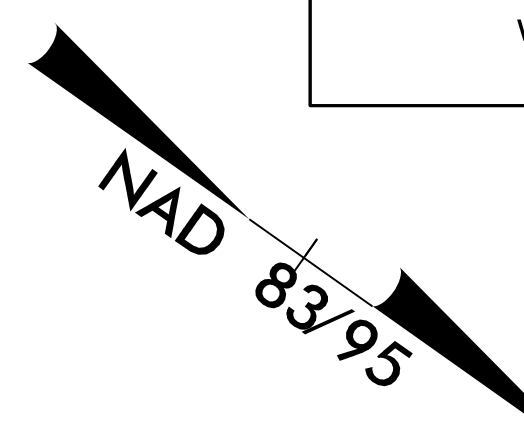
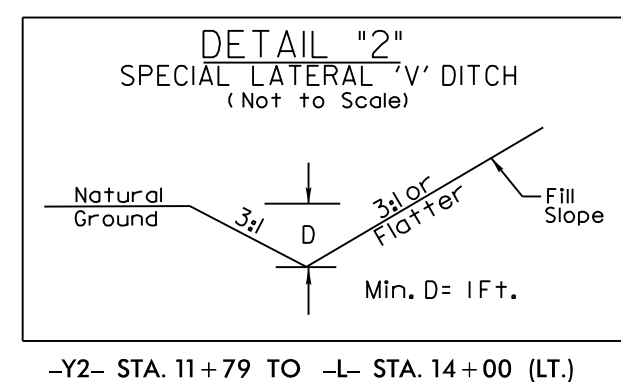


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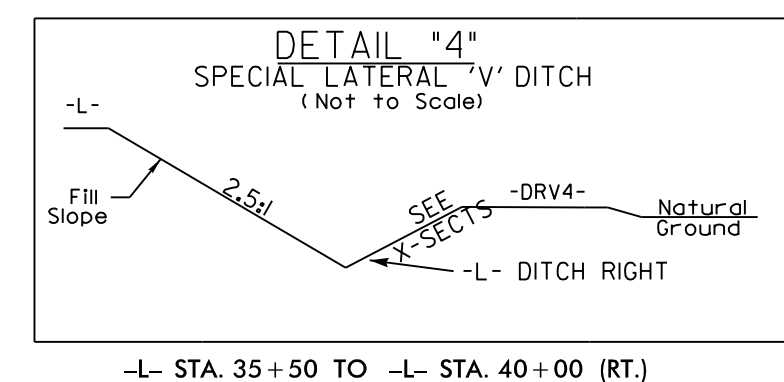
PROJECT REFERENCE NO. B-4616	SHEET NO. 6
ROADWAY DESIGN ENGINEER SEAL 33290 NORTH CAROLINA PROFESSIONAL ENGINEERS BOARD A. THOMPEN 12/16/2017	HYDRAULICS ENGINEER SEAL 019775 NORTH CAROLINA PROFESSIONAL ENGINEERS BOARD ROY LAMM 5/2/2017



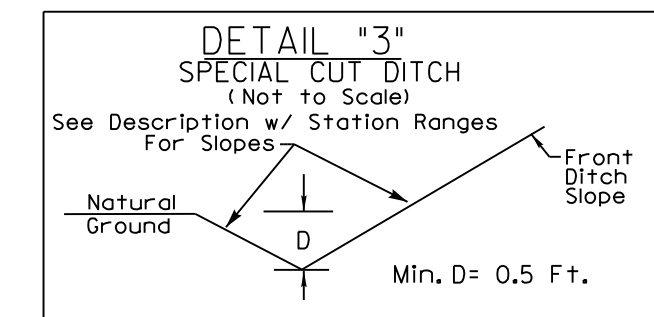
-DRV3-  
 PI Sta 12+92.07  
 $\Delta = 15^\circ 32' 19.2''$  (LT)  
 $D = 11^\circ 27' 33.0''$   
 $L = 135.60'$   
 $T = 68.22'$   
 $R = 500.00'$   
 SE = NC



MATCH LINE -L- STA 38+00.00  
SEE SHEET 5



-DRV4-  
 PI Sta 10+51.55  
 $\Delta = 82^\circ 48' 08.5''$  (RT)  
 $D = 190^\circ 59' 09.4''$   
 $L = 43.36'$   
 $T = 26.45'$   
 $R = 30.00'$   
 SE = NC



**END TIP PROJECT B-4616**  
 -L- STA 46+00.00

- NOTES: (1) SEE SHEET 9 FOR -L- PROFILE  
 (2) SEE SHEET 9 FOR -Y2- PROFILE  
 (3) SEE SHEET 10 FOR -DRV3- PROFILE  
 (4) SEE SHEET 10 FOR -DRV4- PROFILE  
 (5) SEE SHEET 10 FOR -DRV5- PROFILE  
 (6) SEE SHEET 6-A FOR -Y2 DETOUR- & -DRV4 DETOUR- DESIGN