

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5825	08	
WBS NO. 50468.3J			

GENERAL NOTES:

MAINLINE TYPICALS INCLUDE ACCELERATION AND DECELERATION LANES FOR THE RAMPS UP TO THE PHYSICAL GORE. THESE TYPICALS GENERALLY DEPICT MILLING 1.5" AND THEN PAVING ASPHALT CONCRETE SURFACE COURSE S9.5D AT A DEPTH OF 2.0" FOR THE FULL WIDTH OF THE ROADWAY TO INCLUDE SHOULDERS.

THE ULTRATHIN BONDED WEARING COURSE ARE INTENDED TO BE PLACED IN THE TRAVEL LANES AND GORE AREAS BUT NOT ON THE SHOULDERS.

RAMP TYPICALS ARE APPLICABLE BETWEEN PHYSICAL GORE AND INTERSECTING Y-LINE EDGE OF TRAVEL. ULTRATHIN BONDED WEARING COURSE IS NOT INTENDED TO BE PLACED ON THE RAMPS BETWEEN THE PHYSICAL GORE AND THE INTERSECTING Y-LINE.

MAINLINE TYPICALS ON I-77 NORTHBOUND AND SOUTHBOUND DIRECTION INCLUDE THE BRIDGES AT WILKINSON BLVD INTERCHANGE, REMOUNT RD, CLANTON RD, PRESSLEY RD, SOUTH TRYON ST, WOODLAWN RD, AND TWO RAILROAD BRIDGES. THE COMBINATION OF MILLING AND REPLACING ASPHALT PAVEMENT TO THE EXISTING GRADE IS NECESSARY TO MAINTAIN THE EXISTING BRIDGE CLEARANCE.

THE CONTRACTOR SHALL INCREASE HIS MILLING DEPTH FROM 1.5" TO 2.5" 500 FEET PRIOR TO AND AFTER THE CENTER LINE OF EACH OVERHEAD BRIDGE, IN ORDER TO MAINTAIN THE EXISTING BRIDGE CLEARANCE.

THE MILLING AND PAVEMENT STRUCTURE OF THE MAINLINE TRAVEL LANES SHALL BE THE CONTROLLING GRADE. ALL OTHER MILLING AND PAVING SHALL TIE-IN ACCORDINGLY.

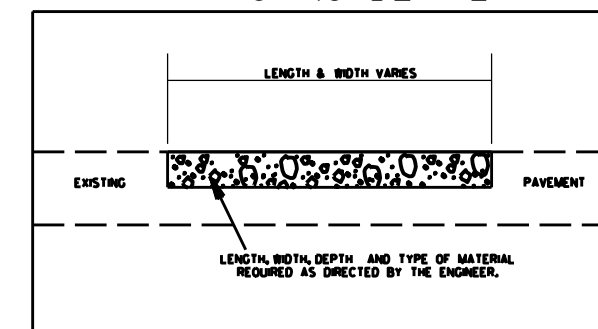
THE MAINLINE CROSS SECTION OF S9.5D SHALL BE PLACED IN SUCH A MANNER THAT THE MAT, AND EACH INDIVIDUAL PULL, WILL BE FLUSH WITH EACH OTHER ACROSS THE ENTIRE WIDTH OF THE ROADWAY TO INCLUDE SHOULDERS. THE TRAVEL LANES WEARING SURFACE OF ULTRATHIN BONDED WEARING COURSE SHALL BE PLACED IN SUCH A MANNER THAT IT WILL EXIST AT AN ELEVATION APPROXIMATELY 5/8" HIGHER THAN THE SURROUNDING SHOULDER PAVEMENT.

THE EXISTING PAVEMENT STRUCTURE CONSISTS OF AN OPEN GRADED FRICTION COURSE OVER ASPHALT CONCRETE PAVEMENT OVER CONCRETE PAVEMENT.

SHOULDER RECONSTRUCTION ON THE MAINLINE WILL BE PLACED AS DIRECTED BY THE ENGINEER.

FOR ALL CONCRETE ON RAMPS WITH ASPHALT OVERLAYS, BUILD UP THICKNESS TO MATCH MAINLINE GRADE AND SUPER. FOR OFF RAMPS TAPER DOWN PAVEMENT THICKNESS FOR SMOOTH TRANSITION.

PATCHING DETAIL



I-5825
I-77 PAVEMENT PRESERVATION

SCALE	-NA-		REVISIONS	
DATE	9/17		2/13/18	
DWG. BY	JHE			
DESIGN BY	JHE			
APPROVED				