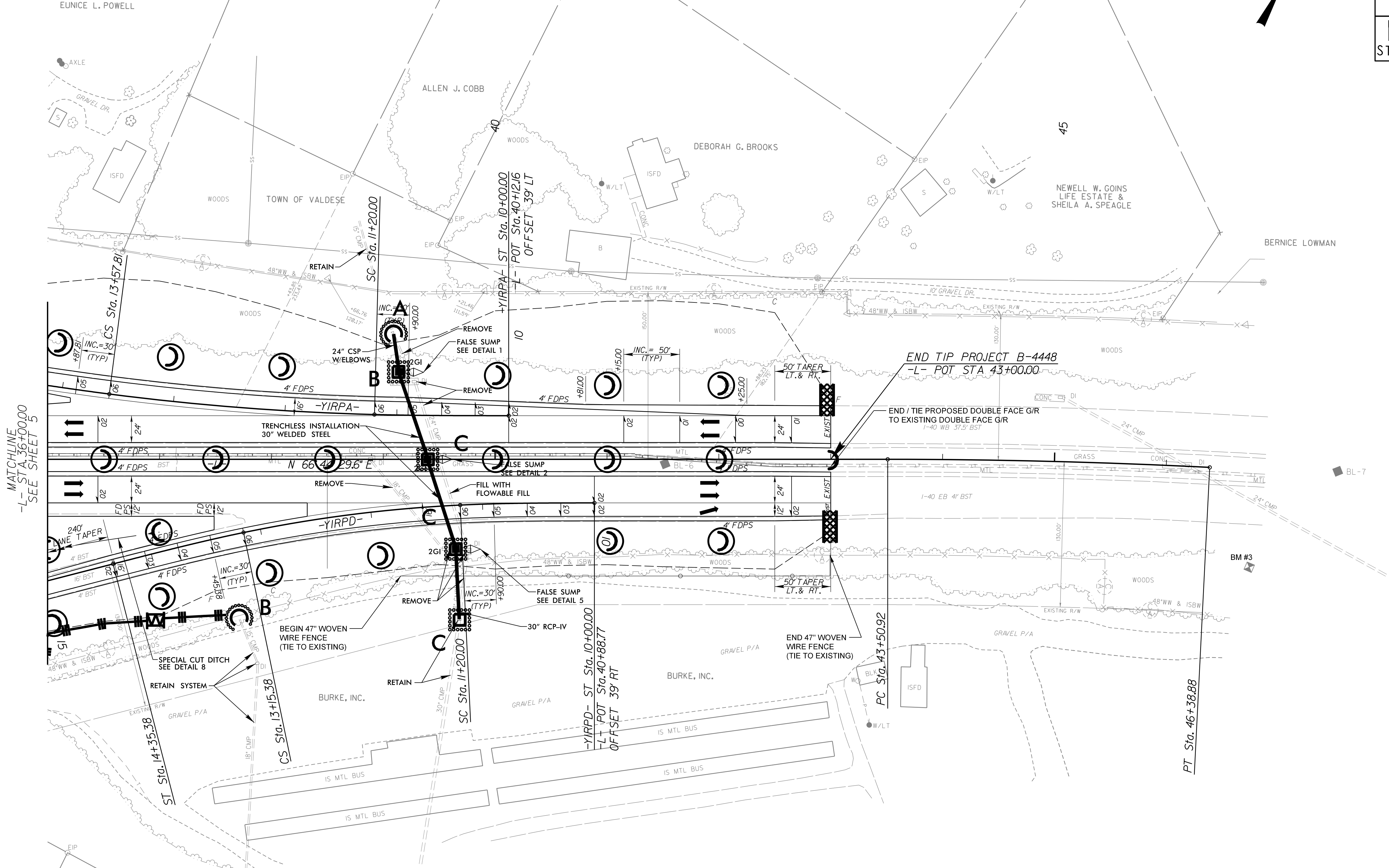
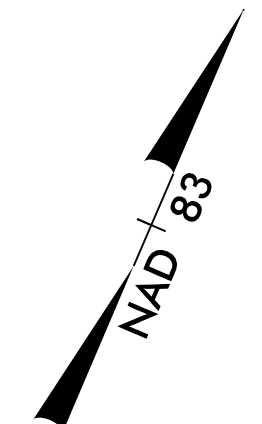
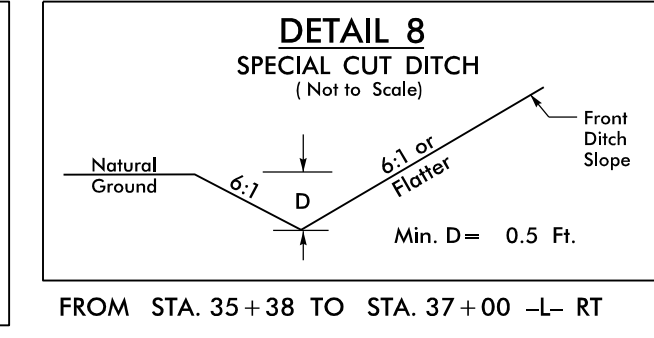
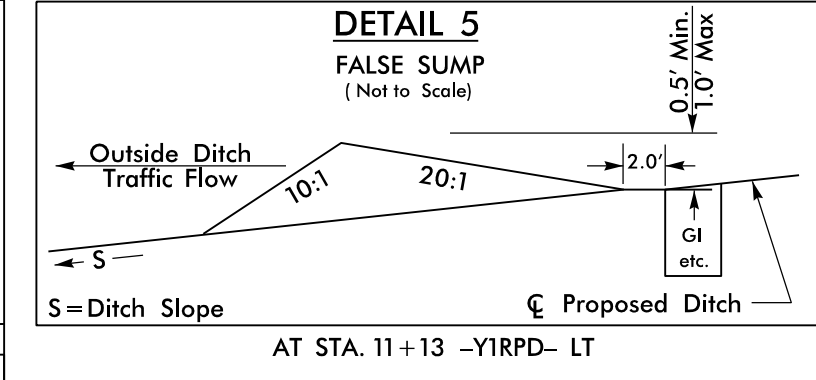
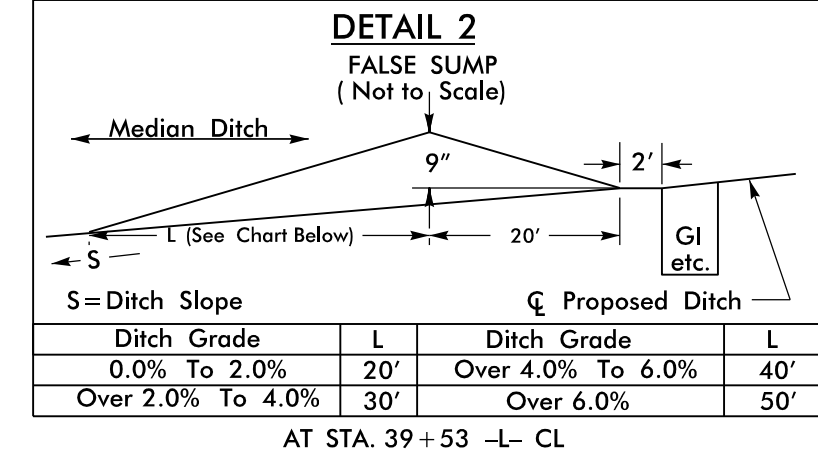
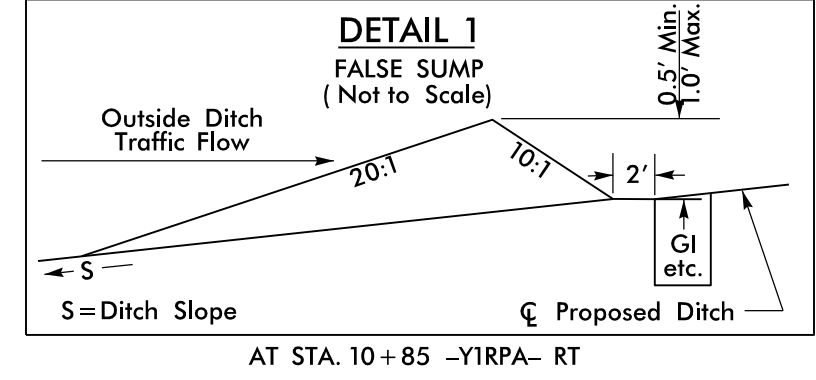


| | | |
|--|--|---|
| -L- | -YIRPA- | -YIRPD- |
| PI Sta 44+94.93 $\Delta = 2'52"46.1"$ (RT) $D = 1'00"00.0"$ $L = 287.95'$ $T = 144.01'$ $R = 5,729.65'$ | Pls Sta 10+80.00 $\Theta_s = 1'28"54.4"$ $L_s = 120.00'$ $LT = 80.00'$ $ST = 40.00'$ | Pls Sta 12+39.01 $\Delta = 5'52"23.0"$ (RT) $D = 2'28"10.7"$ $L = 237.81'$ $T = 119.01'$ $R = 2,320.00'$ $Runoff = 180'$ $Se = 6\%$ |
| | Pls Sta 13+97.81 $\Theta_s = 1'28"54.4"$ $L_s = 120.00'$ $LT = 80.00'$ $ST = 40.00'$ | Pls Sta 10+80.01 $\Theta_s = 2'45"00.7"$ $L_s = 120.00'$ $LT = 80.01'$ $ST = 40.01'$ |
| | | Pls Sta 12+17.89 $\Delta = 8'57"20.4"$ (LT) $D = 4'35"01.2"$ $L = 195.38'$ $T = 97.89'$ $R = 1,250.00'$ $Runoff = 180'$ $Se = 6\%$ $V_d = 45$ MPH |
| | | Pls Sta 13+55.39 $\Theta_s = 2'45"00.7"$ $L_s = 120.00'$ $LT = 80.01'$ $ST = 40.01'$ |



MATCHLINE
-L- STA. 36+00.00
SEE SHEET 5

REVISIONS



THE ALIGNMENTS FOR VERTICAL PROFILES -WBL- & -EBL- ARE BASED ON OFFSETTING THE HORIZONTAL ALIGNMENT -L- 15 FEET LEFT AND RIGHT.

- FOR DETOUR CONSTRUCTION, SEE SHEETS 2B-1 THRU 2B-3
- FOR -L- PROFILE (-WBL-), SEE SHEET 8
- FOR -L- PROFILE (-EBL-), SEE SHEET 10
- FOR -YIRPA- PROFILE, SEE SHEET 12
- FOR -YIRPD- PROFILE, SEE SHEET 13