIGNAL PLANS)
 - REMOVE PORTABLE CONCRETE BARRIER AND TEMPORARY CRASH CUSHION FROM PHASE 1 LOCATION AND RESET AS SHOWN ON SHEETS TMP-19 AND TMP-20
 - INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) AND REMOVE CONFLICTING MARKINGS
 - ACTIVATE TEMPORARY SIGNAL, SHIFT TRAFFIC ONTO NEW PATTERN AND INSTALL TRAFFIC CONTROL DEVICES

STEP 2: AWAY FROM TRAFFIC AND BEHIND BARRIER, COMPLETE CULVERT CONSTRUCTION

STEP 3: CONSTRUCT PROPOSED WIDENING UP TO BUT NOT INCLUDING THE FINAL SURFACE COURSE ALONG -L- NC 125 13+00 TO 56+00 RT (APPROX) AND 56+00 TO 94+00 LT (APPROX)
 - INSTALL ASSOCIATED DRAINAGE, WATER LINE, AND SANITARY SEWER LINES
 - REMOVE TEMPORARY STEEL PLATE AND ADJUST STRUCTURE 802 TO FINAL CURB ELEVATION
 - REMOVE OLD LOCATION THREE BRIDGES RD
 - INSTALL PERMANENT SIGNING ONLY IN LOCATIONS WHERE APPLICABLE, REFER TO SIGNING PLANS
 - PROVIDE WEDGING AS NEEDED FOR POSITIVE DRAINAGE AND TO PROMOTE A SMOOTH TRANSITION
 - MAINTAIN ACCESS TO EXISTING DRIVEWAYS AT ALL POSSIBLE TIMES

NOTE: STEPS 2 AND 3 MAY BE PERFORMED CONCURRENTLY

PHASE III

NOTE: CONSTRUCTION OF NEW LOCATION -L1- PREMIER BOULEVARD, ASSOCIATED BRIDGE, AND -Y3- AMERICAN LEGION ROAD MAY CONTINUE AWAY FROM TRAFFIC

WHILE MAINTAINING TRAFFIC USING RSD 1101.02 FOR TEMPORARY LANE CLOSURES, PERFORM THE FOLLOWING AS SHOWN ON SHEETS TMP-24 THRU TMP-31

STEP 1: REMOVE TEMPORARY SHORING, PORTABLE CONCRETE BARRIER, AND TEMPORARY CRASH CUSHION

STEP 2:
 - INSTALL TEMPORARY SIGNAL AT -L- NC-125\ -L1- PREMIER BLVD, COVER SIGNAL HEADS AND DO NOT ACTIVATE (REFER TO SIGNAL PLANS)
 - INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) PLACE THE PAINT VERSION OF THE FINAL MARKINGS IN AREAS NOT ASSOCIATED WITH THE PHASE 3 TEMPORARY TRAFFIC PATTERN (REFER TO PAVEMENT MARKING PLANS)
 - REMOVE CONFLICTING PAVEMENT MARKINGS
 - ACTIVATE TEMPORARY SIGNAL, SHIFT TRAFFIC ONTO TEMPORARY PATTERN AND INSTALL TRAFFIC CONTROL DEVICES

STEP 3: COMPLETE MEDIAN CONSTRUCTION ALONG -L- NC 125, ALL ASSOCIATED DRAINAGE, AND REMOVE EXISTING SHERATON DRIVE

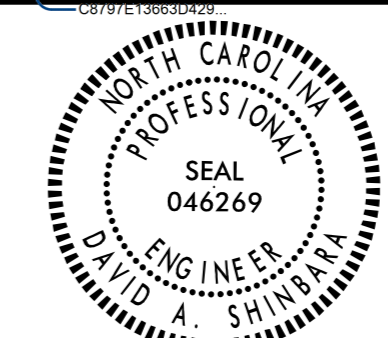
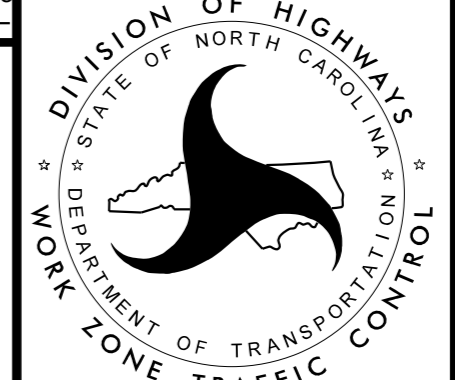
STEP 4: AWAY FROM TRAFFIC, COMPLETE CONSTRUCTION OF NEW LOCATION -L1- PREMIER BOULEVARD, ASSOCIATED BRIDGE, AND -Y3- AMERICAN LEGION ROAD INCLUDING FINAL SURFACE COURSE, FINAL PAVEMENT MARKINGS AND PERMANENT SIGNAGE (REFER TO PAVEMENT MARKING AND SIGNING PLANS)

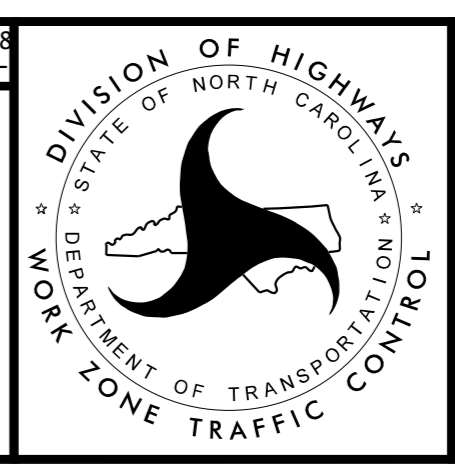
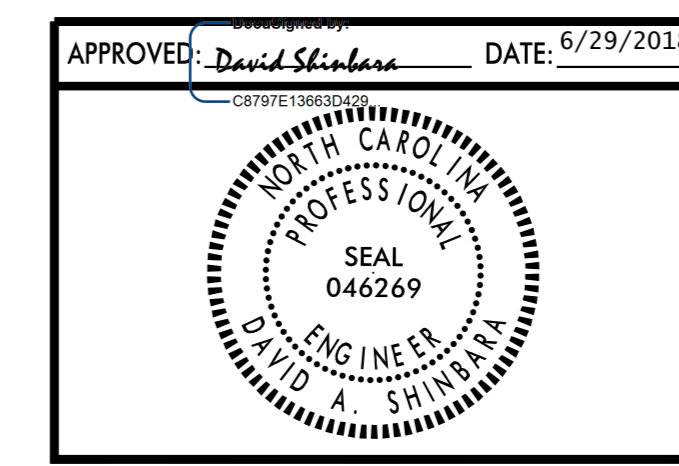
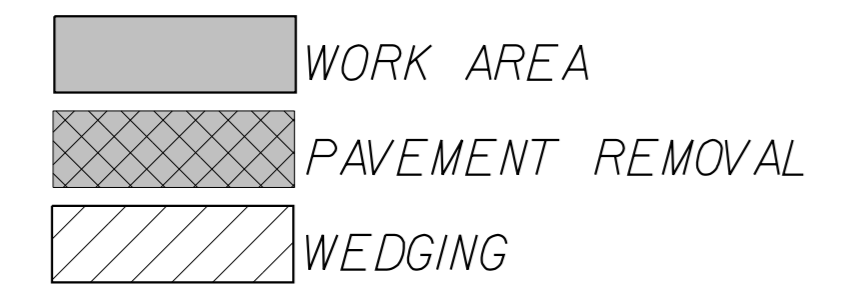
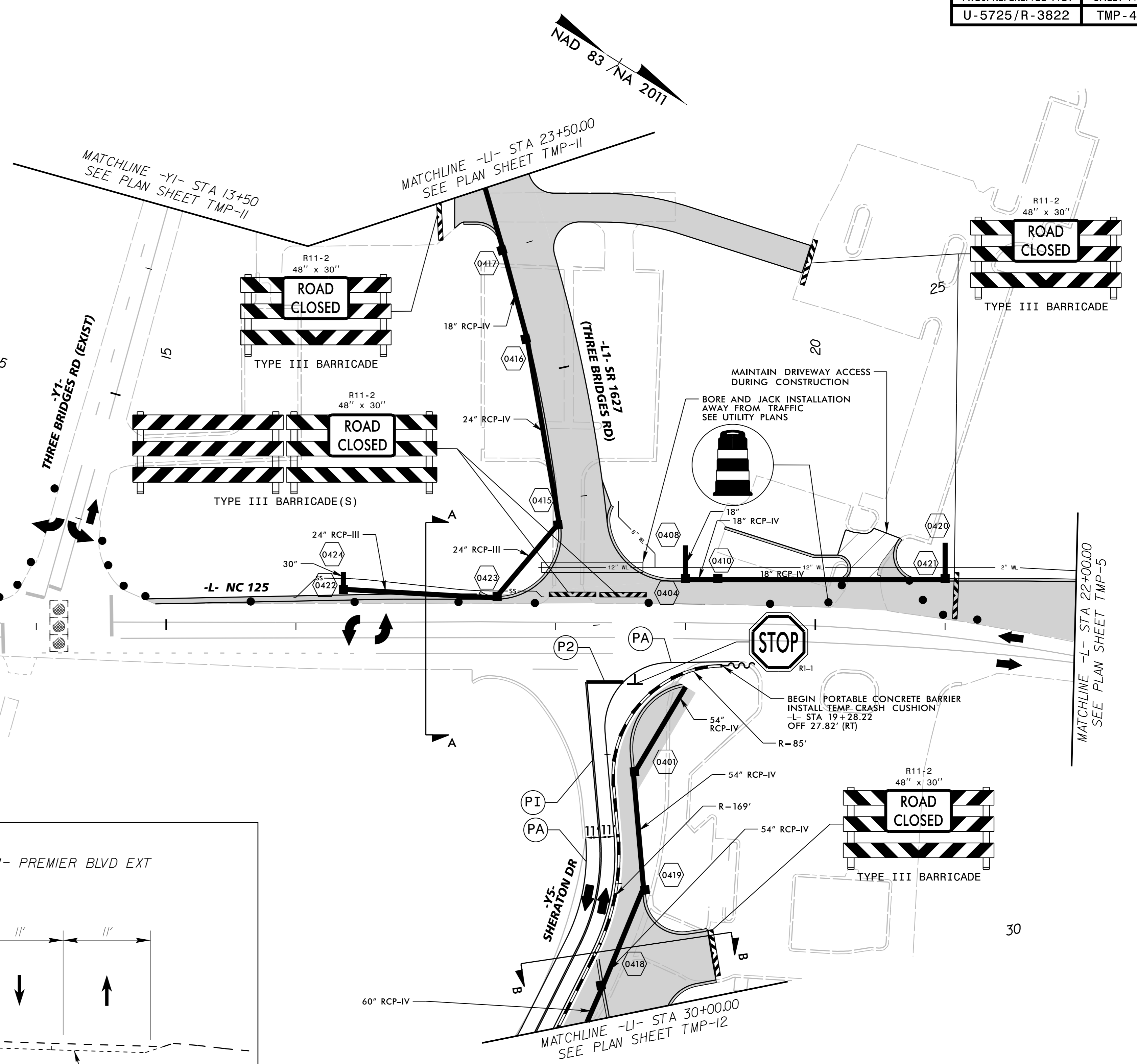
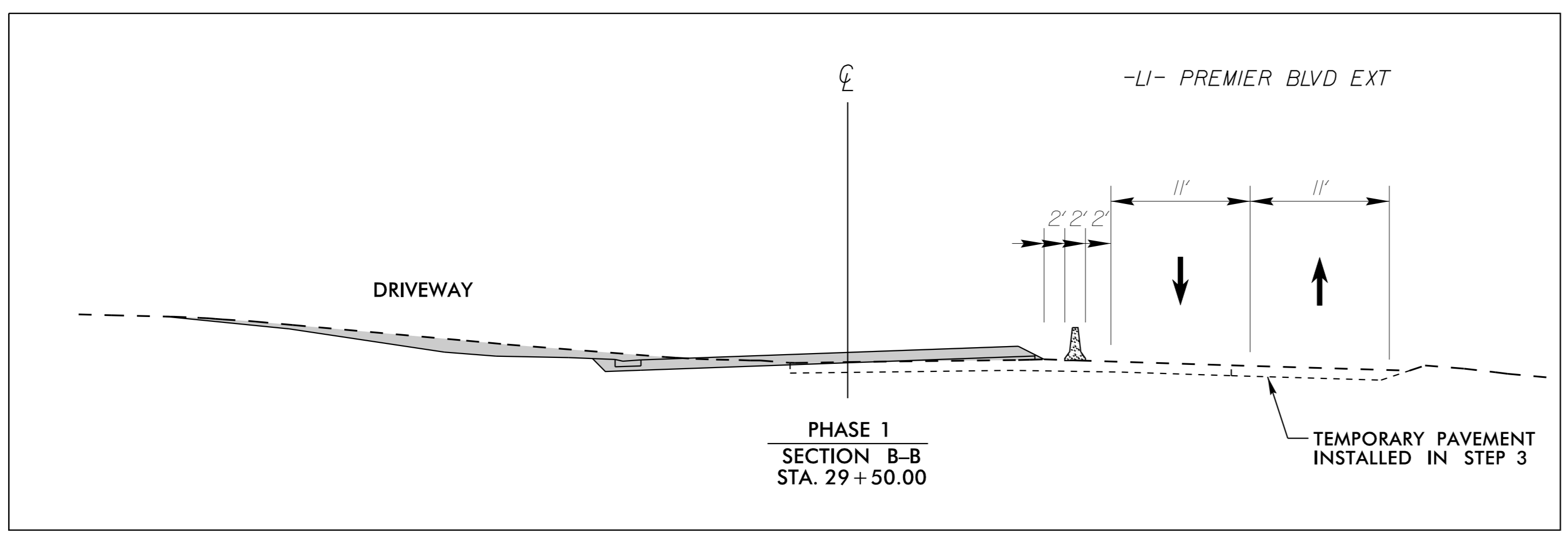
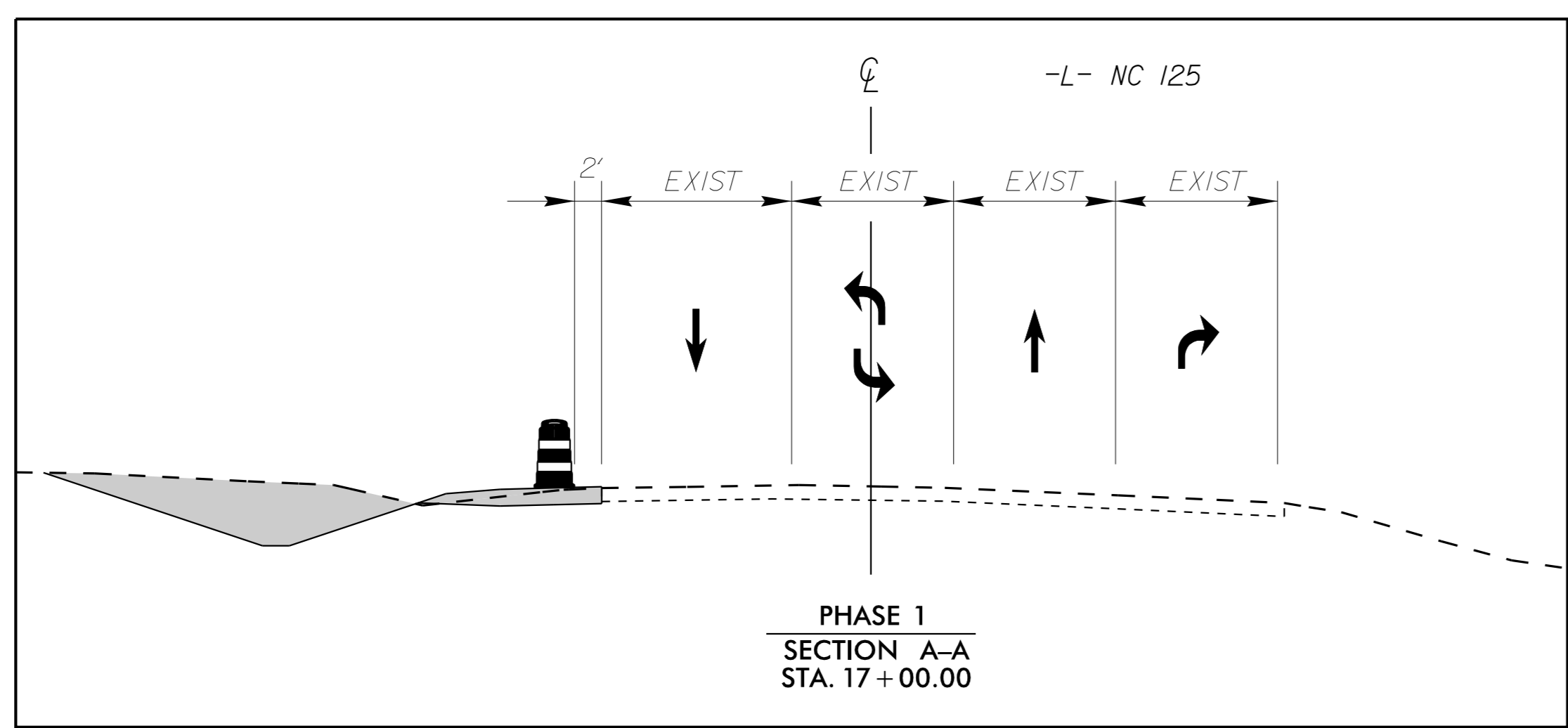
STEP 5: INSTALL FINAL SIGNALS, COVER SIGNAL HEADS, AND DO NOT ACTIVATE.

STEP 6: PLACE FINAL SURFACE COURSE, FINAL PAVEMENT MARKINGS, AND PERMANENT SIGNAGE ON ALL REMAINING ROADS (REFER TO PAVEMENT MARKING AND SIGNING PLANS). ACTIVATE SIGNALS, REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN ALL LANES AND ALL ROADS TO TRAFFIC.

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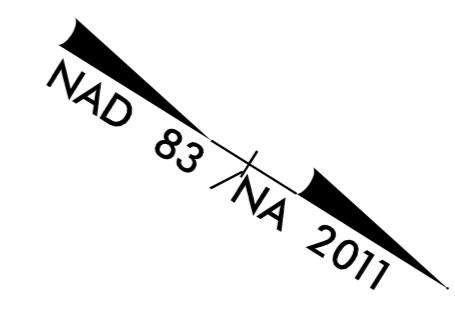
APPROVED: <u>David Steinberg</u> DATE: 6/29/2018 		PHASING
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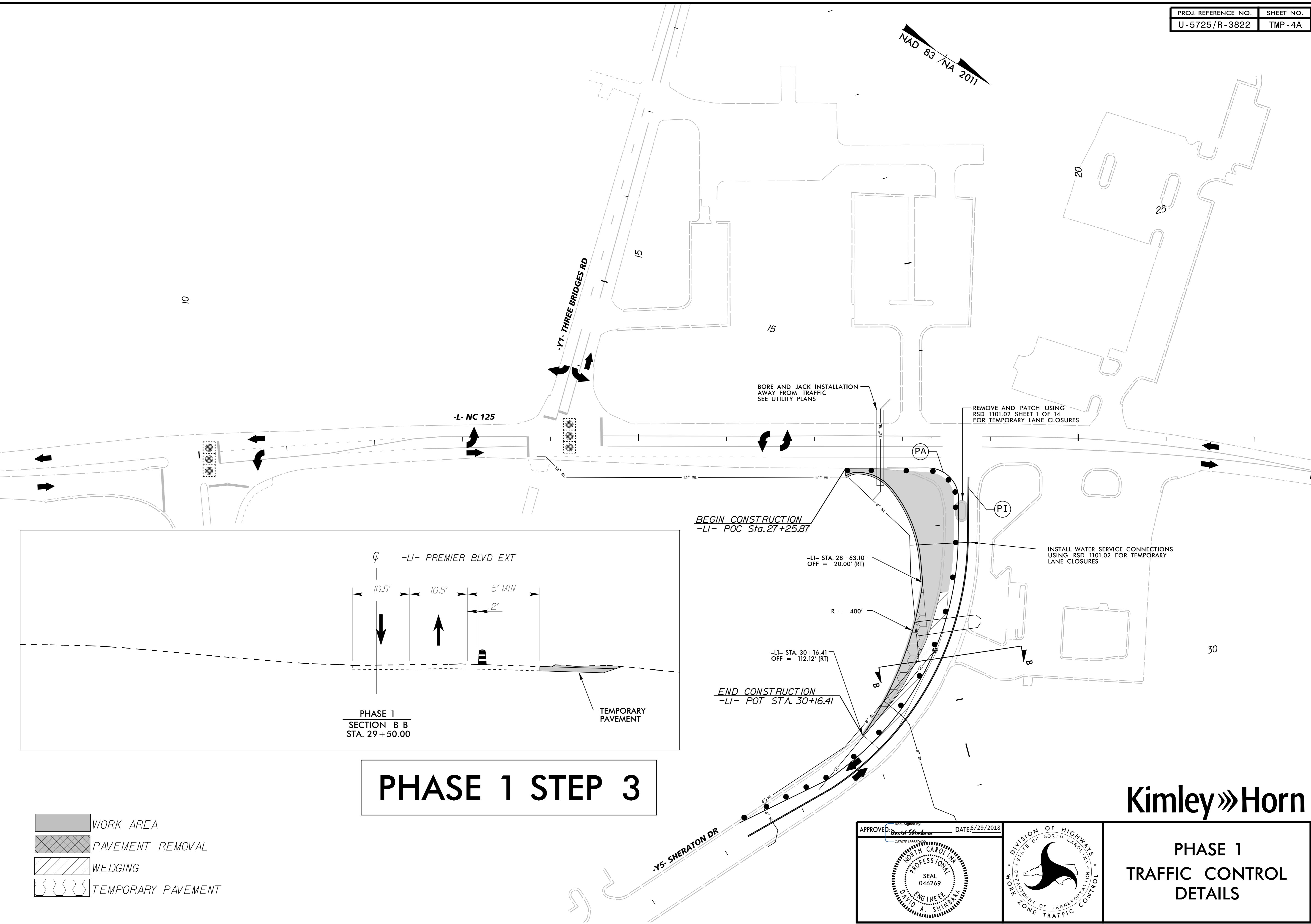
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**PHASE 1
TRAFFIC CONTROL
DETAILS**




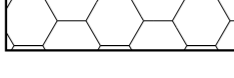
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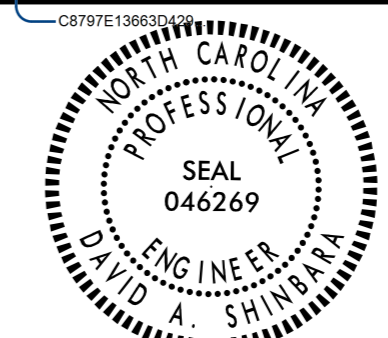
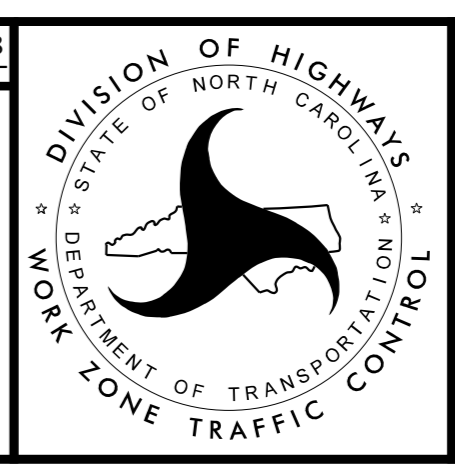
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PHASE 1 STEP 3

-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING
-  TEMPORARY PAVEMENT

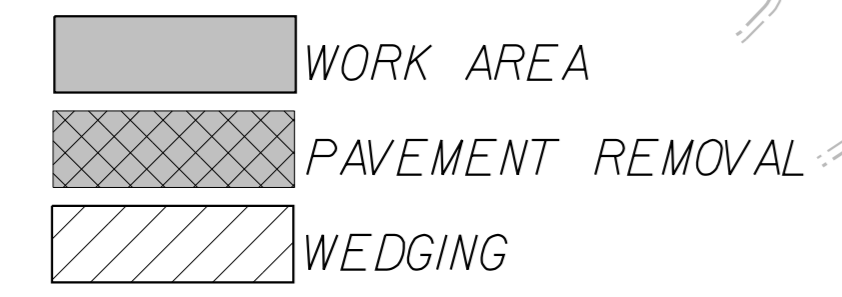
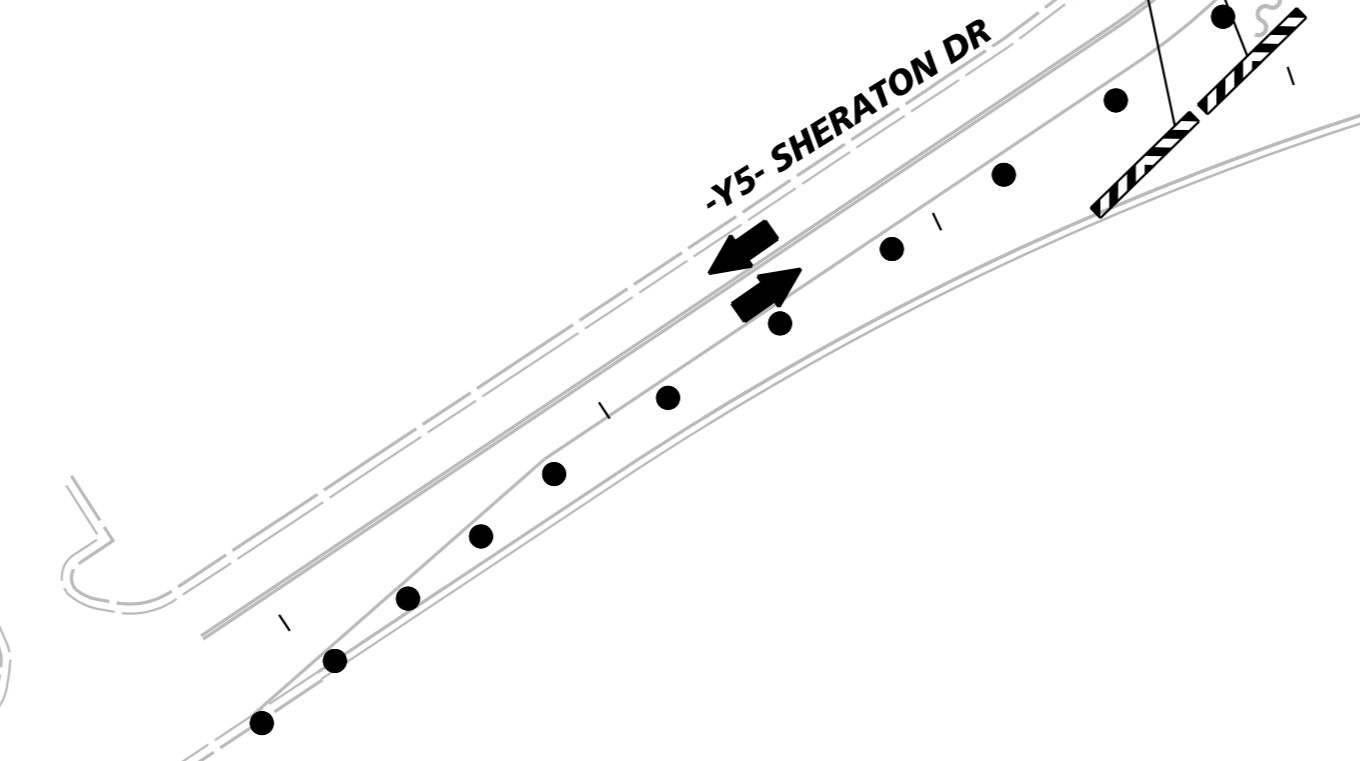
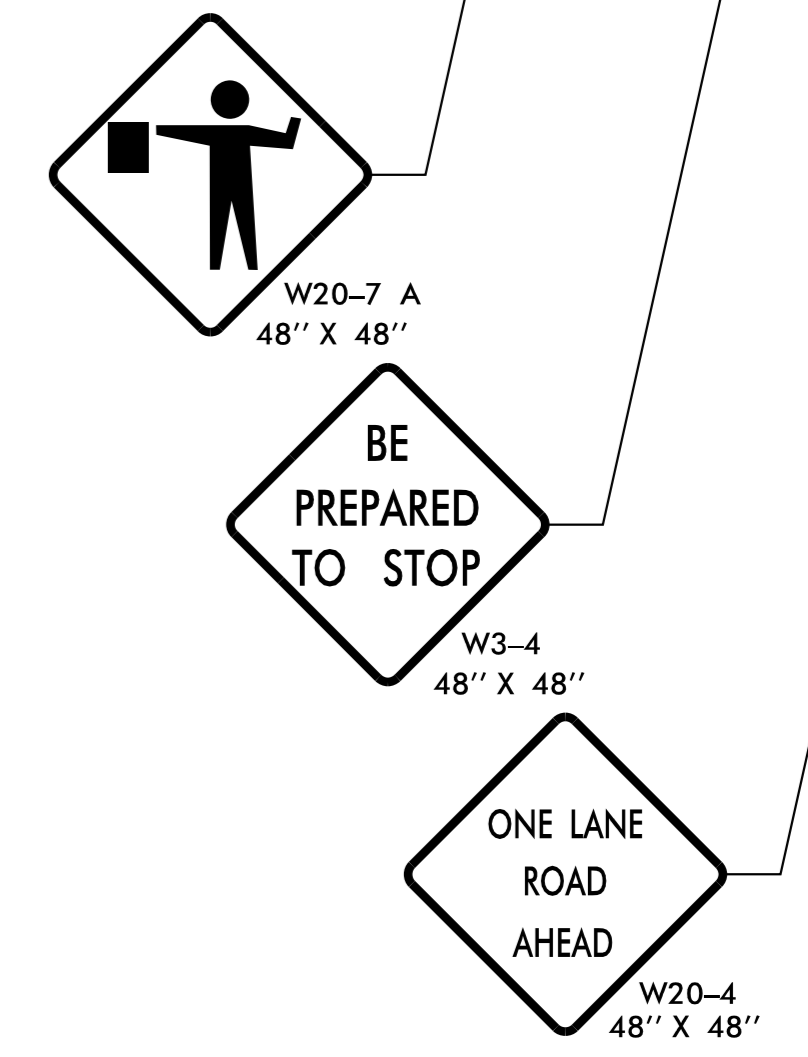
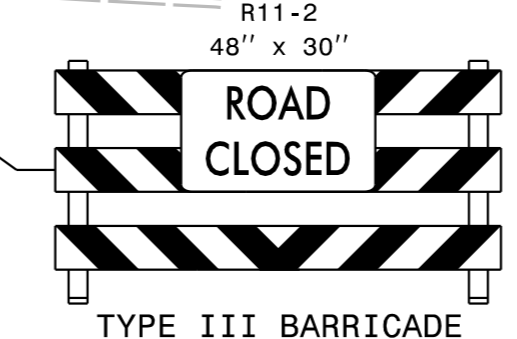
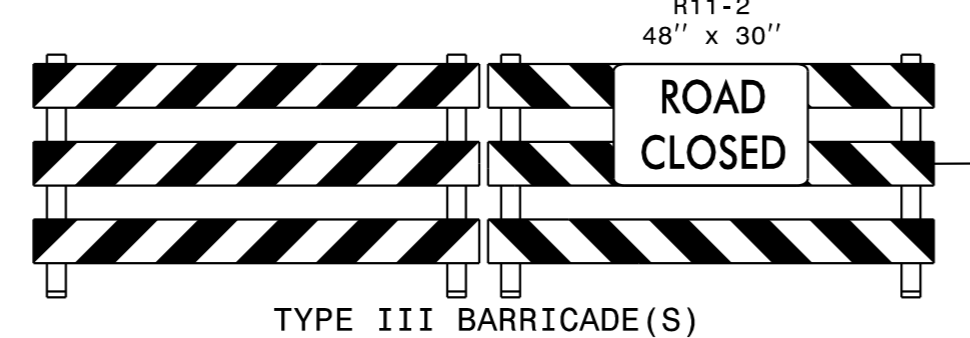
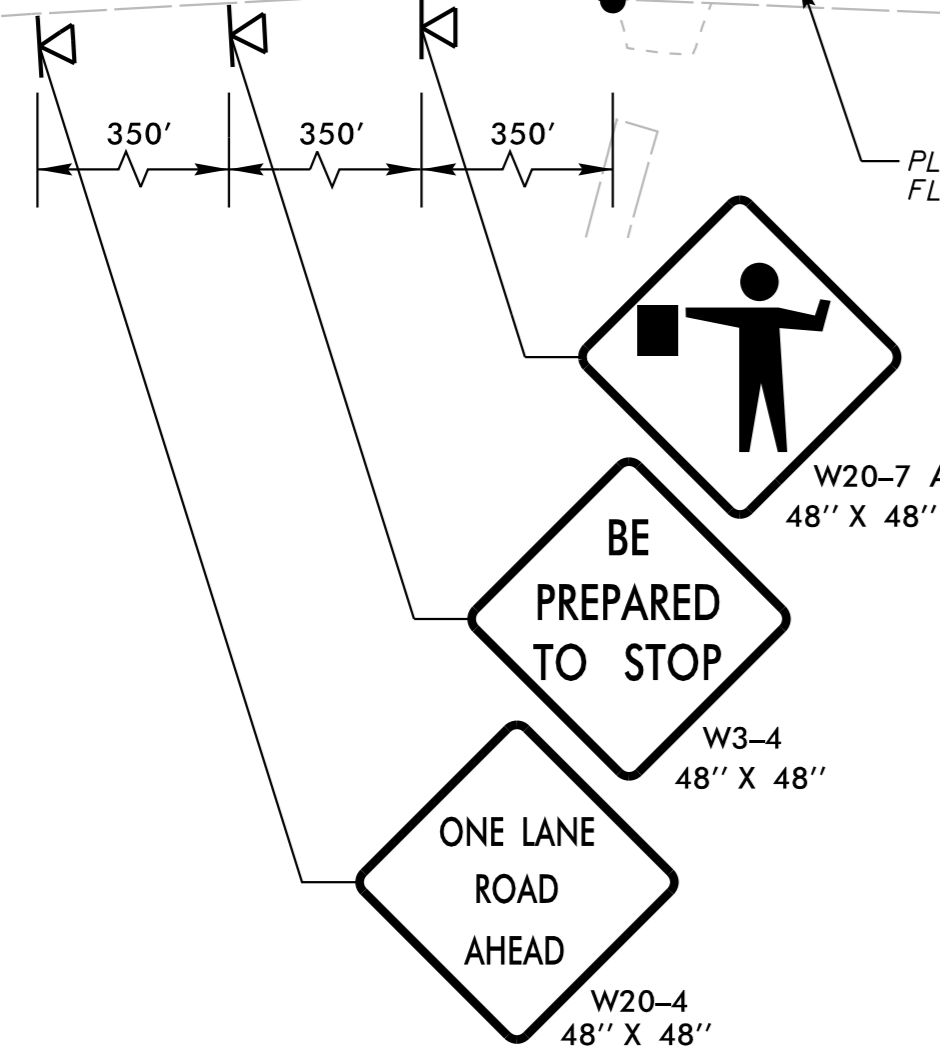
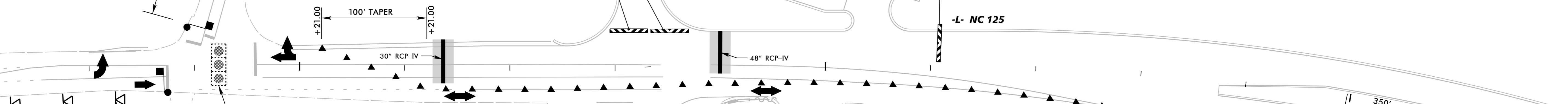
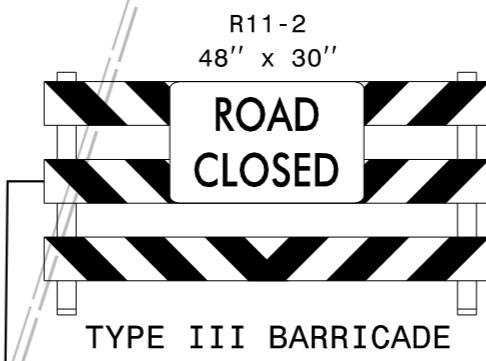
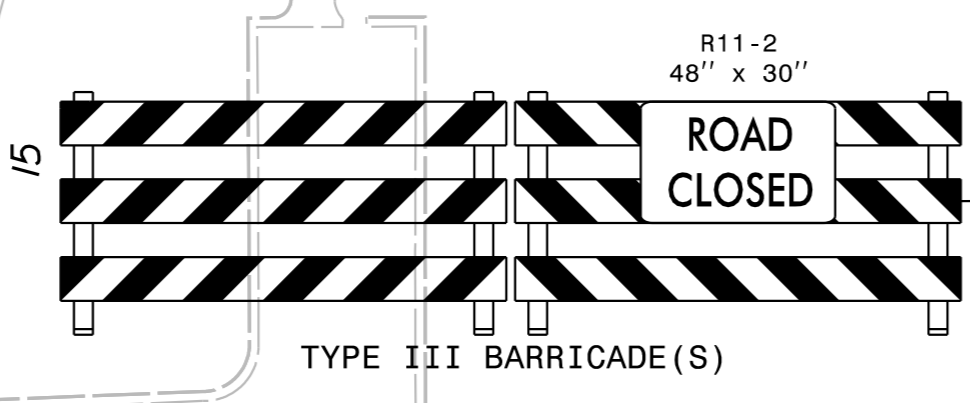
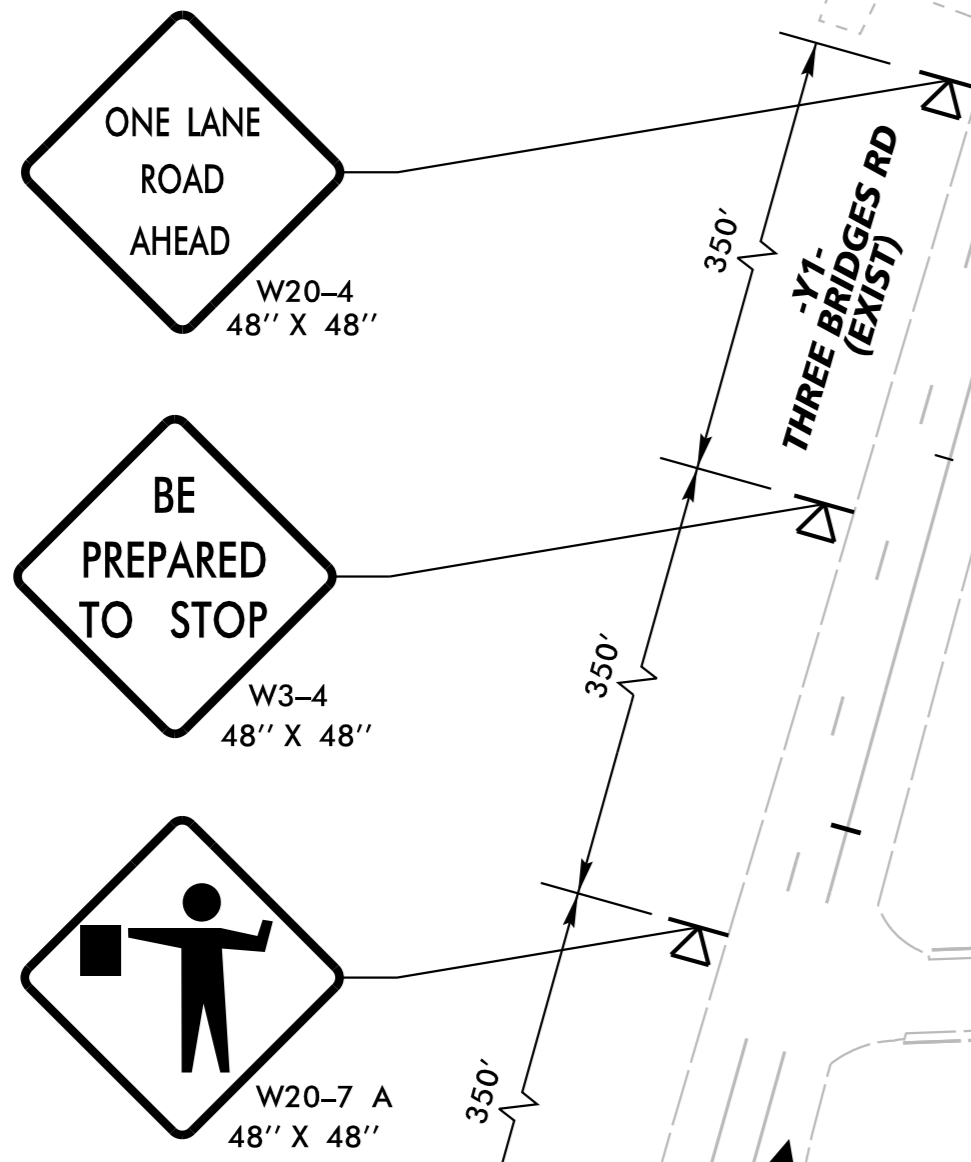
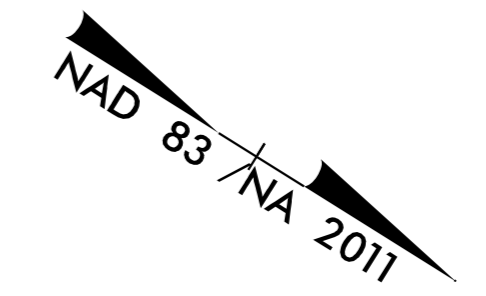
APPROVED: *David Skindera* DATE: 6/29/2018

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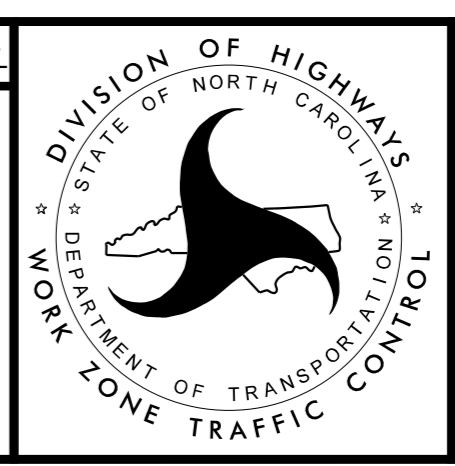
**PHASE 1
TRAFFIC CONTROL
DETAILS**

PHASE 1 STEP 6B



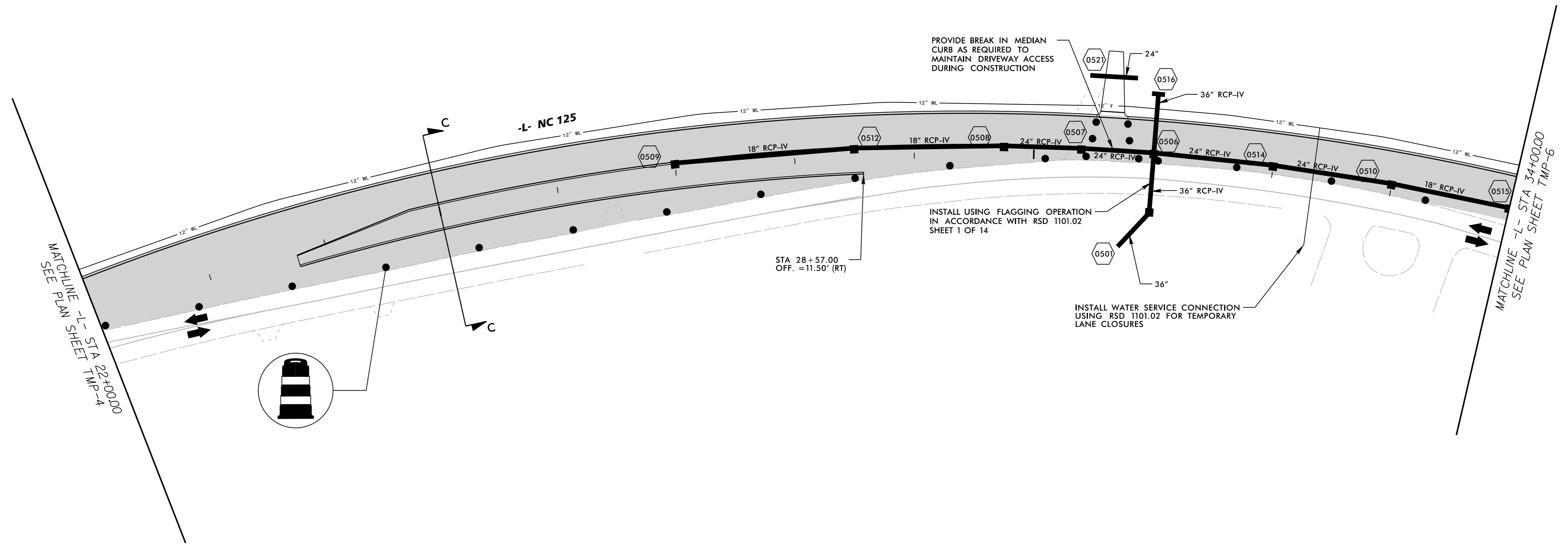
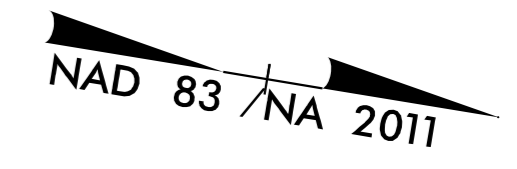
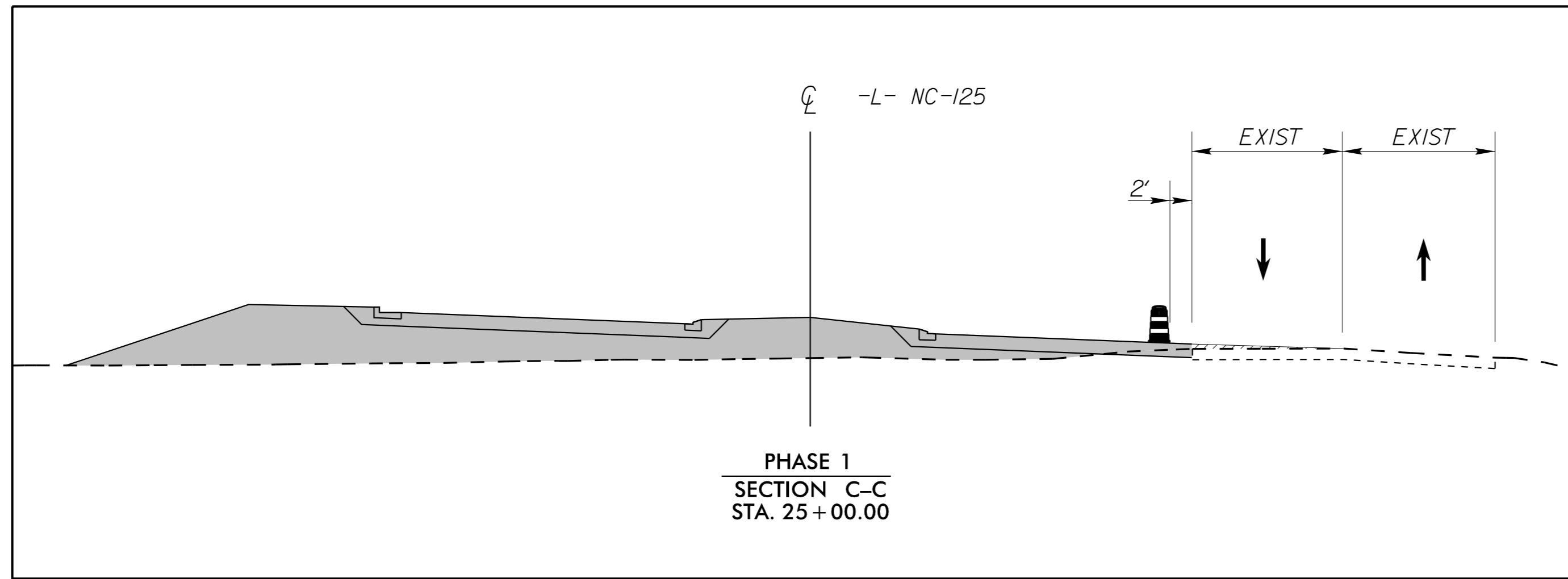
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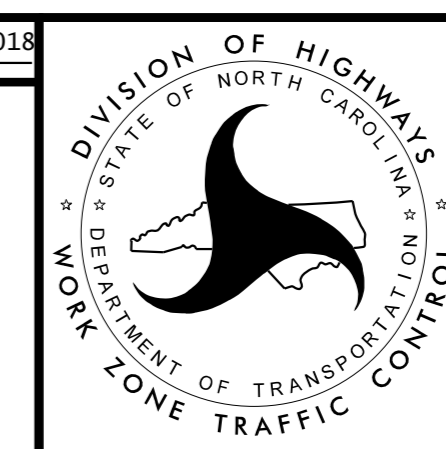
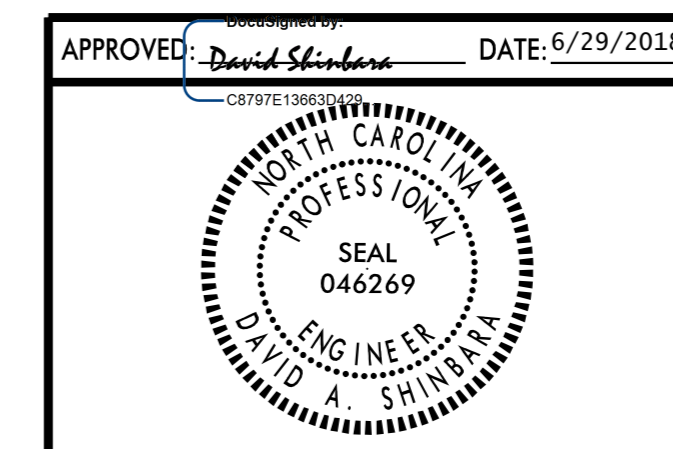
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PHASE 1
TRAFFIC CONTROL
DETAILS

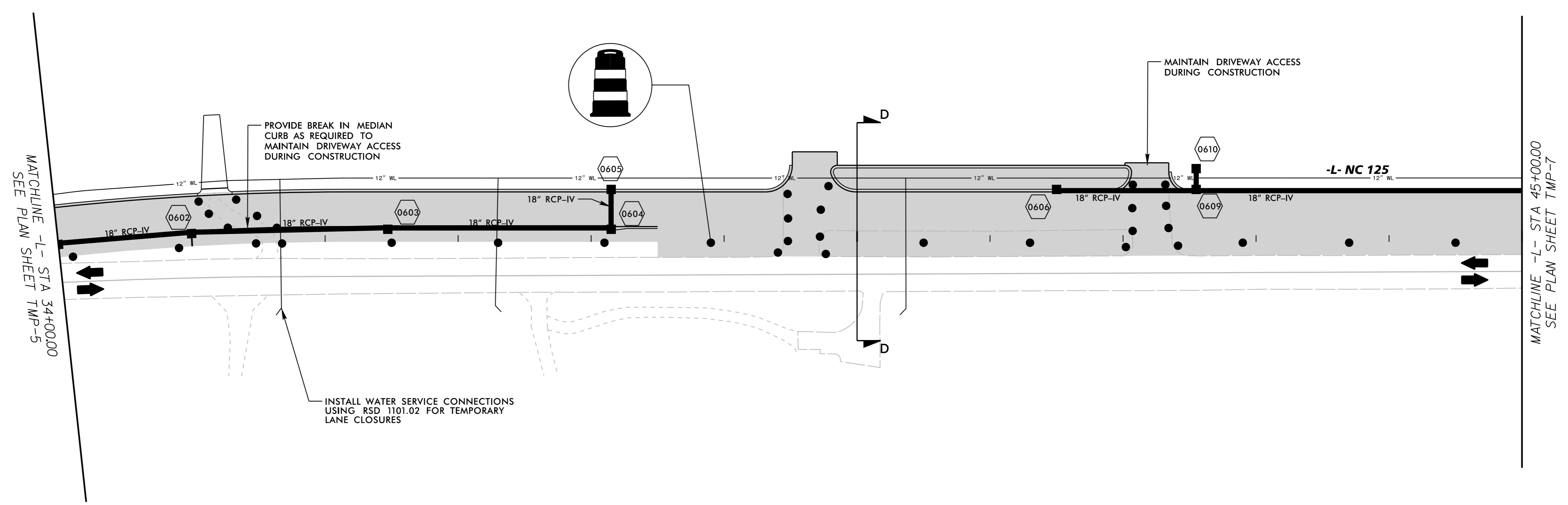
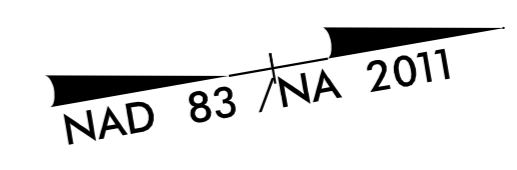
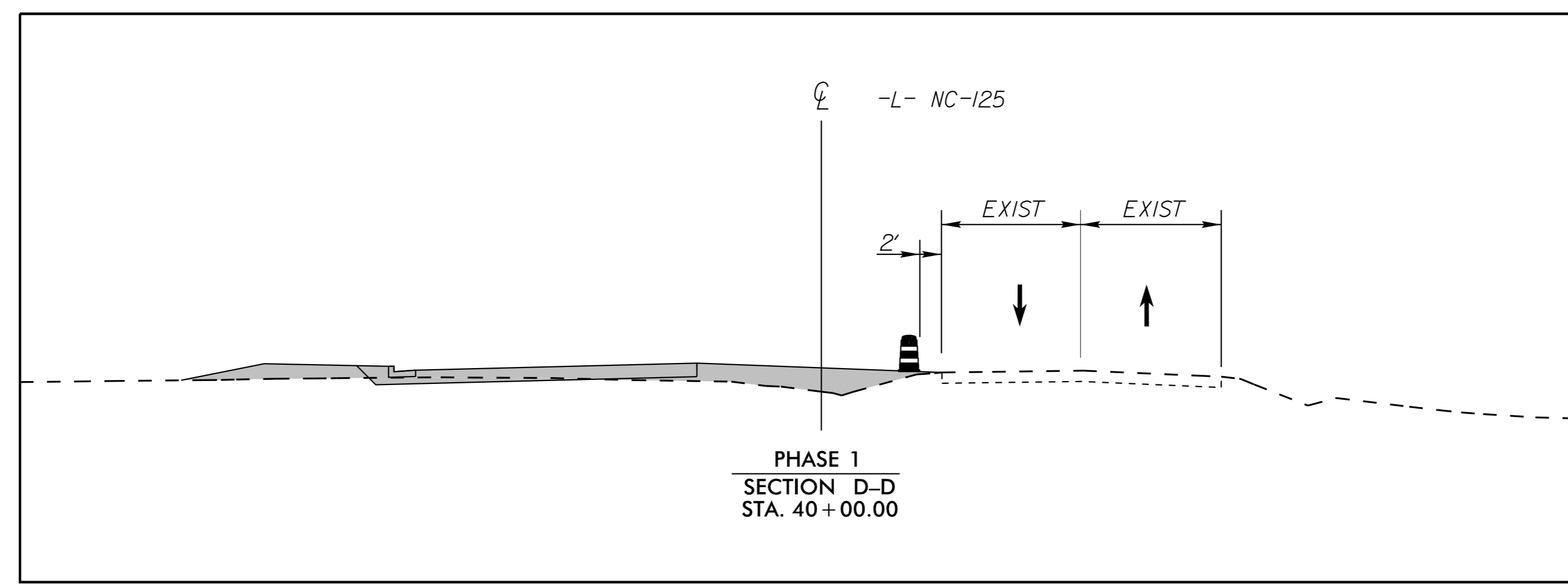


- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

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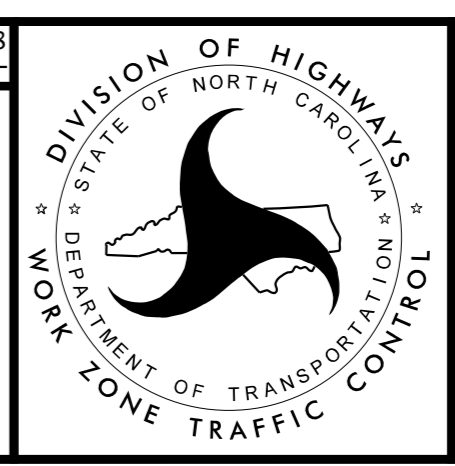


PHASE 1 TRAFFIC CONTROL DETAILS



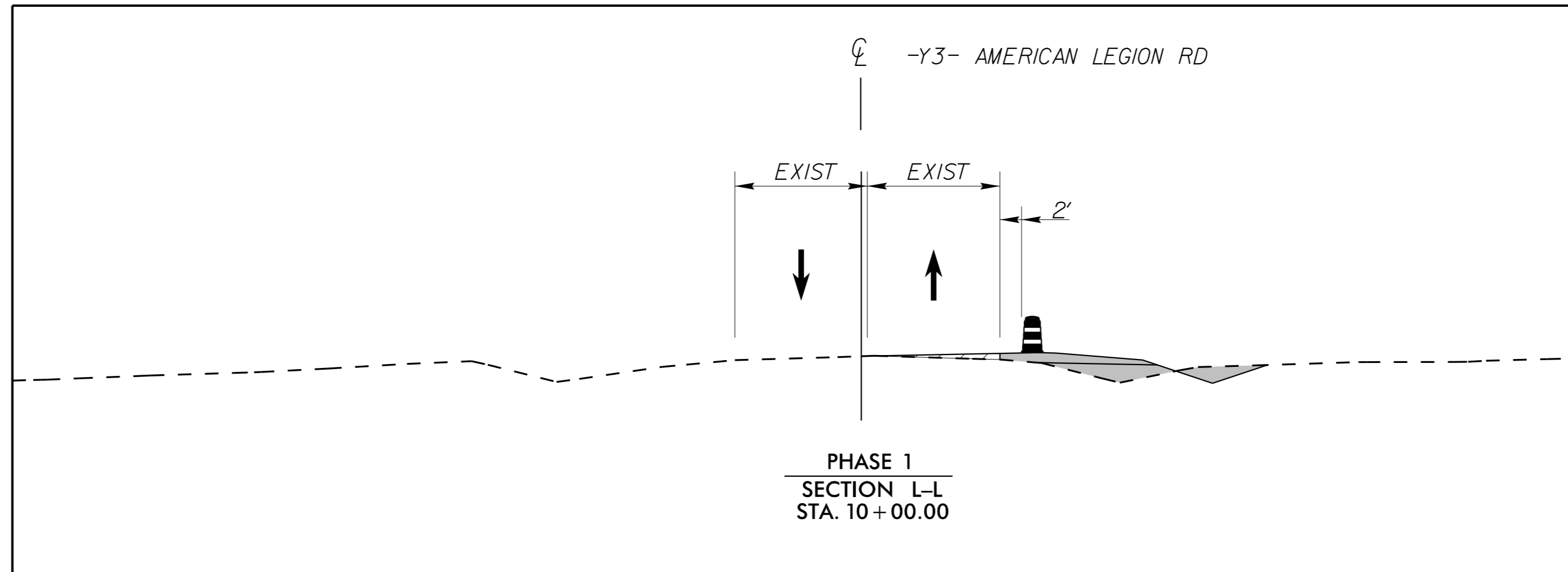
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

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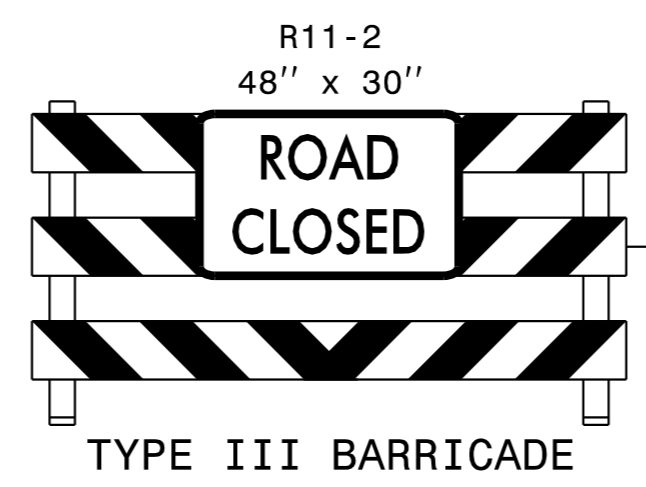


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TRAFFIC CONTROL
DETAILS**

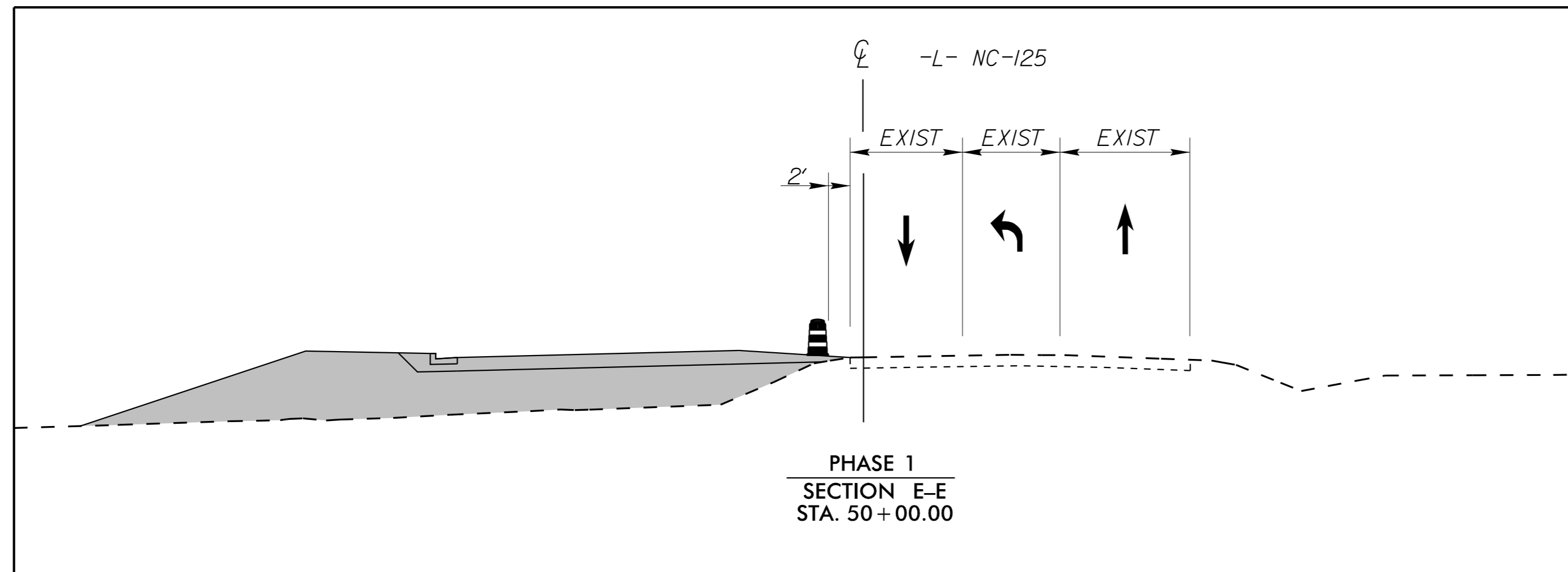
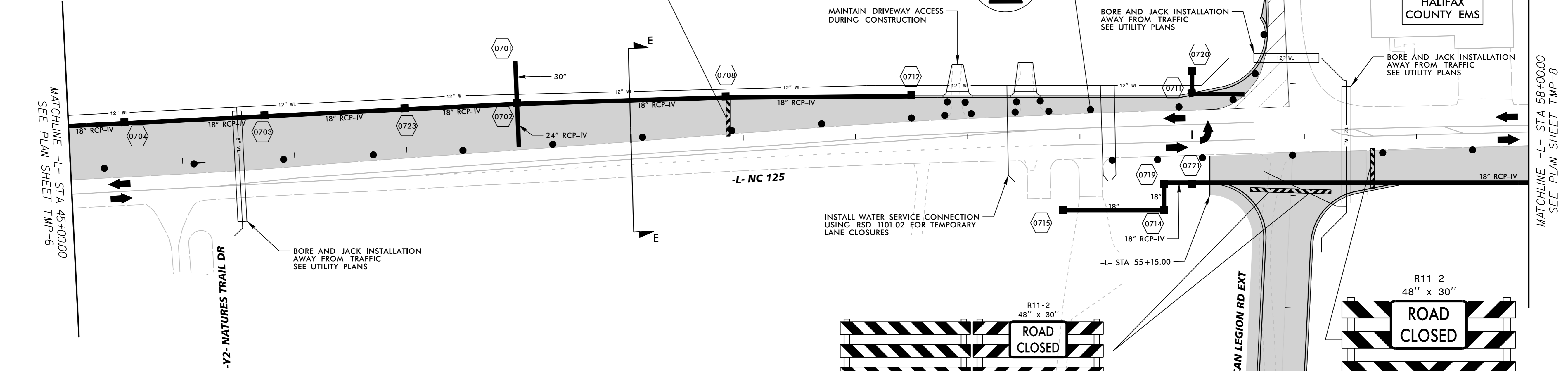
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45
MATCHLINE -L- STA 45+00.00
SEE PLAN SHEET TMP-6



TYPE III BARRICADE



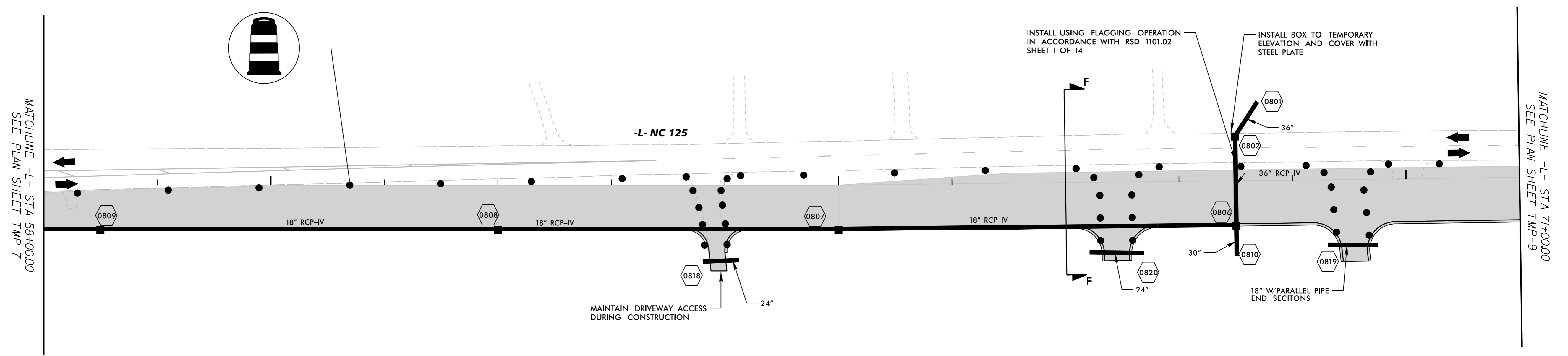
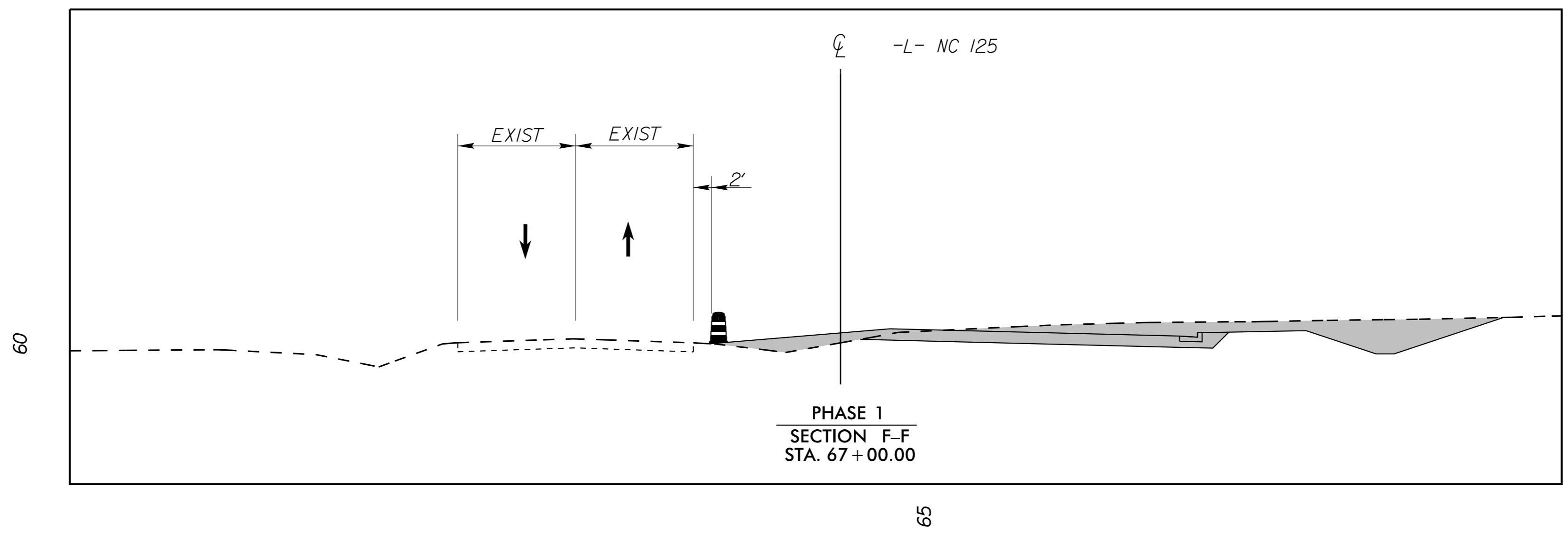
-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

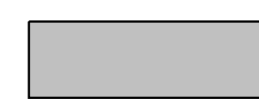
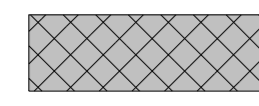
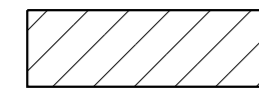
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**PHASE 1
TRAFFIC CONTROL
DETAILS**

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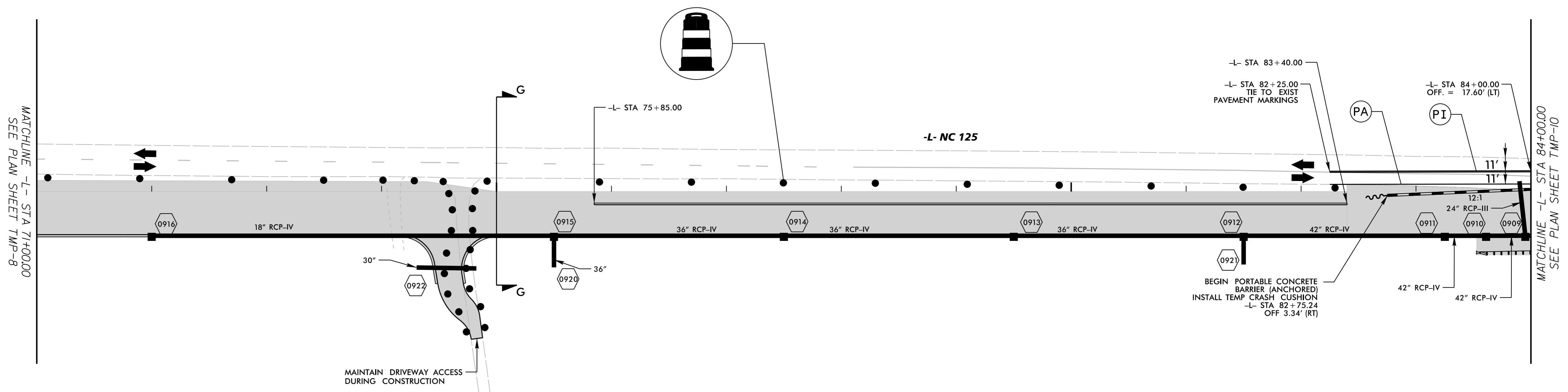
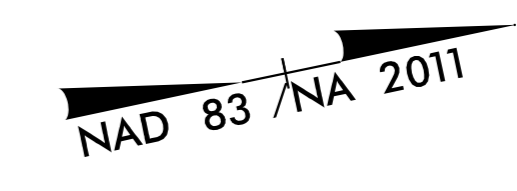
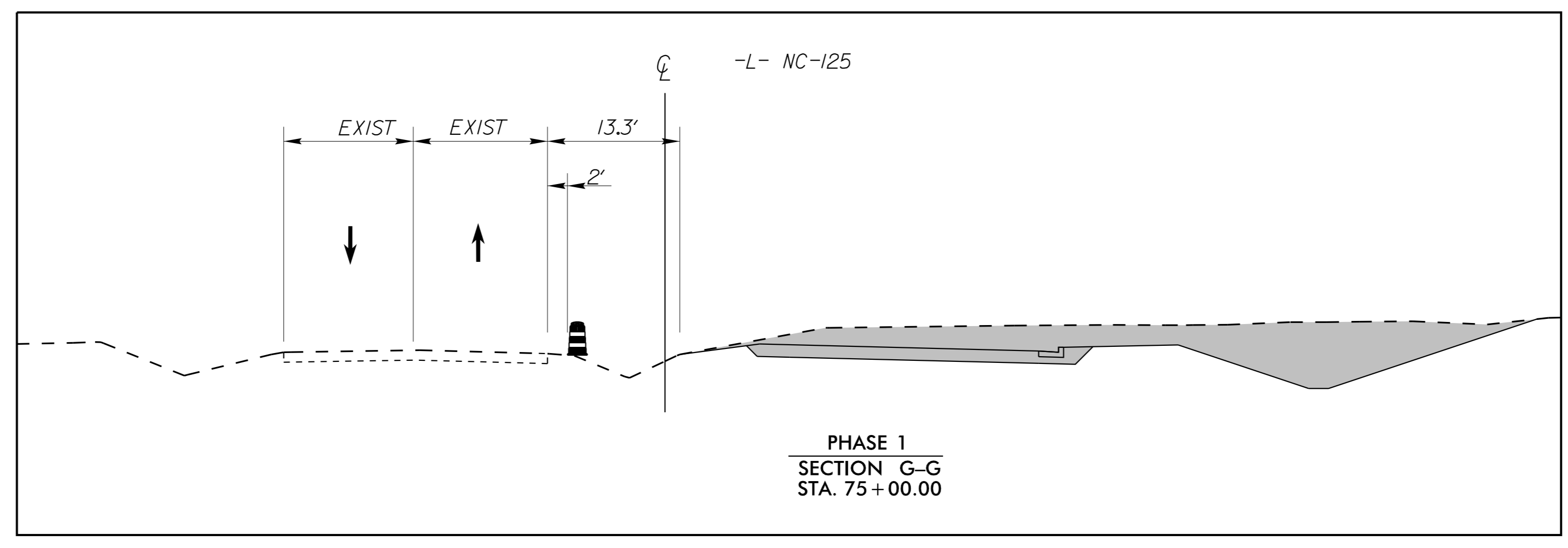
-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

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**PHASE 1
TRAFFIC CONTROL
DETAILS**

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- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

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6/28/2018

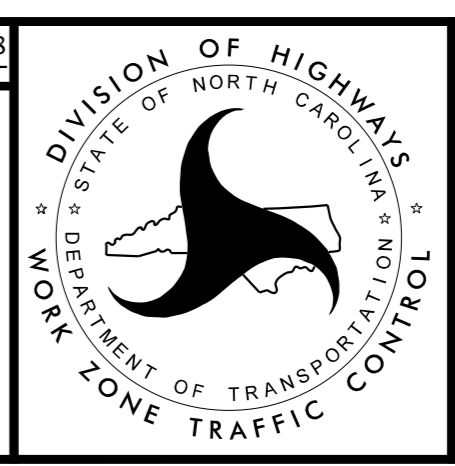
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SEAL 046269

DAVID A. SHINBATA

PROFESSIONAL ENGINEER

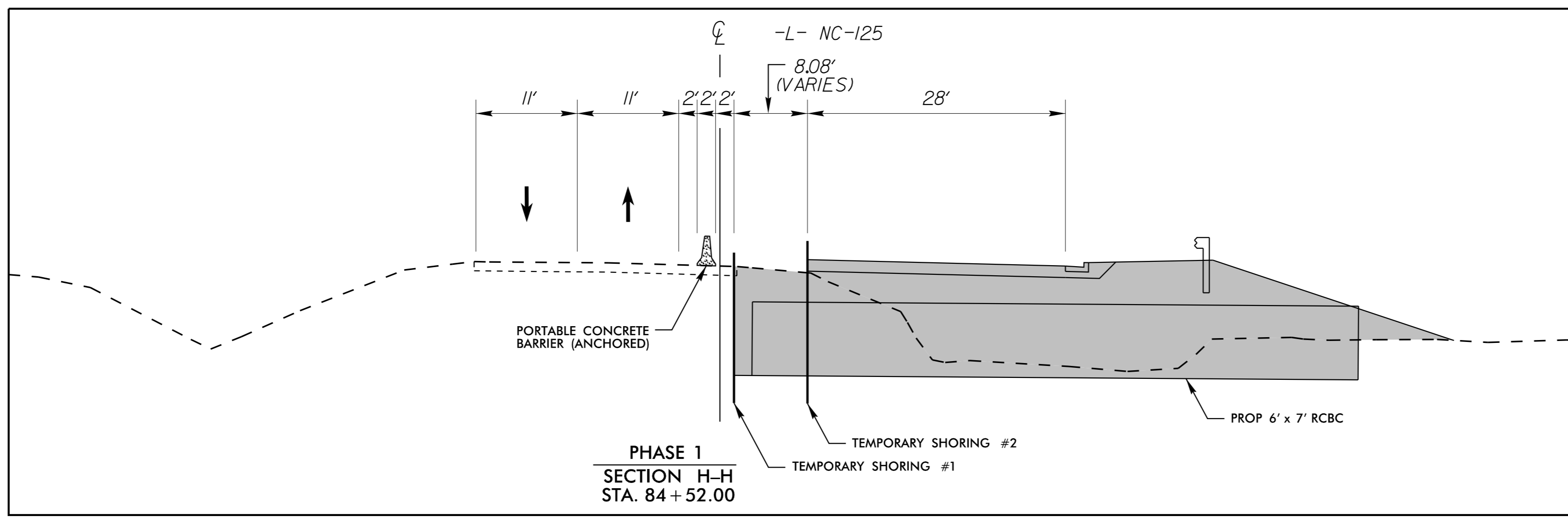
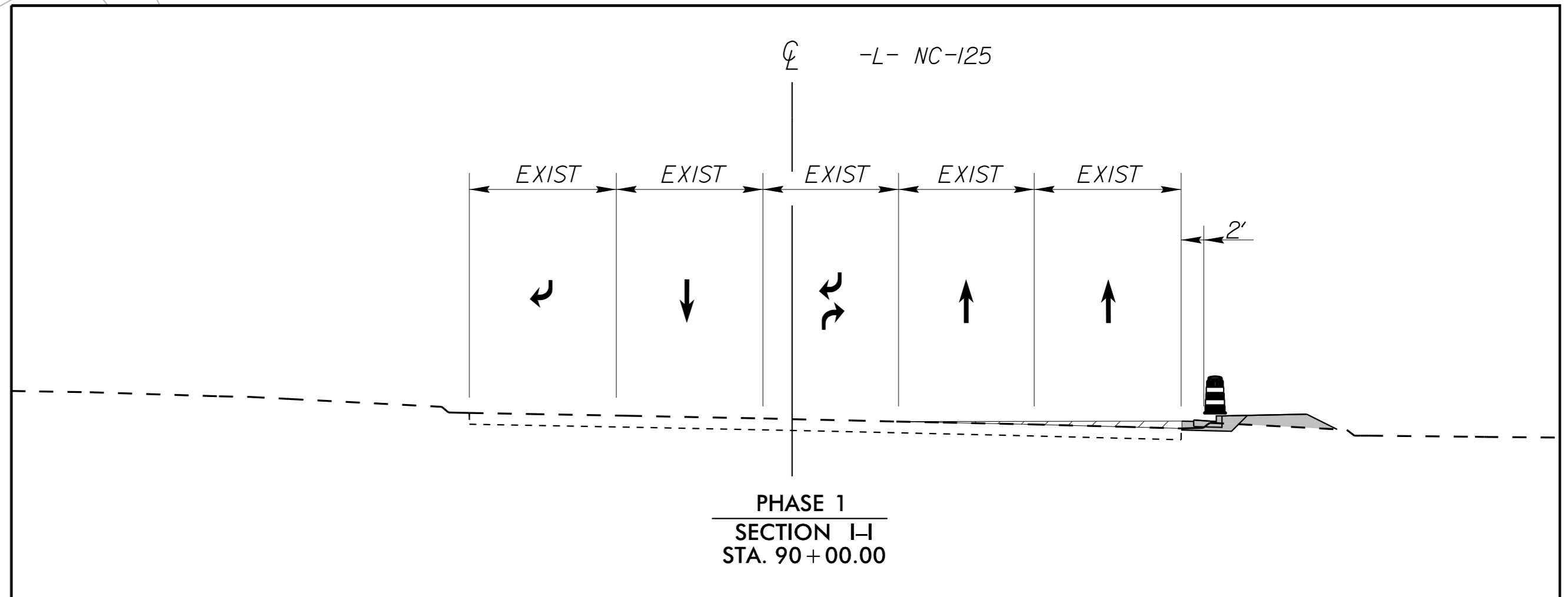
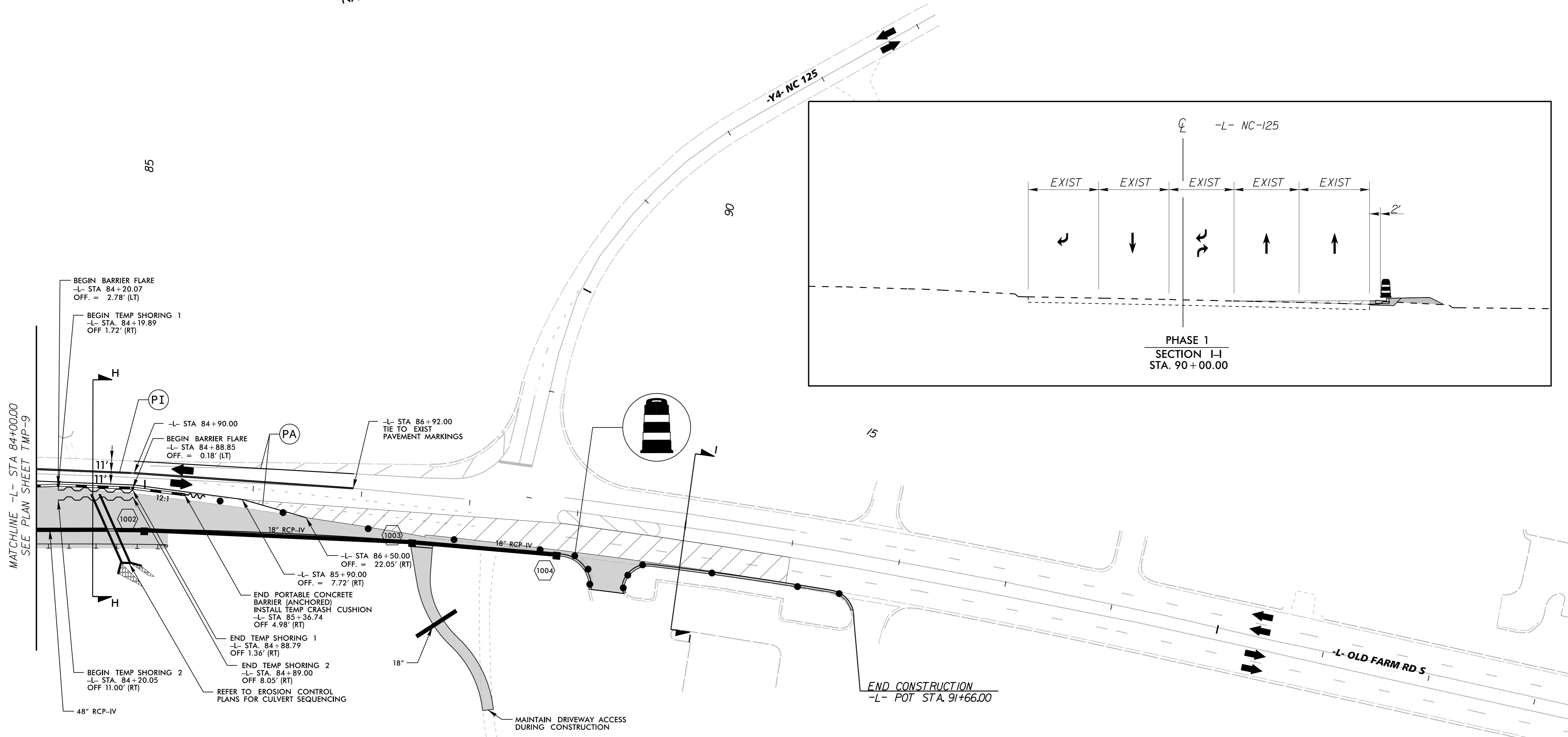
NORTH CAROLINA



Kimley»Horn

**PHASE 1
TRAFFIC CONTROL
DETAILS**

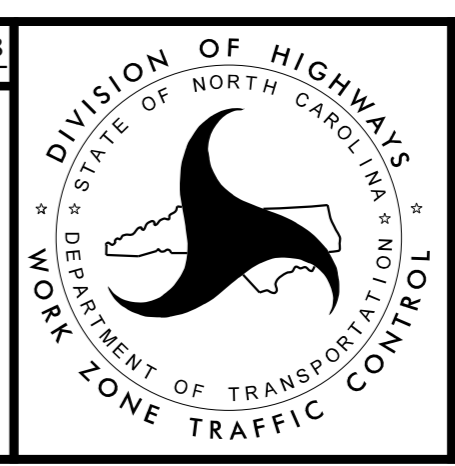
NAD 83 / N.A. 2011



- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

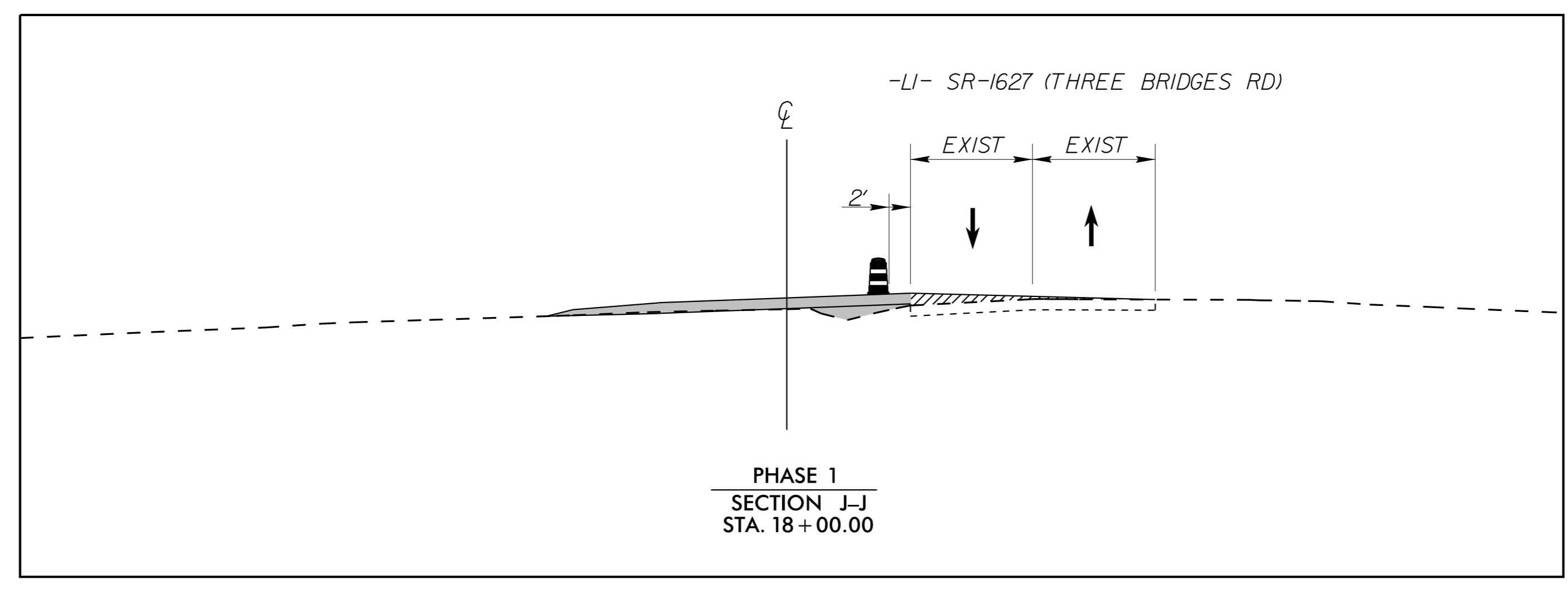
Kimley»Horn

APPROVED: *David Steinhilber* DATE: 6/29/2018
 NORTH CAROLINA PROFESSIONAL SEAL 046269
 ENGINEER DAVID A. SHINGARA



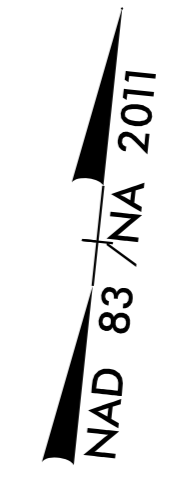
PHASE 1 TRAFFIC CONTROL DETAILS

K:\RAL_Roadway\01036392 - R-3822 and U-5725\TrafficControl\Plan Sheets\R-3822_TCP_psh_10.dgn 6/28/2018



10

15



MATCHLINE -LI- STA 23+50.00
SEE PLAN SHEET TMP-4

MATCHLINE -YI- STA 13+50.00
SEE PLAN SHEET TMP-4

BEGIN CONSTRUCTION
-LI- POT Sta. 14+95.00

MAINTAIN DRIVEWAY ACCESS
DURING CONSTRUCTION

-LI- SR 1627
(THREE BRIDGES RD)

EXIST SR 1627
(THREE BRIDGES RD)

18" RCP-IV

18" RCP-IV

18"

18" RCP-IV

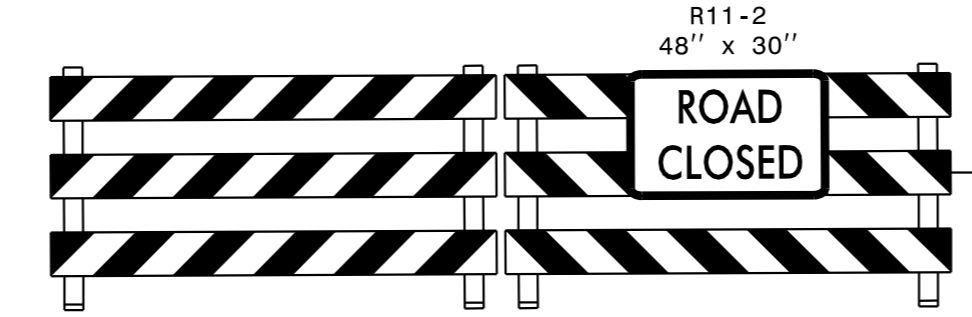
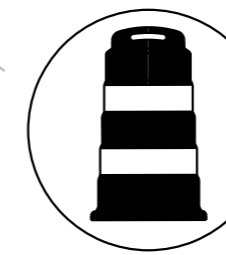
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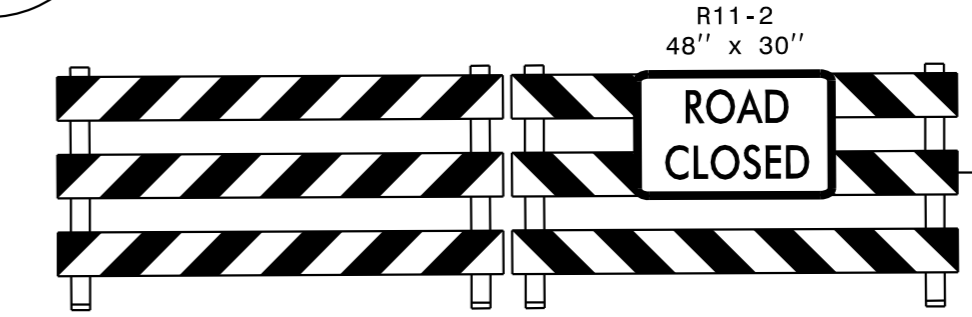
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TYPE III BARRICADE(S)



TYPE III BARRICADE(S)

- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

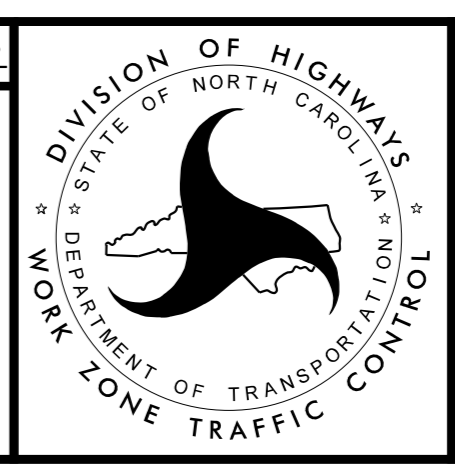
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Kimley»Horn

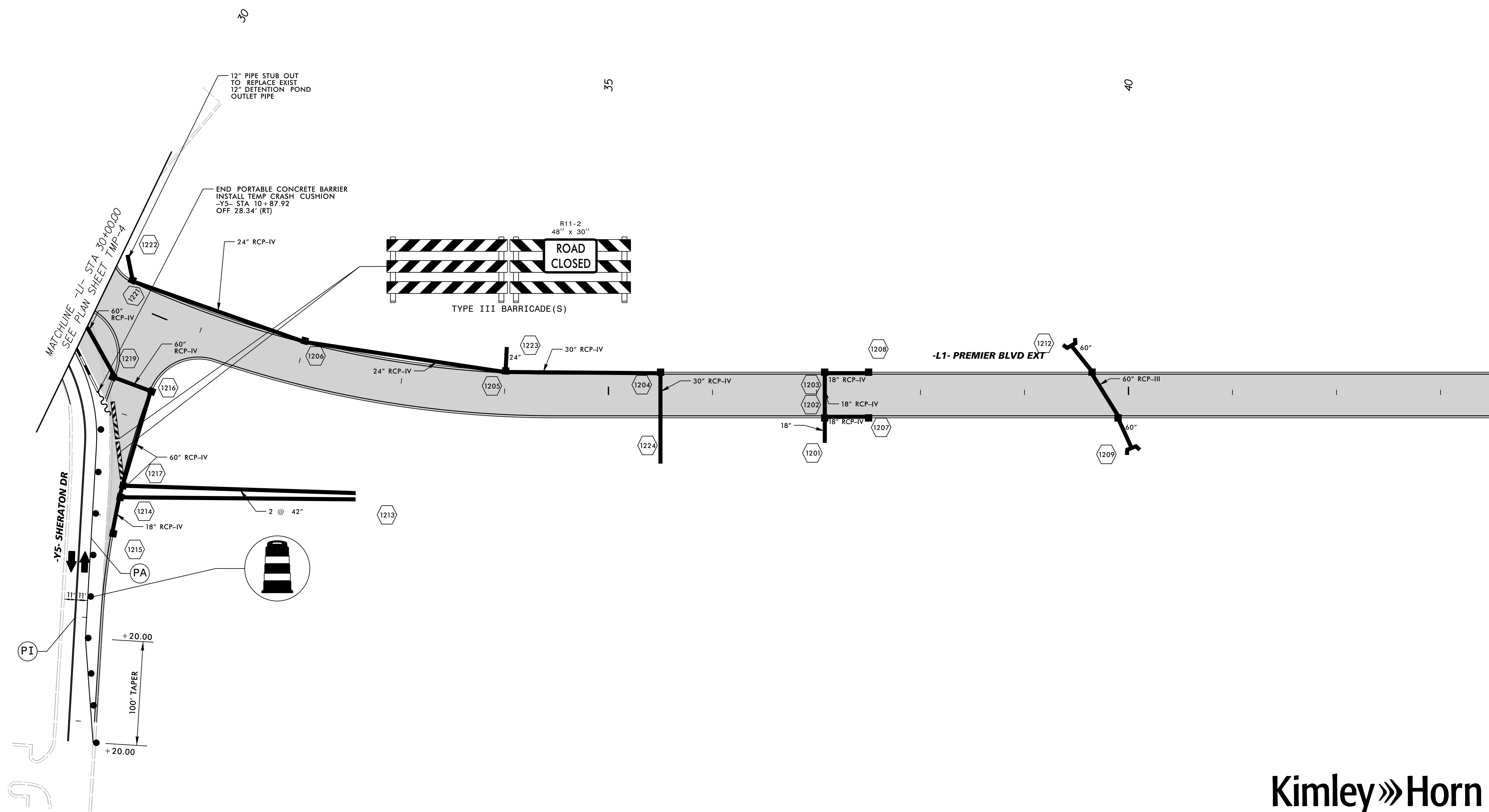
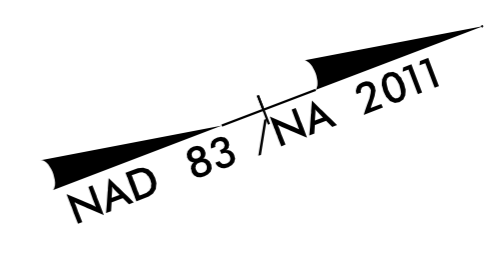
APPROVED: *David Sklar* DATE: 6/29/2018

PROFESSIONAL SEAL 046269

DAVID A. SHINDLER



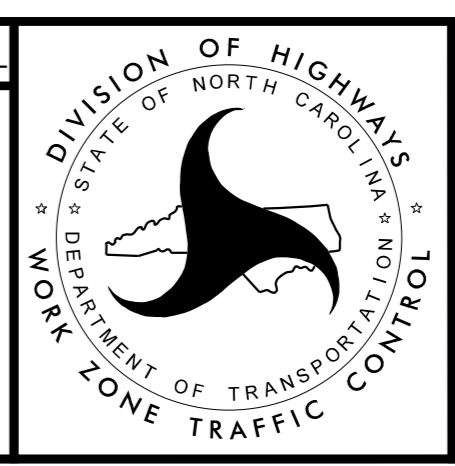
**PHASE 1
TRAFFIC CONTROL
DETAILS**



7/5/2018 K:\RAL_Roadway\01036392 - R-3822 and U-5725\TrafficControl\Plan Sheets\R-3822_TCP_psh_12.dgn

- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

APPROVED: David Skinsara DATE: 7/5/2018

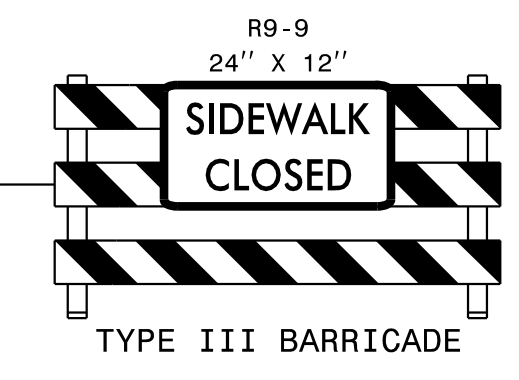
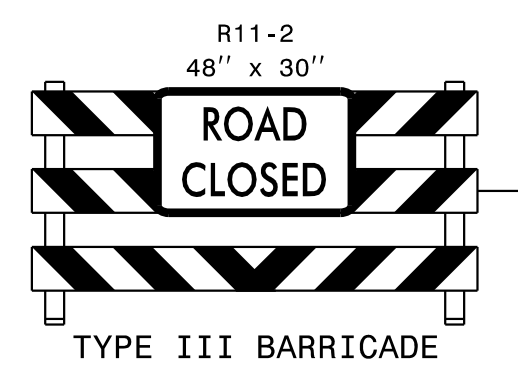
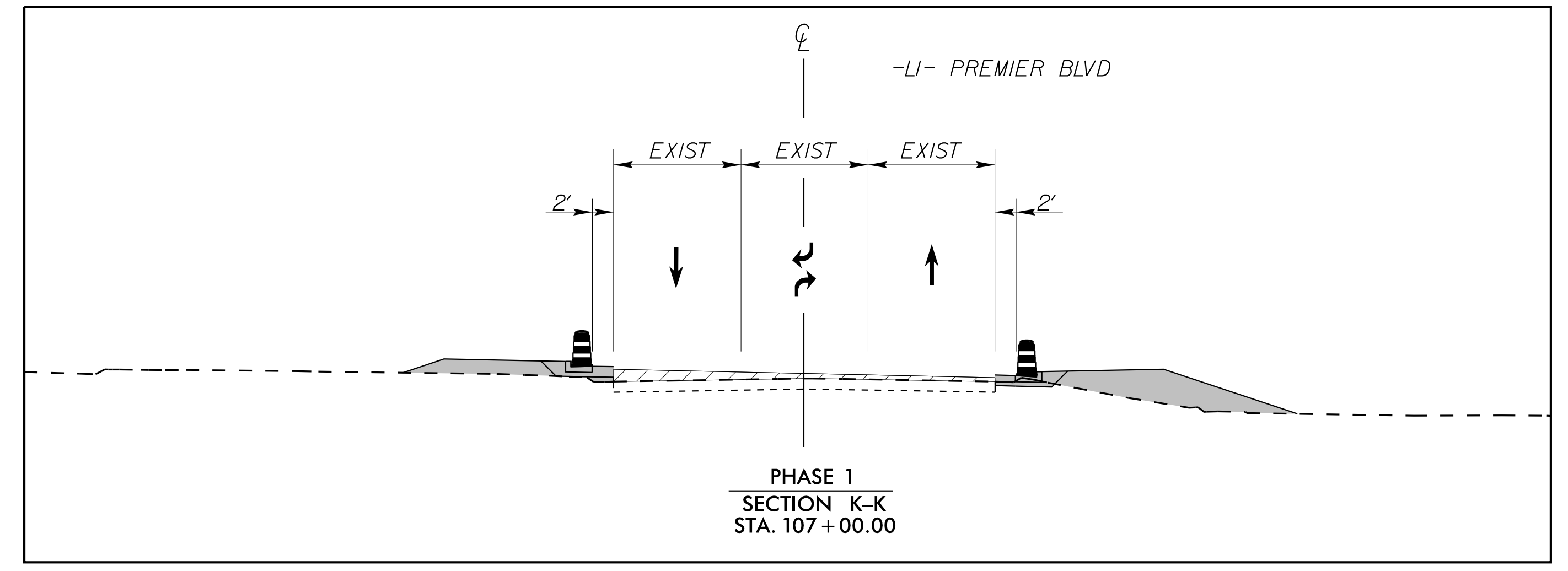


Kimley»Horn

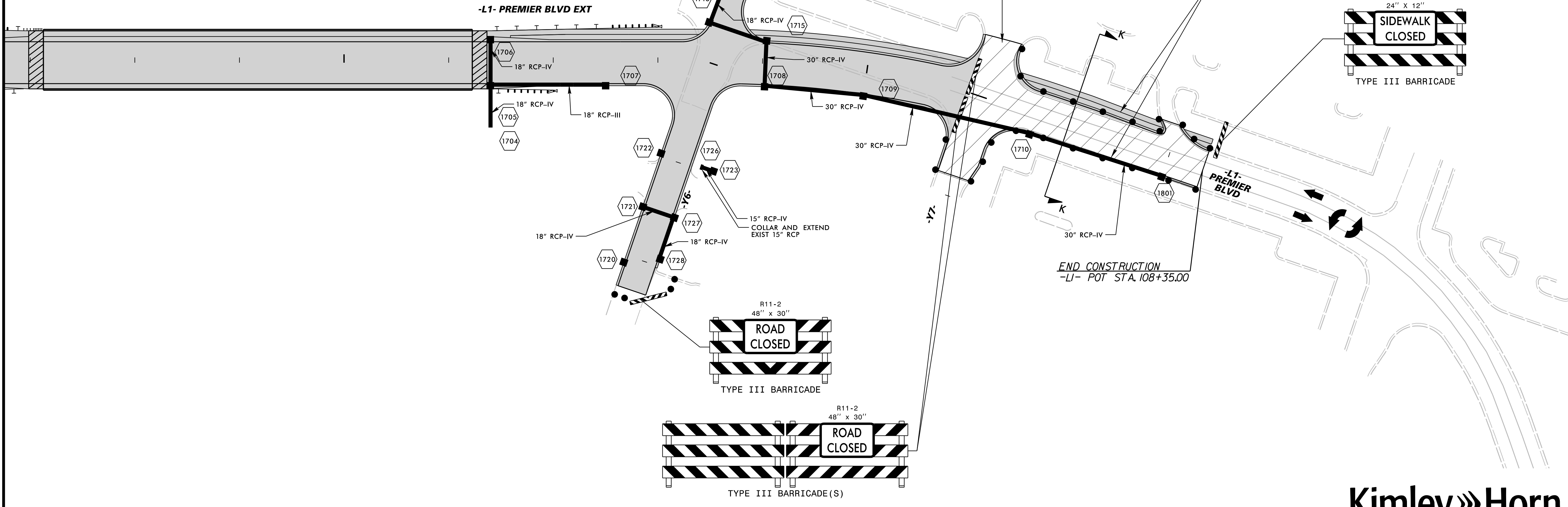
**PHASE 1
TRAFFIC CONTROL
DETAILS**


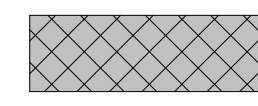
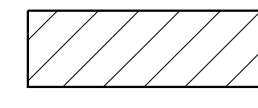
NAD 83 / NA 2011

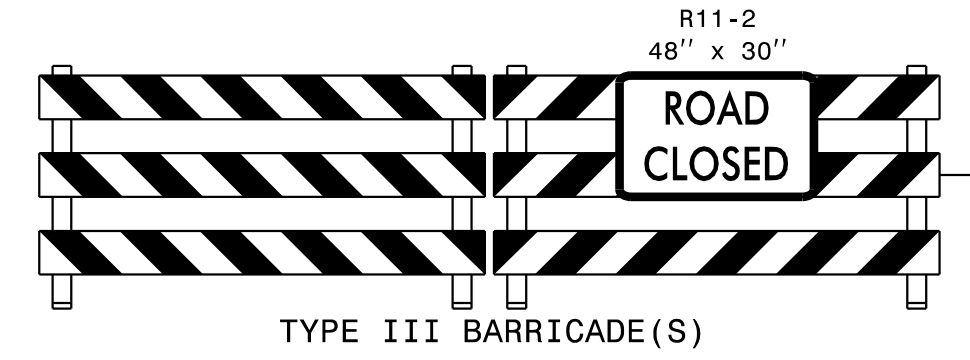
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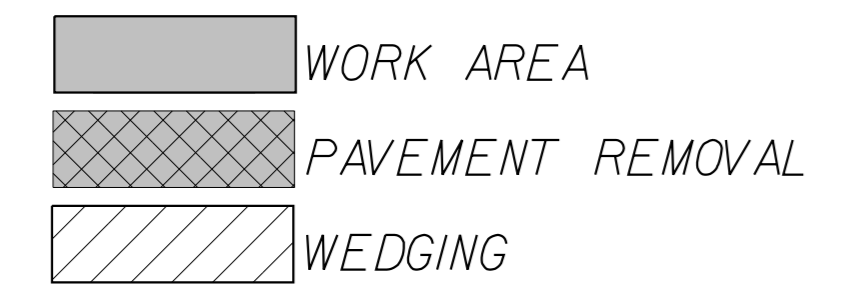
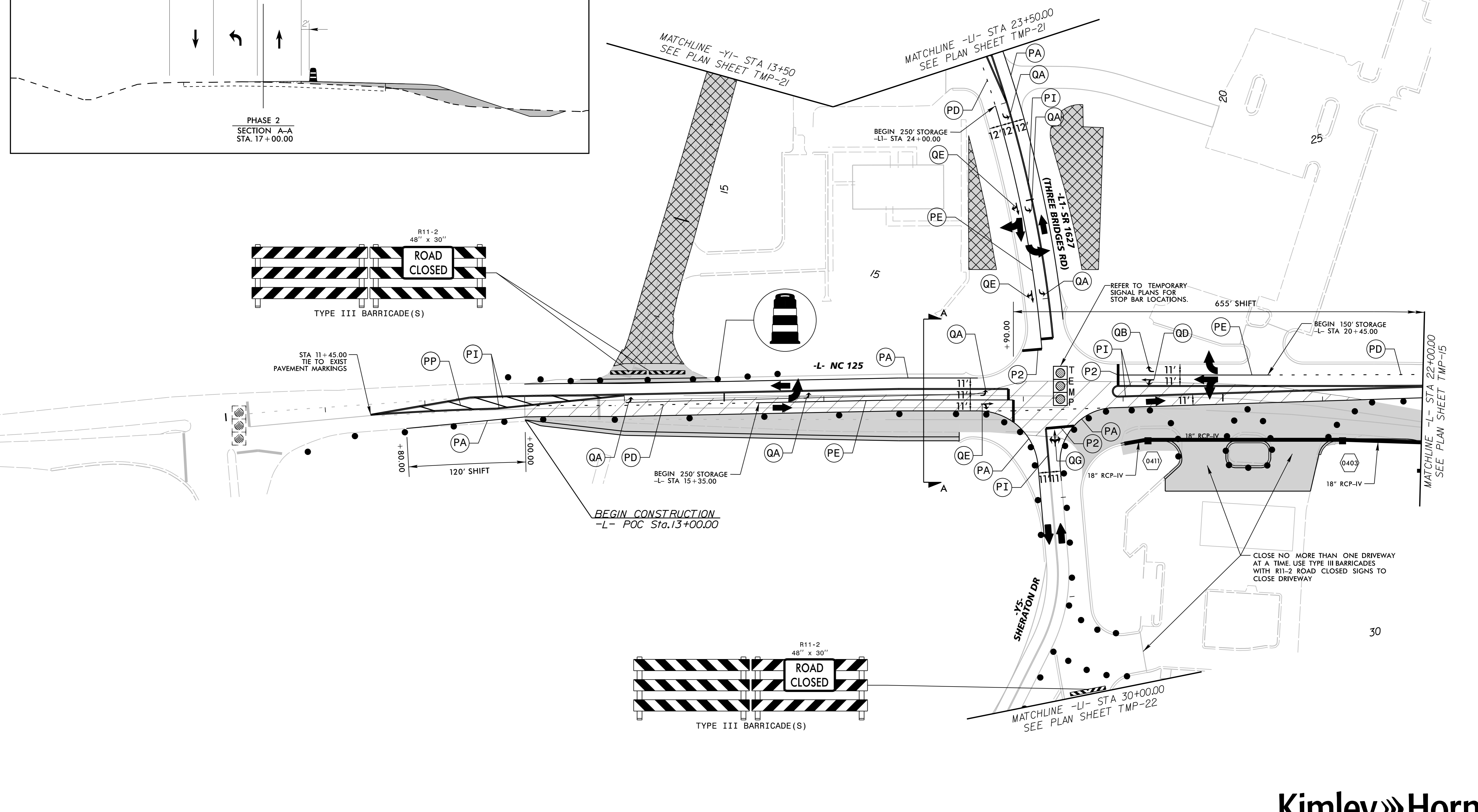
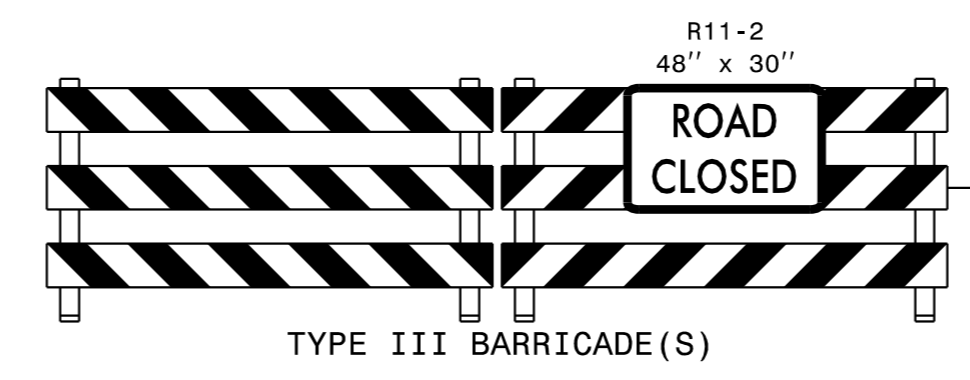
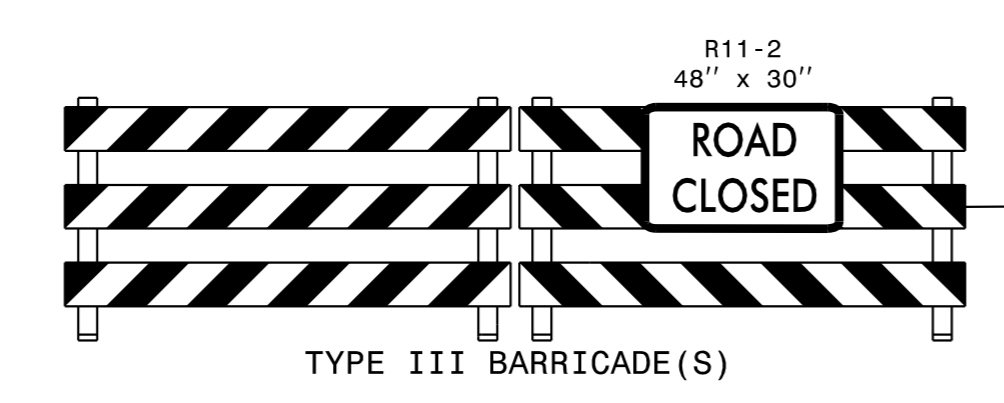
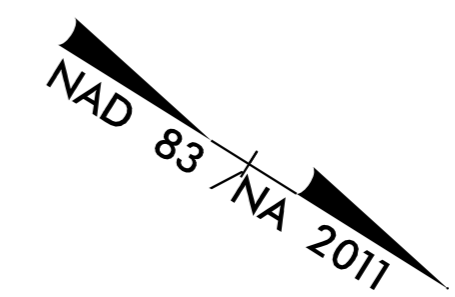
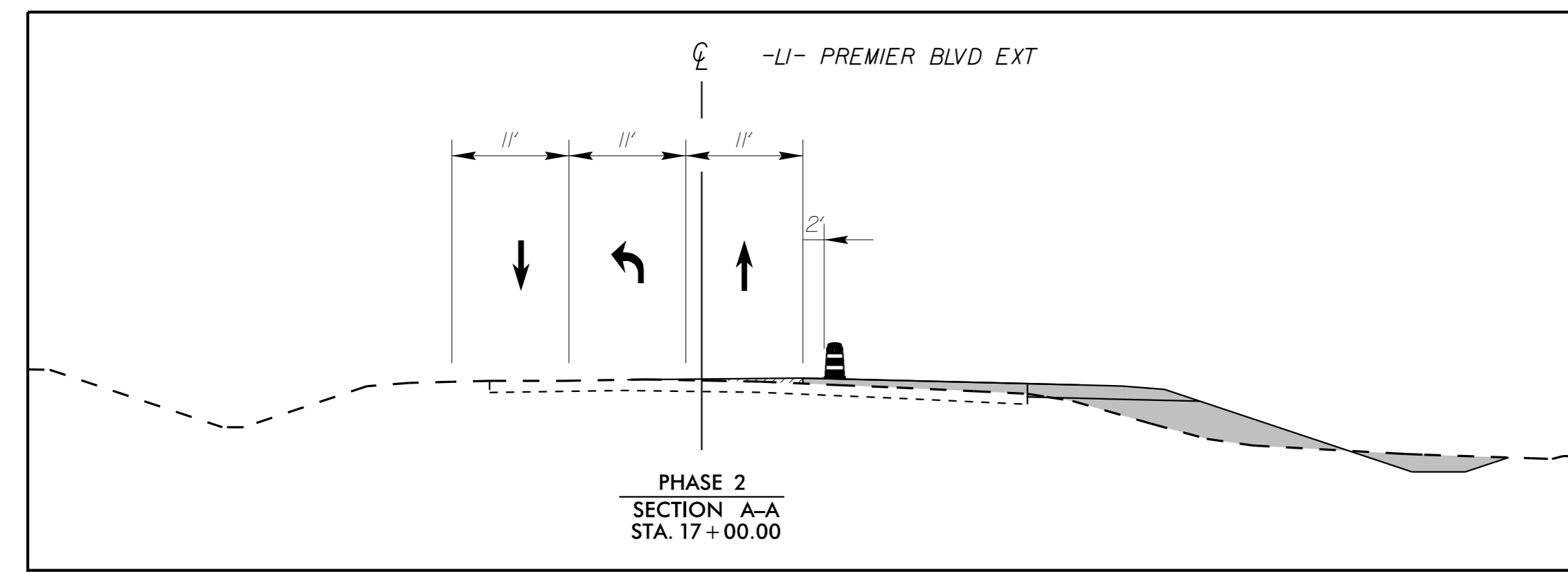
-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING



APPROVED: *David Shink* DATE: 6/29/2018

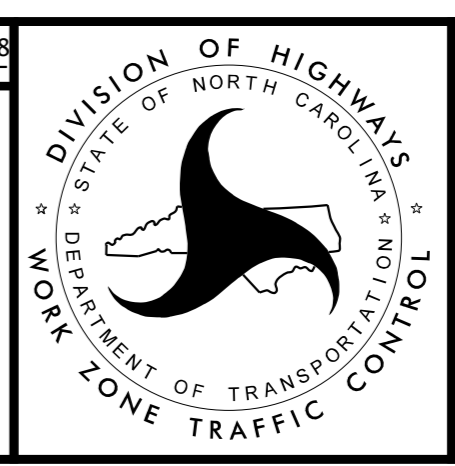
Kimley»Horn

PHASE 1 TRAFFIC CONTROL DETAILS



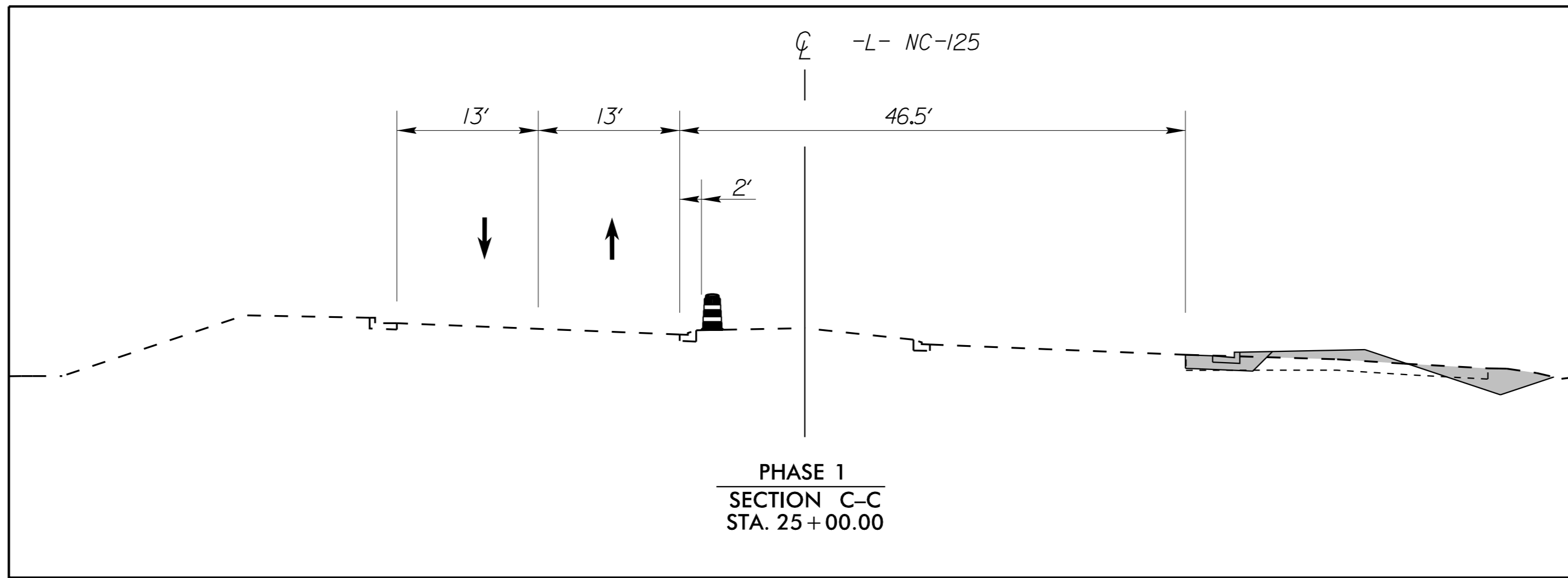
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6/28/2018

APPROVED: David Skiba DATE: 6/29/2018
NORTH CAROLINA PROFESSIONAL SEAL 046269 ENGINEER DAVID A. SHINGARA

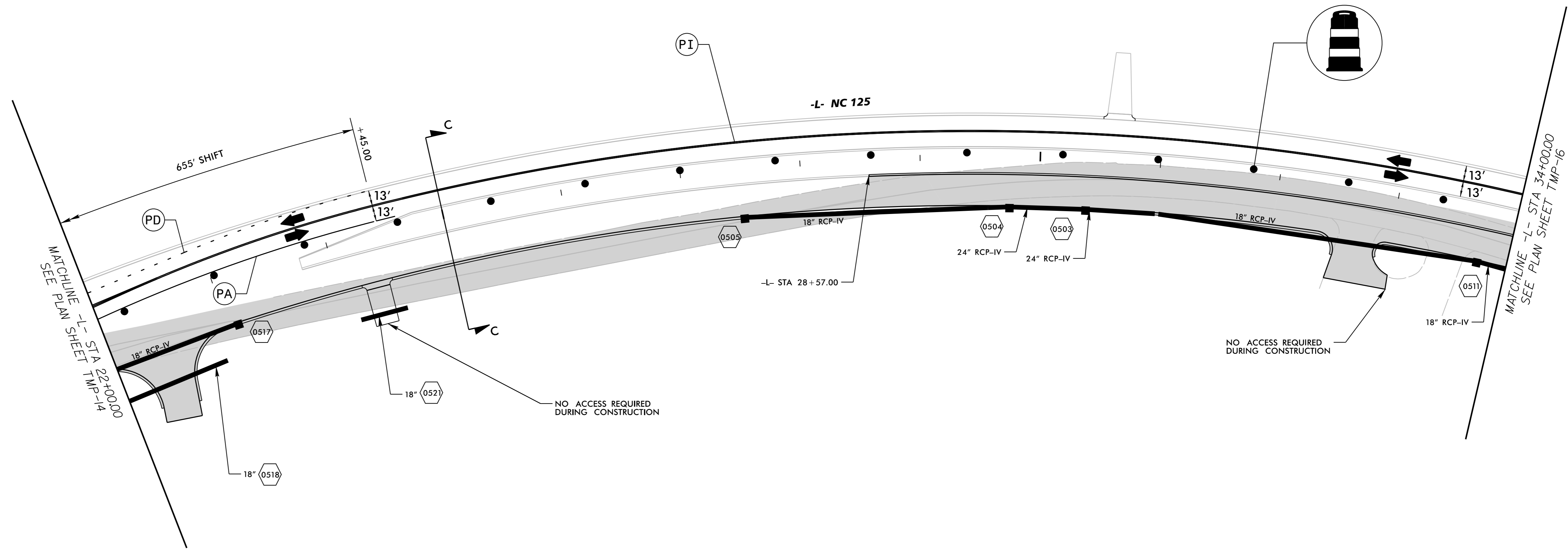
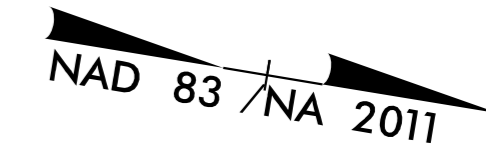





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PHASE 2
TRAFFIC CONTROL
DETAILS



30

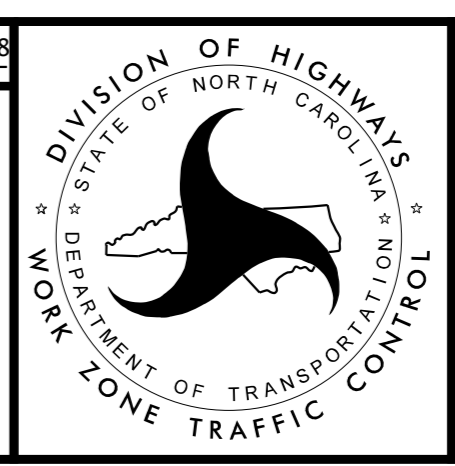


-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

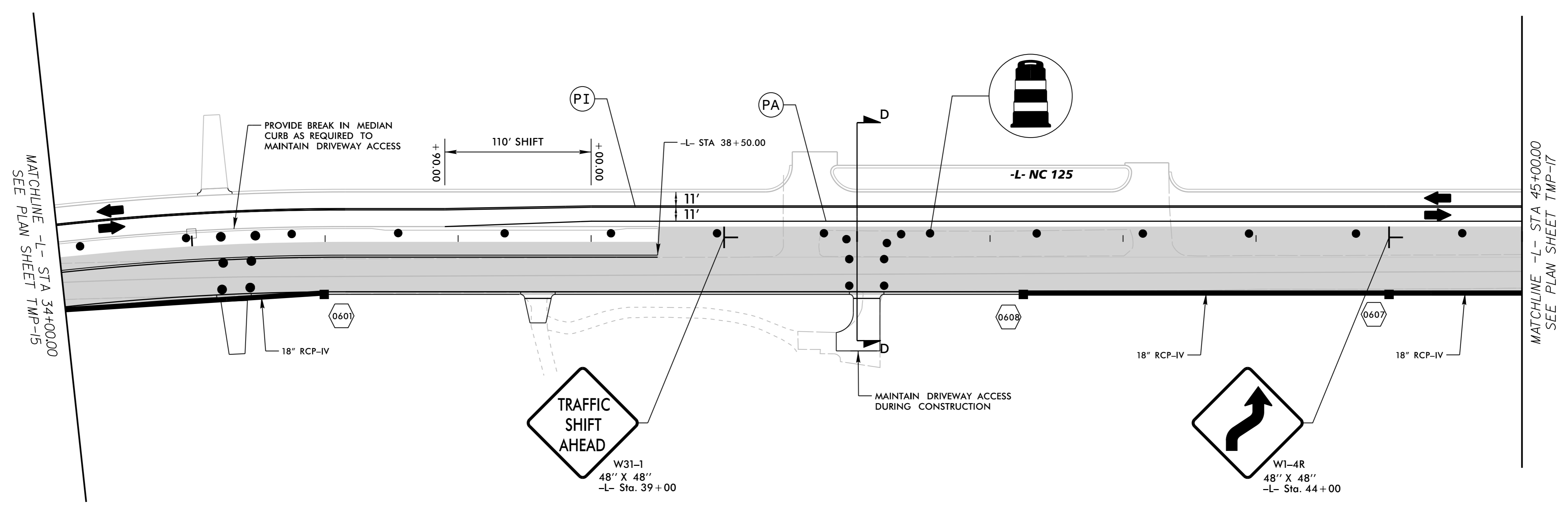
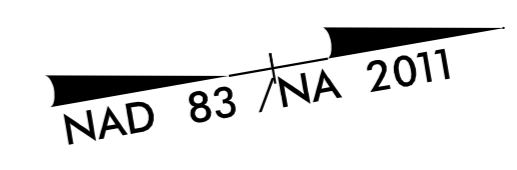
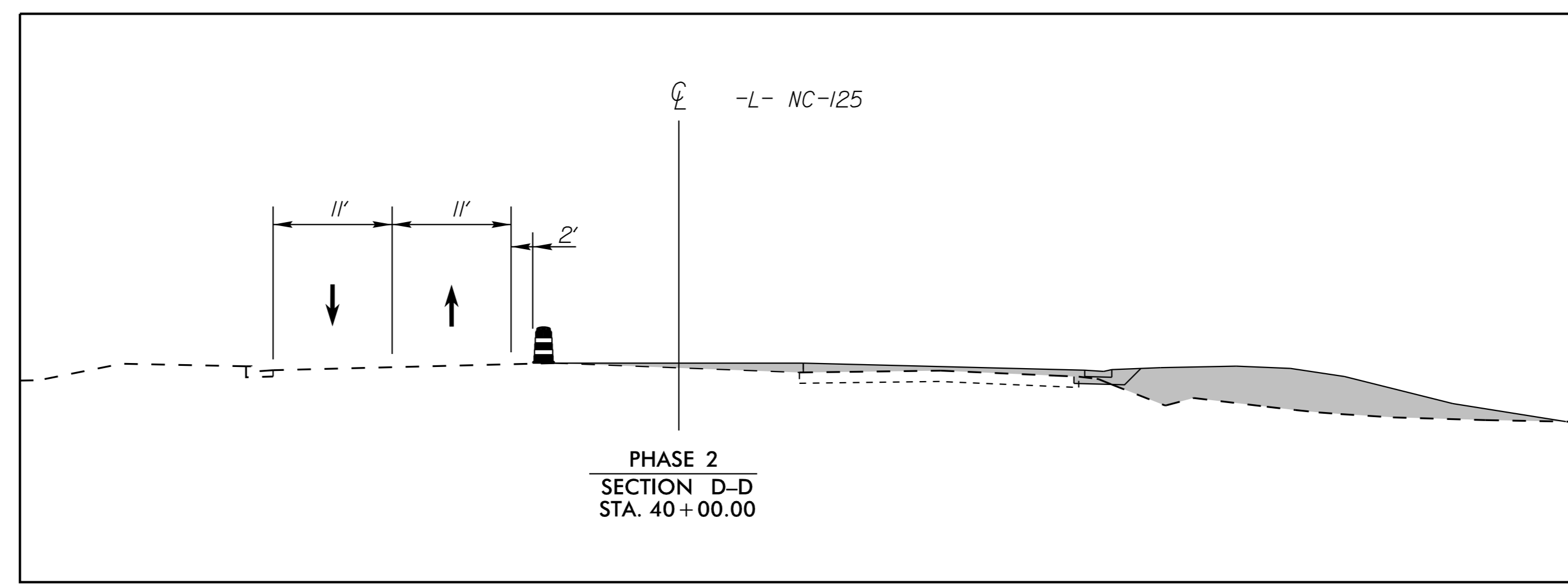
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6/28/2018




Kimley»Horn

APPROVED: *David Skidmore* DATE: 6/29/2018


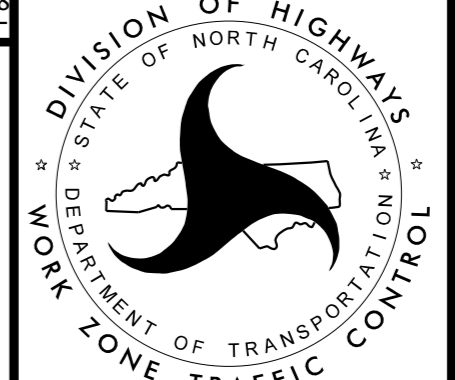


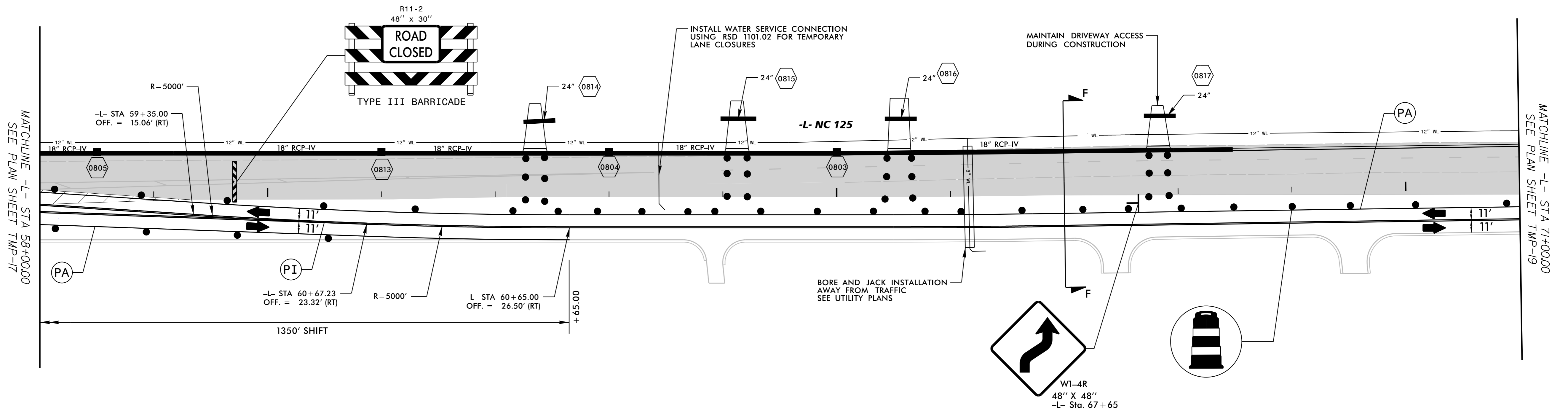
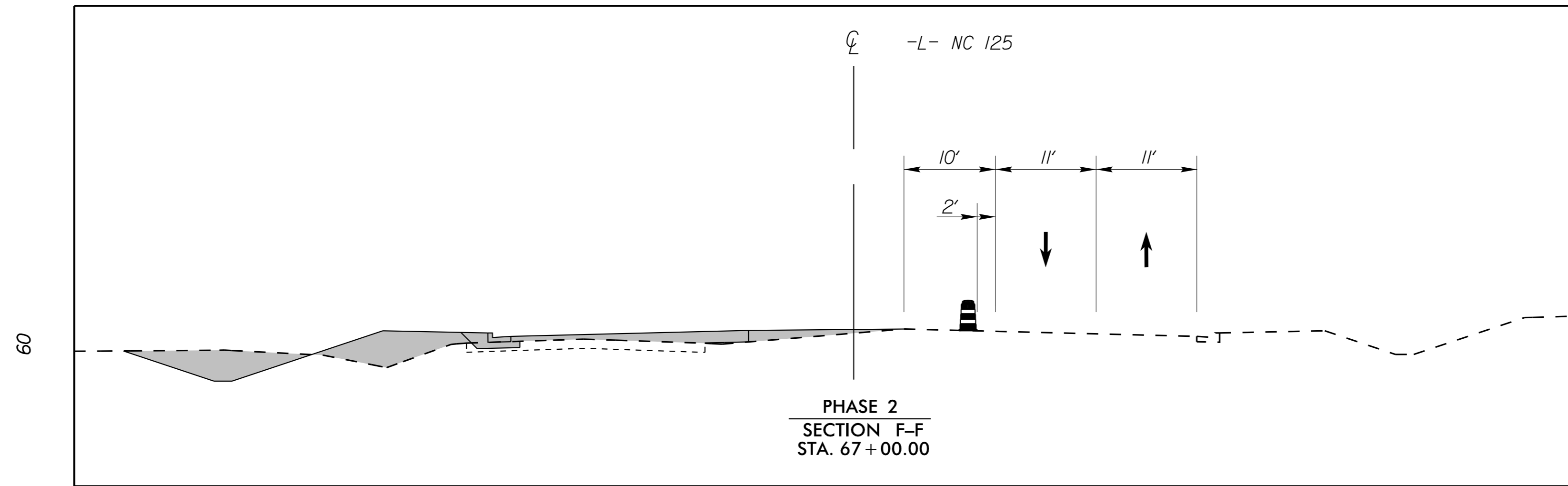
**PHASE 2
TRAFFIC CONTROL
DETAILS**



-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

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6/28/2018

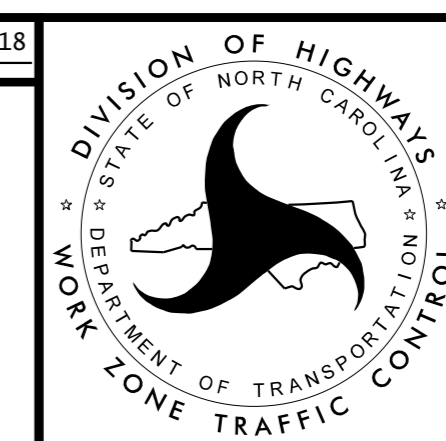
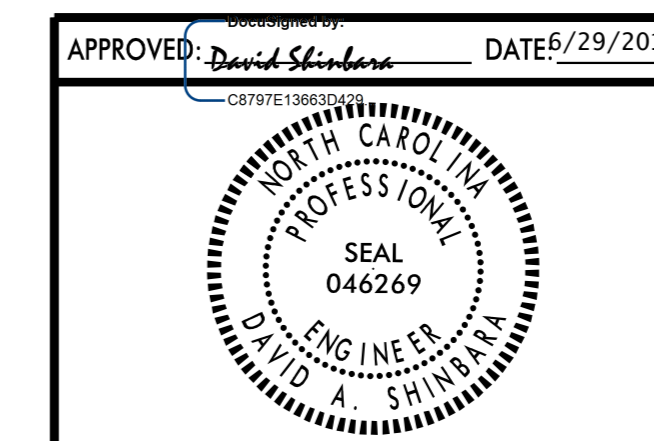
APPROVED: <i>David Shindler</i> DATE: 6/29/2018			<h2 style="margin: 0;">Kimley»Horn</h2> <h3 style="margin: 0;">PHASE 2 TRAFFIC CONTROL DETAILS</h3>



- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

15

Kimley»Horn

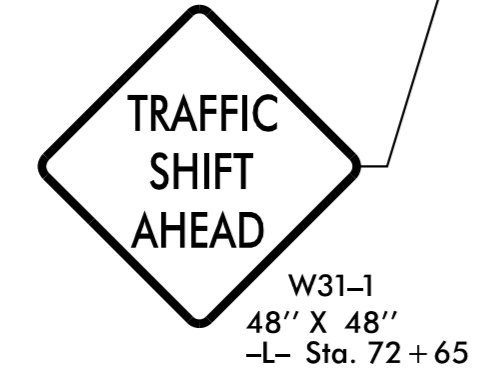
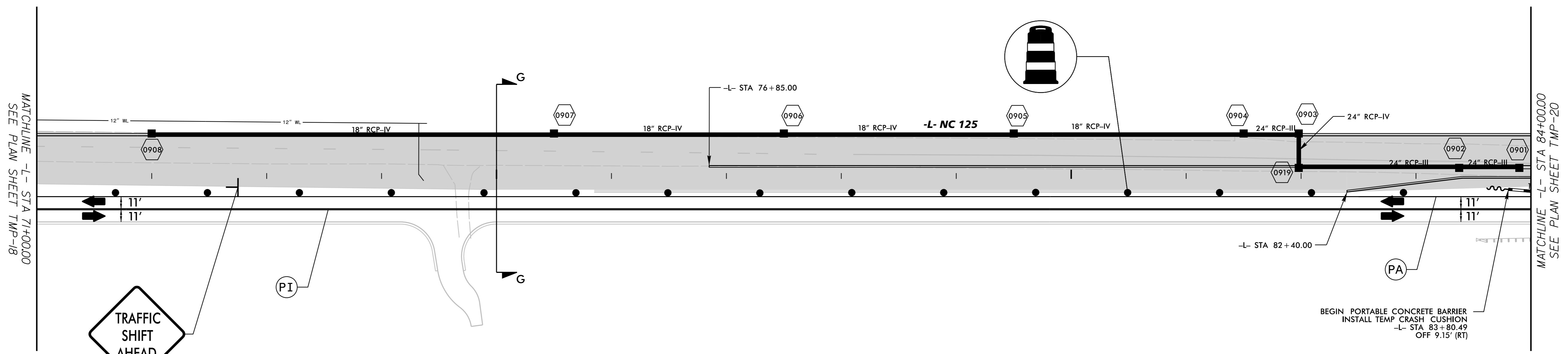
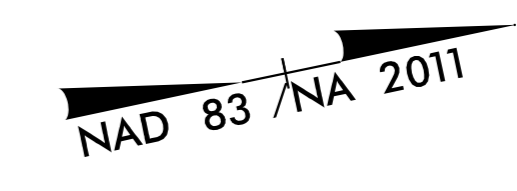
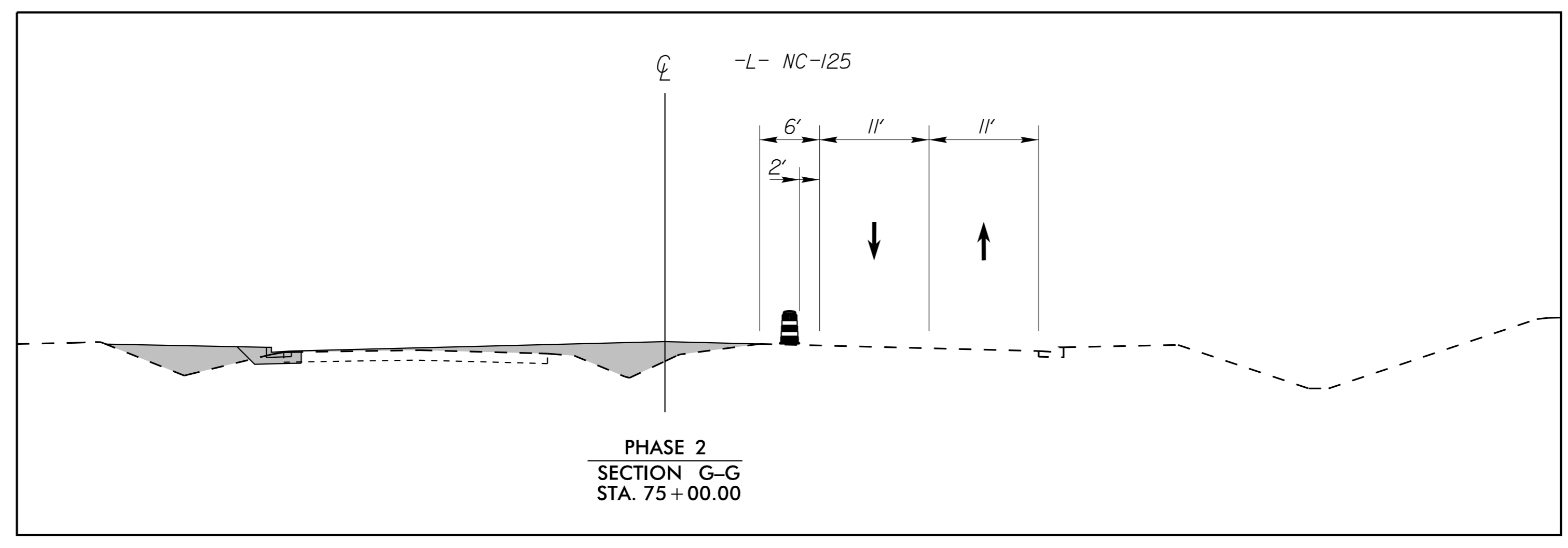


**PHASE 2
TRAFFIC CONTROL
DETAILS**

K:\RAL_Roadway\01036392 - R-3822 and U-5725\TrafficControl\Plan Sheets\R-3822_TCP_psh_18.dgn 6/28/2018

MATCHLINE -L- STA 58+00.00
SEE PLAN SHEET TMP-17

MATCHLINE -L- STA 71+00.00
SEE PLAN SHEET TMP-19



BEGIN PORTABLE CONCRETE BARRIER
INSTALL TEMP CRASH CUSHION
-L- STA 83+80.49
OFF 9.15' (RT)

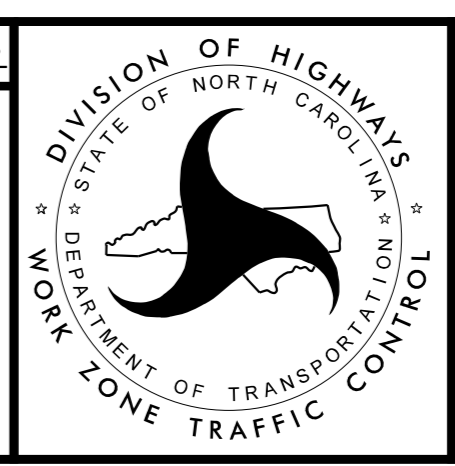
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

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APPROVED: *David Shindler* DATE 6/29/2018

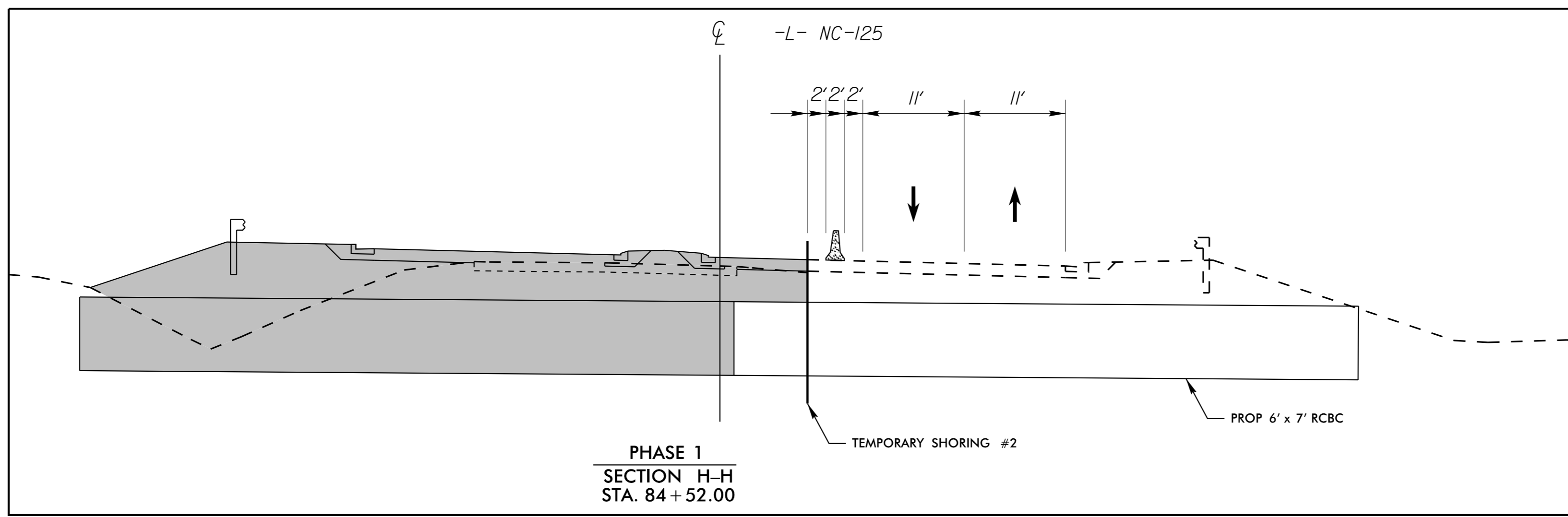
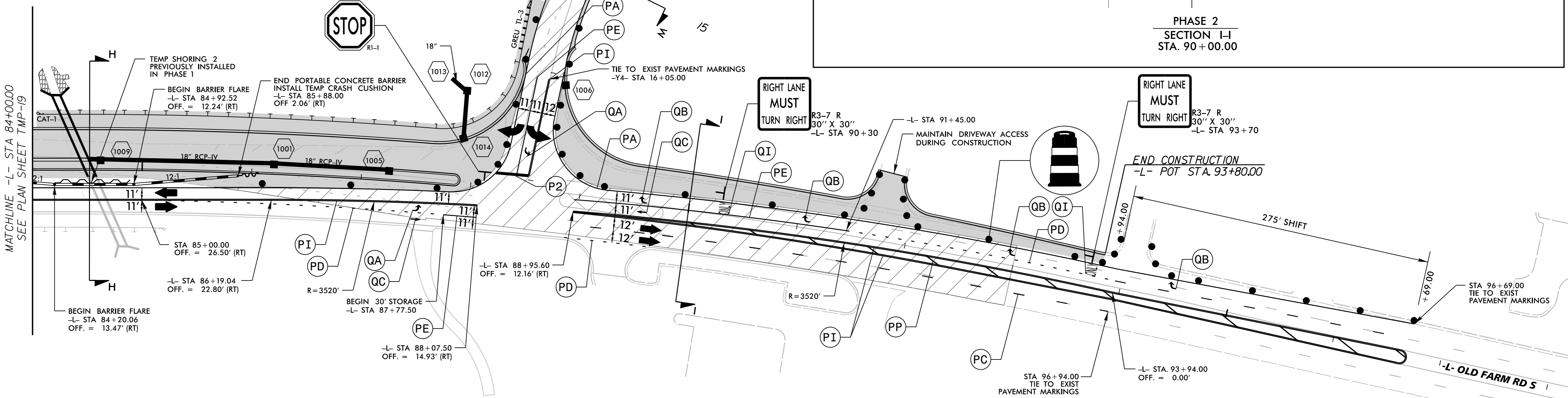
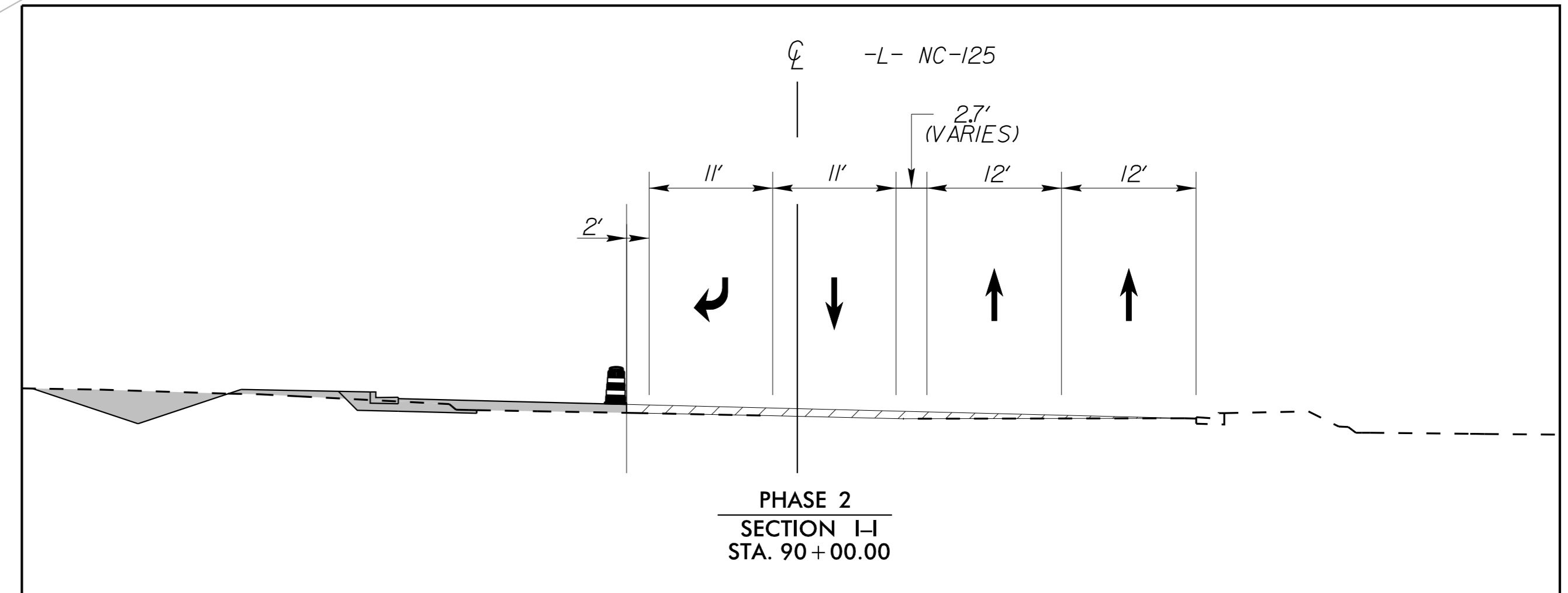
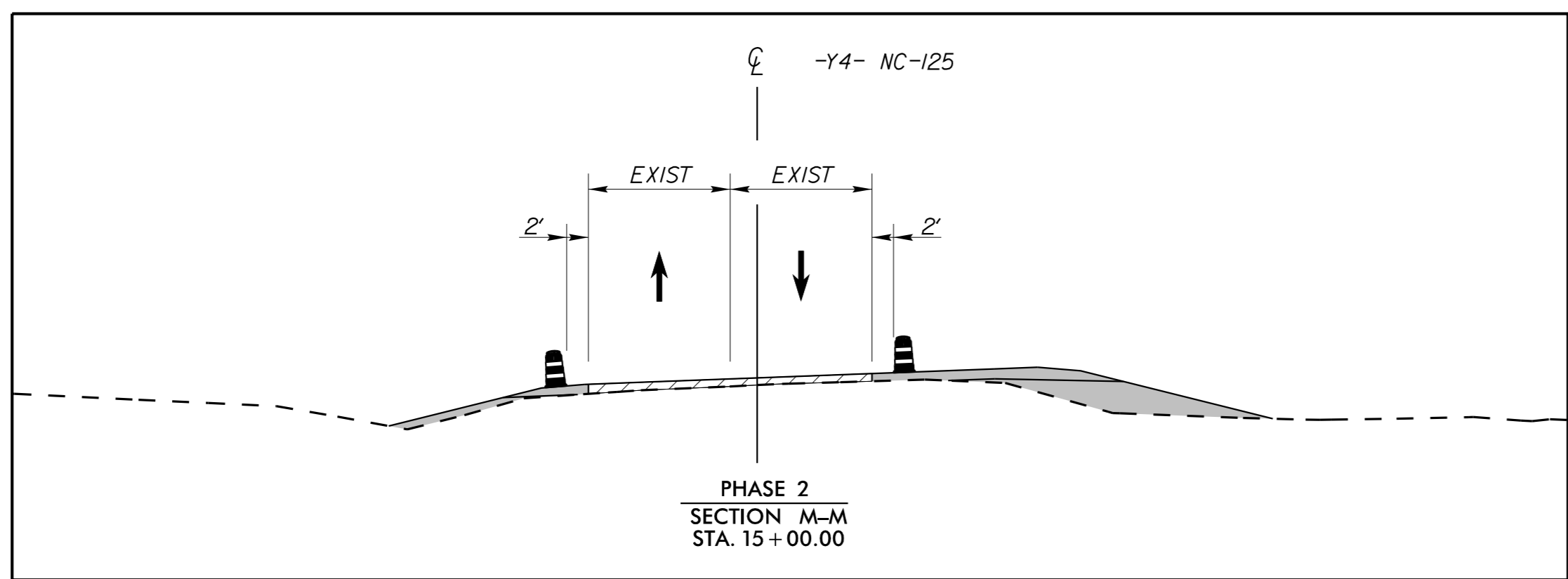
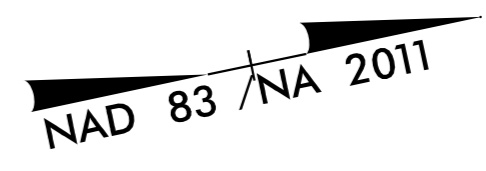
PROFESSIONAL SEAL 046269

DAVID A. SHINDLER, ENGINEER



Kimley»Horn

PHASE 2 TRAFFIC CONTROL DETAILS



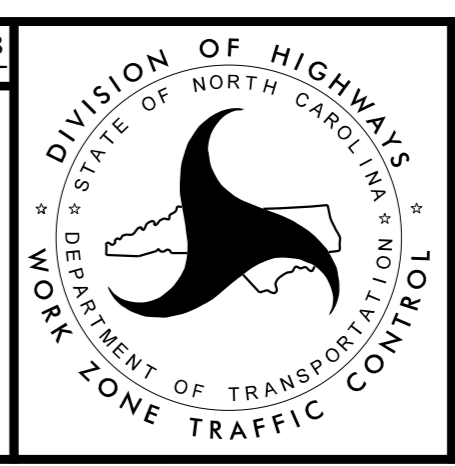
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

Kimley»Horn

APPROVED: *David Stinson* DATE: 6/29/2018

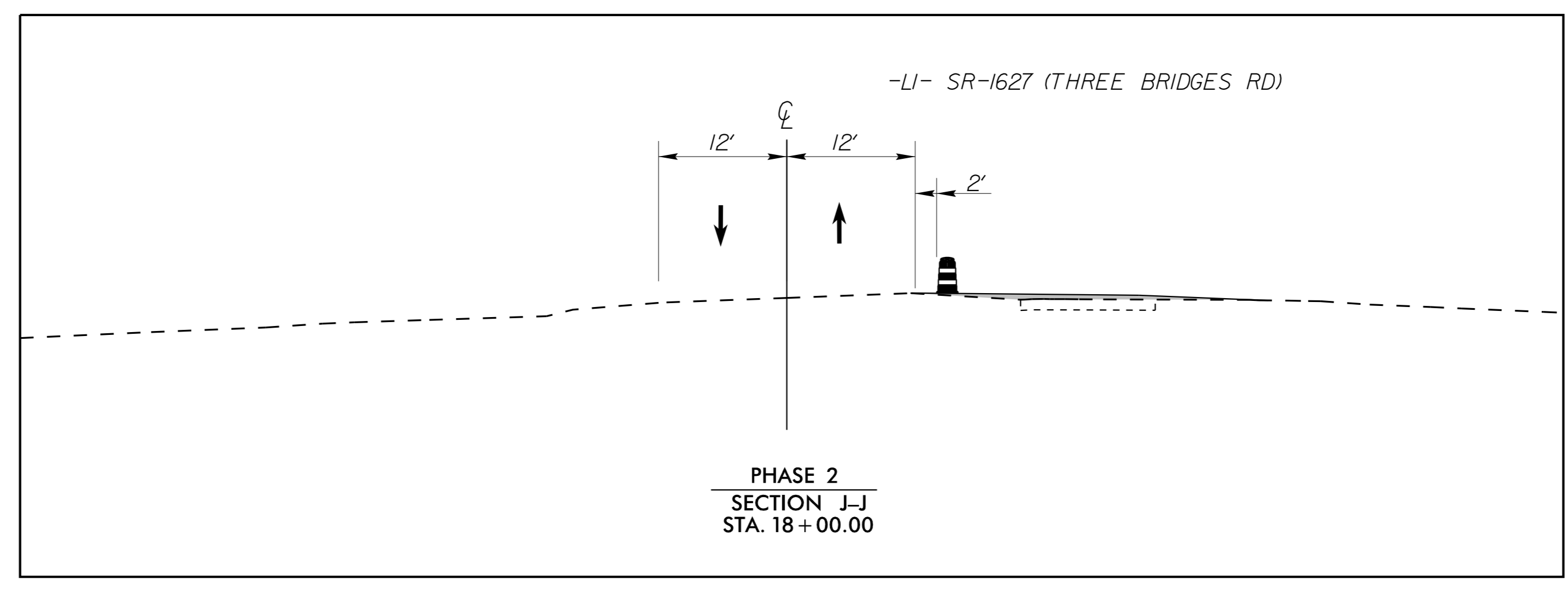
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DAVID A. SHINGARA

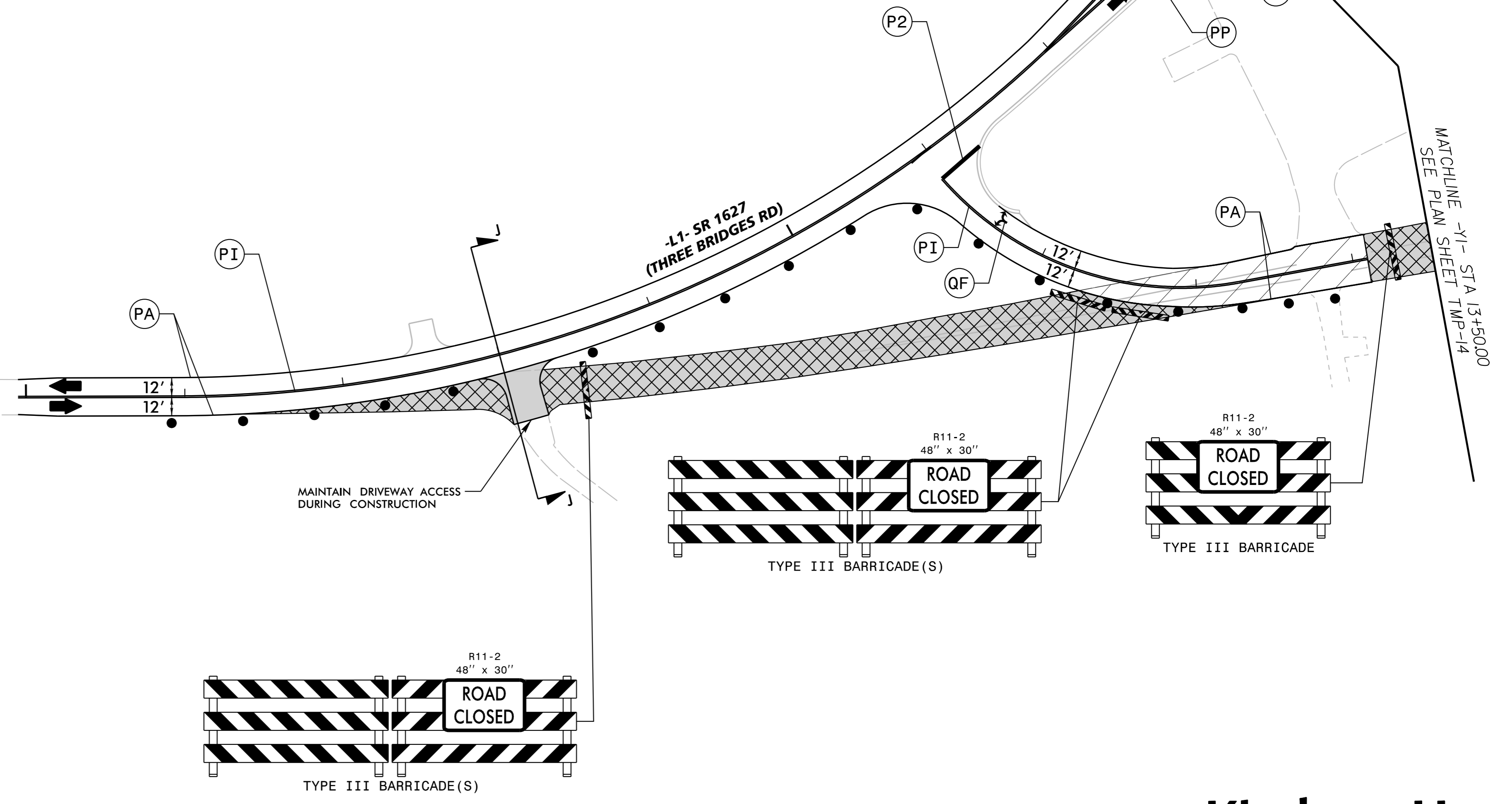


**PHASE 2
TRAFFIC CONTROL
DETAILS**

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15

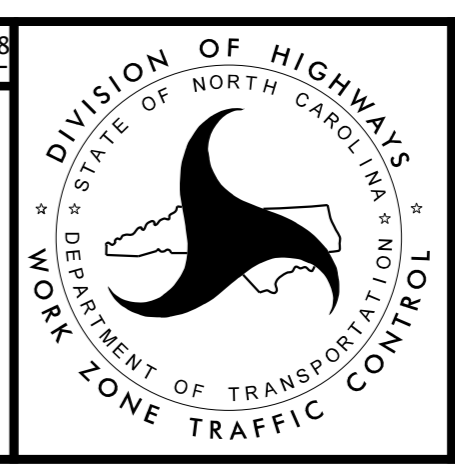


- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

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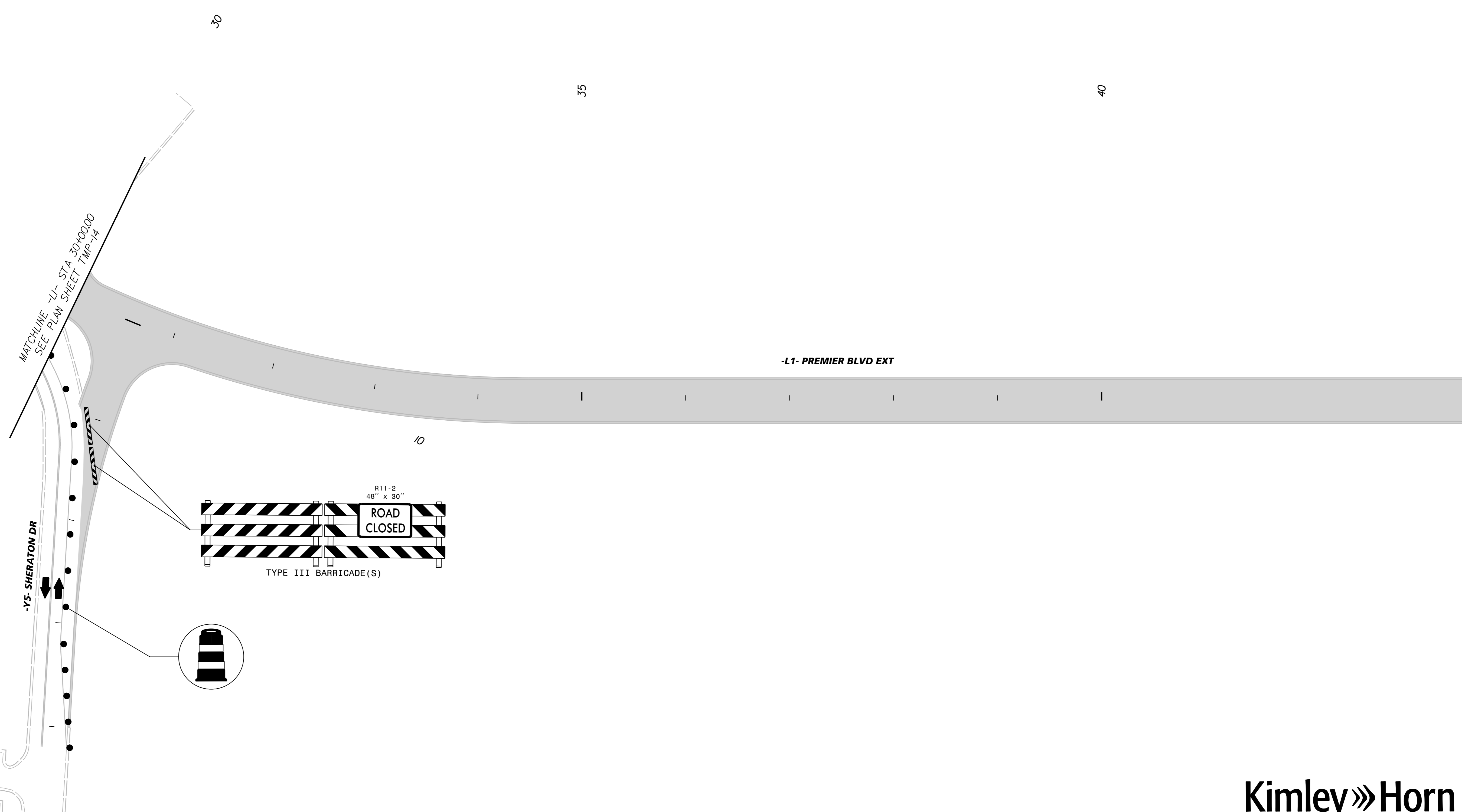
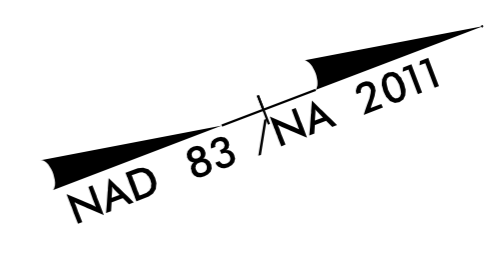
APPROVED: David Shink DATE: 6/29/2018

STATE OF NORTH CAROLINA
 NORTH CAROLINA PROFESSIONAL SEAL
 046269
 ENGINEER
 DAVID A. SHINK



Kimley»Horn

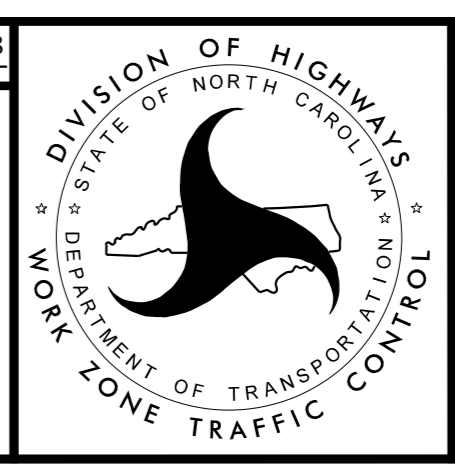
**PHASE 2
TRAFFIC CONTROL
DETAILS**



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6/28/2018

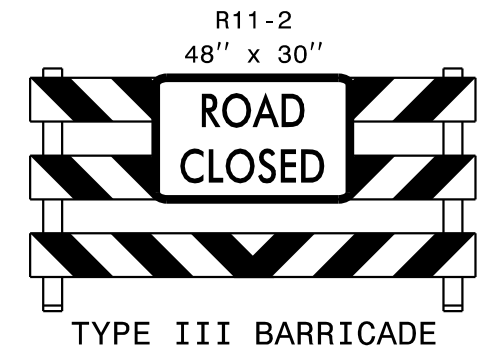
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

APPROVED: *David Skinkens* DATE: 6/29/2018

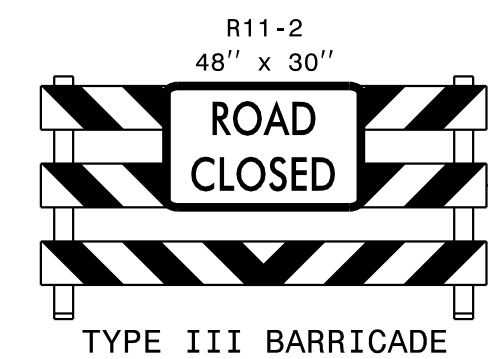


Kimley»Horn

**PHASE 2
TRAFFIC CONTROL
DETAILS**

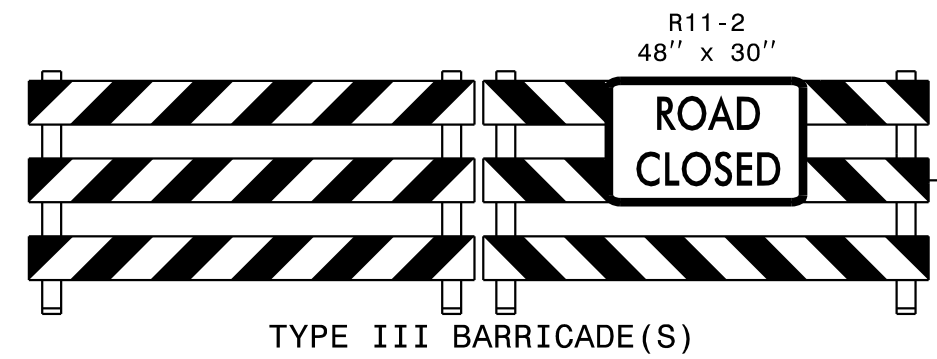


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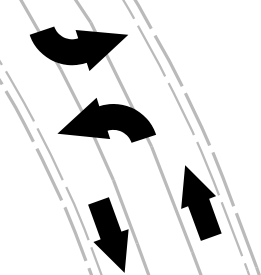


-Y6-

-Y7-



-L1- PREMIER BLVD

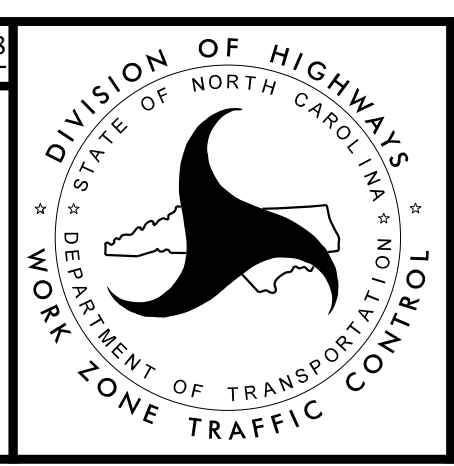


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6/28/2018

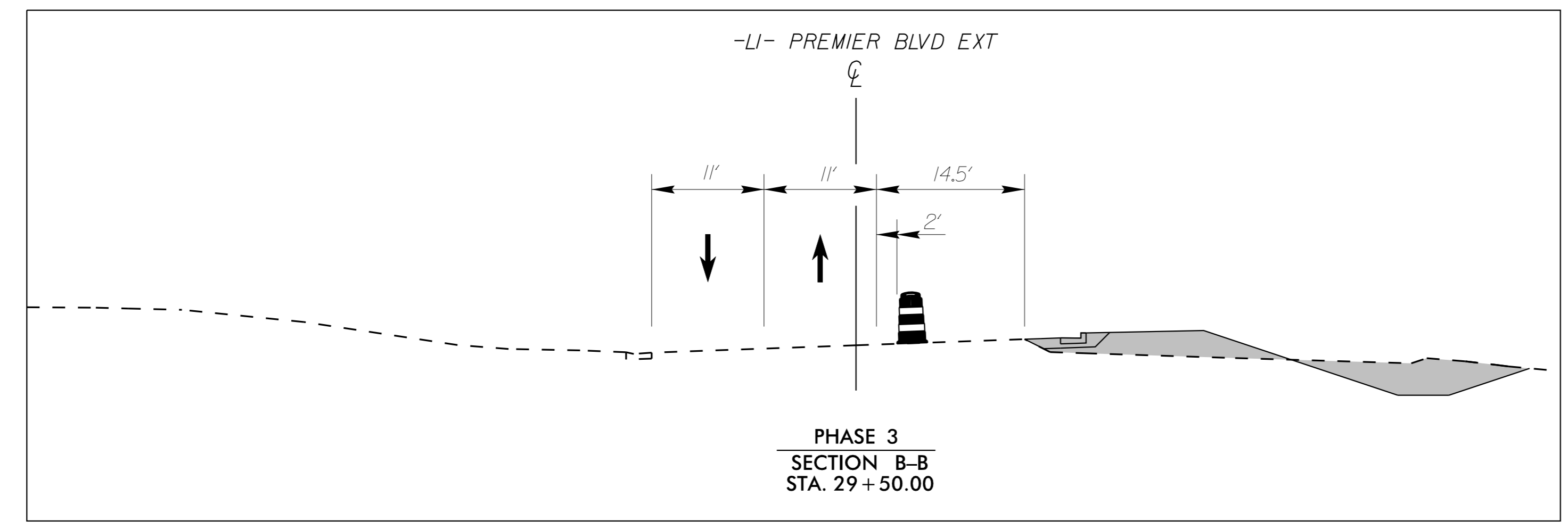
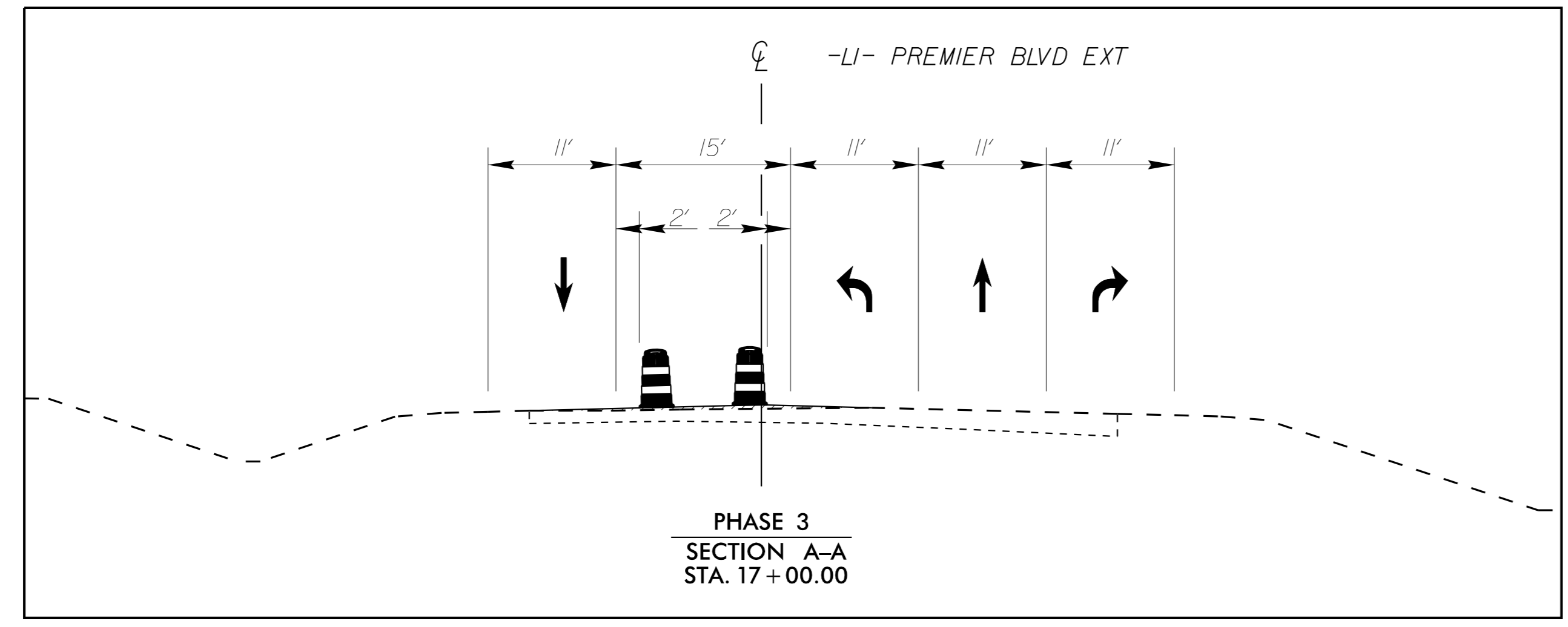
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

Kimley»Horn

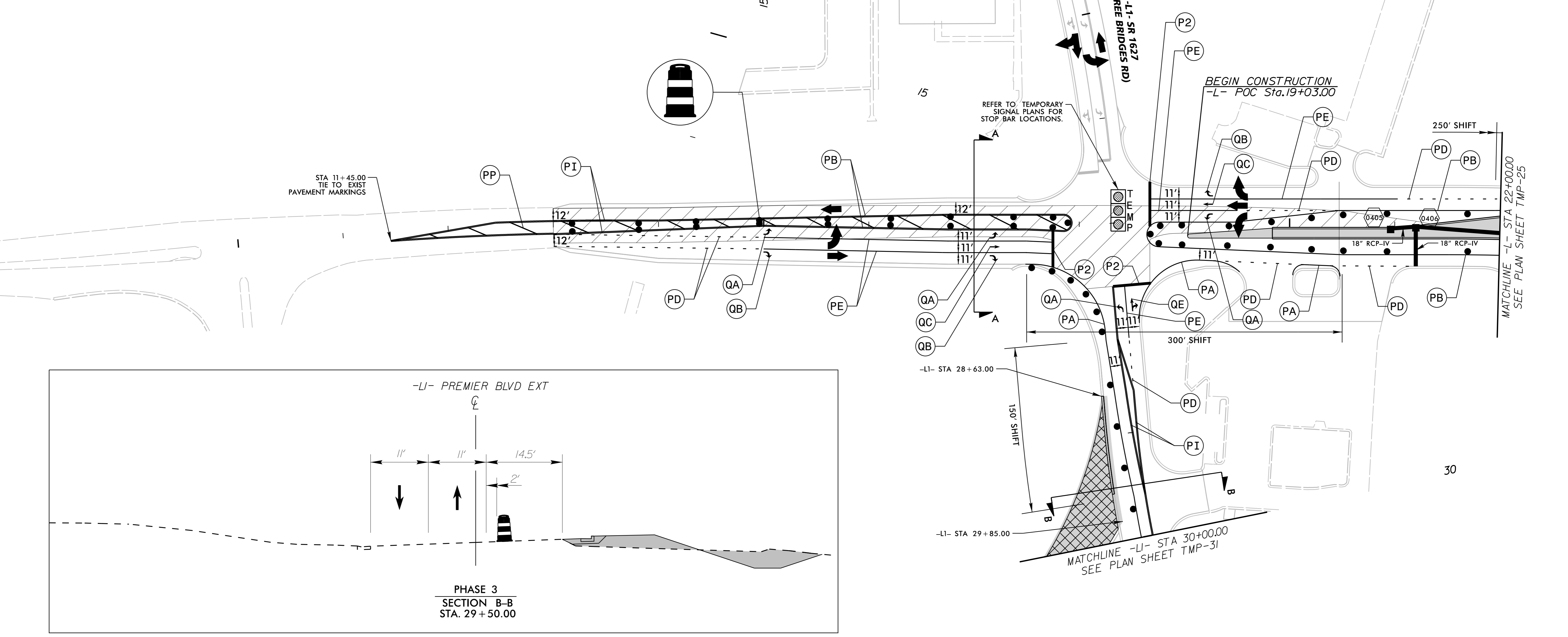
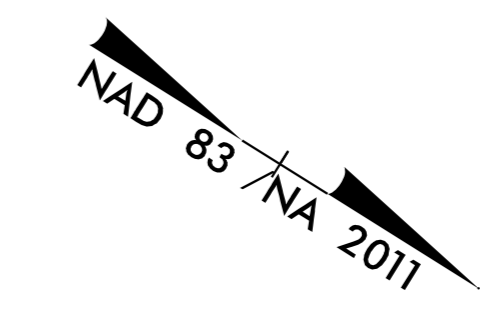
APPROVED: David Shinkens DATE: 6/29/2018



**PHASE 2
TRAFFIC CONTROL
DETAILS**



- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

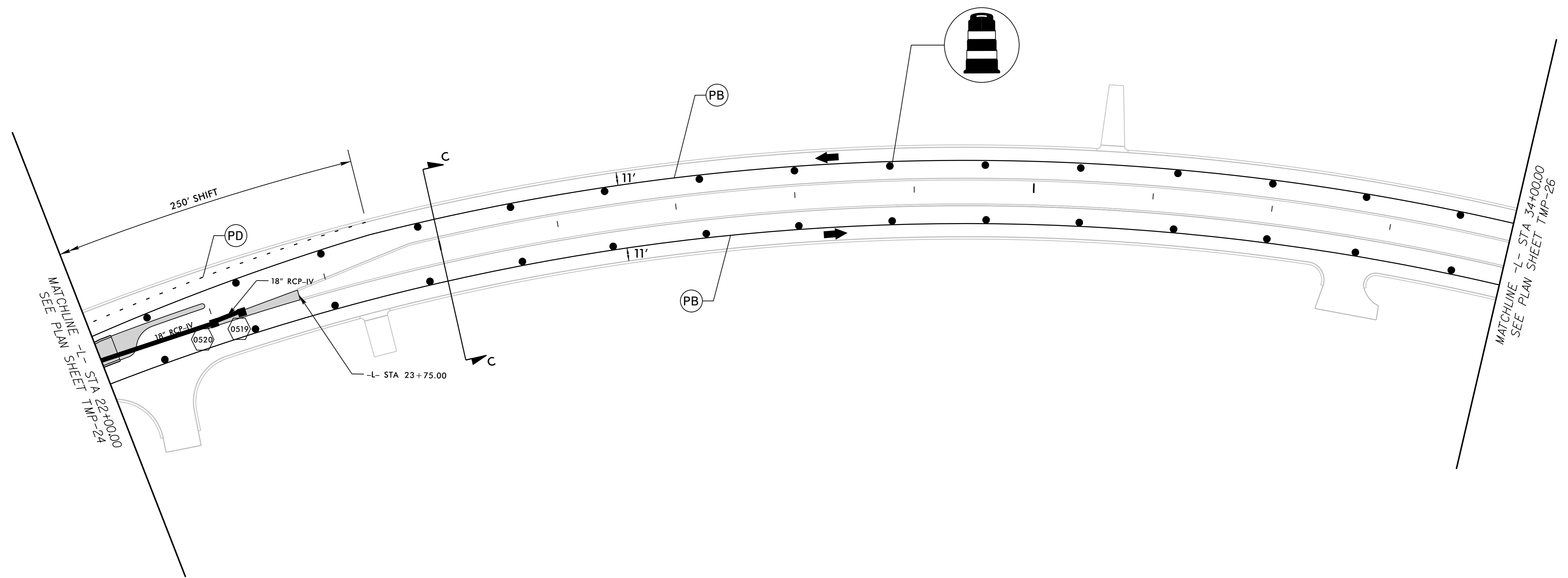
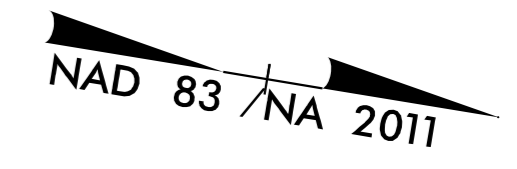
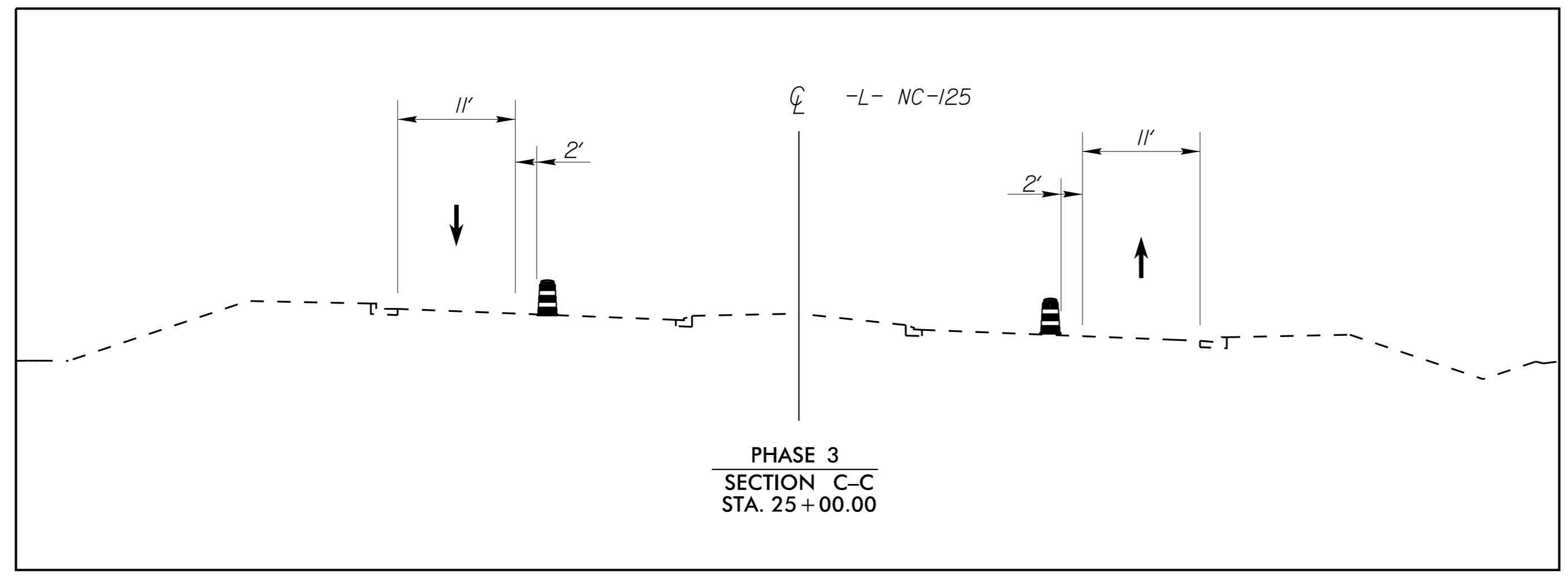





K:\RAL_Roadway\01036392 - R-3822 and U-5725\TrafficControl\Plan Sheets\R-3822_TCP_psh_24.dgn 6/28/2018

Kimley»Horn

APPROVED: *David Skelton* DATE: 6/29/2018

**PHASE 3
TRAFFIC CONTROL
DETAILS**

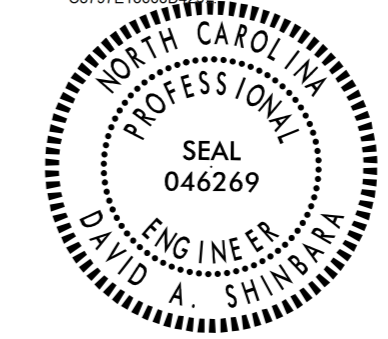
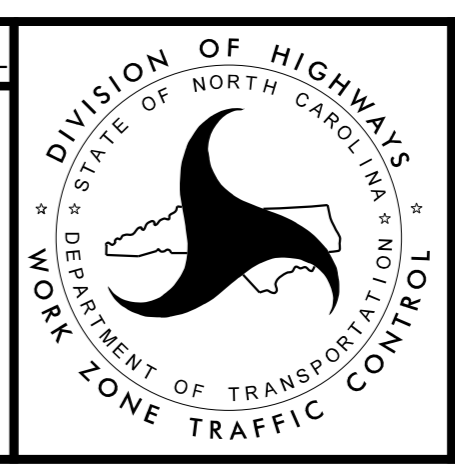


-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

K:\RAL_Roadway\01036392 - R-3822 and U-5725\TrafficControl\Plan Sheets\R-3822_TCP_psh_25.dgn
6/28/2018

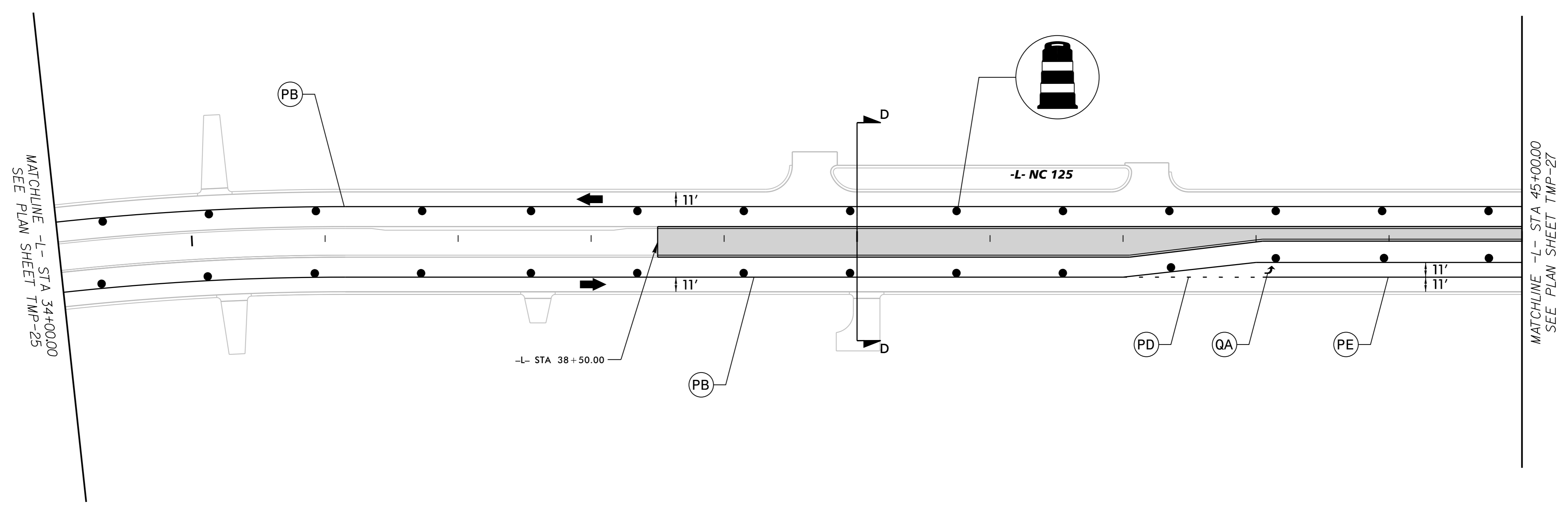
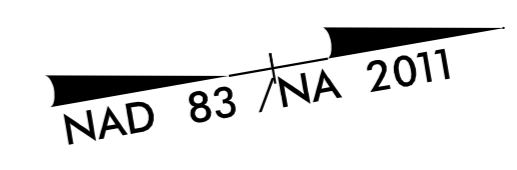
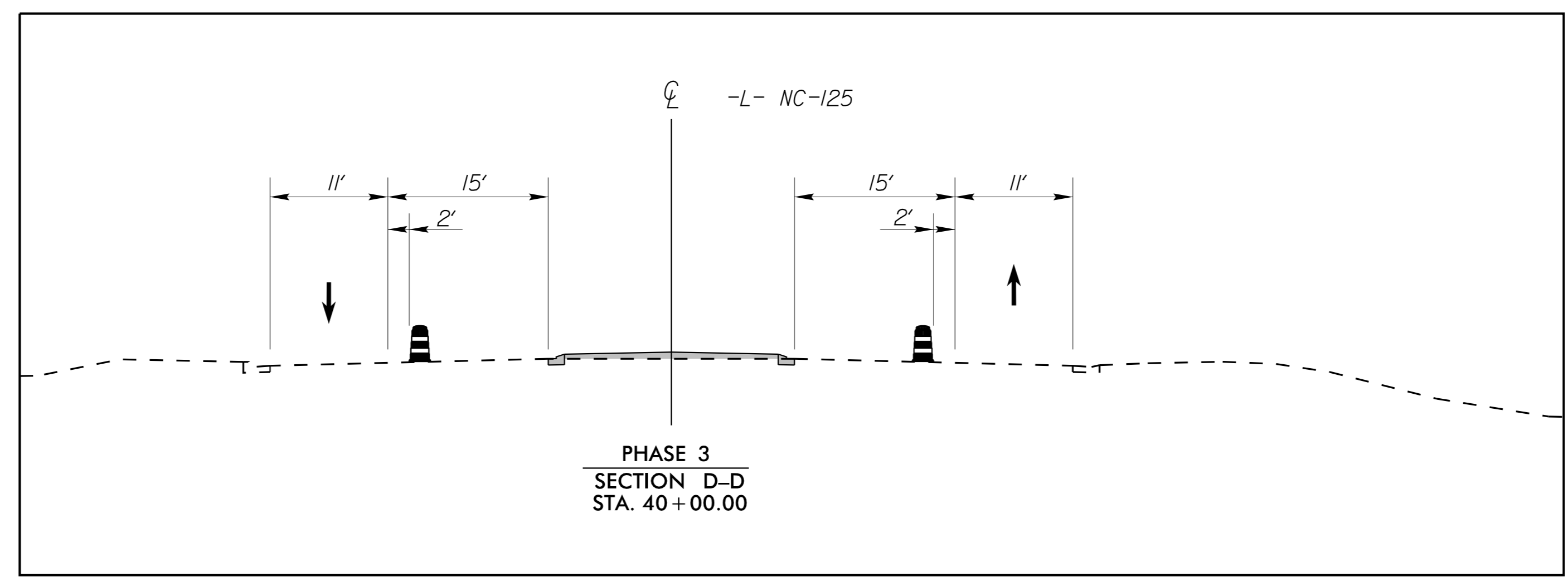
APPROVED: *David Stinson* DATE 6/29/2018




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Kimley»Horn


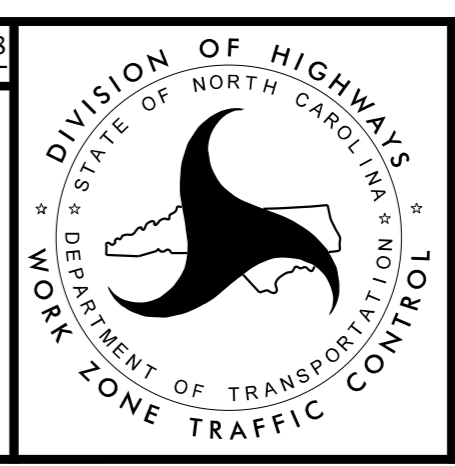
**PHASE 3
TRAFFIC CONTROL
DETAILS**



-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

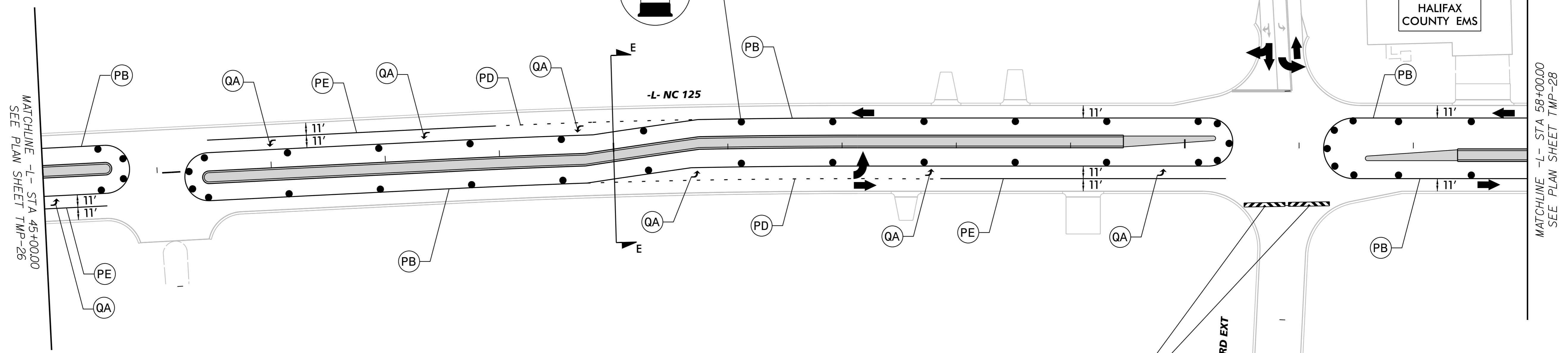
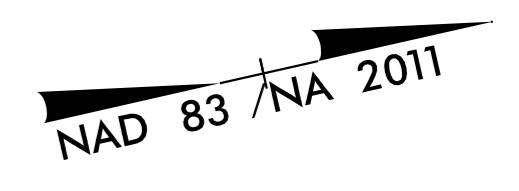
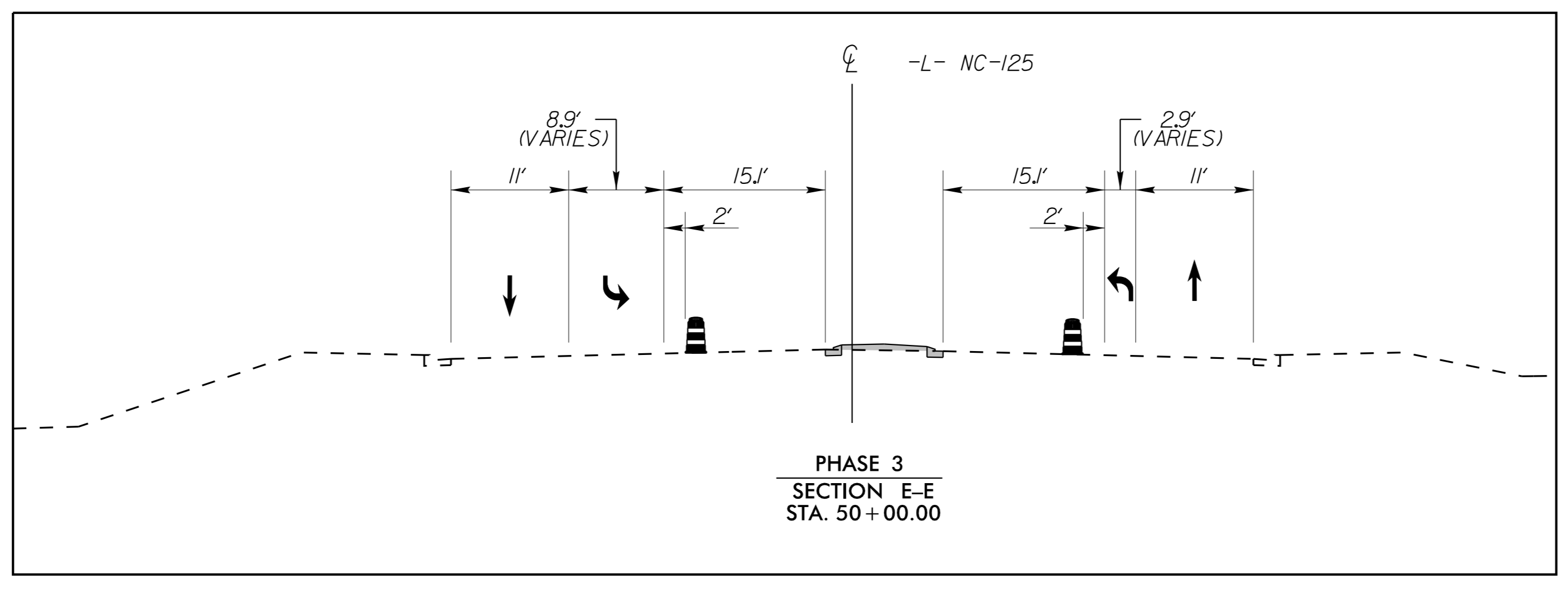
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6/28/2018

APPROVED: David Steinborn DATE: 6/29/2018

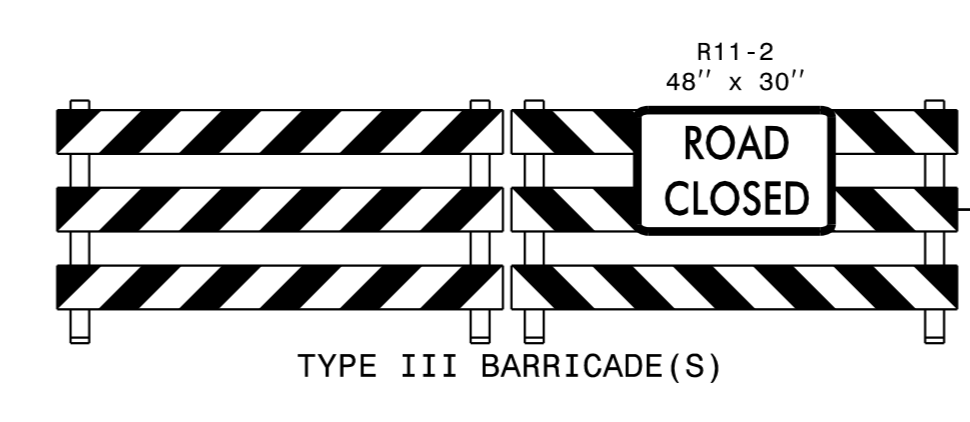
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**PHASE 3
TRAFFIC CONTROL
DETAILS**



MATCHLINE -L- STA 45+00.00
SEE PLAN SHEET TMP-26

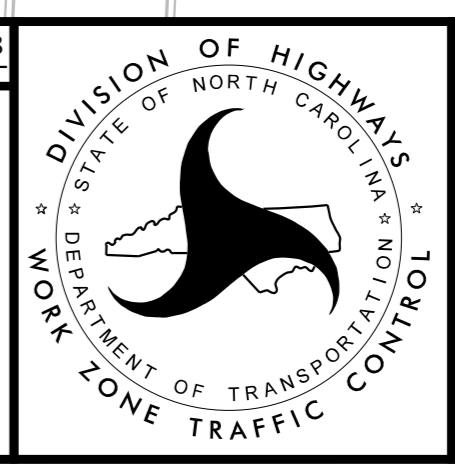
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SEE PLAN SHEET TMP-28



-Y3- AMERICAN LEGION RD EXT

- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

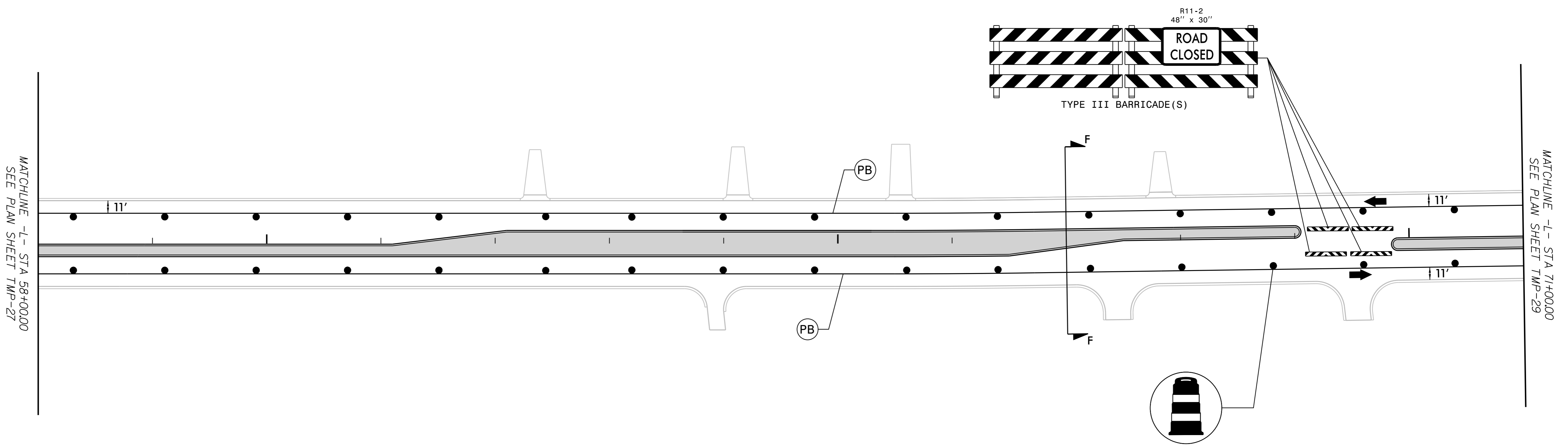
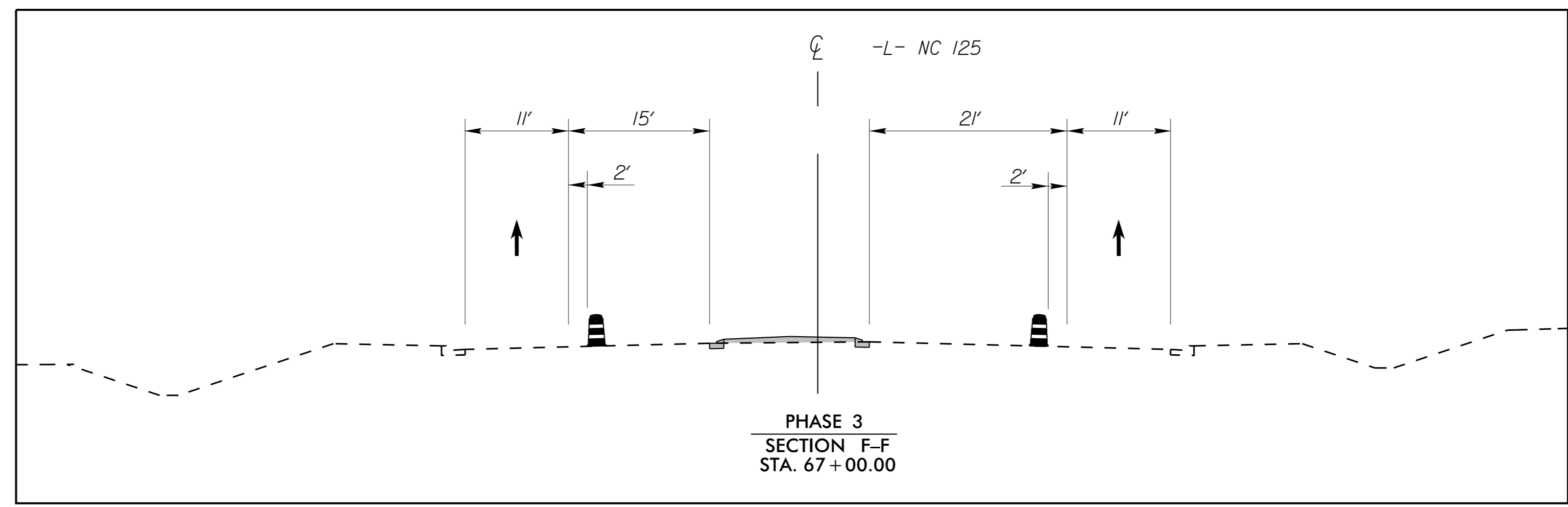
APPROVED: *David Skelton* DATE: 6/29/2018



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**PHASE 3
TRAFFIC CONTROL
DETAILS**

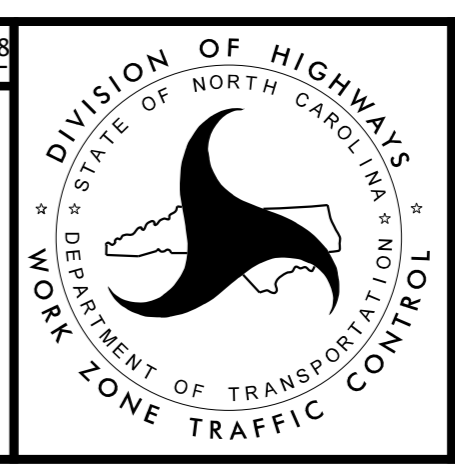
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- WORK AREA
- PAVEMENT REMOVAL
- WEDGING

APPROVED: David Skirbom DATE: 6/29/2018

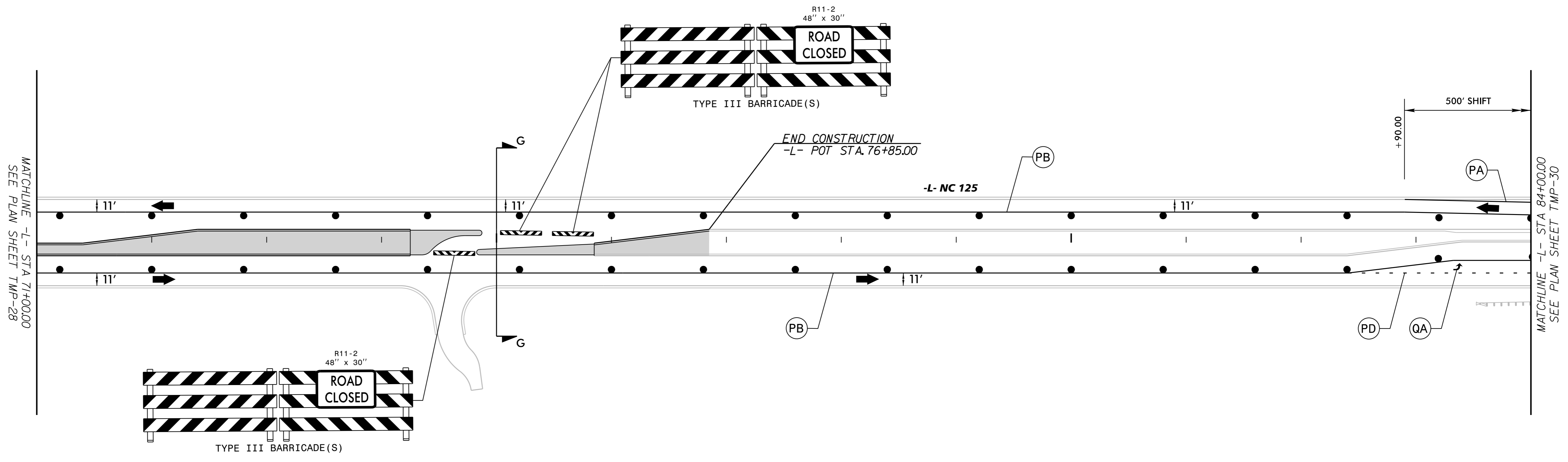
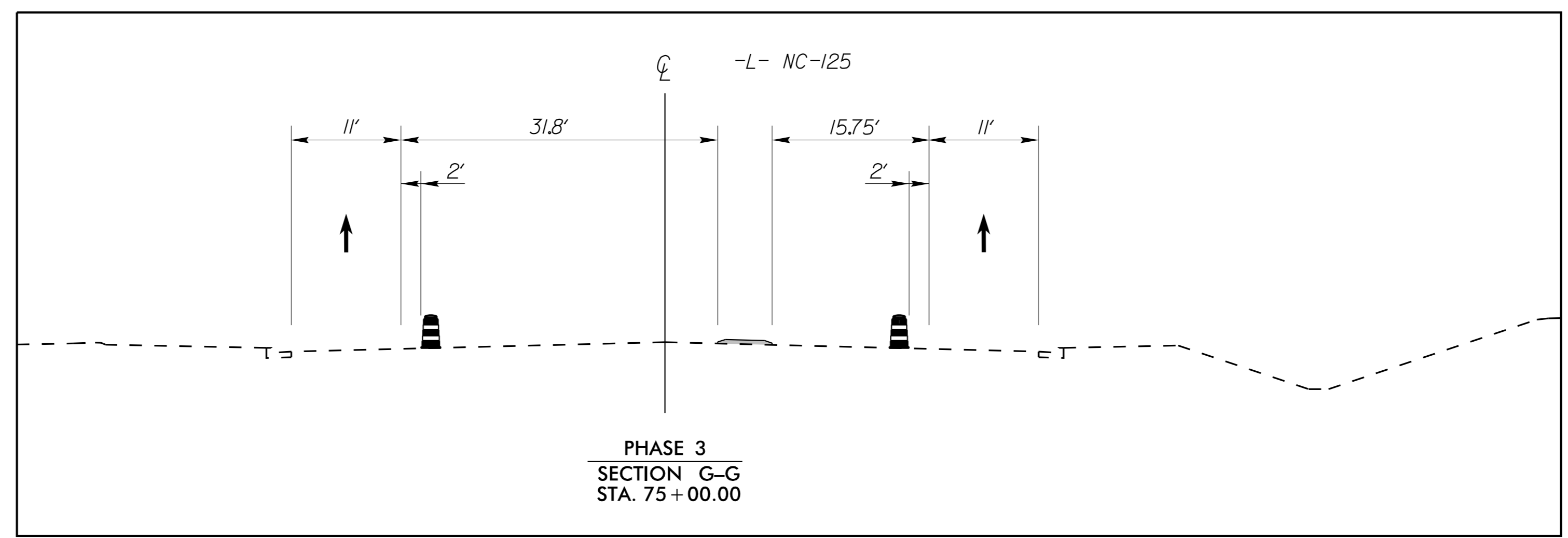
NORTH CAROLINA
PROFESSIONAL
SEAL
046269
ENGINEER
DAVID A. SHINGARA

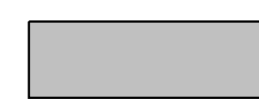
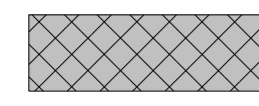
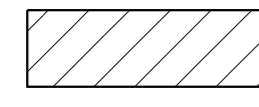


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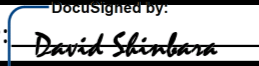
**PHASE 3
TRAFFIC CONTROL
DETAILS**

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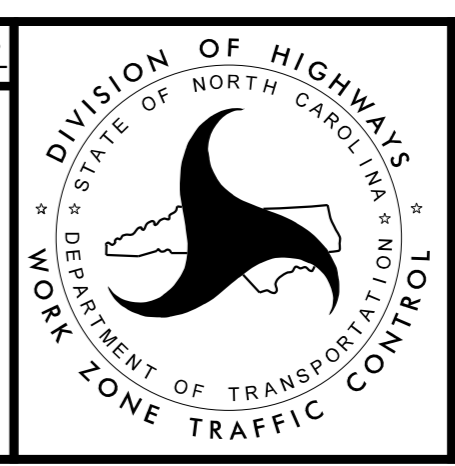
-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

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APPROVED:  DATE: 6/29/2018

SEAL: 046269

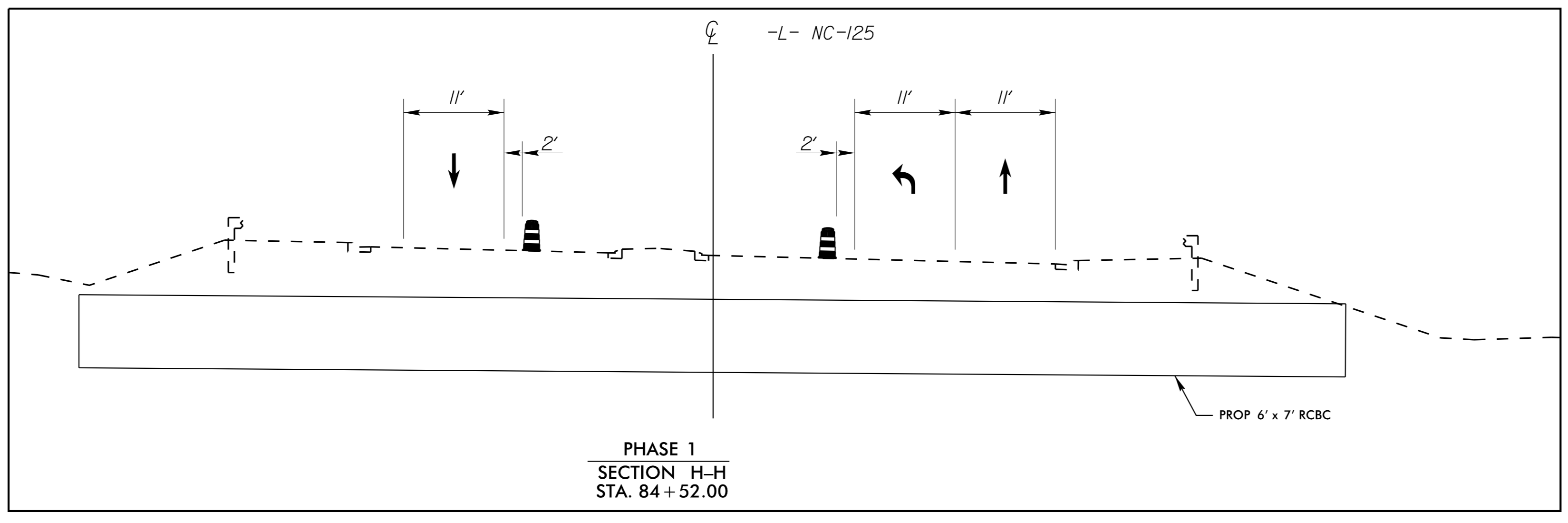
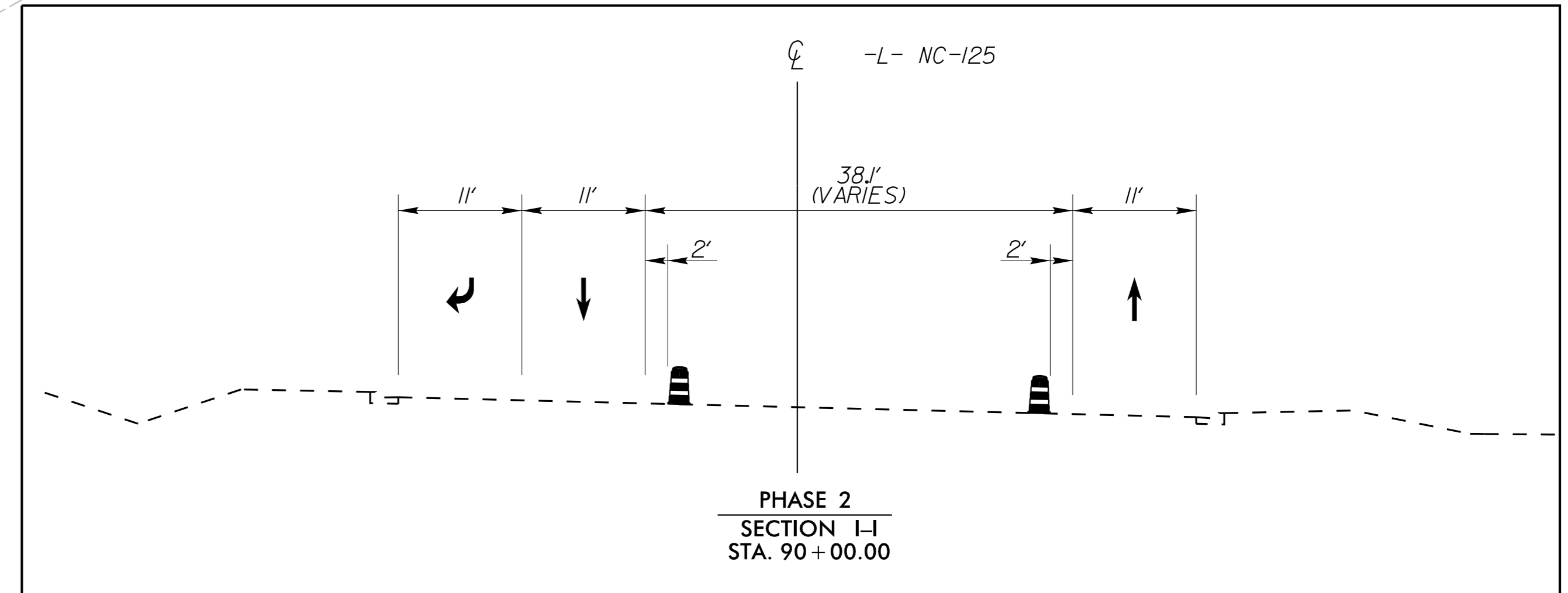
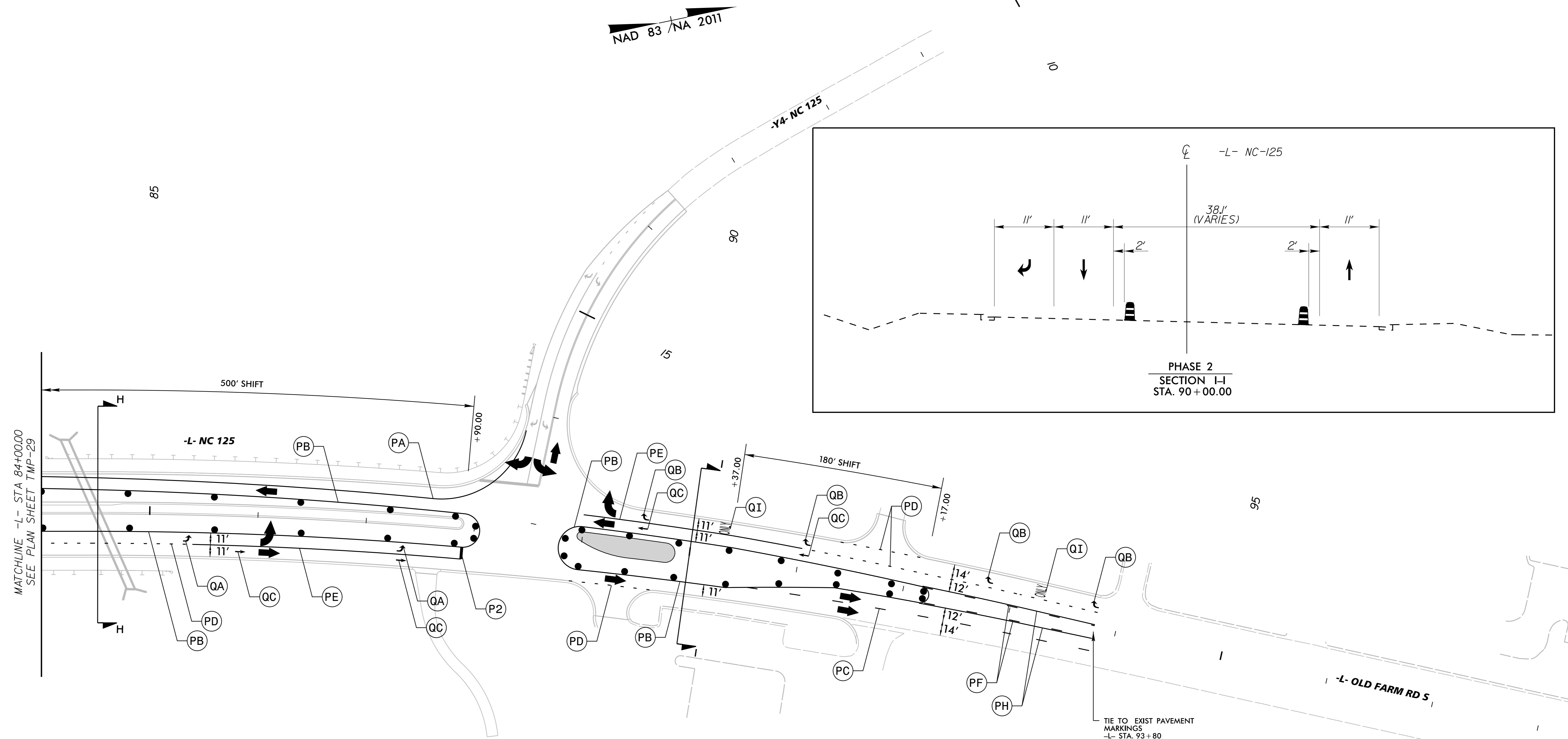
DAVID A. SHINN, P.E.



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PHASE 3 TRAFFIC CONTROL DETAILS

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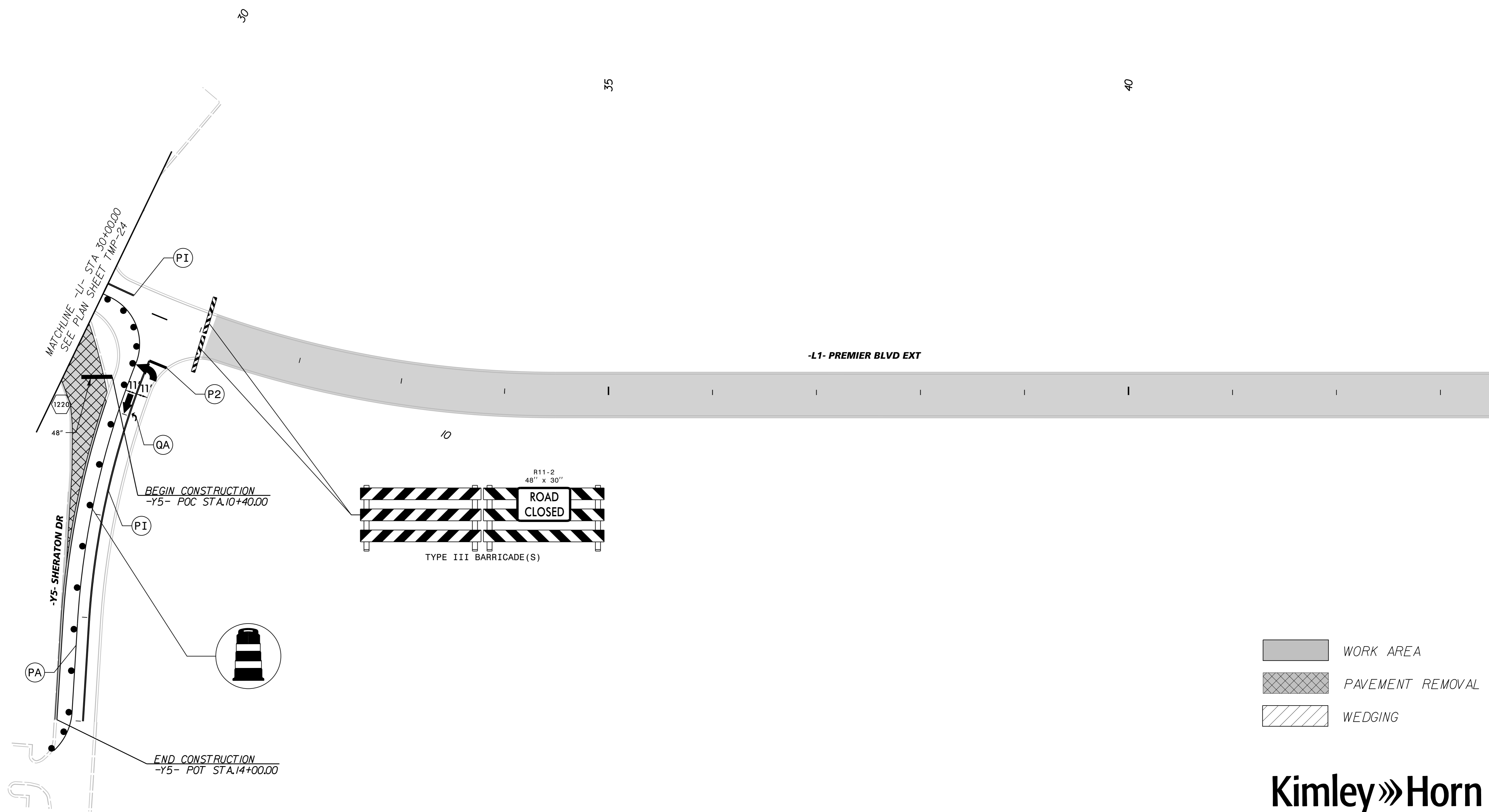
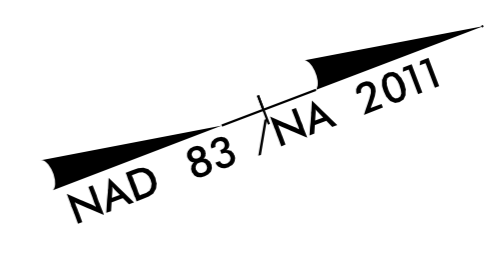
- WORK AREA
- PAVEMENT REMOVAL
- WEDGING




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APPROVED: *David Sklar* DATE: 6/29/2018

**PHASE 3
TRAFFIC CONTROL
DETAILS**

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-  WORK AREA
-  PAVEMENT REMOVAL
-  WEDGING

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APPROVED: David Steinborn DATE: 6/29/2018

**PHASE 3
TRAFFIC CONTROL
DETAILS**

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