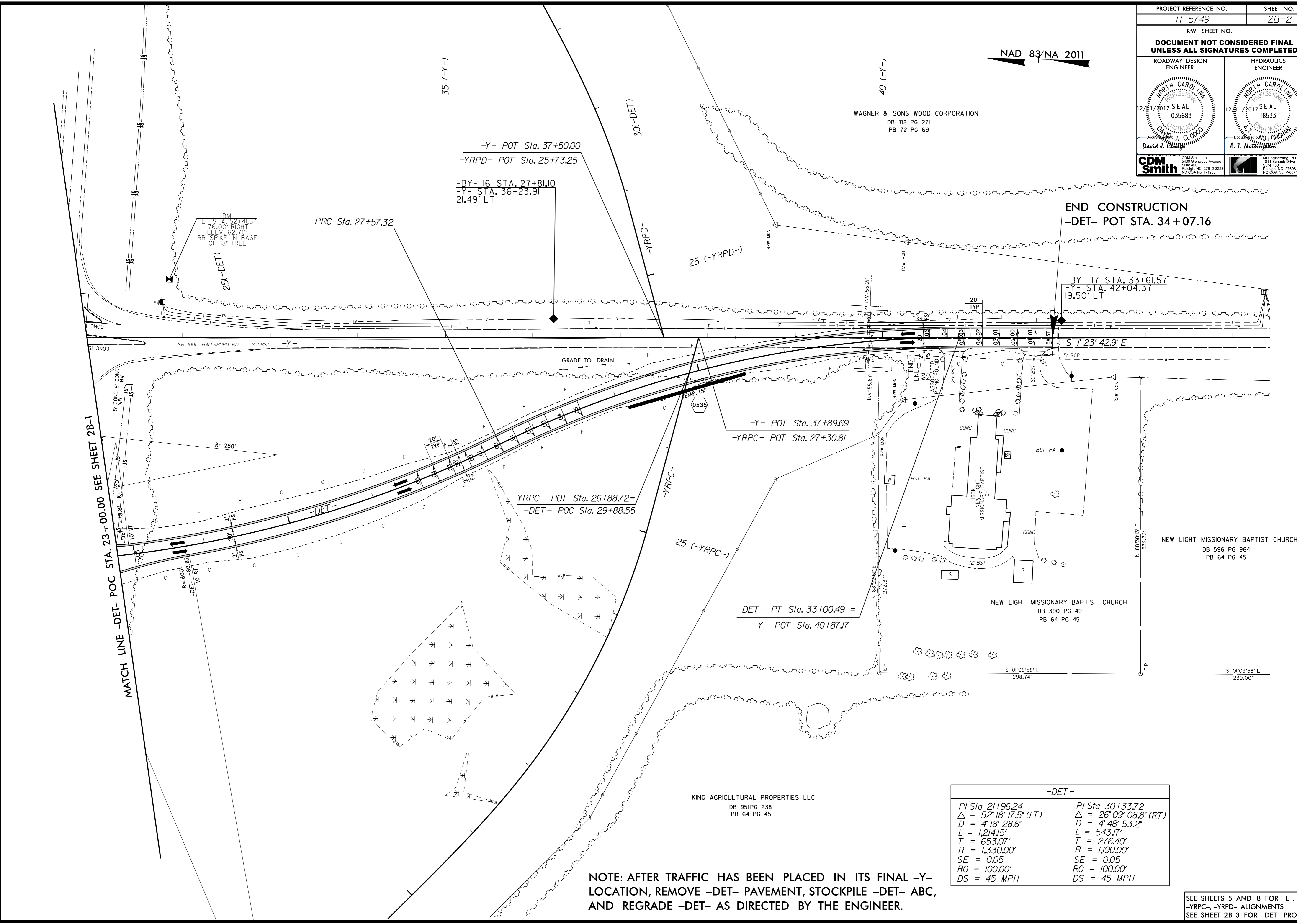


PROJECT REFERENCE NO. <i>R-5749</i>	SHEET NO. <i>2B-2</i>
R/W SHEET NO.	
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER
David J. Clouso	A.T. Neill
CDM Smith Inc. 5400 Glenwood Avenue Suite 400 Raleigh, NC 27612-3228 NC CDA No. P-1255	ME Engineering, PLLC 5115 Schuch Drive Suite 100 Raleigh, NC 27608 NC CDA No. P-0871

NAD 83/NA 2011



MATCH LINE -DET- POC STA. 23+00.00 SEE SHEET 2B-1

-Y- POT Sta. 37+50.00
 -YRPD- POT Sta. 25+73.25
 -BY- 16 STA. 27+81.10
 -Y- STA. 36+23.91
 21.49' LT

END CONSTRUCTION
 -DET- POT STA. 34+07.16

-BY- 17 STA. 33+61.57
 -Y- STA. 42+04.37
 19.50' LT

-YRPC- POT Sta. 26+88.72=
 -DET- POC Sta. 29+88.55

-Y- POT Sta. 37+89.69
 -YRPC- POT Sta. 27+30.81

-DET- PT Sta. 33+00.49 =
 -Y- POT Sta. 40+87.17

KING AGRICULTURAL PROPERTIES LLC
 DB 951 PG 238
 PB 64 PG 45

NEW LIGHT MISSIONARY BAPTIST CHURCH
 DB 596 PG 964
 PB 64 PG 45

NEW LIGHT MISSIONARY BAPTIST CHURCH
 DB 390 PG 49
 PB 64 PG 45

-DET-	
PI Sta 21+96.24	PI Sta 30+33.72
$\Delta = 52' 18" 17.5" (LT)$	$\Delta = 26' 09" 08.8" (RT)$
$D = 4' 18" 28.6"$	$D = 4' 48" 53.2"$
$L = 1,214.15'$	$L = 543.17'$
$T = 653.07'$	$T = 276.40'$
$R = 1,330.00'$	$R = 1,190.00'$
$SE = 0.05$	$SE = 0.05$
$RO = 100.00'$	$RO = 100.00'$
$DS = 45 MPH$	$DS = 45 MPH$

NOTE: AFTER TRAFFIC HAS BEEN PLACED IN ITS FINAL -Y- LOCATION, REMOVE -DET- PAVEMENT, STOCKPILE -DET- ABC, AND REGRADE -DET- AS DIRECTED BY THE ENGINEER.

SEE SHEETS 5 AND 8 FOR -L-, -Y-,
 -YRPC-, -YRPD- ALIGNMENTS
 SEE SHEET 2B-3 FOR -DET- PROFILE

5/14/19

Invalid expression: Rdj_psh_2B2.dgn