

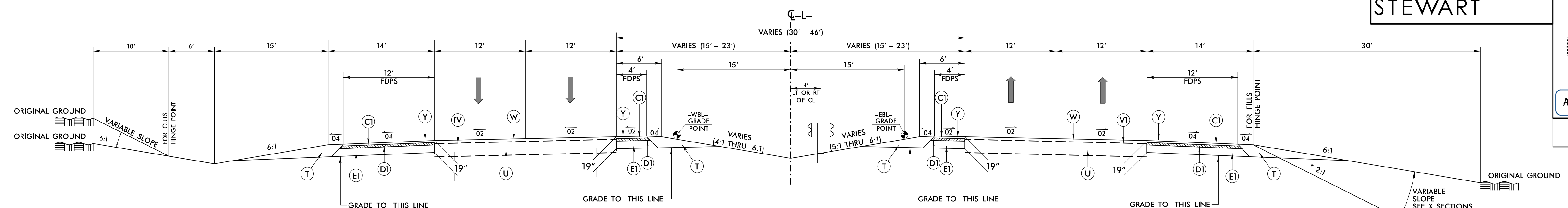
8.17/99



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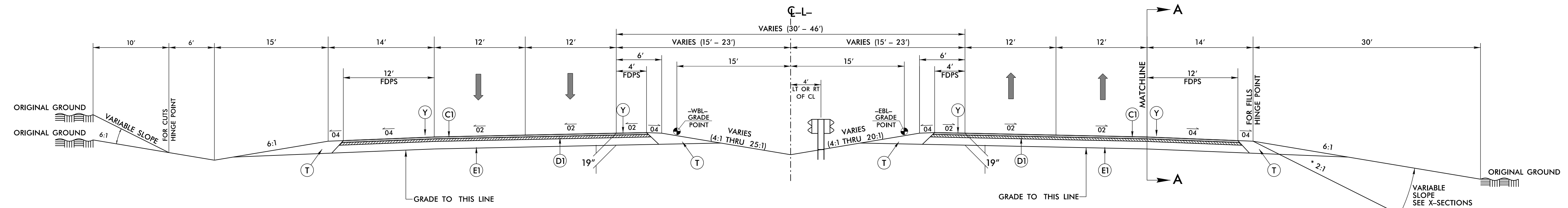
PROJECT REFERENCE NO. B-4448	SHEET NO. 2A-2
ROADWAY DESIGN ENGINEER ANDREW P. YOUNG SEAL 034407 5/10/2018	PAVEMENT DESIGN ENGINEER CLARK HARRISON SEAL 22896 5/12/2018

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED



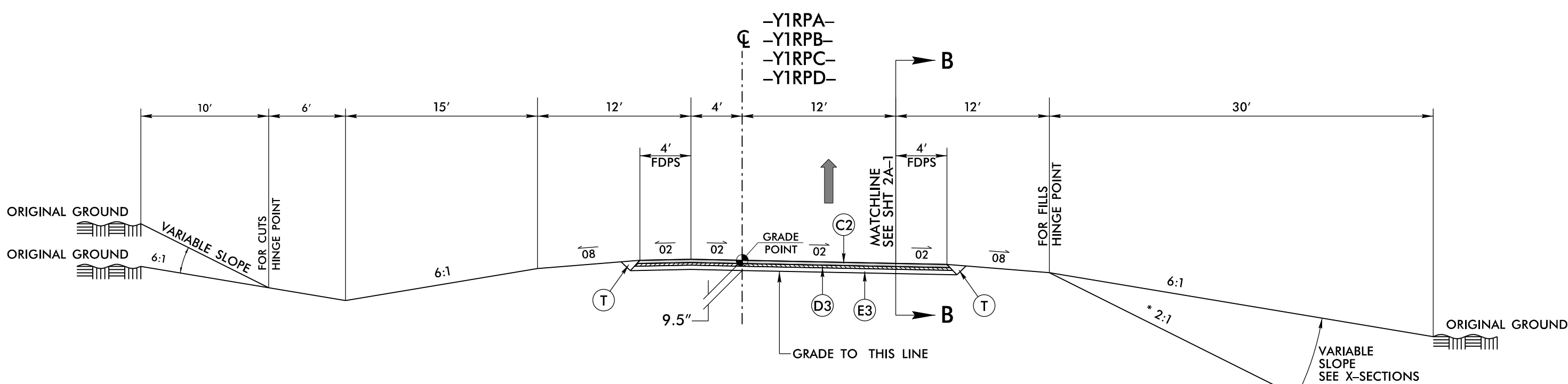
TYPICAL SECTION NO. 1
 -L- STA. 13+00.00 TO -L- STA. 22+00.00
 -L- STA. 34+40.00 TO -L- STA. 43+00.00

NOTES:
 * 4:1 MAX ON INTERIOR OF INTERCHANGE
 UTILIZE 3:1 SLOPES ON THE INTERIOR OF THE INTERCHANGE AT THE FOLLOWING:
 -L- STA. 18+75.00 TO 27+00.00 RT
 EXISTING ULTRA THIN ASPHALT PAVEMENT TO BE MILLED PRIOR TO WEDGING.



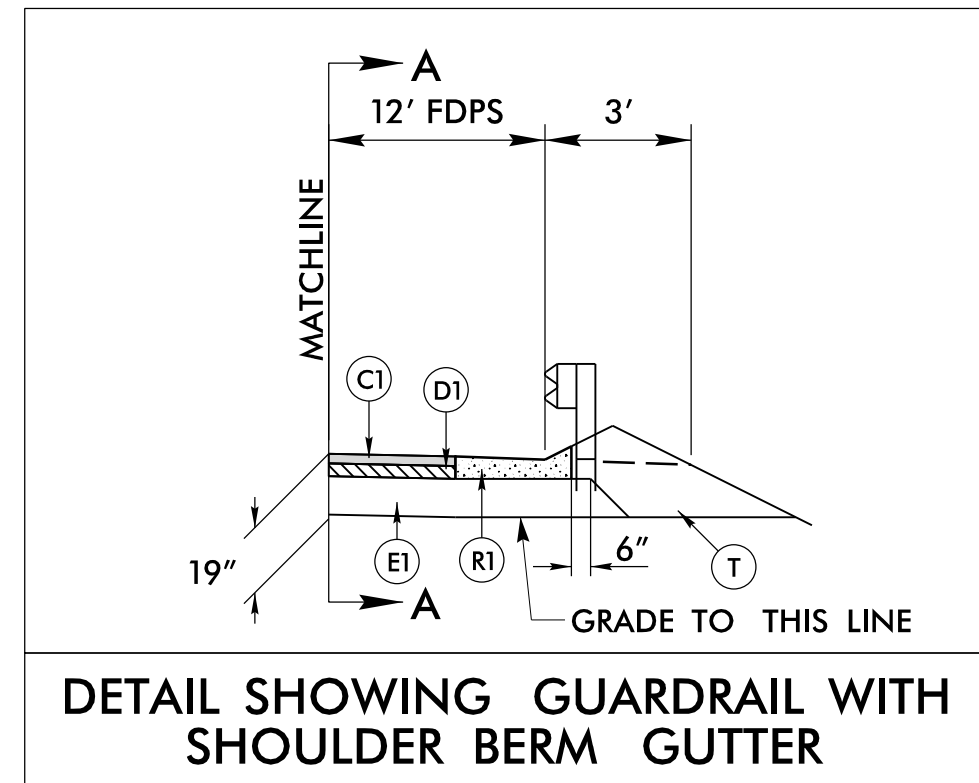
TYPICAL SECTION NO. 2
 -WBL- STA. 22+00.00 TO -WBL- STA. 27+76.49 (BEGIN BRIDGE)
 -WBL- STA. 28+76.49 (END BRIDGE) TO -WBL- STA. 34+40.00
 -EBL- STA. 23+83.00 TO -EBL- STA. 27+95.42 (BEGIN BRIDGE)
 -EBL- STA. 28+95.42 (END BRIDGE) TO -EBL- STA. 32+60.00

NOTES:
 * 4:1 MAX ON INTERIOR OF INTERCHANGE
 UTILIZE 3:1 SLOPES ON THE INTERIOR OF THE INTERCHANGE AT THE FOLLOWING:
 -L- STA. 18+75.00 TO 27+00.00 RT

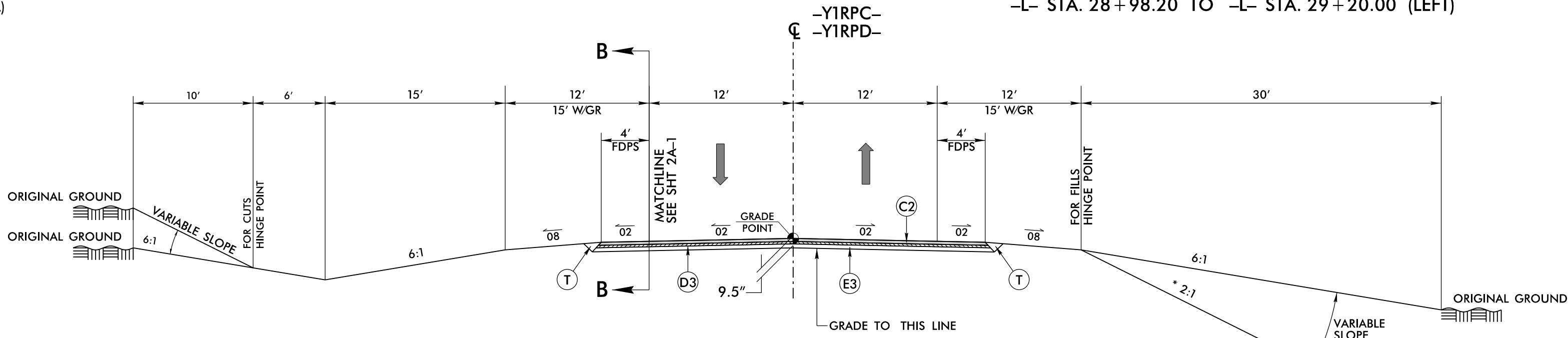


TYPICAL SECTION NO. 3
 -YIRPA- STA. 14+83.03 TO -L- STA. 22+13.62
 -YIRPB- STA. 14+20.80 TO -L- STA. 21+93.79 (MIRROR TYPICAL)
 -YIRPC- STA. 12+94.25 TO -L- STA. 15+61.00
 -YIRPD- STA. 13+40.51 TO -L- STA. 16+74.00 (MIRROR TYPICAL)

NOTES:
 * 4:1 MAX ON INTERIOR OF INTERCHANGE
 USE MAINLINE PAVEMENT AS SHOWN ON TYPICAL SECTION NO. 2 THRU GORE AREAS.



DETAIL SHOWING GUARDRAIL WITH SHOULDER BERM GUTTER
 -L- STA. 25+50.00 TO -L- STA. 27+73.70 (RIGHT)
 -L- STA. 29+21.54 TO -L- STA. 29+44 (RIGHT)
 -L- STA. 28+98.20 TO -L- STA. 29+20.00 (LEFT)



TYPICAL SECTION NO. 4
 -YIRPC- STA. 15+61.00 TO -L- STA. 23+08.87
 -YIRPD- STA. 16+74.00 TO -L- STA. 20+28.60

NOTES:
 * 4:1 MAX ON INTERIOR OF INTERCHANGE

FINAL PAVEMENT SCHEDULE	
C1	3" S9.5D
C2	3" S9.5B
C3	3" TYPE S9.5C
C4	VAR. S9.5D
C5	VAR. S9.5B
C6	VAR. S9.5C
D1	4" I19.0C
D2	3" I19.0C
D3	2.5" I19.0C
D4	VAR. I19.0C
E1	12" B25.0C
E2	3" B25.0C
E3	4" B25.0C
E4	4 1/2" B25.0C
E5	VAR. B25.0C
J1	8" AGGREGATE BASE COURSE.
R1	SHOULDER BERM GUTTER.
T	EARTH MATERIAL.
U	EXISTING PAVEMENT.
V1	0.5" MILLING
V2	1.5" MILLING
W	ASPHALT WEDGING (SEE DETAIL).
Y	MILLED RUMBLE STRIPS.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1, UNLESS SHOWN OTHERWISE

REVISIONS

4/13/2016 09:11:00 N:\Projects\B4448_RDY_TYP.dgn