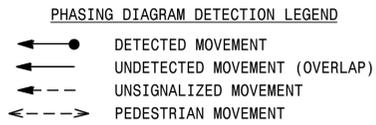
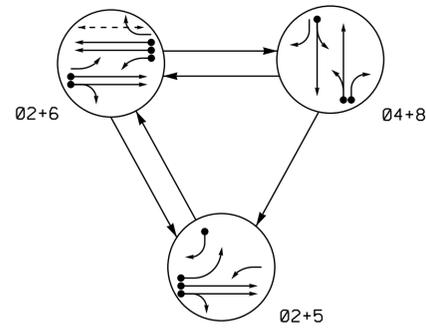


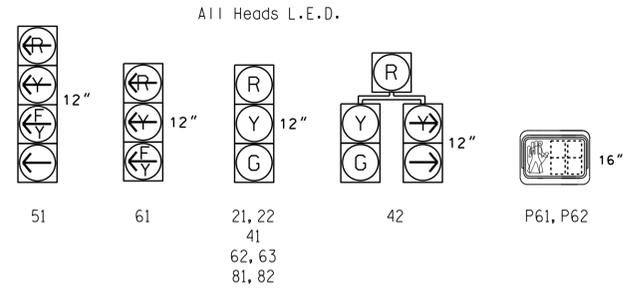
PHASING DIAGRAM



SIGNAL FACE	PHASE			
	02+5	02+6	04+8	FLASH
21, 22	G	G	R	Y
41	R	R	G	R
42	R	R	G	R
51	F	F	FR	FR
61	F	F	FR	FR
62, 63	R	G	R	Y
81, 82	R	R	G	R
P61, P62	DW	W	DW	DRK

W - Walk
 DW - Don't Walk
 DRK - Dark

SIGNAL FACE I.D.



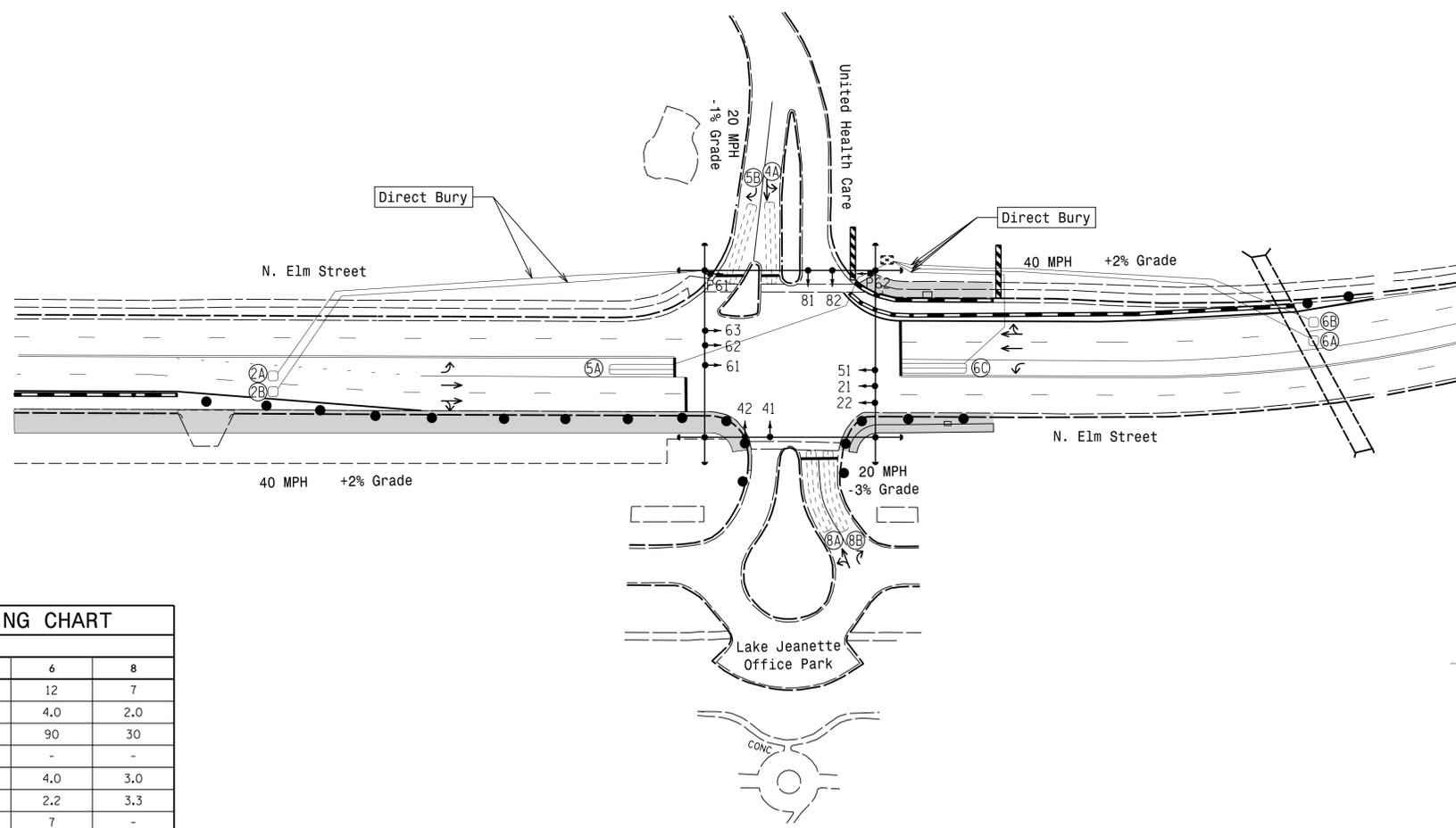
LOOP & DETECTOR UNIT INSTALLATION CHART

LOOP	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	DETECTOR PROGRAMMING								
					PHASE	SWITCH (PHASE)	DELAY TIME	STRETCH TIME	CALLING	EXTENSION	ADDED INIT.	SYSTEM LOOP	NEW CARD
2A	6X6	250	4	X	2	-	-	-	X	X	X	-	-
2B	6X6	250	4	X	2	-	-	-	X	X	X	-	-
4A	6X50	+5	2-4-2	-	4	-	-	-	X	X	-	-	-
5A	6X40	0	2-4-2	X	5	-	15	-	X	X	-	-	-
5B	6X50	+5	2-4-2	-	5	-	15	-	X	X	-	-	-
6A	6X6	250	4	X	6	-	-	-	X	X	X	-	-
6B	6X6	250	4	X	6	-	-	-	X	X	X	-	-
6C	6X40	0	2-4-2	X	6	-	-	-	X	X	-	-	-
8A	6X50	+5	2-4-2	-	8	-	-	-	X	X	-	-	-
8B	6X50	+5	2-4-2	-	8	-	10	-	X	X	-	-	-

3 Phase Fully Actuated (Greensboro Signal System)

NOTES

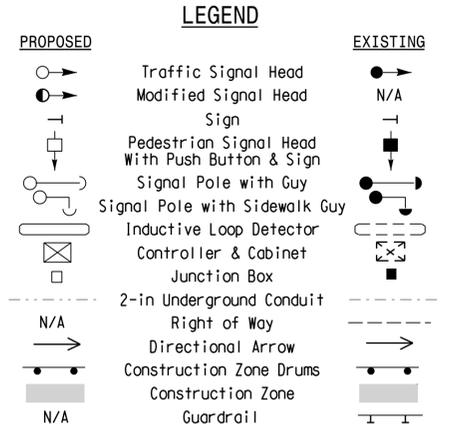
- Refer to "Roadway Standard Drawings NCDOT" dated January 2018 and "Standard Specifications for Roads and Structures" dated January 2018.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Phase 5 may be lagged.
- Re-number existing phases, loops, and signal heads as shown.
- Set all detector units to presence mode.
- Omit "WALK" and flashing "DON'T WALK" with no pedestrian calls.
- Program pedestrian heads to countdown the flashing "Don't Walk" time only.
- Pedestrian pedestals are conceptual and shown for reference only. See sheets P1-P3 for pushbutton location details.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.



NAZTEC APOGEE 2070 TIMING CHART

FEATURE	PHASE				
	2	4	5	6	8
Min Green *	12	7	7	12	7
Gap, Extension *	4.0	2.0	2.0	4.0	2.0
Maximum Green 1 *	90	30	30	90	30
Maximum Green 2 *	-	-	-	-	-
Yellow Clear	4.0	3.0	3.0	4.0	3.0
Red Clear	2.2	3.3	3.2	2.2	3.3
Walk *	-	-	-	7	-
Pedestrian Clear	-	-	-	23	-
Added Initial *	2.0	-	-	2.0	-
Maximum Initial *	29	-	-	29	-
Time Before Reduction *	15	-	-	15	-
Time To Reduce *	30	-	-	30	-
Minimum Gap	3.0	-	-	3.0	-
Recall Mode	MIN RECALL	-	-	MIN RECALL	-
Lock Calls	YES	NO	NO	YES	NO
Dual Entry	-	ON	-	-	ON
Simultaneous Gap	ON	ON	ON	ON	ON

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.



Signal Upgrade Temporary Design 1 (TMP Phase I)

Prepared in the Offices of: **TRANSPORTATION MOBILITY AND SAFETY SOLUTIONS**
 NORTH CAROLINA PROFESSIONAL ENGINEERS
 750 N. Greenfield Pkwy, Garner, NC 27529

N. Elm Street at United Health Care and Lake Jeanette Office Park
 Division 7 Guilford County Greensboro
 PLAN DATE: October 2017 REVIEWED BY: I. O. Umozurike

SCALE: 1" = 50'

REVISIONS: _____ INIT. DATE

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL: ROBERT J. ZIEMBA, ENGINEER, 026486, 10/31/2017

SIG. INVENTORY NO. GBO-559T1

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