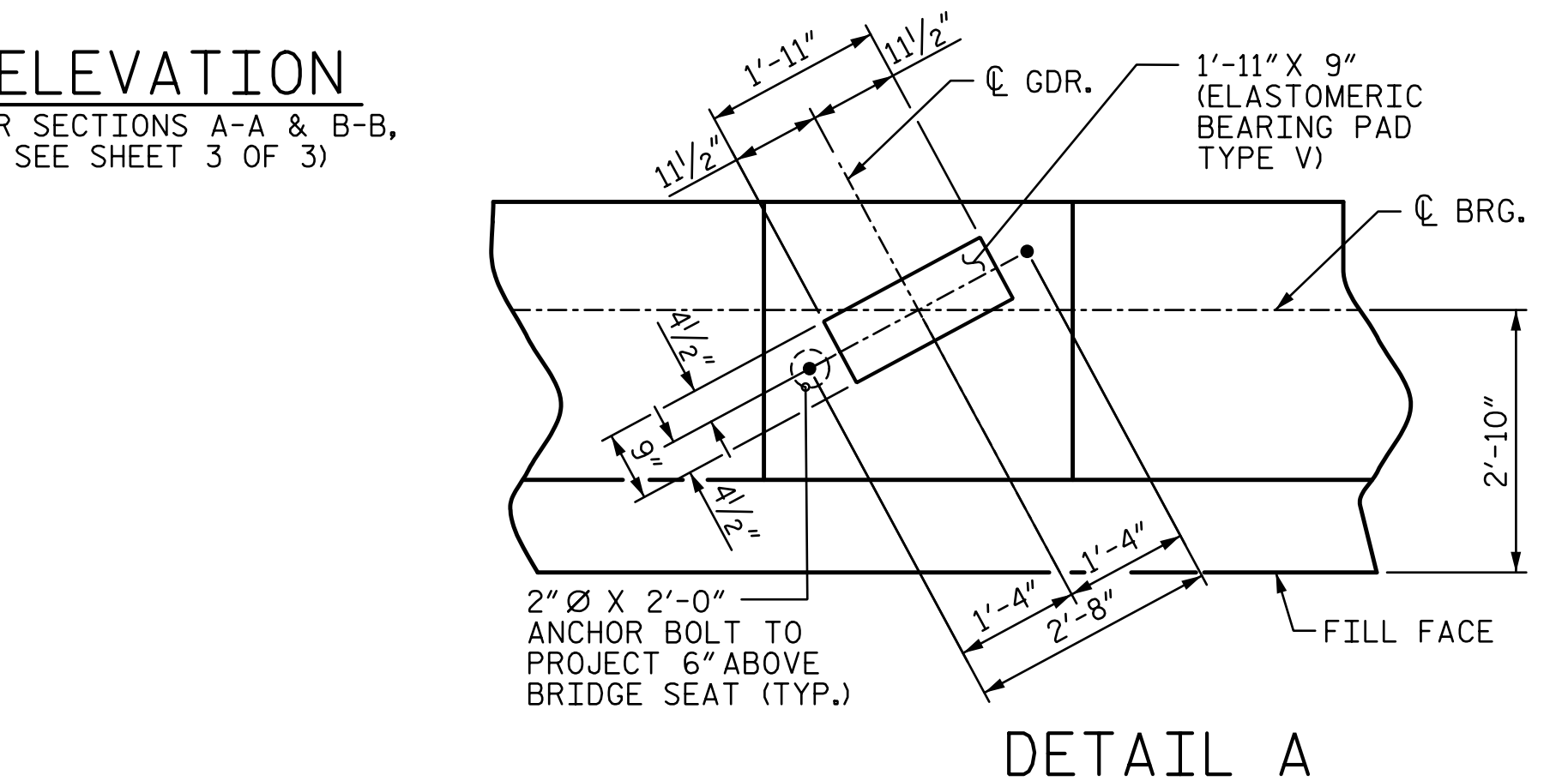
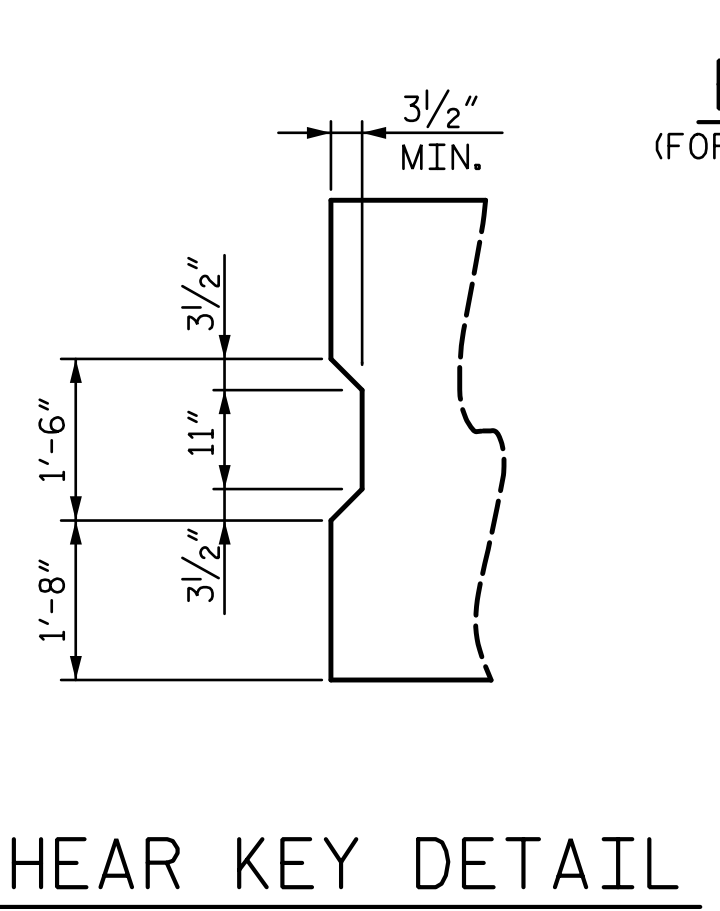


**NOTES:**  
 STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.  
 BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.  
 THE TOP SURFACE OF THE END BENT CAP SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, EXCEPT THAT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.  
 THE TOP SURFACE OF THE CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE OUTSIDE FACE AT THE RATE OF 2%.  
 DIMENSIONS LOCATING THE PILES ARE SHOWN TO THE CENTERLINE OF THE PILE AT THE BOTTOM OF THE END BENT CAP.  
 INSTALL THE 4" Ø DRAIN PIPE THROUGH THE WING WALL AS REQUIRED FOR THE REINFORCED BRIDGE APPROACH FILL, SEE ROADWAY PLANS. REINFORCING STEEL IN THE WING WALL MAY BE SHIFTED AS NECESSARY TO CLEAR THE DRAIN PIPE.  
 CONCRETE IN THE HATCHED AREA OF THE BACKWALL SHALL BE POURED AFTER THE BARRIER RAIL IS CAST IF SLIP FORMING IS USED.  
 PILE SPLICE SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND AWS D1.1.



PROJECT NO. U-4751  
 NEW HANOVER COUNTY  
 STATION: 62+99.10 -L-  
 SHEET 1 OF 3

STATE OF NORTH CAROLINA  
 DEPARTMENT OF TRANSPORTATION  
 RALEIGH

SUBSTRUCTURE  
 END BENT 1

6/29/2017

STV ENGINEERS, INC.  
 900 West Trade St., Suite 715  
 Charlotte, NC 28202  
 NC License Number F-5991

REVISIONS			
NO.	BY:	DATE:	DATE:
1			
2			
3			
4			

SHEET NO. S2-22  
 TOTAL SHEETS 30

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