

LEGEND

├ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

TOUTDED OTATIONADY OTONIANO FOR THE

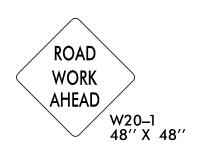
-Y- LINE SIGNING

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE

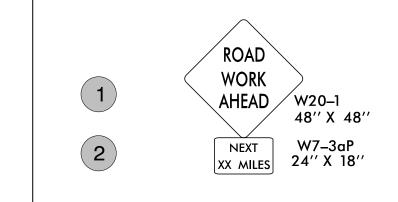
-Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.





PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

SIGNING NOTES AND CEMENT PER DIRECTION



PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS.
ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.

#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)

SP 13107 48" X 48"

ROAD

UNDER

48" X 24"

- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.
- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.
 - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.
 - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.
 - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.
 - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.
 - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.

5 END ROAD WORK

PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

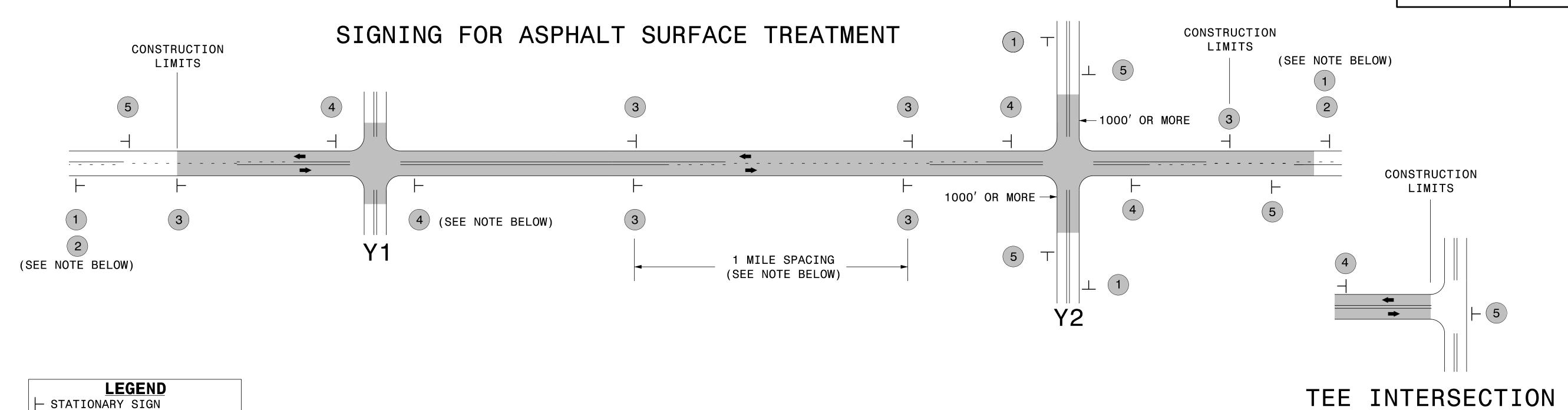
OF HIGH NORTH CARPOLINA WORTH CARPOLINA ** DEEPPRAMENTO OF TRANSPOOD TRAFFIC

RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS

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4/20/2016 C:\Users\rmgarrett\Desktop

PROJ. REFERENCE NO. SHEET NO. 2019CPT.13.03.10571 TMP-2



MAINLINE (-L-) SIGNING

PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. AHEAD W20-1 NO O XX MILES 24" X 18" ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS) AND **LOOSE** - ALTERNATE THE FOLLOWING TWO SIGNS: \mathbf{T} **GRAVEL** SH - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". E 48" X 48" - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART NO ER THEREAFTER. (UNMARKED) \Box - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION PAVEMENT ING AND SPACE 1 MILE APART THEREAFTER. 48'' X 48'' I GN EME - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. **ROAD** - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. SO UNDER - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE. PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN ROAD WORK WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION. G20–2 A

48" X 24"

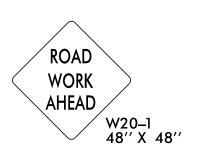
-Y- LINE SIGNING

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

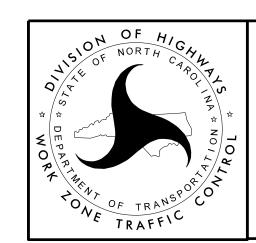
WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED
-Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE

-Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.



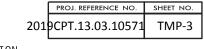


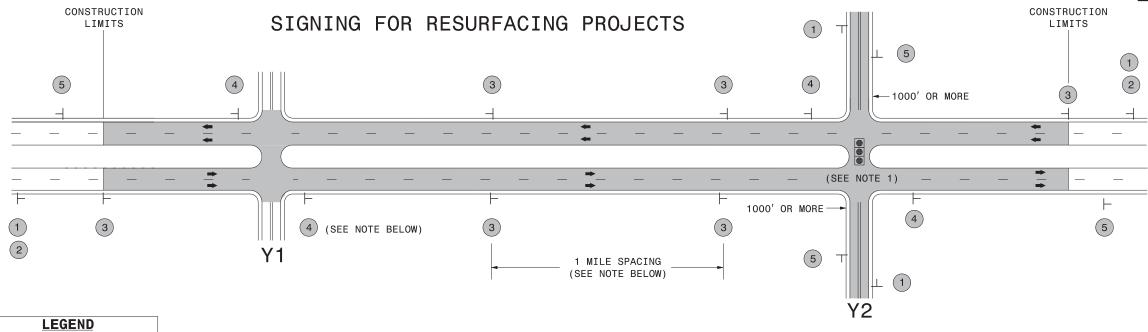
PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.



ADVANCE WARNING SIGNS
FOR
ASPHALT SURFACE TREATMENTS
2 LANE ROADWAYS

SYK MGARKETT VDOWNIOAAS NRESUK TACING_AAV WARN_ZEN - ASI.AGK garrett ← DIRECTION OF TRAFFIC FLOW

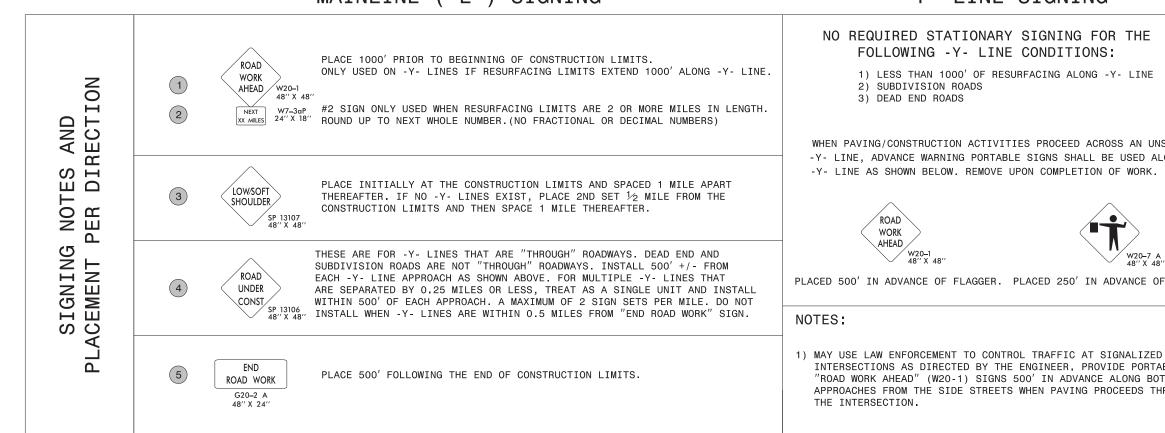




- STATIONARY SIGN ← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING



NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE

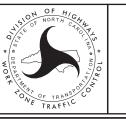
WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE

-Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.



PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH



RESURFACING ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN MULTI-LANE ROADWAYS W/ SHOULDER SECTIONS

RUMBLE STRIPS SPACED 5' ON CENTER

175'

RUMBLE STRIPS SPACED 2'

ON CENTER

11-13 STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

Y THERMOPLASTIC STRIPS TWO-WAY RUMBLE

ENGLISH STANDARD DRAWING FOR -LANE . OM L

SHEET 1 OF 1

STATE OF UT-13
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. RUMBLE STRIPS SPACED 2' ON CENTER RUMBLE STRIPS SPACED 5' ON CENTER NOTE 3 22' TWO-LANE,

22'

175′

(IF SIGNALIZED)

GENERAL NOTES:

50'

- ALL RUMBLE STRIPS SHALL BE CENTERED IN THE LANE AND SHALL BE 2 FEET LESS THAN THE WIDTH OF THE TRAVEL LANE
- RUMBLE STRIPS SHALL BE PLACED USING 4" x 240 MIL WHITE THERMOPLASTIC PAVEMENT MARKING MATERIAL. 2.

NOTE 3

PLACEMENT OF STOP-AHEAD (W3-1) OR SIGNAL-AHEAD (W3-3) SIGNS SHALL COMPLY WITH THE 2009 MUTCD SECTION 5C.04.

SHEET 1 OF 1

ENGLISH

STANDARD

DRAWING

FOR

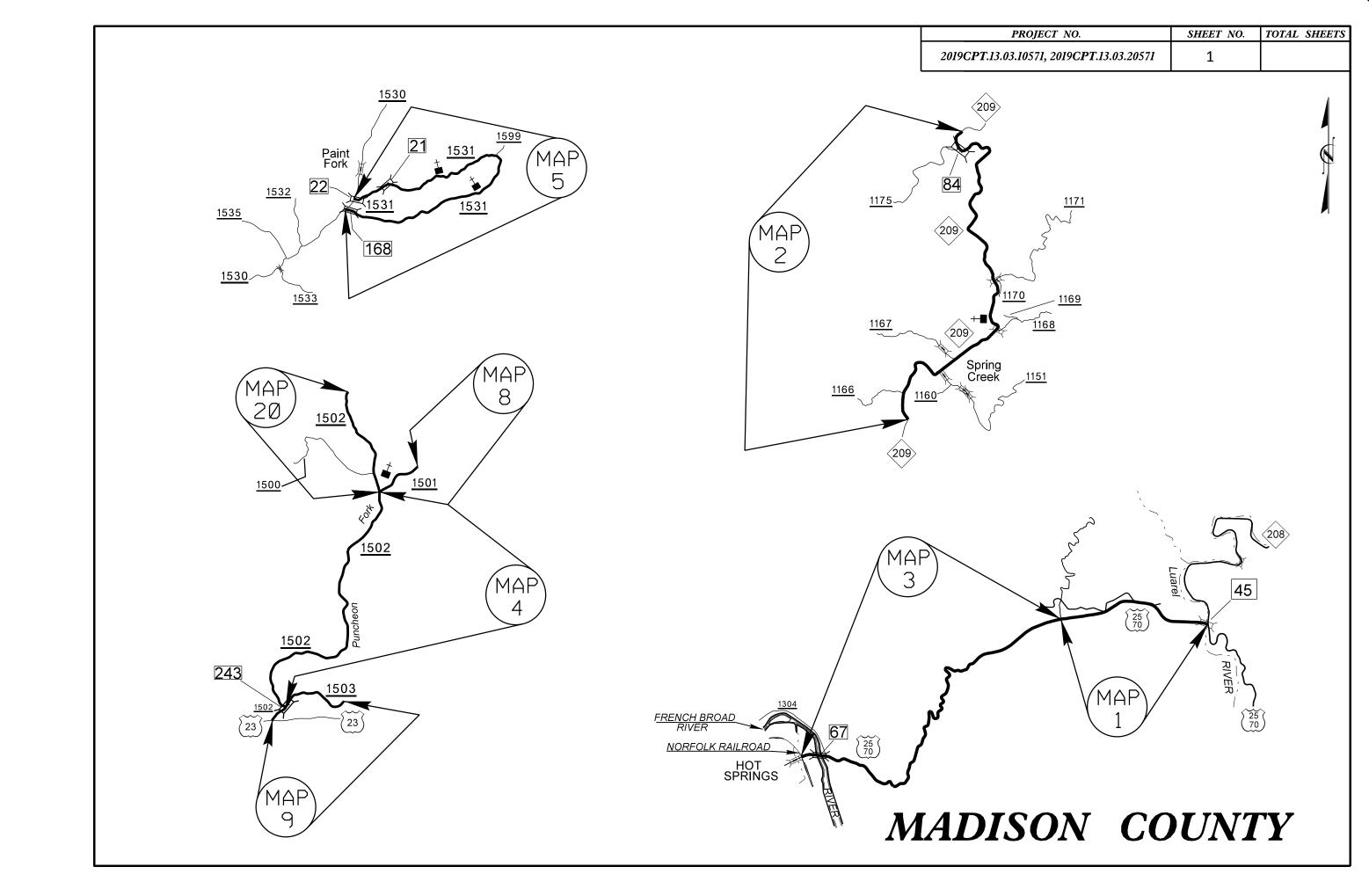
RUMBLE

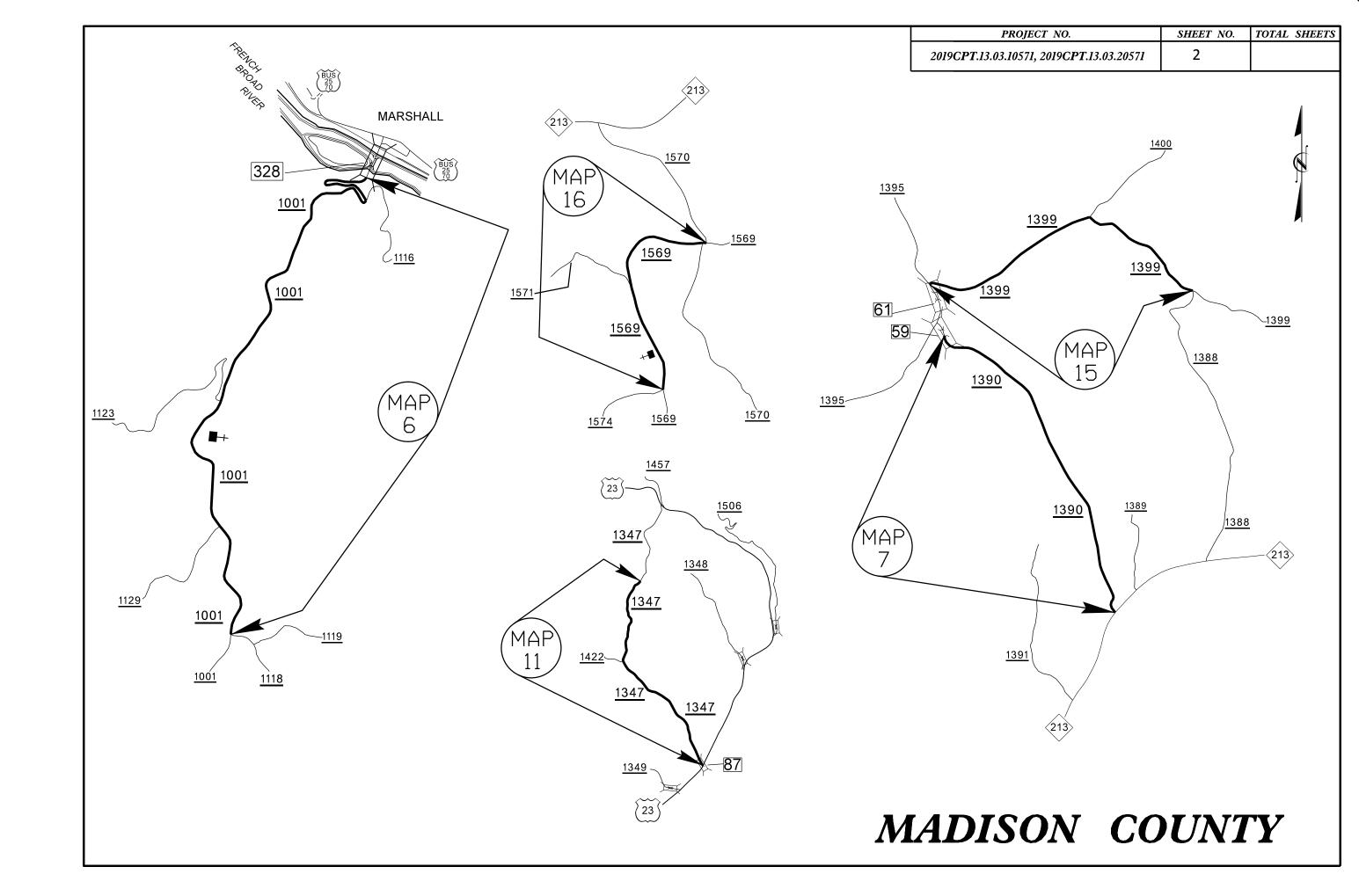
STRIPS

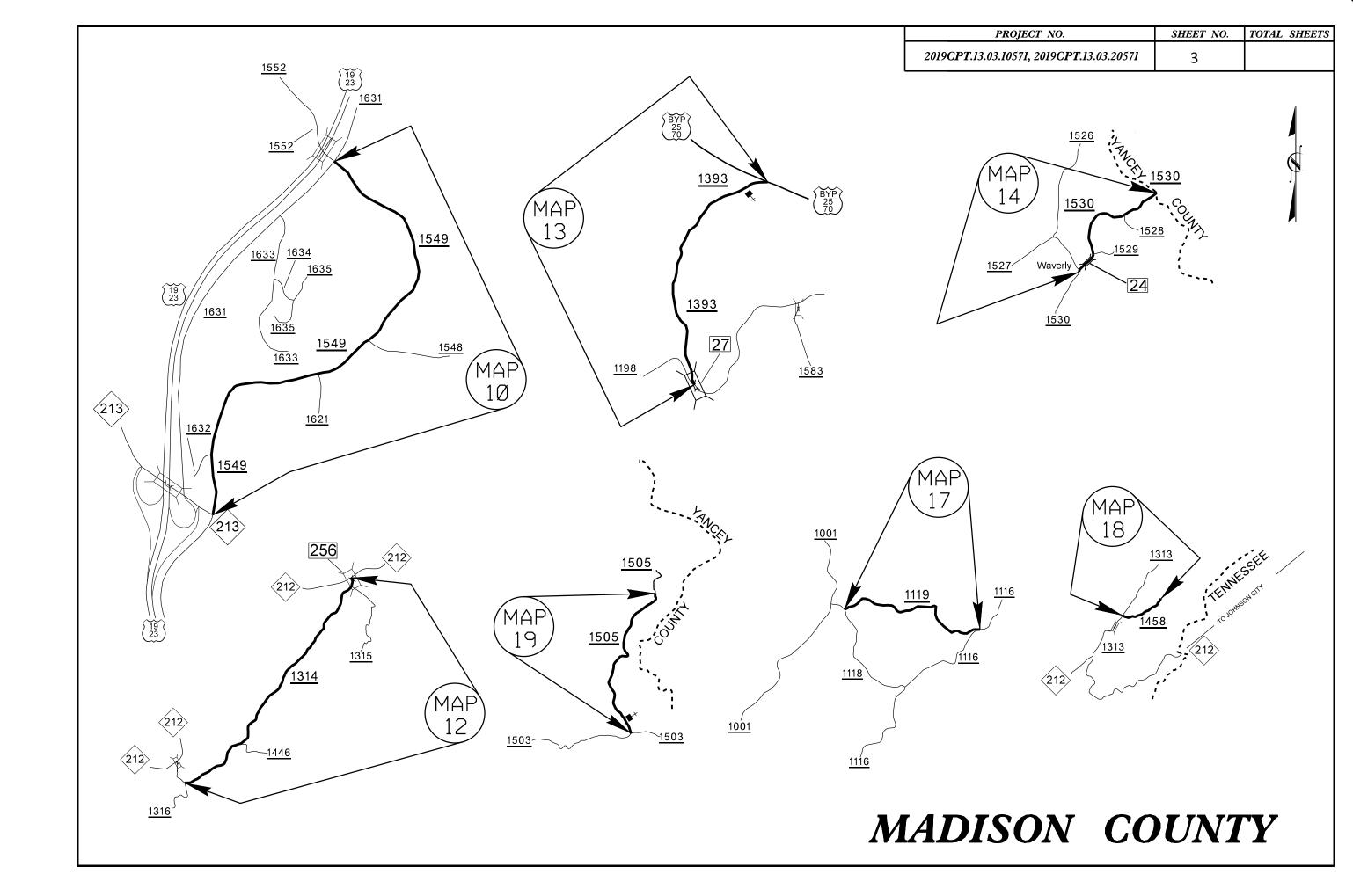
TWO-WAY

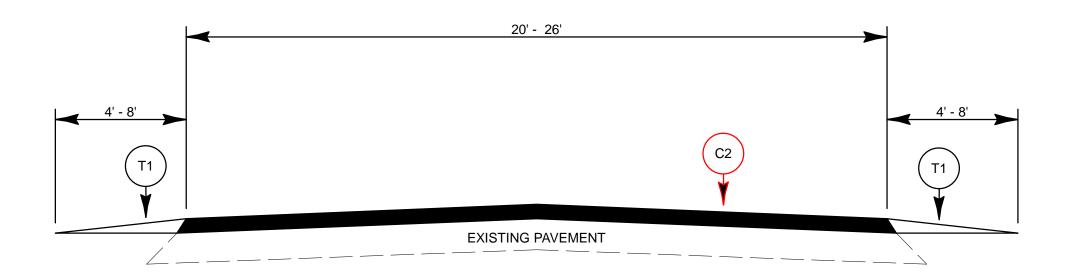
THERMOPLAS

TIC

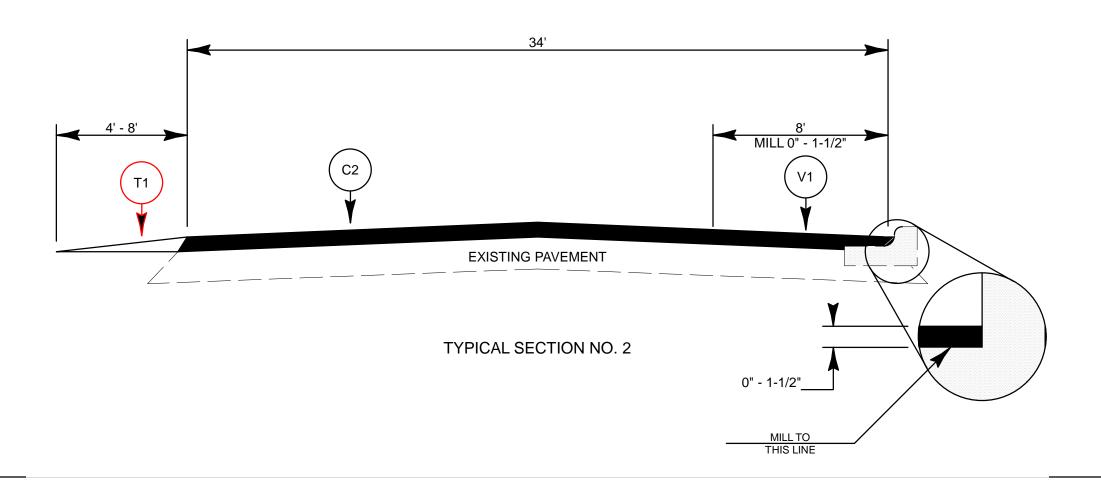




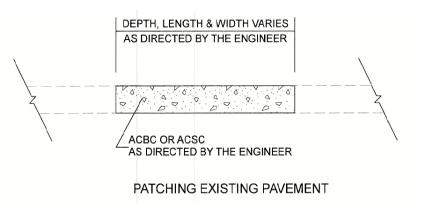




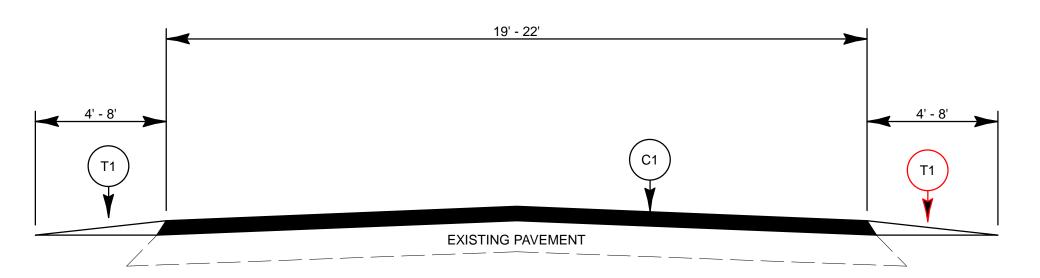
TYPICAL SECTION NO. 1



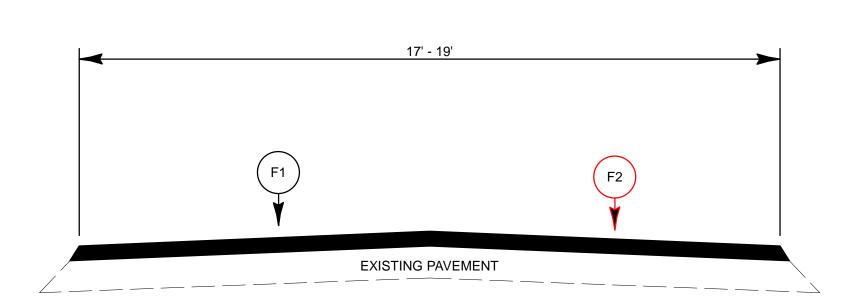
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571	4	
2019CF1.13.03.20371		



	PAVEMENT SCHEDULE
C1	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YARD
C2	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
F1	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F2	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT, 0 TO 1-1/2" DEPTH
V2	INCIDENTAL MILLING
V3	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH
Y1	LATEX MODIFIED MICRO-SURFACING, TYPE III



TYPICAL SECTION NO. 3

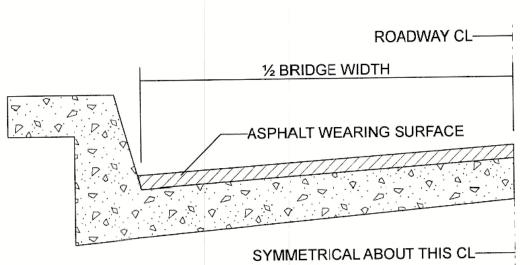


TYPICAL SECTION NO. 4

PROJECT NO. SHEET NO. TOTAL SHEETS

2019CPT.13.03.10571,
2019CPT.13.03.20571

5



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: \$4.75A ½", \$F9.5A 1.0", \$9.5X 1.5", \$12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A ¾", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C ½". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: \$4.75A 1.0", \$F9.5A 1.5",\$9.5X 2.0", \$12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C ½".

NOTES

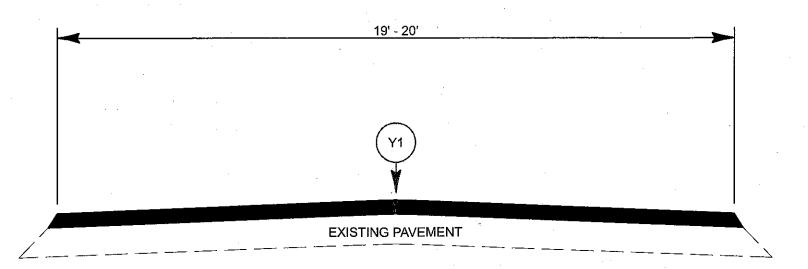
ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.

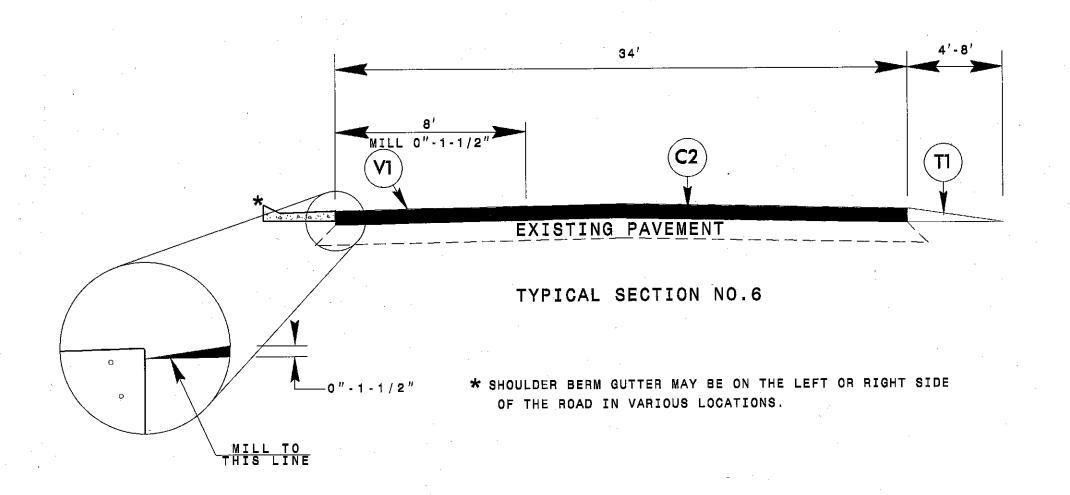
SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE.

BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

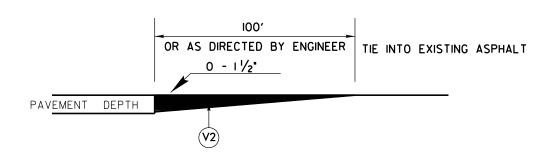
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571		
2019CF1.13.03.20371	6	



TYPICAL SECTION NO. 5

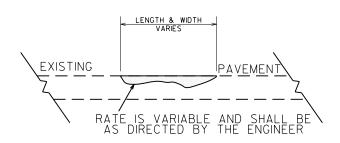


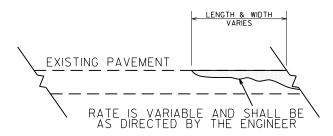
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571	7	



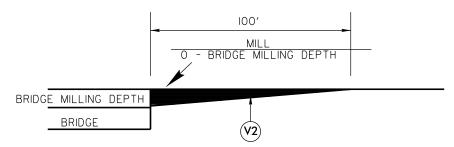
DETAIL TO TIE INTO EXIST PAVEMENT

THE CONTRACTOR'S ATTENTION IS DIRECTED TO
THE FACT THAT HE WILL BE REQUIRED TO MILL
THE EXISTING ASPHALT PAVEMENT TO ENSURE A PROPER
TIE-IN WITH THE EXISTING SURFACE AT THE BEGINNING, END
AND Y LINES OF EACH MAP TO BE RESURFACED WITH
ASPHALT CONC SURFACE COURSE, TYPE S9.5C.
THIS WILL BE PAID FOR AS INCIDENTAL MILLING.



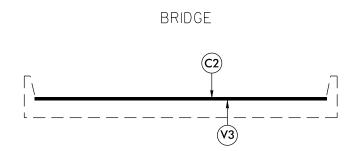


DETAIL SHOWING METHOD OF WEDGING



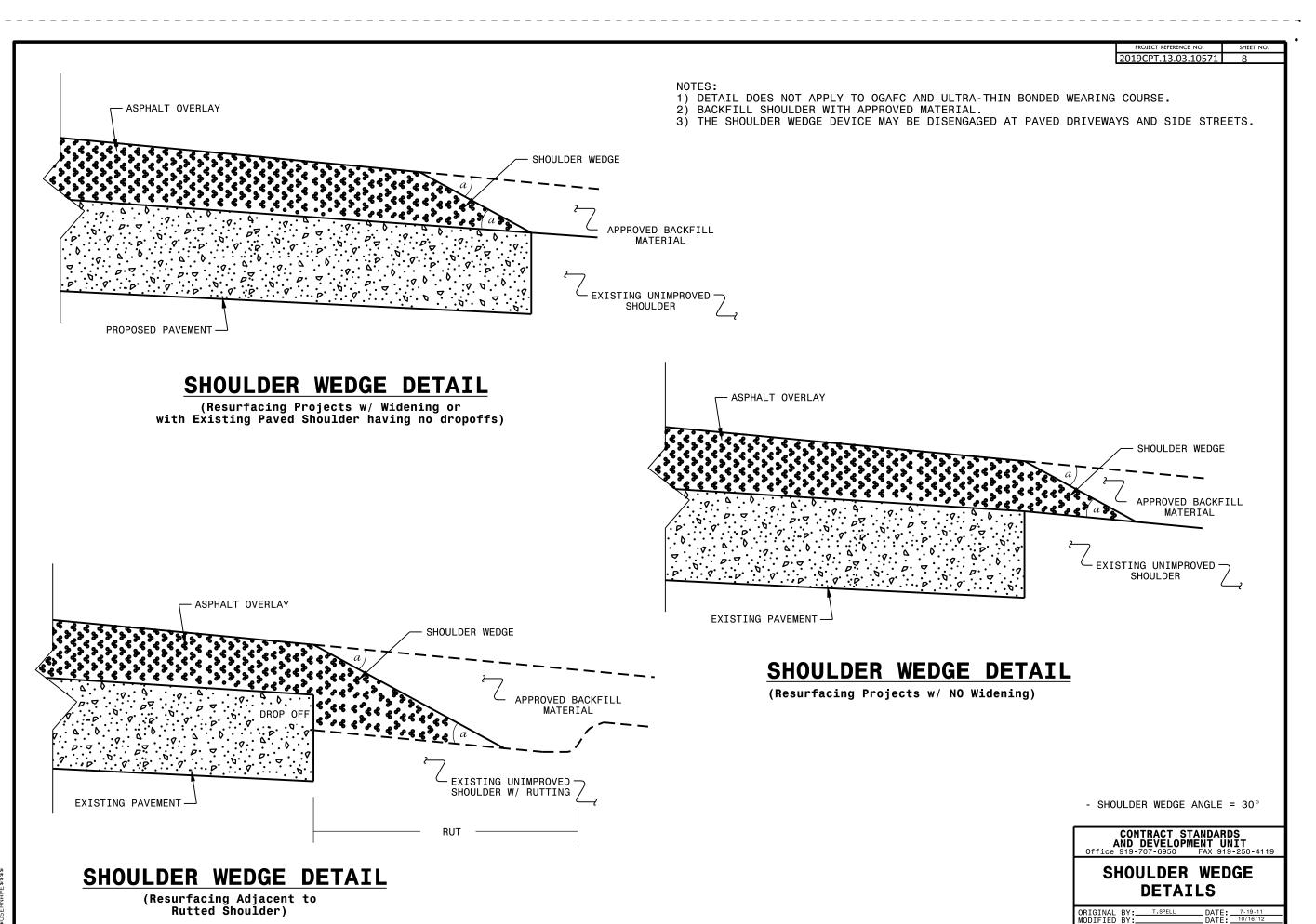
MILLING DETAIL AT BRIDGE APPROACHES

WHERE BRIDGES WILL BE MILLED THEN RESURFACED. THIS WILL BE PAID FOR AS INCIDENTAL MILLING. USE AT BRIDGE NUMBERS: 45 MAP 1, 84 MAP 2, 67 MAP 3, AND 243 MAP 9.



BRIDGE DETAIL

BRIDGE 45 MAP 1, 84 MAP 2, 67 MAP 3, AND 243 MAP 9. MILL 1⁻¹/₂" OFF EXISTING PAVEMENT SEE MAPS FOR BRIDGE LOCATION



PROJECT NO.	SHEET NO.	TOTAL NO.
2019CPT.13.03.10571, 2019CPT.13.03.20571		
2019CP1.13.03.103/1, 2019CP1.13.03.203/1	9	

SUMMARY OF QUANTITIES

										1	4220000000 F	3 0 101 101		OF Q			4540000000	1 453300000 F	457500000 5	1 4704000000 5	1 4003500000 5	1 403000000 F	403000000 F	4020F00000 N	4004000000 F	200000000000000000000000000000000000000
PROJECT NO	COLINITY	MAP NO	POLITE	DESCRIPTION	TYP NO	LANES LANE	FINAL	WARM MIX	LENGTH	WIDTH	1220000000-E INCIDENTAL	1245000000-E SHOULDER	1260000000-E AGGREGATE	1297000000-E MILLING	1308000000-E MILLING	1330000000-E INCIDENTAL	1519000000-E ASPHALT CONC			1704000000-E PATCHING	1803500000-E ASPHALT	1820000000-E ASPHALT	1838000000-E EMULSION FOR	1838500000-N VACUUM	1891000000-E LATEX	2605000000-N CONCRETE
PROJECT NO	COUNTY	I WAP NO	KOUTE	DESCRIPTION	TYPNO		SURFACE		LENGIA	WIDIH	STONE BASE	RECONSTRUCTION	SHOULDER	ASPHALT	ASPHALT	MILLING	SURFACE	SURFACE	BINDER FOR	EXISTING	SURFACE	SURFACE	ASPHALT	TRUCK	MODIFIED	CURB RAMP
							TESTING				STORE DASE	RECONSTRUCTION	BORROW	PAVEMENT,	PAVEMENT,	WILLEAN	COURSE, TYPE	COURSE, TYPE		PAVEMENT	TREATMENT,	TREATMENT,	SURFACE	mock	MICRO-	CONDINAIN
							REQUIRE						20	1-1/2" DEPTH	0" TO 1-1/2"		\$9.5B	\$9.5C	1 2		DOUBLE SEAL	FOG SEAL	TREATMENT		SURFACING,	
														,	DEPTH										TYPE III	
									MI	FT	TON	SMI	TON	SY	SY	SY	TON	TON	TON	TON	SY	SY	GAL	WK	SY	EA
				FROM NC 208 TO BEG PASSING																						
				ANE AT TOP OF THE MTN (MP 15.92																						
2019CPT.13.03.10571	Madisor	n 1	US 25-70	TO MP 17.85)	1	2 2WL	J NO	NO	1.93	26	97	3.86	502	340	2,400	1,800		2,733	164	500						+
				FROM PVMNT JOINT NORTH OF SR																						
				1175 TO PVMNT JOINT AT SPRING																						
				CREEK UNITED METHODIST CHURCH																						
2019CPT.13.03.10571	Madisor	n 2	NC 209	(MP 13.91 TO MP 8.63)	1	2 2WU	J NO	NO	5.28	20	264	10.56	1,373	89		600		5,760	346	900						
				FROM THE END OF MAP 1 TO THE																						
				R/R TRACKS IN HOT SPRINGS (MP						_																
2019CPT.13.03.10571			US 25/70	17.85 TO MP 20.86)	1,2,6	136 2WL	J NO	NO	3.01 10.22	34	151	6.02	783	1,560	4,150	712		5,568	334 844	300						7
TOTAL FOR PROJ	NO. 2019	CP1.13.03.	105/1						10.22		512	20.44	2,658	1,989	6,550	3,112	1	14,061	844	1,700						+
		1	1	FROM SR 1503 TO SR 1501 (MP 0.01			1	+								1	+	1	1		1	1	 			+
2019CPT.13.03.20571	Madisor	n 4	SR 1502	TO MP 3.62)	3	2 2WD	NO	NO	3.61	22	181	7.22	939				4,330		260	650						
				FROM SR 1530 TO SR 1530 (MP 0.00																				*		
2019CPT.13.03.20571	Madisor	n 5	SR 1531	TO MP 4.25)	4	2 2WU	J NO	NO	4.25	18						1,355				900	44,880	44,880	24,684			
				FROM BRIDGE #328 TO SR 1118 (MP	_																					
2019CPT.13.03.20571	Madisor	n 6	SR 1001	8.37 TO MP 5.94) FROM NC 213 TO JOINT AT BRIDGE	5	2 2WL	J NO	NO	2.43	20						-				400					28,512	
2019CPT.13.03.20571	Madisor	7	SR 1390	#59 (MP 1.47 TO MP 0.07)	3	2 2WL	J NO	NO	1.4	20	70	2.80	364				1,527		92	400						
2013011.13.03.20371	ividaisoi	,	3K 1330	FROM SR 1502 TO EOM (MP 0.01 TO		2 2000	110	140	1.7	20	70	2.00	304				1,527		32	400						+
2019CPT.13.03.20571	Madisor	n 8	SR 1501	MP 0.58)	3	2 2WU	NO NO	NO	0.57	21	28	1.14	148				653		39	175						
				FROM US 23 TO SR 1502 (MP 0.01																						
2019CPT.13.03.20571	Madisor	n 9	SR 1503	TO MP 0.24)	3	2 2WU	J NO	NO	0.23	20	12	0.46	60	219		533	251		15	125						
				FROM SR 1631 TO NC 213 (MP 0.06	_																					
2019CPT.13.03.20571	Madisor	n 10	SR 1549	TO MP 1.41) FROM US 23 TO EOP (MP 0.00 TO	3	2 2WL	J NO	NO	1.35	22	68	2.70	351			-	1,619		97	350						
2019CPT.13.03.20571	Madisor	n 11	SR 1347	MP 2.15	4	2 2WU	NO NO	NO	2.15	19										550	23,965	23,965	13,181	*		
		1		FROM SR 1316 TO NC 212 (MP 0.01			1	1																		+
2019CPT.13.03.20571	Madisor	n 12	SR 1314	TO MP 3.43)	5	2 2WU	NO NO	NO	3.42	19										125					38,122	
				FROM US 25/70 TO SR 1198 (MP																				*		
2019CPT.13.03.20571	Madisor	n 13	SR 1393	0.76 TO MP 0.01)	4	2 2WL	J NO	NO	0.75	18						-		ļ	ļ	150	7,920	7,920	4,356			
2019CPT.13.03.20571	Madicar	1.4	SR 1530	FROM SR 1527 TO YANCEY COUNTY LINE (MP 5.37-MP 7.13)	2	2 2WL	J NO	NO	1.76	19	88	3.52	458				1,825		109	700						
2013CF1.13.03.203/1	iviauisor	n 14		FROM SR 1395 TO SR 1388 (MP 0.01	3	2 ZWU	, INU	INU	1./0	13	00	3.34	438	 		+	1,823	1	109	700	1	 	+			+
2019CPT.13.03.20571	Madisor	n 15	SR 1399	TO MP 1.31)	4	2 2WU	NO NO	NO	1.3	18			1	1		1	1			60	13,728	13,728	7,551			
				FROM SR 1570 TO SR 1574 (MP 1.4																	, , , , , , , , , , , , , , , , , , ,	<u> </u>				1
2019CPT.13.03.20571	Madisor	n 16	SR 1569	TO MP 2.17P	4	2 2WL	J NO	NO	0.77	17						1	1			100	7,679	7,679	4,224			
				FROM SR 1118 TO SR 1116 (MP 0.00									1	1		1	1							*		
2019CPT.13.03.20571	Madisor	n 17	SR 1119	TO MP 1.05)	4	2 2WL	J NO	NO	1.05	18		1	 	 		+	+	1	1	300	11,088	11,088	6,099			+
2019CPT.13.03.20571	Madisor	n 18	SR 1458	FROM SR 1313 TO EOP (MP 0.00 TO MP 0.68)	4	2 2WL	J NO	NO	0.68	17			1	1		1	1			50	6,782	6,782	3,730			1
2013011.13.03.20371		. 10	511 1450	FROM SR 1503 TO EOP (MP 0.00 TO	-	2 2000	1,10	140	0.00	1,						1	+	1	1	30	0,702	0,702	3,730			+
2019CPT.13.03.20571	Madisor	n 19	SR 1505	MP 1.94)	4	2 2WU	NO NO	NO	1.94	18			ĺ							700	20,486	20,486	11,268			
				FROM SR 1501 TO EOP (MP 3.62 TO																						1
2019CPT.13.03.20571			SR 1502	MP 4.92)	3	2 2WL	J NO	NO	1.3	21	65	2.60	338			1	1,489		89	600						
TOTAL FOR PROJ NO. 2019CPT.13.03.20571							1		28.96		512	20.44	2,658	219		1,888	11,694	1	701	6,335	136,528	136,528	75,093	7	66,634	
	AND TOT	1	1				-		20.40		1 024	40.00	F 24.0	2 200	6.550	F 000	11 504	14.004	1 545	0.005	120 520	120 520	75 000	7	66.634	7
GR/	AND TOT	AL			l		1		39.18		1,024	40.88	5,316	2,208	6,550	5,000	11,694	14,061	1,545	8,035	136,528	136,528	75,093	7	66,634	

PROJECT NO.	SHEET NO.	TOTAL NO.					
2019CPT.13.03.10571,							
2019CPT.13.03.20571	10						

THERMOPLASTIC AND PAINT QUANTITIES

										4413000000-E	4457000000-N	4687000000-E	4705000000-E	4710000000-E	4721000000-E	481000	00000-E	4847	7010000-E	485000000-E	4905000000-N
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE	LENGTH	WIDTH	WORK ZONE	TEMPORARY	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	PAINT	PAINT	POLYUREA	POLYUREA PAVEMENT	REMOVAL OF	SNOWPLOWABLE
							TYPE			ADVANCE/GENERAL	TRAFFIC	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	MARKING LINES	PAVEMENT MARKING	PAVEMENT
										WARNING SIGNING	CONTROL	MARKING LINES	MARKING LINES	MARKING LINES	MARKING	MARKING	MARKING	MARKING LINES	YELLOW (4", 20 MILS)	LINES (4")	MARKERS
												(4", 240 MILS)	(16", 120 MILS)	(24", 120 MILS)	CHARACTER	LINES (4")	LINES (4")	WHITE (4", 20 MILS)	(HIGHLY REFLECTIVE		
												WHITE	WHITE	WHITE	(120 MILS) RXR	WHITE	YELLOW	(HIGHLY REFLECTIVE	MEDIA)		
																		MEDIA)			
								MI	FT	SF	LS	LF	LF	LF	EA	LF	LF	LF	LF	LF	EA
				FROM NC 208 TO BEG PASSING LANE																	
2040007 42 02 40574			25 70	AT TOP OF THE MTN (MP 15.92 TO		_	21.00	4.00	2.5			300						20.204	20.004		
2019CPT.13.03.10571	Madison	1	US 25-70	MP 17.85)	1	2	2WU	1.93	26	-								20,381	20,381		145
				FROM PVMNT JOINT NORTH OF SR																	
				1175 TO PVMNT JOINT AT SPRING						1,147	*										
				CREEK UNITED METHODIST CHURCH						,											
2019CPT.13.03.10571	Madison	2	NC 209	(MP 13.91 TO MP 8.63)	1	2	2WII	5.28	20									56,813	56,813		700
20130: 1113103110371	Maaison	_	110 203	FROM THE END OF MAP 1 TO THE R/	-			5.20										30,013	30,013		700
				TRACKS IN HOT SPRINGS (MP 17.85																	
2019CPT.13.03.10571	Madison	3	US 25/70	TO MP 20.86)	1,2,6	2	2WU	3.01	34				40	132	4			34,000	32,550		365
		•	•	,				10.22		1,147	1	300	40	132	4			111,194	109,744		1,210
TOTAL FOR PROJ	NO. 20190	PT.13.03.1	10571																20,938		
				<u> </u>														-			·
				FROM SR 1503 TO SR 1501 (MP 0.01																_	
2019CPT.13.03.20571	Madison	4	SR 1502	TO MP 3.62)	3	2	2WD	3.61	22	1		1				76,243	76,243				
				FROM SR 1530 TO SR 1530 (MP 0.00								1									
2019CPT.13.03.20571	Madison	5	SR 1531	TO MP 4.25)	4	2	2WU	4.25	18							89,760	89,760				
				FROM BRIDGE #328 TO SR 1118 (MP																	
2019CPT.13.03.20571	Madison	6	SR 1001	8.37 TO MP 5.94)	5	2	2WU	2.43	20							51,322	51,322			102644	
				FROM NC 213 TO JOINT AT BRIDGE																	
2019CPT.13.03.20571	Madison	7	SR 1390	#59 (MP 1.47 TO MP 0.07)	3	2	2WU	1.4	20							29,568	29,568				
				FROM SR 1502 TO EOM (MP 0.01 TO																	
2019CPT.13.03.20571	Madison	8	SR 1501	MP 0.58)	3	2	2WU	0.57	21							12,038	12,038				
2040007 42 02 20574			CD 4500	FROM US 23 TO SR 1502 (MP 0.01 TO		_	21.00	0.00								4.050					
2019CPT.13.03.20571	Madison	9	SR 1503	MP 0.24)	3	2	2WU	0.23	20							4,858	4,858				
2010CDT 12 02 20F71	Madison	10	SR 1549	FROM SR 1631 TO NC 213 (MP 0.06 TO MP 1.41)	3	2	214/11	1 25	22							28,512	28,512				
2019CPT.13.03.20571	iviauison	10	3K 1549	FROM US 23 TO EOP (MP 0.00 TO MF			2WU	1.35	22							28,512	28,512				
2019CPT.13.03.20571	Madison	11	SR 1347	2.15	4	2	2WU	2.15	19							45,408	45,408				
2013CF1.13.03.20371	iviauison	11	31(1347	FROM SR 1316 TO NC 212 (MP 0.01	4		2000	2.13	13							43,400	43,408				
2019CPT.13.03.20571	Madison	12	SR 1314	TO MP 3.43)	5	2	2WU	3.42	19	3,251	*					72,230	72,230			144460	
2013011113103120371	- Widaison		511 1511	FROM US 25/70 TO SR 1198 (MP 0.76		Ť	20	52								72,230	72,230			111100	
2019CPT.13.03.20571	Madison	13	SR 1393	TO MP 0.01)	4	2	2WU	0.75	18			1	1			15,840	15,840				
	1	1		FROM SR 1527 TO YANCEY COUNTY			T			1		1		1	1						İ
2019CPT.13.03.20571	Madison	14	SR 1530	LINE (MP 5.37-MP 7.13)	3	2	2WU	1.76	19							37,171	37,171				
				FROM SR 1395 TO SR 1388 (MP 0.01						1		1									
2019CPT.13.03.20571	Madison	15	SR 1399	TO MP 1.31)	4	2	2WU	1.3	18]		1				27,456	27,456				
				FROM SR 1570 TO SR 1574 (MP 1.4						1		1									
2019CPT.13.03.20571	Madison	16	SR 1569	TO MP 2.17P	4	2	2WU	0.77	17	1		1				16,262	16,262				
				FROM SR 1118 TO SR 1116 (MP 0.00								1									
2019CPT.13.03.20571	Madison	17	SR 1119		4	2	2WU	1.05	18	1		1				22,176	22,176				ļ
				FROM SR 1313 TO EOP (MP 0.00 TO			1					1	1						1		1
2019CPT.13.03.20571	Madison	18	SR 1458	MP 0.68)	4	2	2WU	0.68	17	4		1							ļ		ļ
	1			FROM SR 1503 TO EOP (MP 0.00 TO			1					1	1						1		1
2019CPT.13.03.20571	Madison	19	SR 1505	MP 1.94)	4	2	2WU	1.94	18	4		1		1	1	40,973	40,973		 		
2010CDT 42 02 20574	Madie	20	CD 4500	FROM SR 1501 TO EOP (MP 3.62 TO	,		214/11	1.3	34			1	1			27.456	27.456		1		1
2019CPT.13.03.20571	Madison	20	SR 1502	MP 4.92)	3	2	2WU	1.3 28.96	21	2.251	1	 	 	1		27,456	27,456		 	247 104	_
TOTAL FOR PROJ	NO. 20190	PT.13.03.2	20571			1	1	28.96	-	3,251	1	 	 	 	1	597,273	597,273 4,546		<u> </u>	247,104	
						<u> </u>	<u> </u>		<u> </u>	1		1	1	1	ı	1,194	7,340	1			1
						I	T	39.18	1	4,398	1	300	40	132	4	597,273	597,273	111,194	109,744	247,104	1,210
GR	RAND TOTA	L					1	33.10		7,330	<u> </u>	300	+0	132	1		4,546		20,938	277,104	1,210
						<u> </u>	1	l		1	l	l	I	1	i	1,13	7,370		-0,550		1