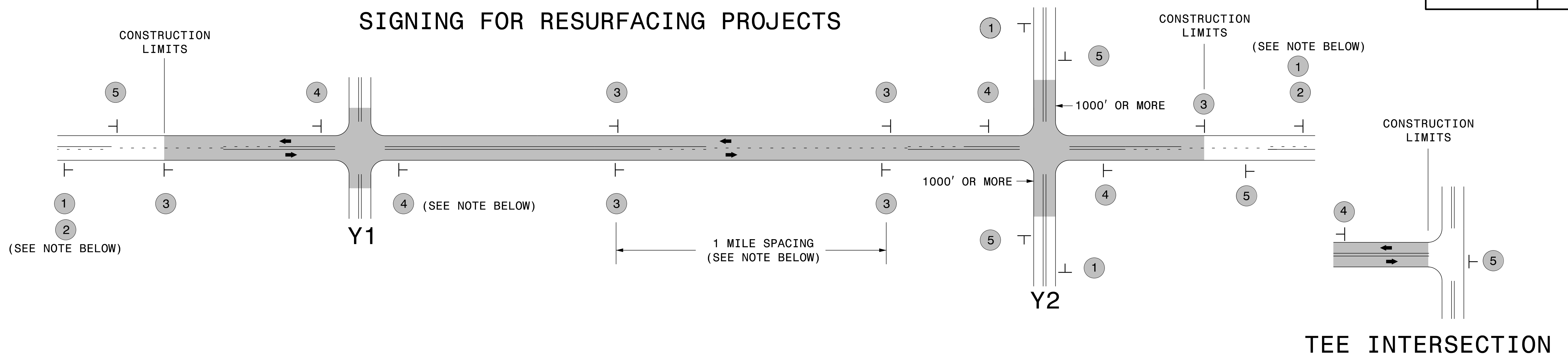


SIGNING FOR RESURFACING PROJECTS



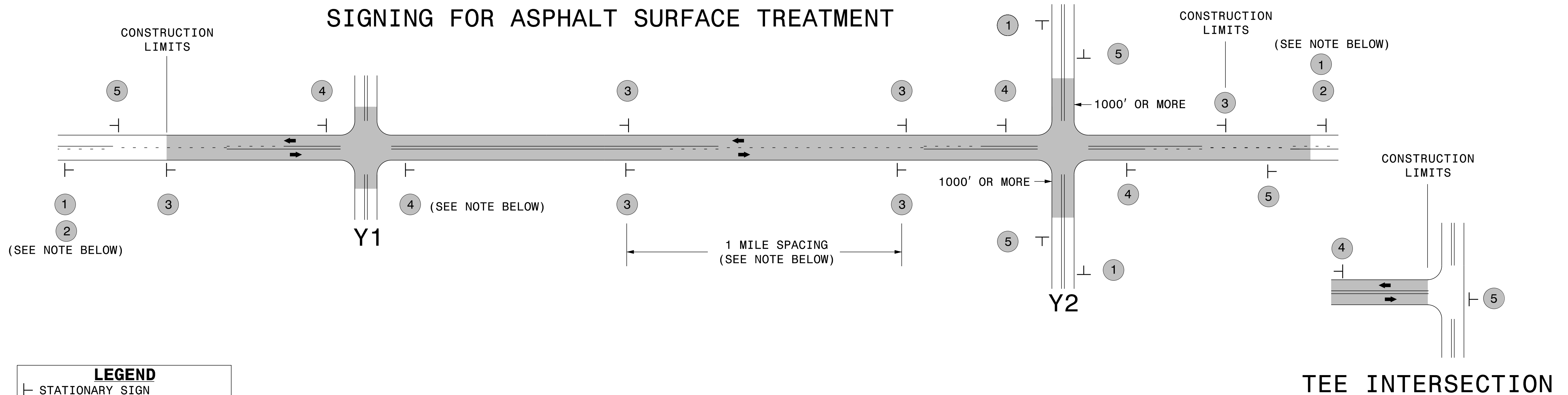
LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	2	3	4	5		
						<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <p> </p> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
						<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
						<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
						<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	

**RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS**



LEGEND

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	 <small>W20-1 48" X 48"</small>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.
	2	 <small>W7-3aP 24" X 18"</small>	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)
	3	 <small>W8-7 48" X 48"</small>	<ul style="list-style-type: none"> - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
		 <small>SP 48" X 48"</small>	
	4	 <small>SP 13106 48" X 48"</small>	<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.
5	 <small>G20-2 A 48" X 24"</small>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

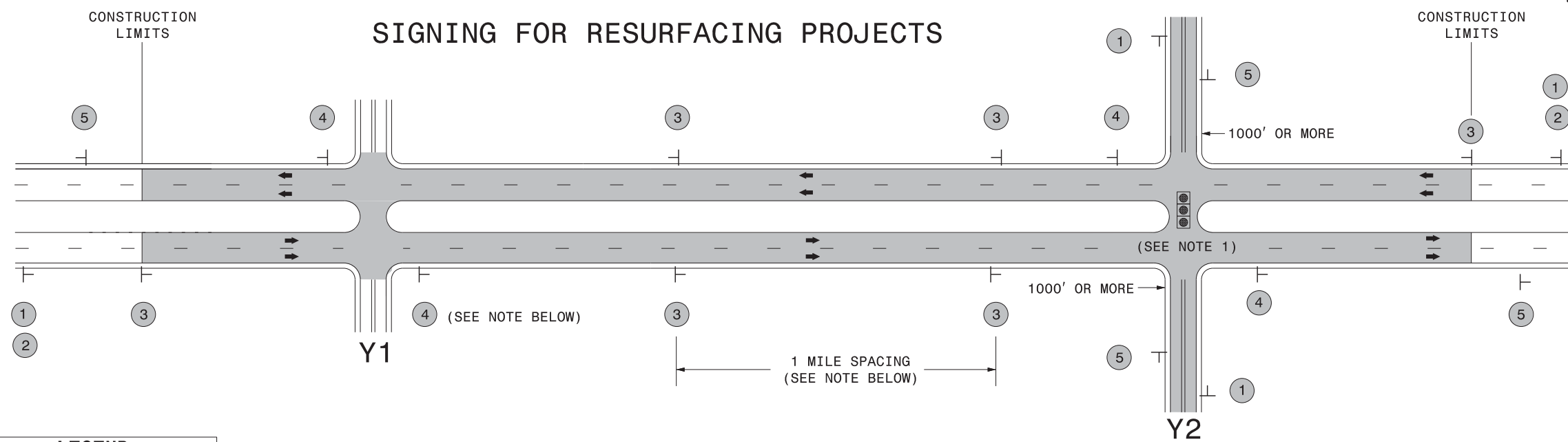
WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

W20-1
48" X 48"

W20-7 A
48" X 48"

PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

ADVANCE WARNING SIGNS FOR ASPHALT SURFACE TREATMENTS 2 LANE ROADWAYS



LEGEND
 | STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <small>W20-1 48" X 48"</small> <small>W20-7 A 48" X 48"</small> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
		<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
		<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	

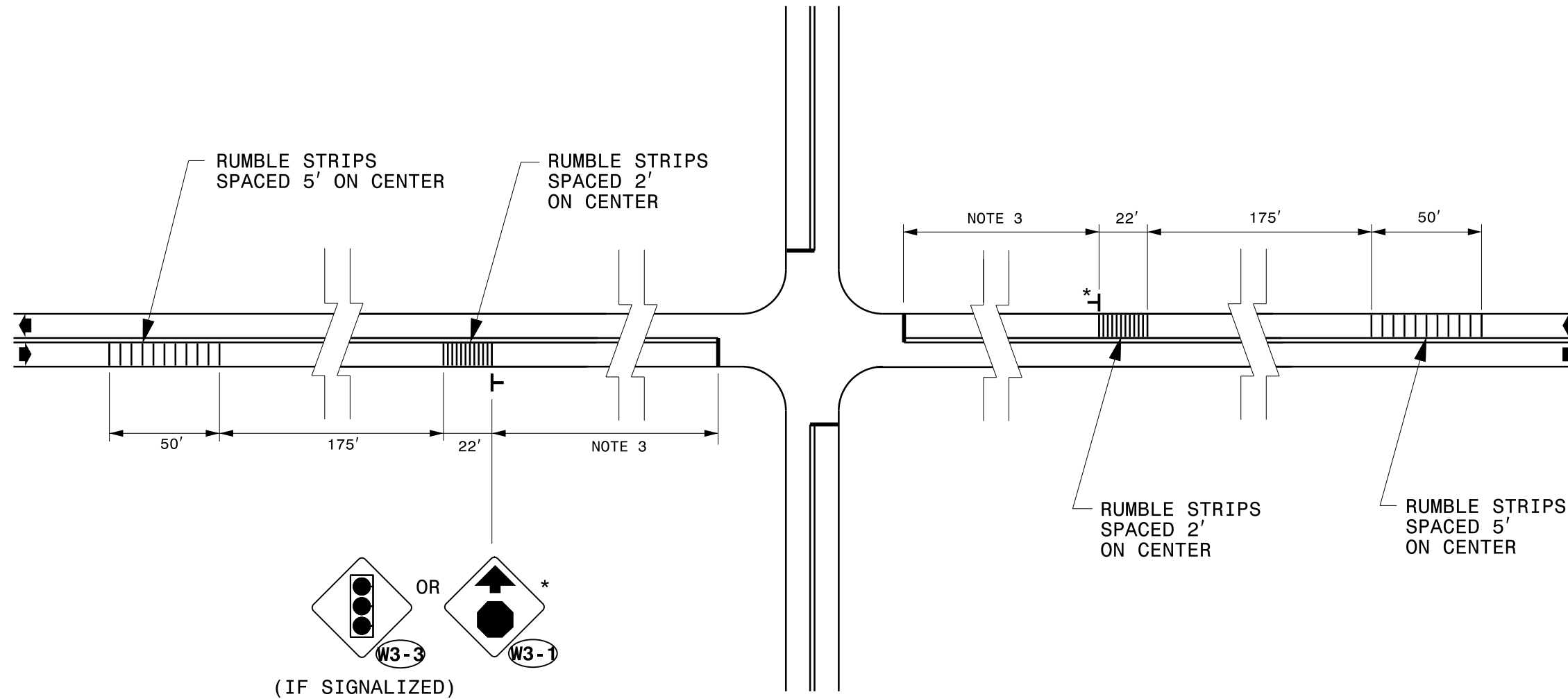
3/23/2015 C:\Users\rmgarrrett\Downloads\Resurfacing_AdvWarn_LrSu_Shldr.dgn User:rmgarrrett

**RESURFACING
 ADVANCE WARNING SIGNS
 FOR RURAL AND SUBURBAN
 MULTI-LANE ROADWAYS
 W/ SHOULDER SECTIONS**

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

11-13

ENGLISH STANDARD DRAWING FOR
**TWO-LANE, TWO-WAY THERMOPLASTIC
RUMBLE STRIPS**



GENERAL NOTES:

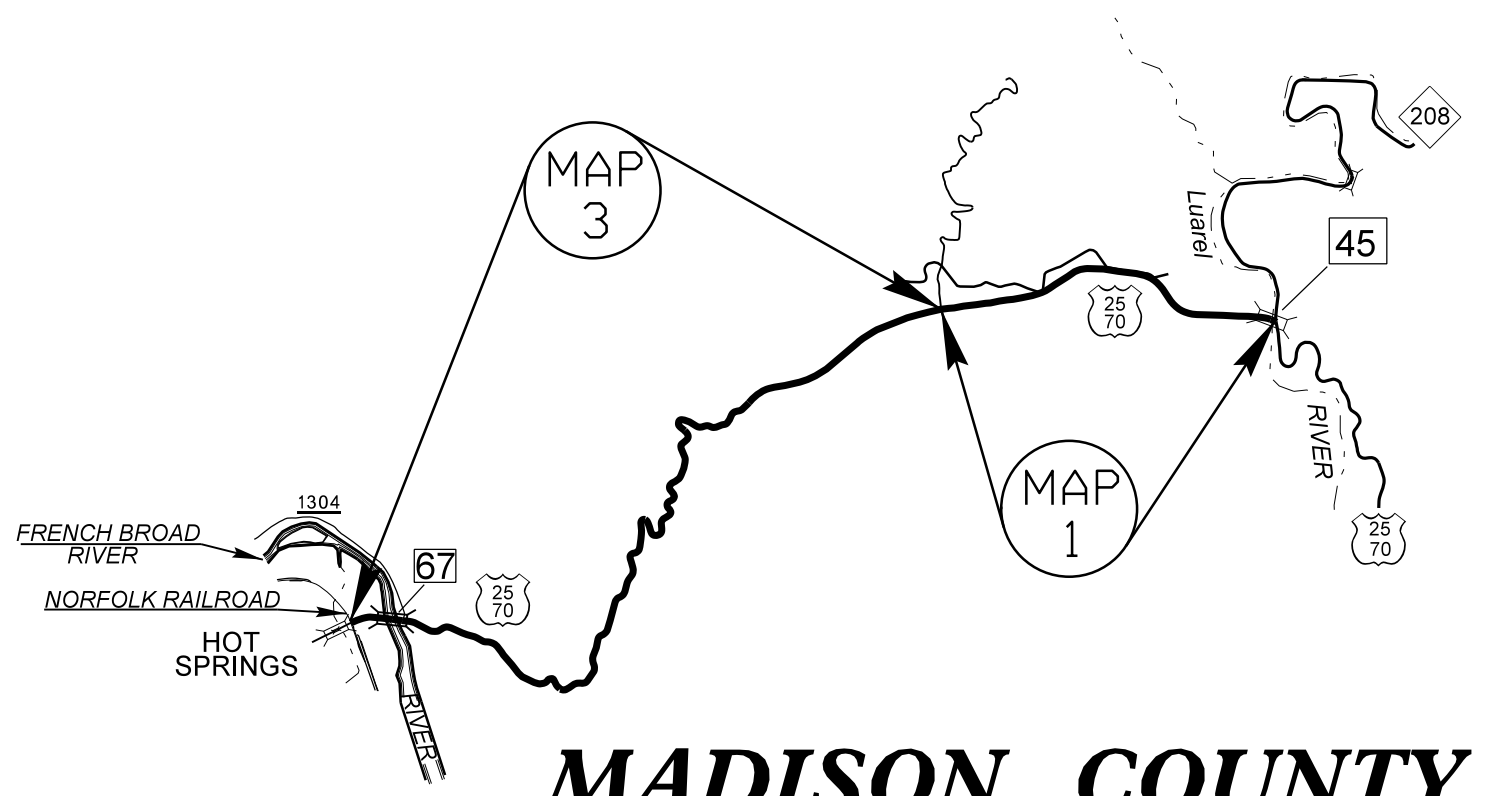
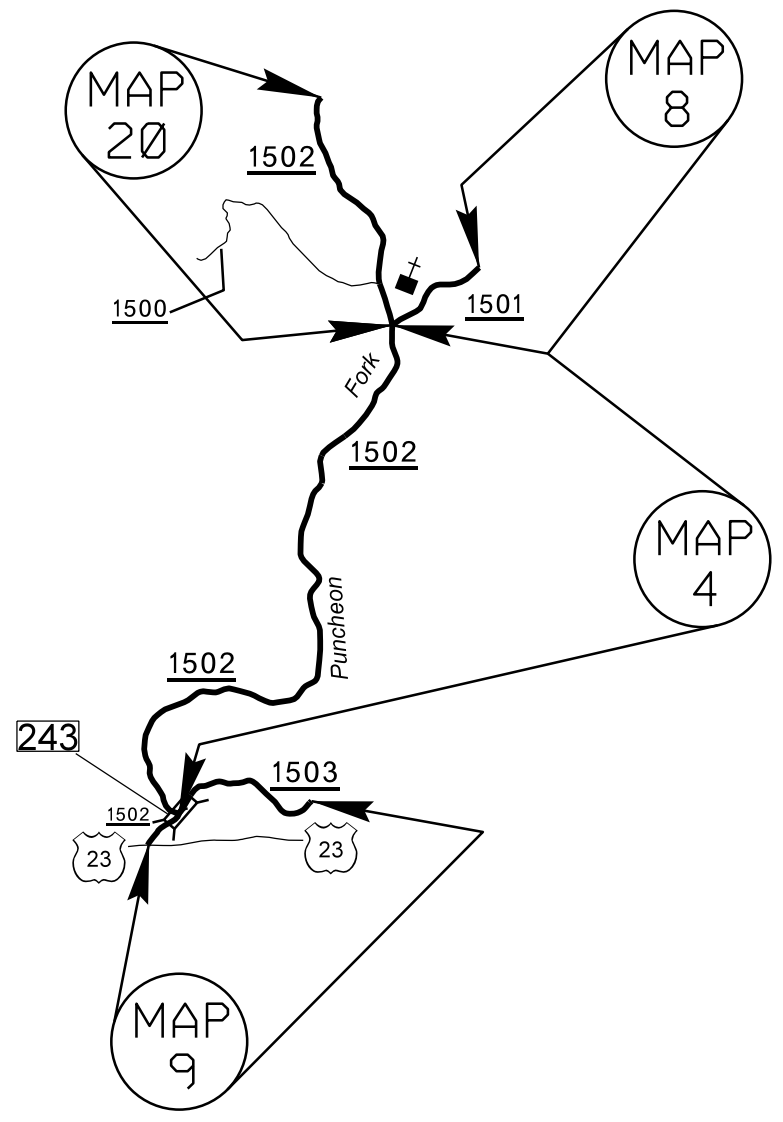
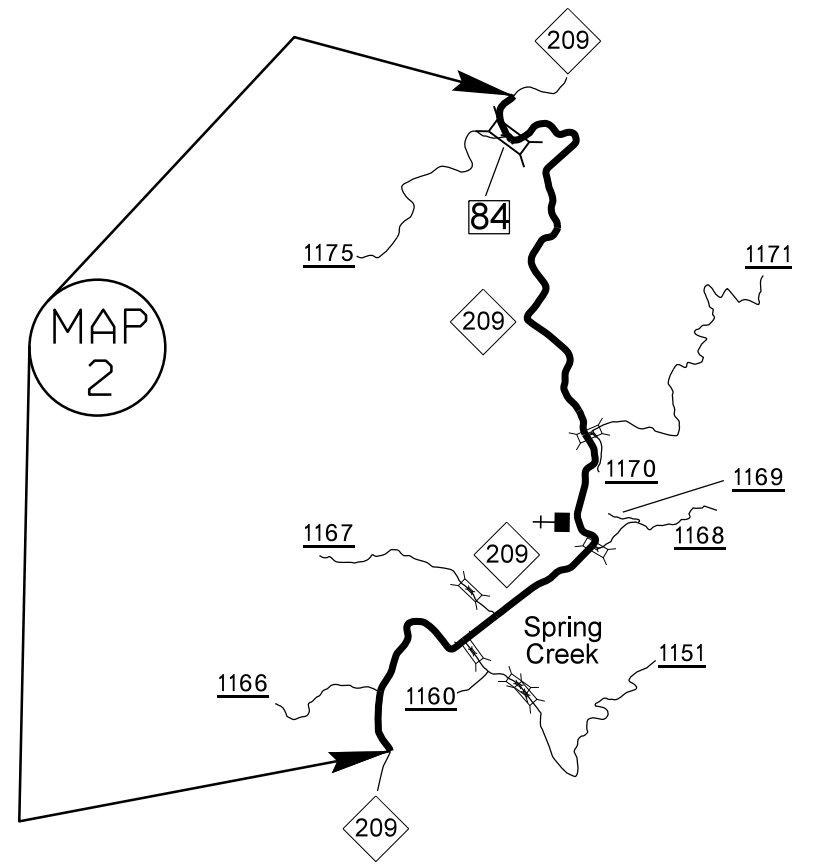
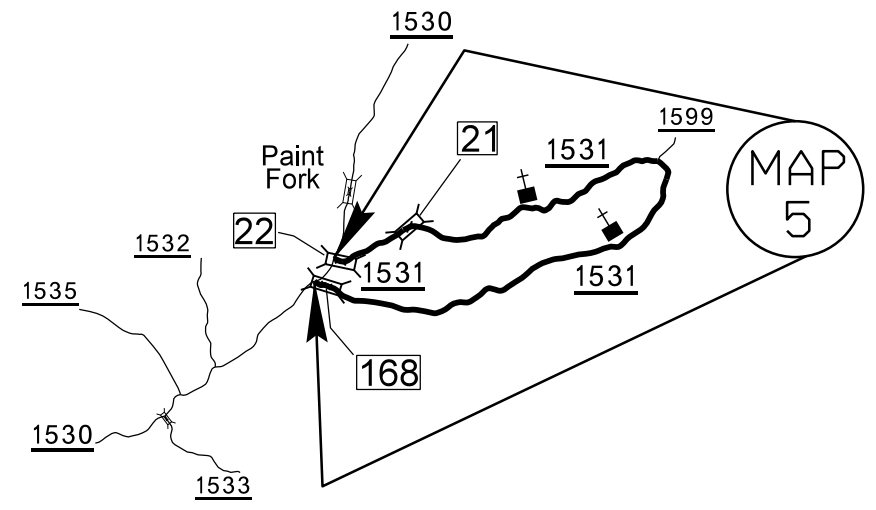
1. ALL RUMBLE STRIPS SHALL BE CENTERED IN THE LANE AND SHALL BE 2 FEET LESS THAN THE WIDTH OF THE TRAVEL LANE
2. RUMBLE STRIPS SHALL BE PLACED USING 4" x 240 MIL WHITE THERMOPLASTIC PAVEMENT MARKING MATERIAL.
3. PLACEMENT OF STOP-AHEAD (W3-1) OR SIGNAL-AHEAD (W3-3) SIGNS SHALL COMPLY WITH THE 2009 MUTCD SECTION 5C.04.

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

11-13

ENGLISH STANDARD DRAWING FOR
**TWO-LANE, TWO-WAY THERMOPLASTIC
RUMBLE STRIPS**

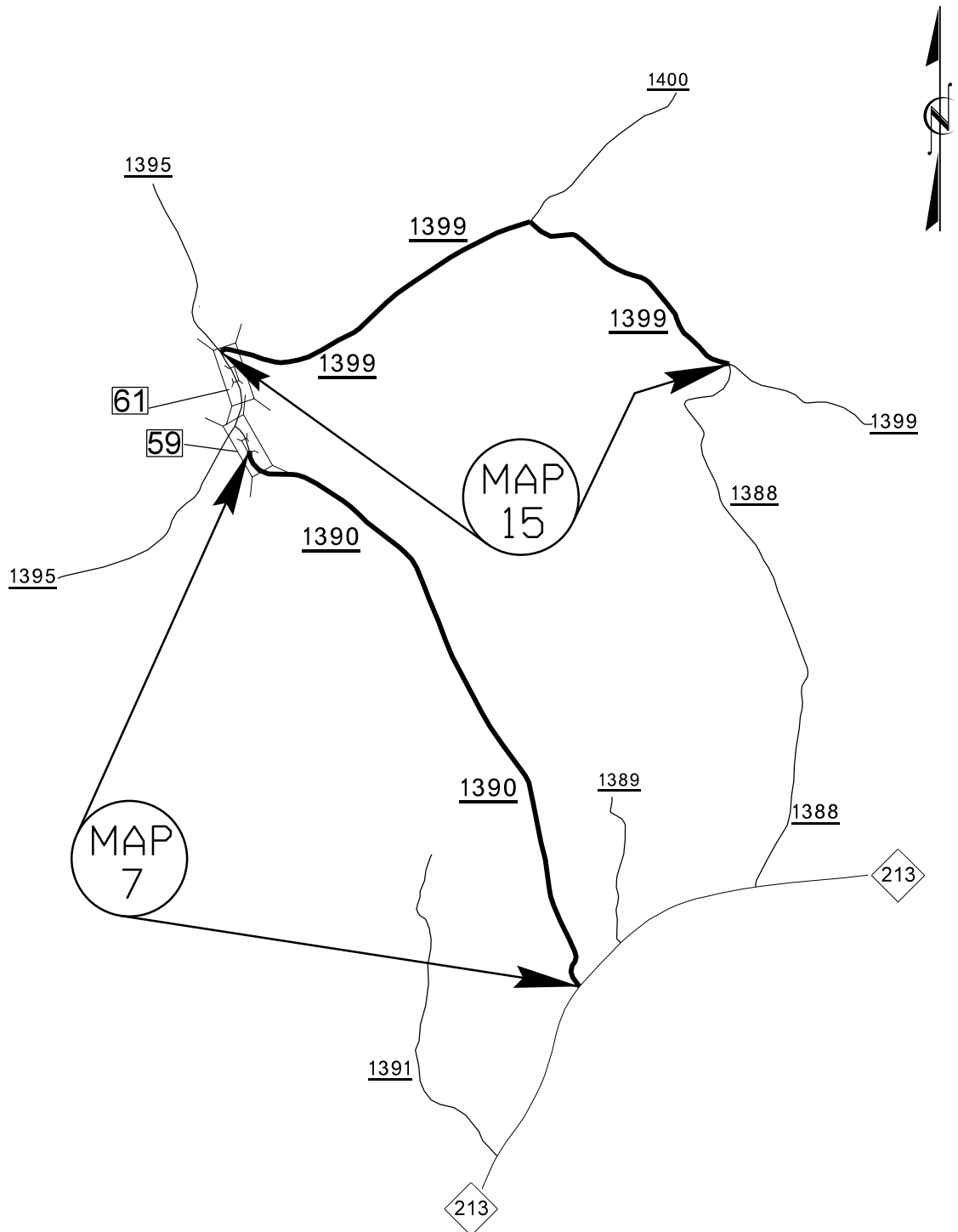
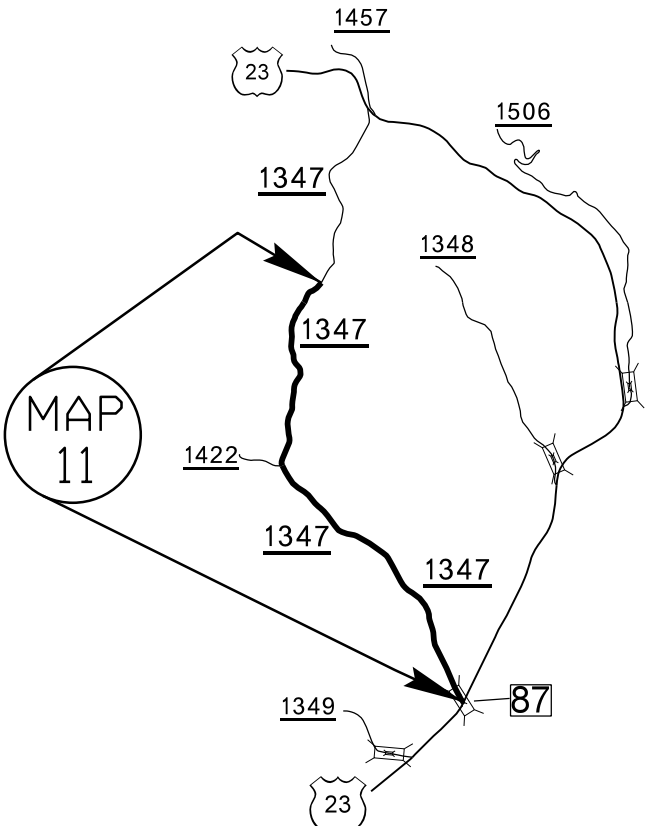
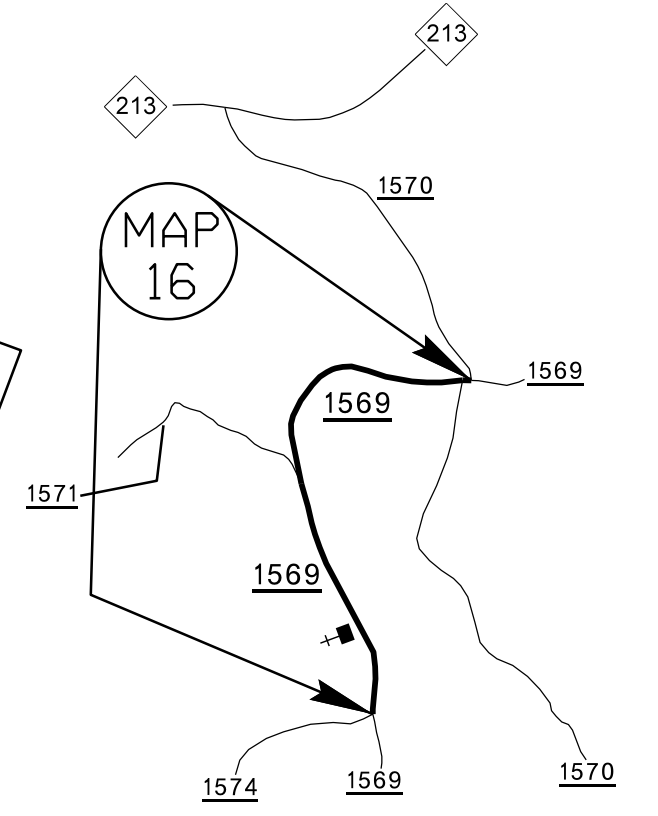
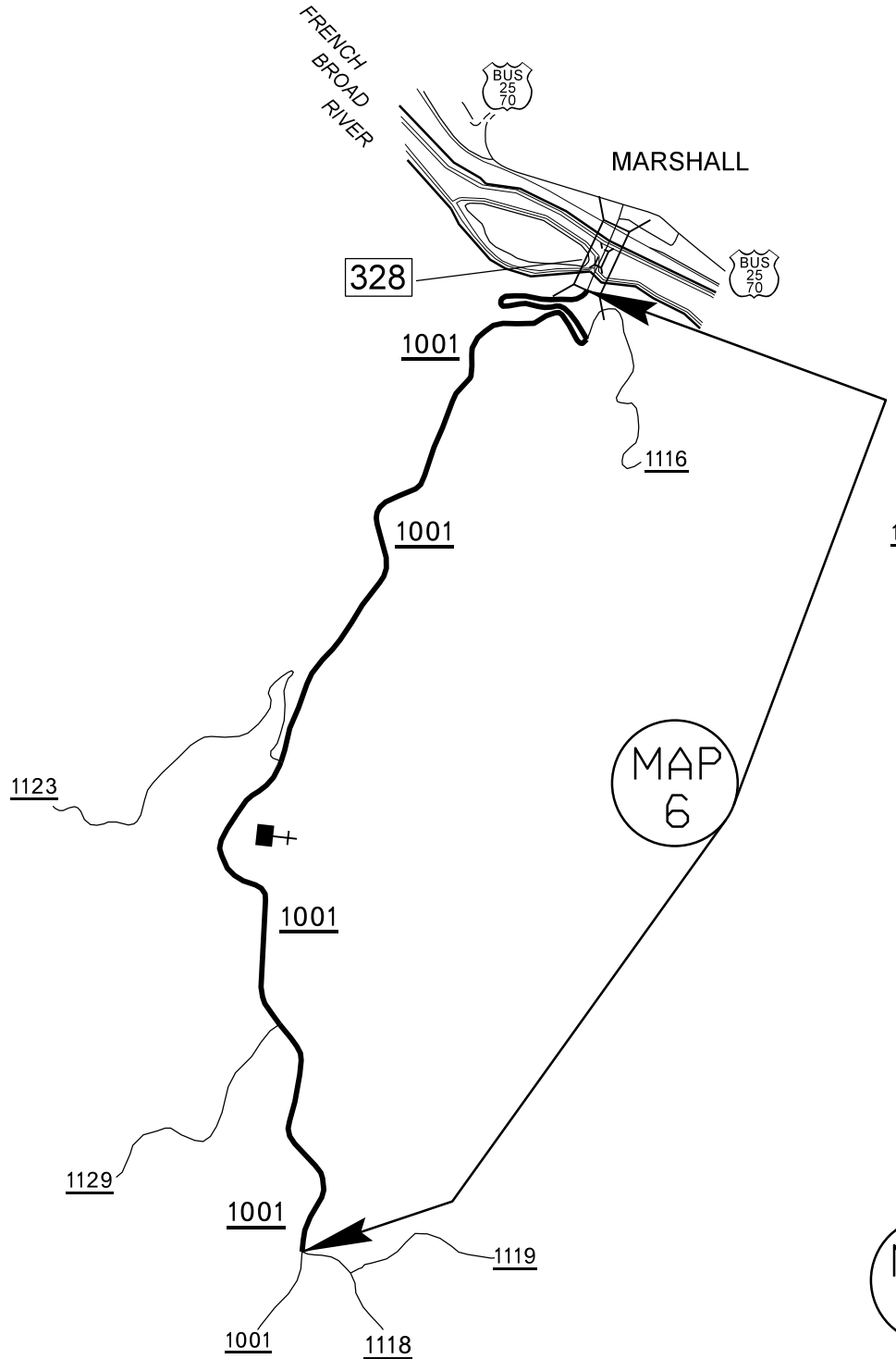
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571	1	



MADISON COUNTY

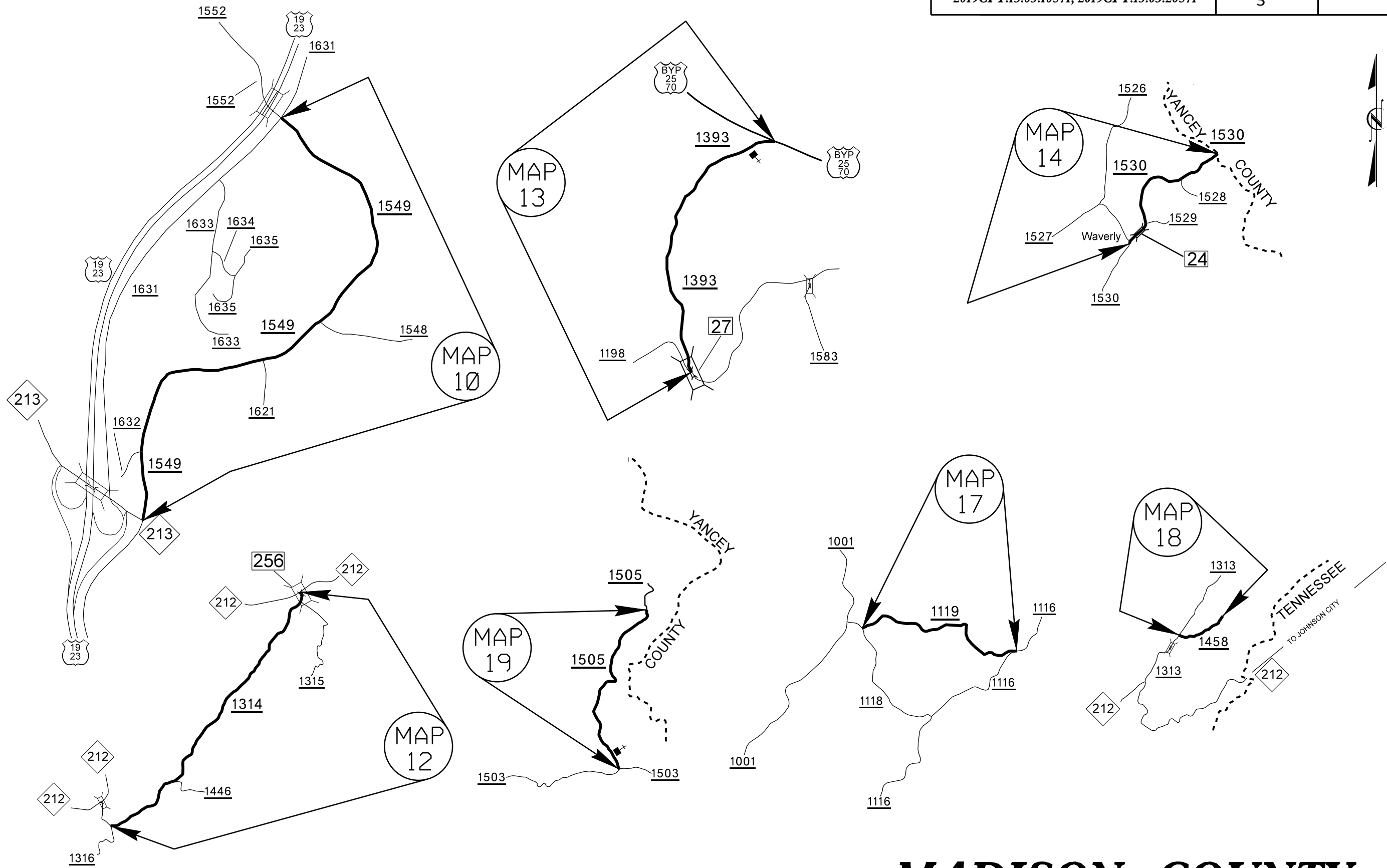


PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571	2	

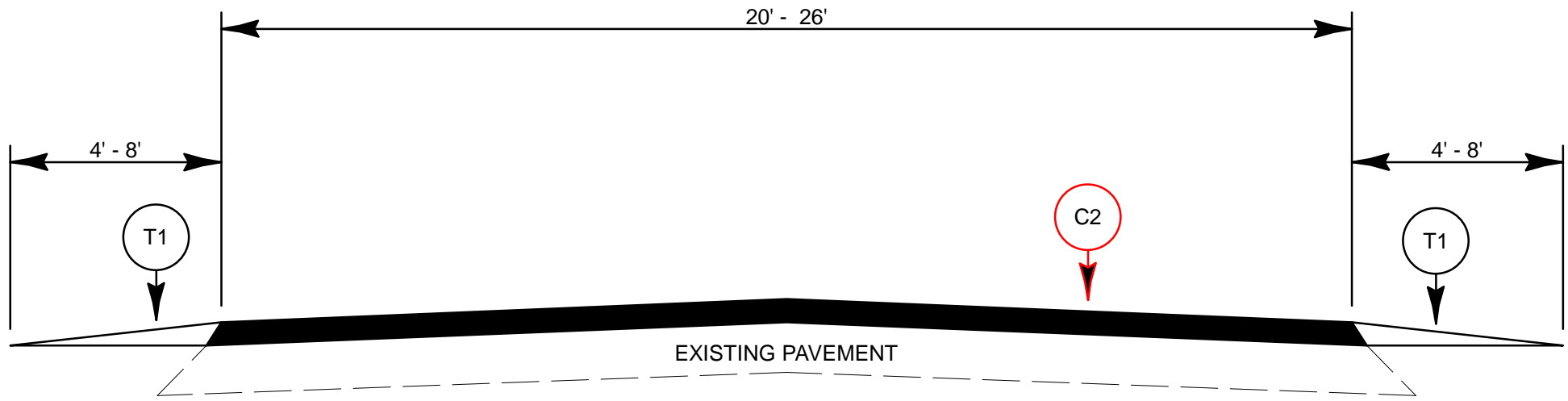


MADISON COUNTY

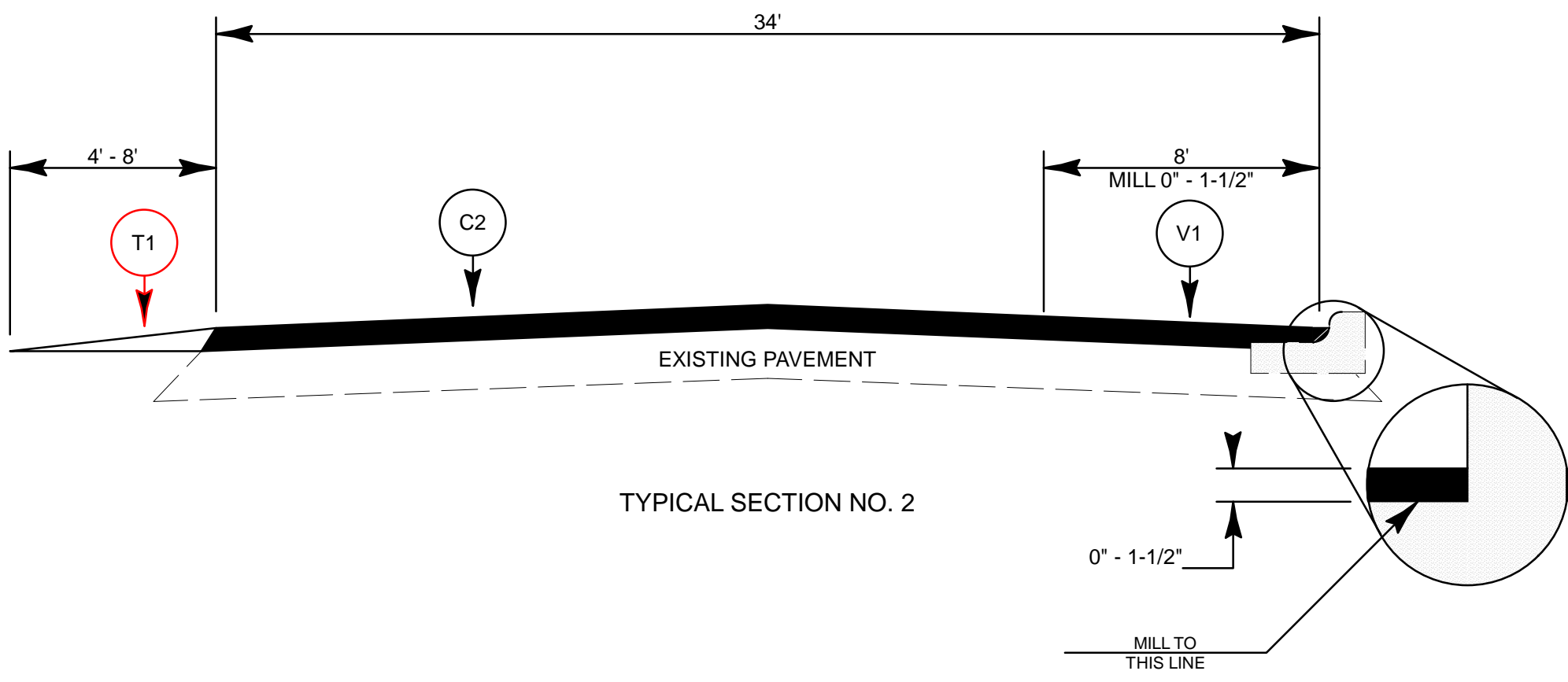
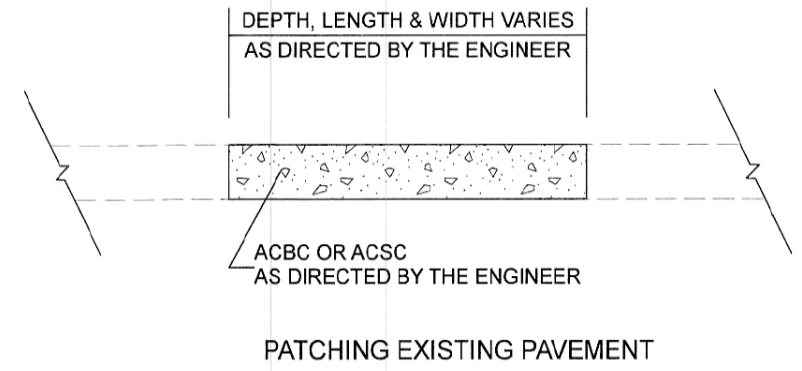
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571	3	



MADISON COUNTY

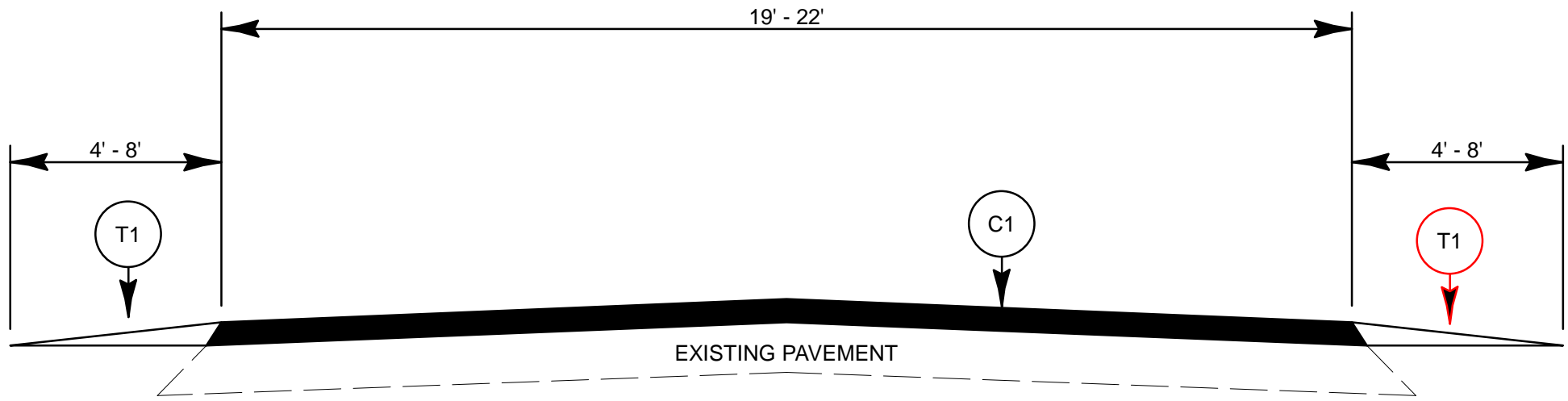


TYPICAL SECTION NO. 1

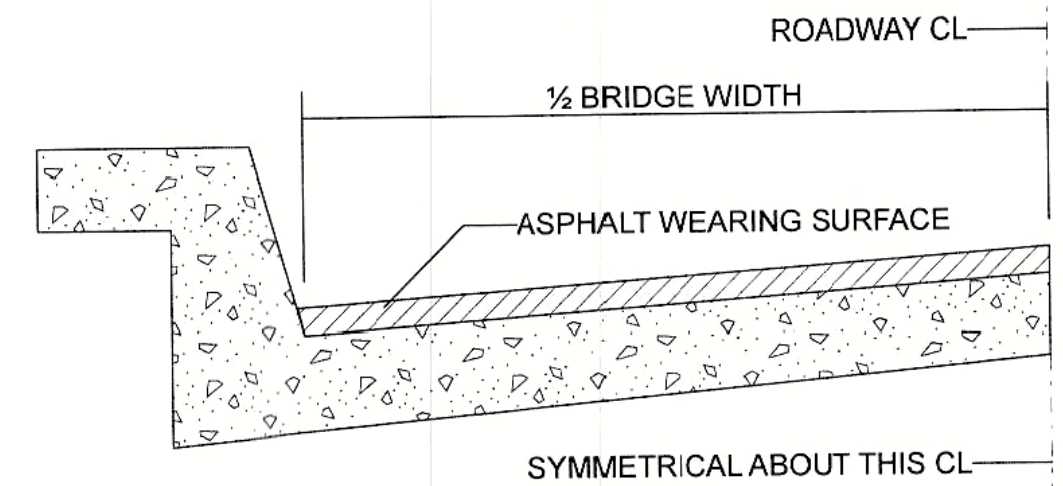


TYPICAL SECTION NO. 2

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YARD
C2	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
F1	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F2	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT, 0 TO 1-1/2" DEPTH
V2	INCIDENTAL MILLING
V3	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH
Y1	LATEX MODIFIED MICRO-SURFACING, TYPE III



TYPICAL SECTION NO. 3



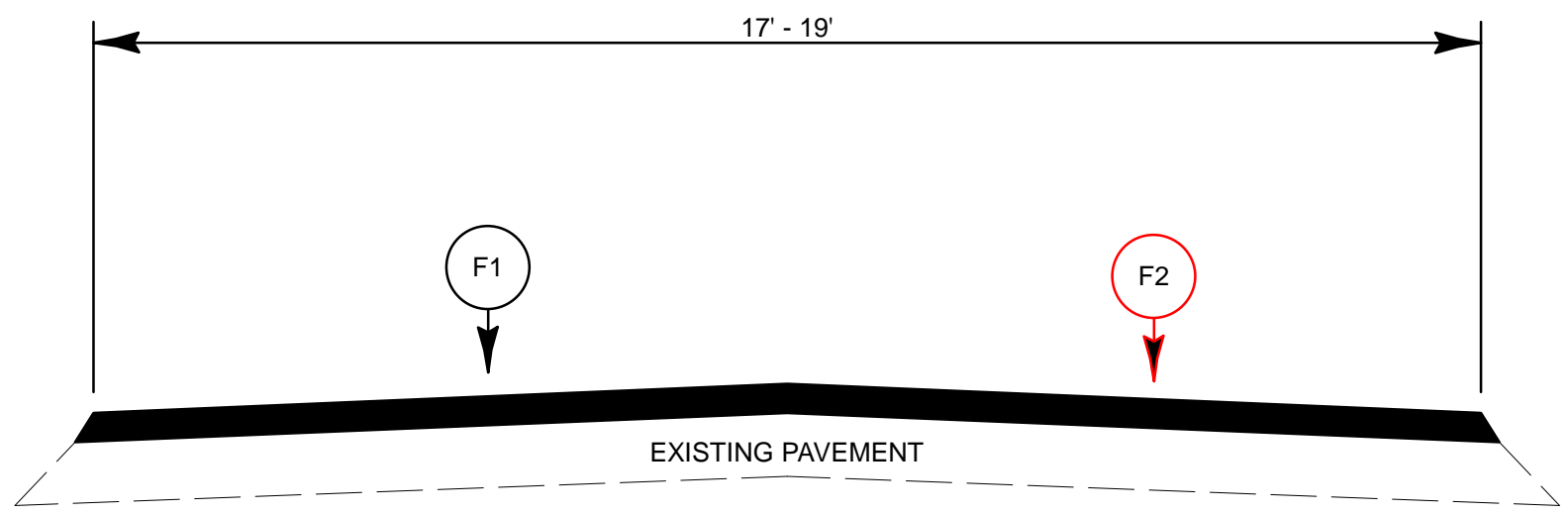
BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", SF9.5A 1.0", S9.5X 1.5", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 3/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1.0", SF9.5A 1.5", S9.5X 2.0", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 3/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2".

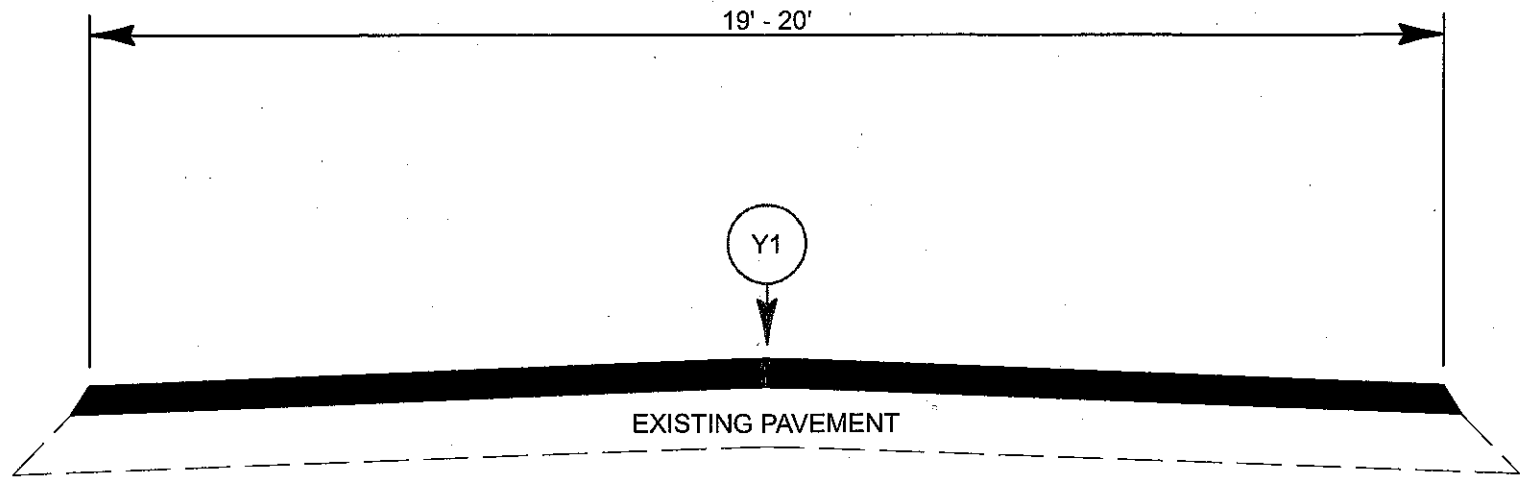
NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.
 ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.
 EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.
 SHOULDER AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.
 BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

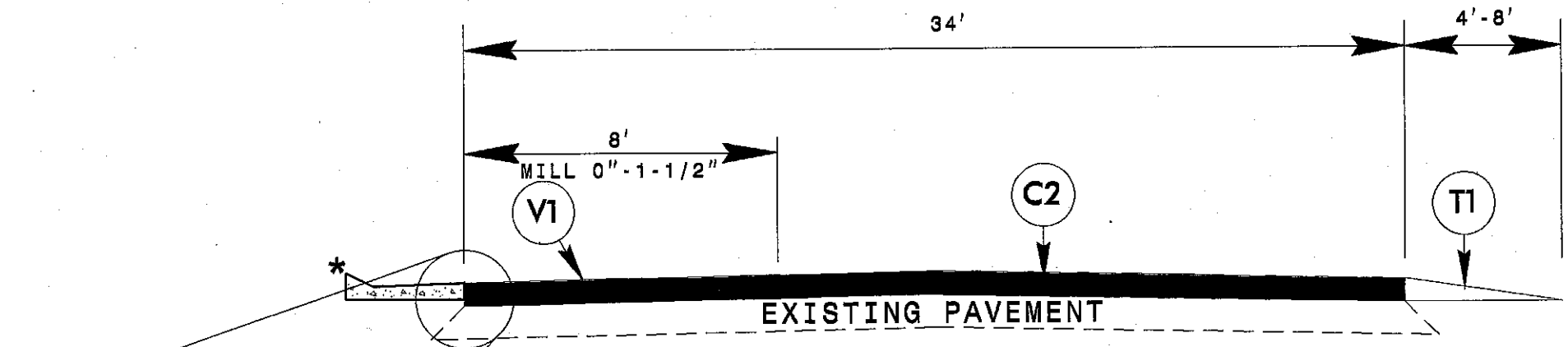


TYPICAL SECTION NO. 4

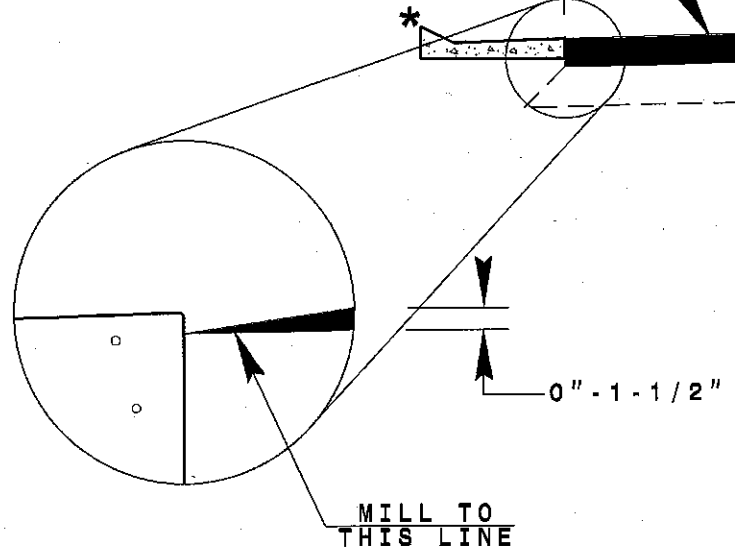
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571, 2019CPT.13.03.20571	6	



TYPICAL SECTION NO. 5

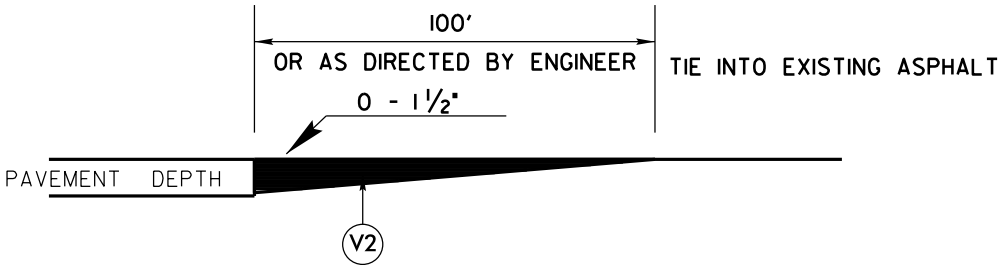


TYPICAL SECTION NO. 6



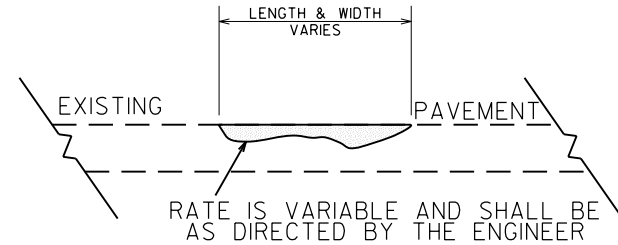
* SHOULDER BERM GUTTER MAY BE ON THE LEFT OR RIGHT SIDE OF THE ROAD IN VARIOUS LOCATIONS.

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2019CPT.13.03.10571	7	

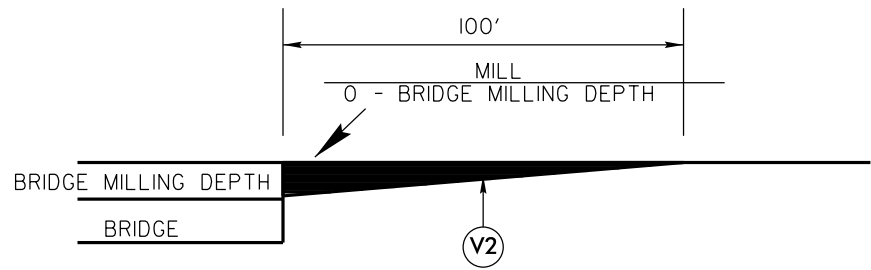
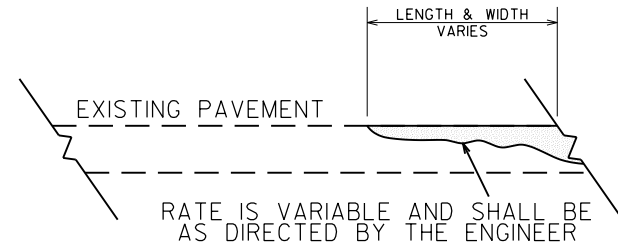


DETAIL TO TIE INTO EXIST PAVEMENT

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT HE WILL BE REQUIRED TO MILL THE EXISTING ASPHALT PAVEMENT TO ENSURE A PROPER TIE-IN WITH THE EXISTING SURFACE AT THE BEGINNING, END AND Y LINES OF EACH MAP TO BE RESURFACED WITH ASPHALT CONC SURFACE COURSE, TYPE S9.5C. THIS WILL BE PAID FOR AS INCIDENTAL MILLING.

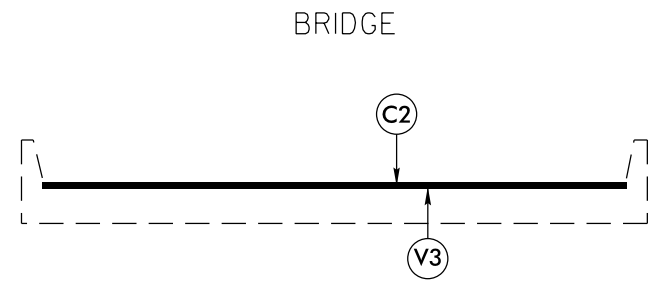


DETAIL SHOWING METHOD OF WEDGING



MILLING DETAIL AT BRIDGE APPROACHES

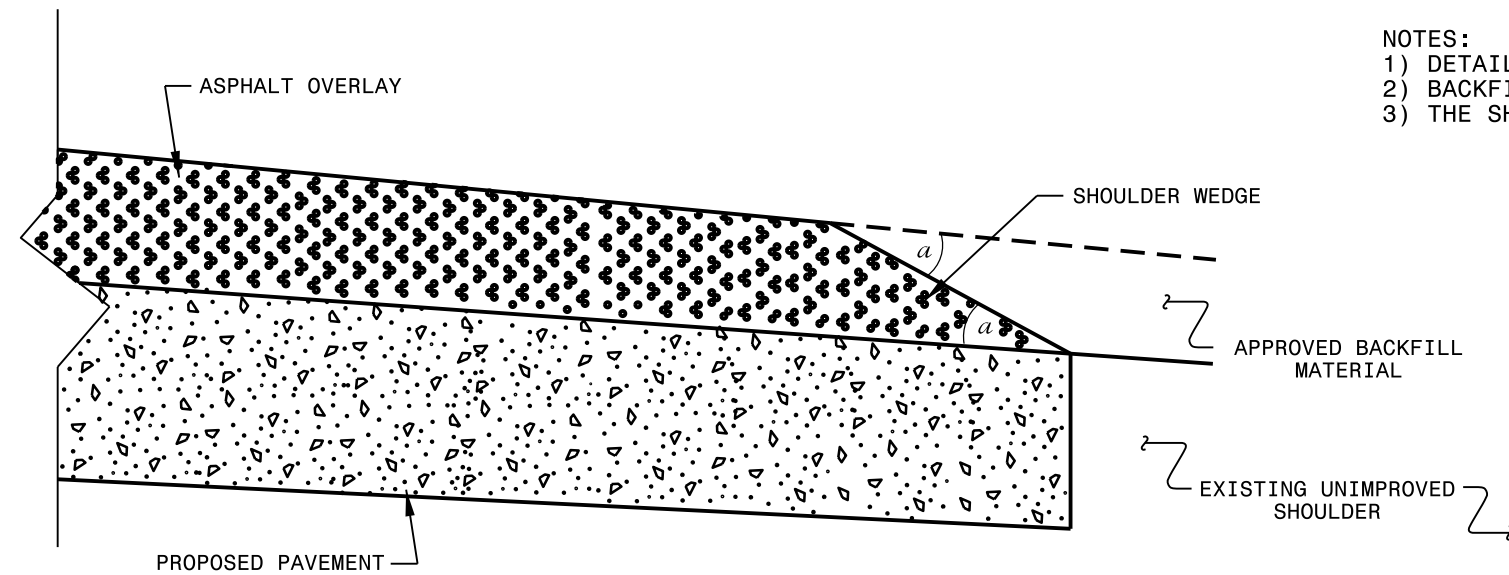
WHERE BRIDGES WILL BE MILLED THEN RESURFACED. THIS WILL BE PAID FOR AS INCIDENTAL MILLING. USE AT BRIDGE NUMBERS: 45 MAP 1, 84 MAP 2, 67 MAP 3, AND 243 MAP 9.



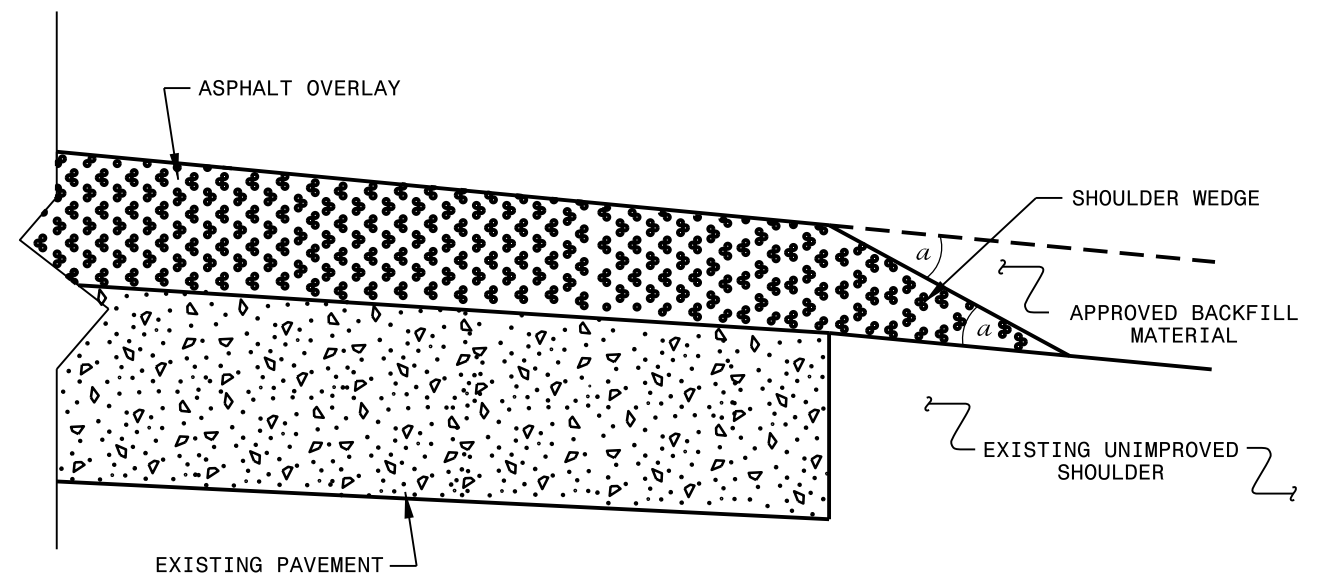
BRIDGE DETAIL

BRIDGE 45 MAP 1, 84 MAP 2, 67 MAP 3, AND 243 MAP 9. MILL 1-1/2" OFF EXISTING PAVEMENT SEE MAPS FOR BRIDGE LOCATION

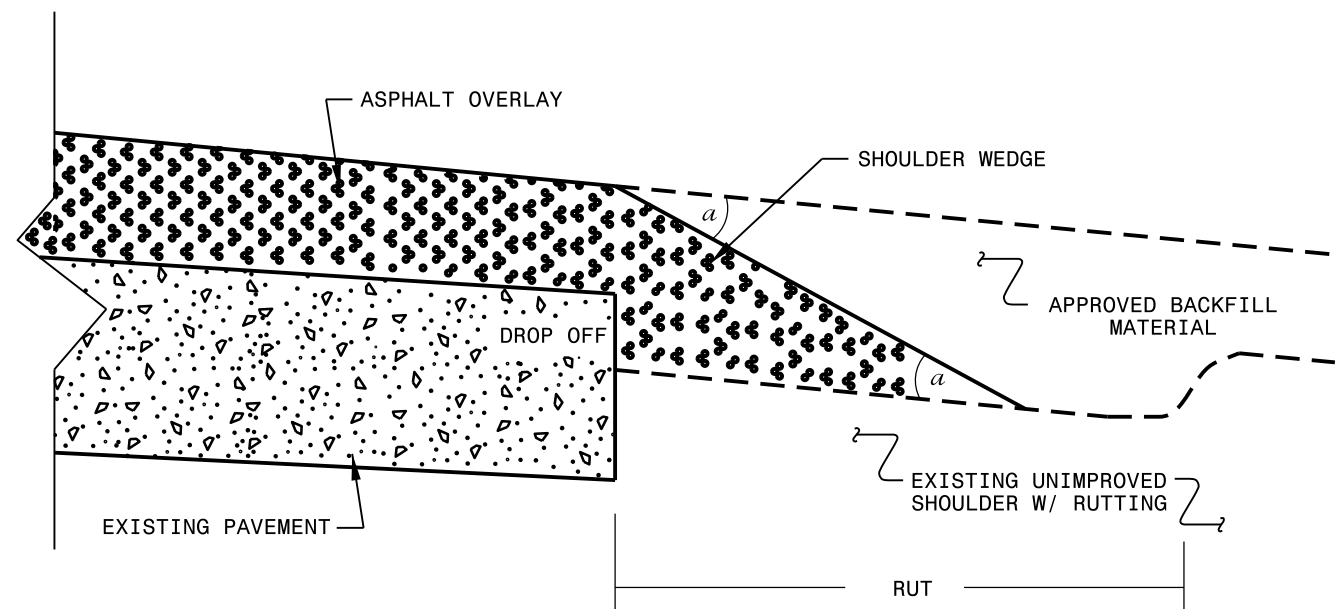
- NOTES:
 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: susr/details/stand/shoulderwedgedetail.dgn	

SYSTEM: \$\$\$\$
 USER: \$\$\$\$
 DATE: \$\$\$\$

PROJECT NO.	SHEET NO.	TOTAL NO.
2019CPT.13.03.10571,		
2019CPT.13.03.20571	10	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	441300000-E	445700000-N	468700000-E	470500000-E	471000000-E	472100000-E	481000000-E		484701000-E		485000000-E	490500000-N															
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	THERMOPLASTIC PAVEMENT MARKING LINES (4", 240 MILS) WHITE	THERMOPLASTIC PAVEMENT MARKING LINES (16", 120 MILS) WHITE	THERMOPLASTIC PAVEMENT MARKING LINES (24", 120 MILS) WHITE	THERMOPLASTIC PAVEMENT MARKING CHARACTER (120 MILS) RXR	PAINT PAVEMENT MARKING LINES (4") WHITE	PAINT PAVEMENT MARKING LINES (4") YELLOW	POLYUREA PAVEMENT MARKING LINES WHITE (4", 20 MILS) (HIGHLY REFLECTIVE MEDIA)	POLYUREA PAVEMENT MARKING LINES YELLOW (4", 20 MILS) (HIGHLY REFLECTIVE MEDIA)	REMOVAL OF PAVEMENT MARKING LINES (4")	SNOWPLOWABLE PAVEMENT MARKERS															
									MI	FT	SF	LS	LF	LF	LF	EA	LF	LF	LF	LF	EA															
2019CPT.13.03.10571	Madison	1	US 25-70	FROM NC 208 TO BEG PASSING LANE AT TOP OF THE MTN (MP 15.92 TO MP 17.85)	1	2	2WU	1.93	26			300							20,381	20,381		145														
2019CPT.13.03.10571	Madison	2	NC 209	FROM PVMNT JOINT NORTH OF SR 1175 TO PVMNT JOINT AT SPRING CREEK UNITED METHODIST CHURCH (MP 13.91 TO MP 8.63)	1	2	2WU	5.28	20	1,147	*								56,813	56,813		700														
2019CPT.13.03.10571	Madison	3	US 25/70	FROM THE END OF MAP 1 TO THE R/ TRACKS IN HOT SPRINGS (MP 17.85 TO MP 20.86)	1,2,6	2	2WU	3.01	34				40	132	4				34,000	32,550		365														
TOTAL FOR PROJ NO. 2019CPT.13.03.10571																																				
										10.22		1,147	1	300	40	132	4			111,194	109,744		1,210													
																							220,938													
2019CPT.13.03.20571	Madison	4	SR 1502	FROM SR 1503 TO SR 1501 (MP 0.01 TO MP 3.62)	3	2	2WD	3.61	22	3,251	*																									
2019CPT.13.03.20571	Madison	5	SR 1531	FROM SR 1530 TO SR 1530 (MP 0.00 TO MP 4.25)	4	2	2WU	4.25	18																											
2019CPT.13.03.20571	Madison	6	SR 1001	FROM BRIDGE #328 TO SR 1118 (MP 8.37 TO MP 5.94)	5	2	2WU	2.43	20																										102644	
2019CPT.13.03.20571	Madison	7	SR 1390	FROM NC 213 TO JOINT AT BRIDGE #59 (MP 1.47 TO MP 0.07)	3	2	2WU	1.4	20																											
2019CPT.13.03.20571	Madison	8	SR 1501	FROM SR 1502 TO EOM (MP 0.01 TO MP 0.58)	3	2	2WU	0.57	21																											
2019CPT.13.03.20571	Madison	9	SR 1503	FROM US 23 TO SR 1502 (MP 0.01 TO MP 0.24)	3	2	2WU	0.23	20																											
2019CPT.13.03.20571	Madison	10	SR 1549	FROM SR 1631 TO NC 213 (MP 0.06 TO MP 1.41)	3	2	2WU	1.35	22																											
2019CPT.13.03.20571	Madison	11	SR 1347	FROM US 23 TO EOP (MP 0.00 TO MP 2.15)	4	2	2WU	2.15	19																											
2019CPT.13.03.20571	Madison	12	SR 1314	FROM SR 1316 TO NC 212 (MP 0.01 TO MP 3.43)	5	2	2WU	3.42	19																										144460	
2019CPT.13.03.20571	Madison	13	SR 1393	FROM US 25/70 TO SR 1198 (MP 0.76 TO MP 0.01)	4	2	2WU	0.75	18																											
2019CPT.13.03.20571	Madison	14	SR 1530	FROM SR 1527 TO YANCEY COUNTY LINE (MP 5.37-MP 7.13)	3	2	2WU	1.76	19																											
2019CPT.13.03.20571	Madison	15	SR 1399	FROM SR 1395 TO SR 1388 (MP 0.01 TO MP 1.31)	4	2	2WU	1.3	18																											
2019CPT.13.03.20571	Madison	16	SR 1569	FROM SR 1570 TO SR 1574 (MP 1.4 TO MP 2.17P)	4	2	2WU	0.77	17																											
2019CPT.13.03.20571	Madison	17	SR 1119	FROM SR 1118 TO SR 1116 (MP 0.00 TO MP 1.05)	4	2	2WU	1.05	18																											
2019CPT.13.03.20571	Madison	18	SR 1458	FROM SR 1313 TO EOP (MP 0.00 TO MP 0.68)	4	2	2WU	0.68	17																											
2019CPT.13.03.20571	Madison	19	SR 1505	FROM SR 1503 TO EOP (MP 0.00 TO MP 1.94)	4	2	2WU	1.94	18																											
2019CPT.13.03.20571	Madison	20	SR 1502	FROM SR 1501 TO EOP (MP 3.62 TO MP 4.92)	3	2	2WU	1.3	21																											
TOTAL FOR PROJ NO. 2019CPT.13.03.20571																																				
									28.96															3,251	1											
																																				1,194,546
GRAND TOTAL																																				
									39.18		4,398	1	300	40	132	4					597,273	597,273		111,194	109,744	247,104	1,210									
																									1,194,546	220,938										