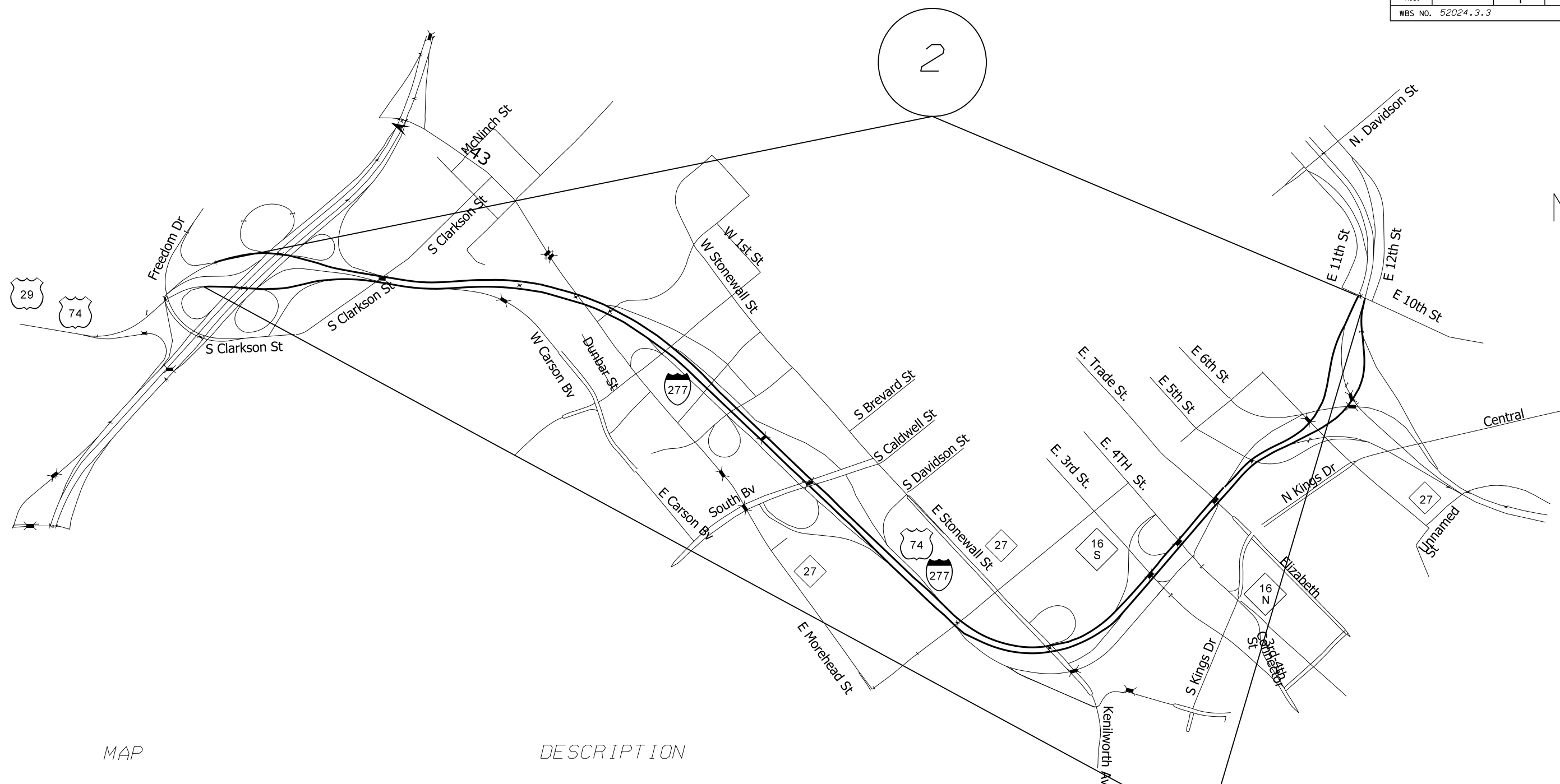


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STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	1	
WBS NO. 52024.3.3			

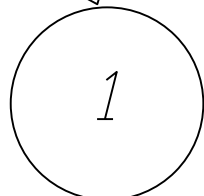


MAP

DESCRIPTION

- # 1 INTERSTATE 277 (OUTER LOOP)
- # 2 INTERSTATE 277 (INNER LOOP)

FROM I-77 SOUTH AROUND CENTER CITY CHARLOTTE TO BRIDGE OVER 10th ST.
 FROM BRIDGE OVER 10th ST SOUTH AROUND CENTER CITY CHARLOTTE TO I-77



I-5746A/I-5746B
INTERSTATE PAVEMENT PRESERVATION

SCALE	-NA-		REVISIONS
DATE	3/17		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	2	
WBS NO. 52024.3.3			

MAP

3 DELETED

4 I-277 INNER TO I-77 S

5 I-277 INNER TO I-77 N

6 FREEDOM DR TO I-277 OUTER

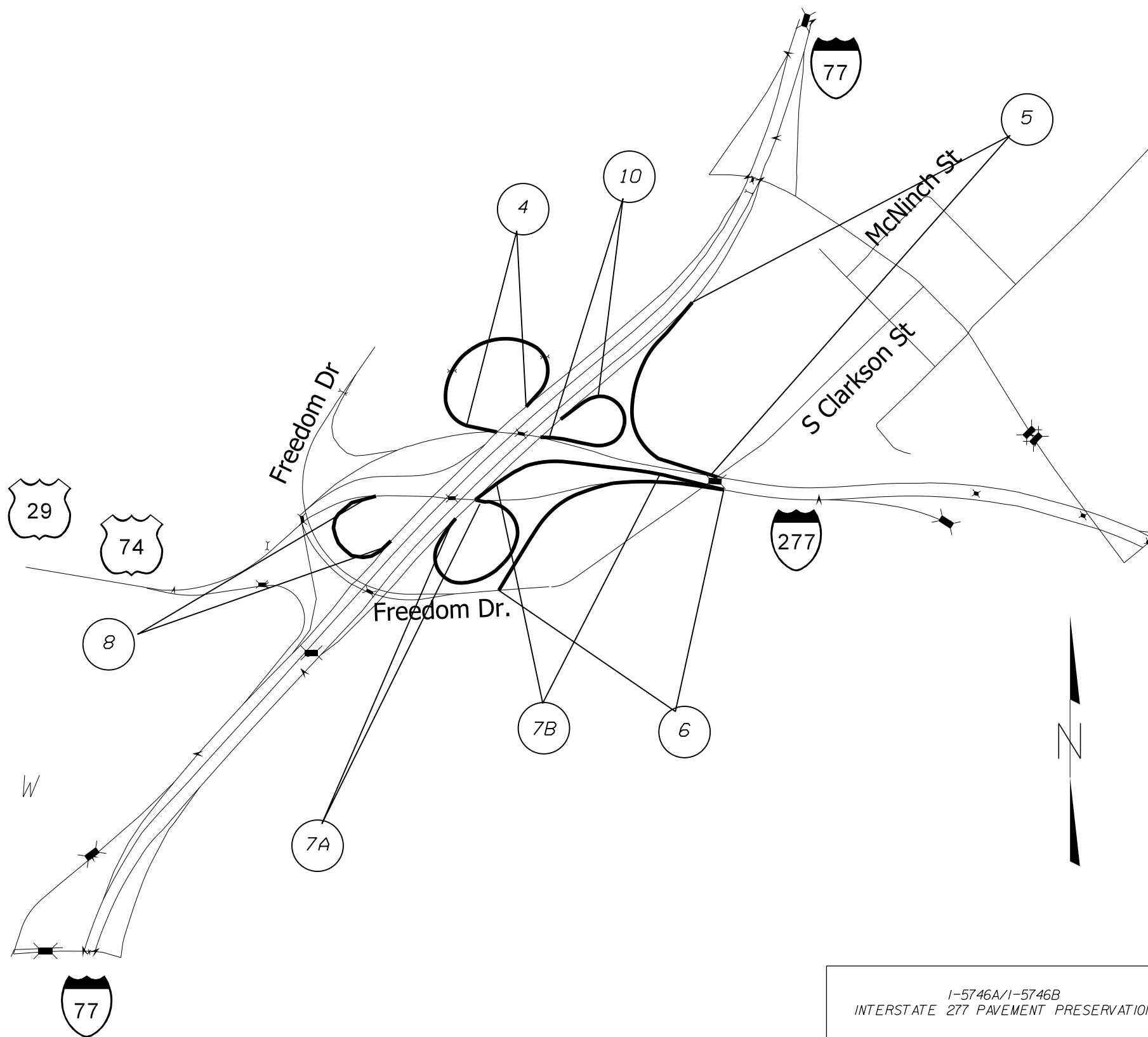
7A I-277 OUTER TO I-77 N


7B I-77 N TO I-277 OUTER

8 I-77 S TO I-277 OUTER

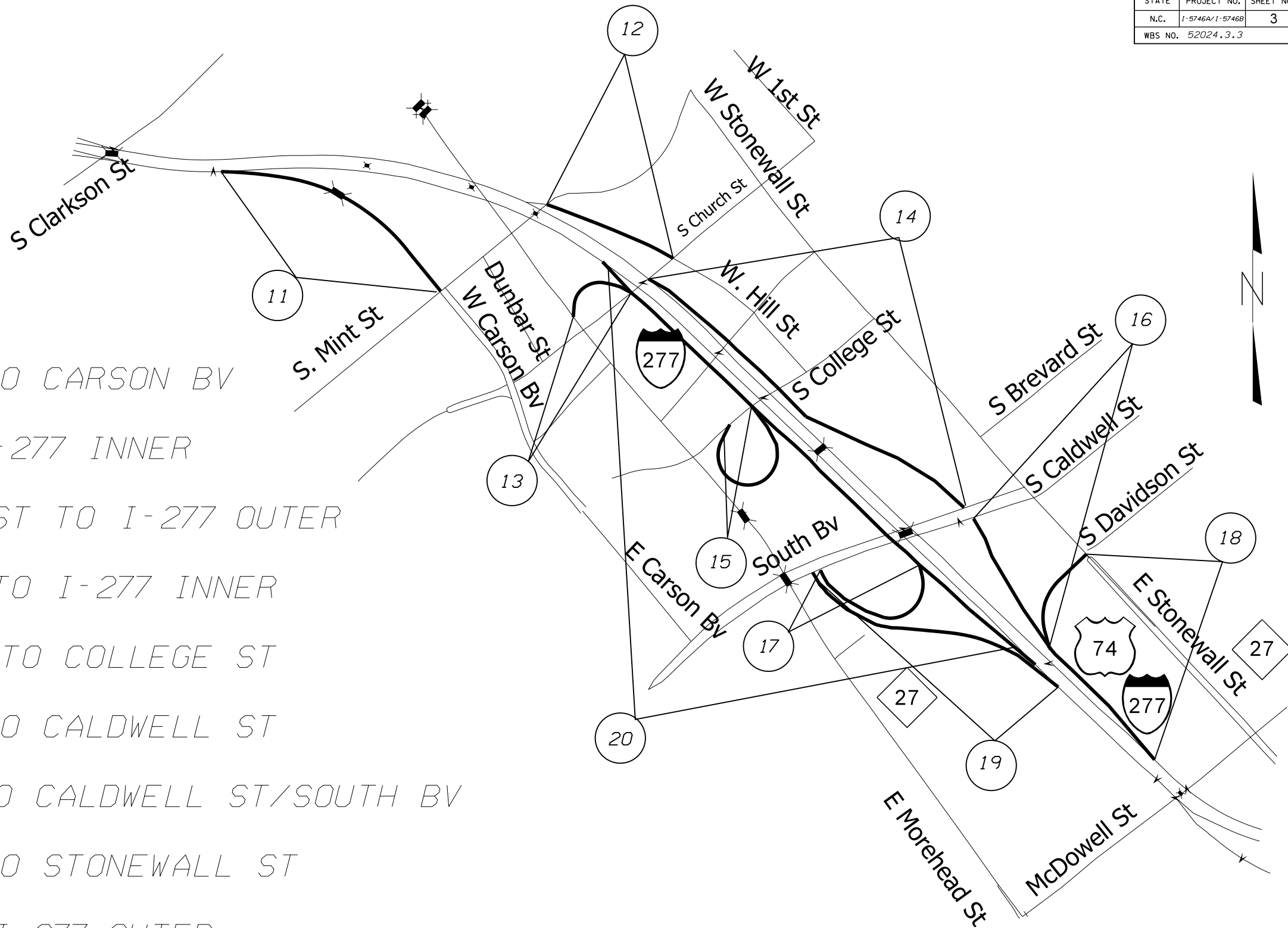
9 DELETED

10 I-77 N TO I-277 INNER/US 74 W



I-5746A/I-5746B INTERSTATE 277 PAVEMENT PRESERVATION			REVISIONS	
SCALE	-NA-			
DATE	3/17			
DWG. BY	TJP			
DESIGN BY	TJP			
APPROVED	WAT			


STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	3	
WBS NO. 52024.3.3			



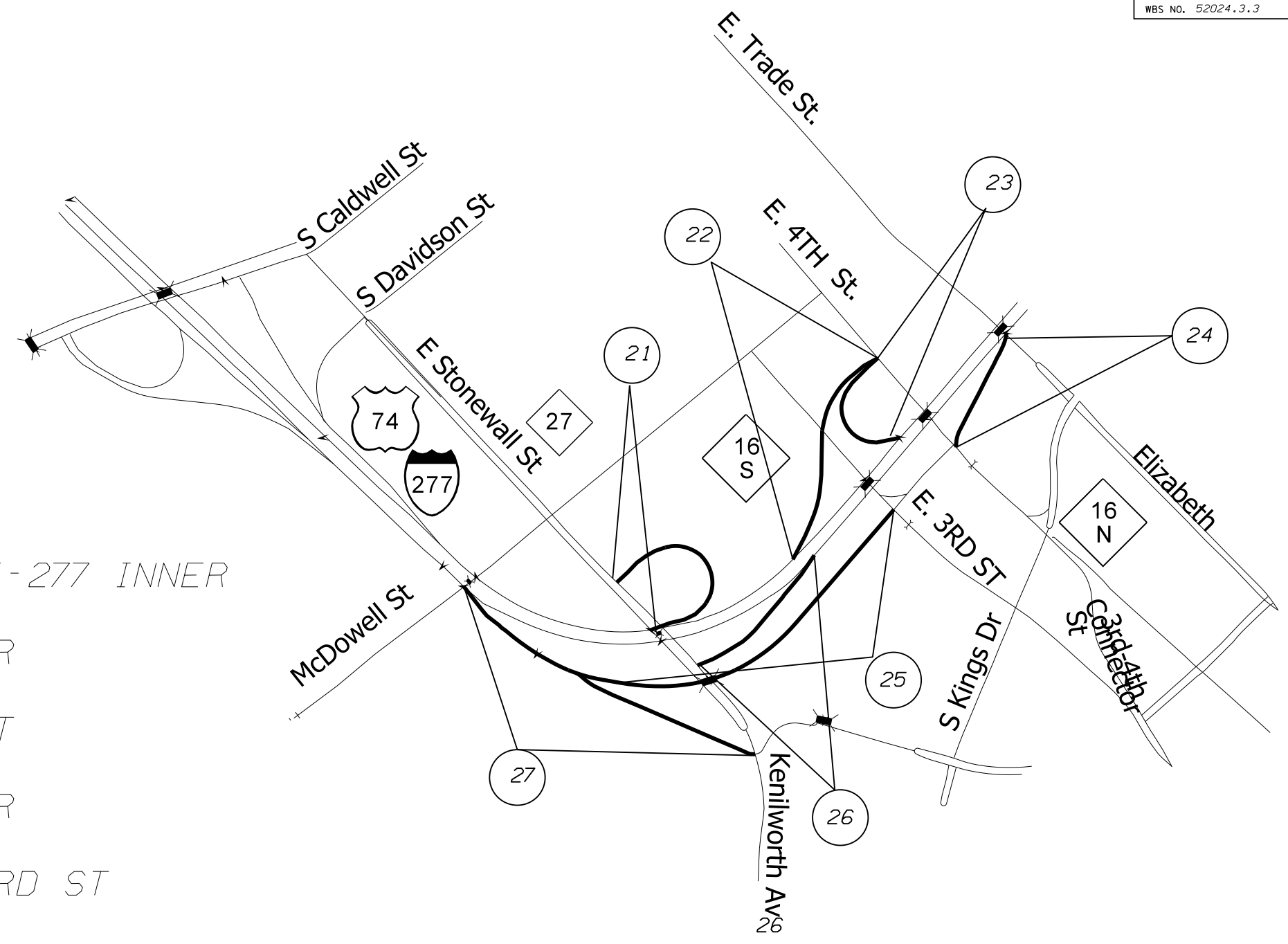
MAP

- # 11 I-277 OUTER TO CARSON BV
- # 12 HILL ST TO I-277 INNER
- # 13 W. MOREHEAD ST TO I-277 OUTER
- # 14 CALDWELL ST TO I-277 INNER
- # 15 I-277 OUTER TO COLLEGE ST
- # 16 I-277 INNER TO CALDWELL ST
- # 17 I-277 OUTER TO CALDWELL ST/SOUTH BV
- # 18 I-277 INNER TO STONEWALL ST
- # 19 SOUTH BV TO I-277 OUTER
- # 20 I-277 OUTER COLLECTOR RAMP

I-5746A/I-5746B
INTERSTATE 277 PAVEMENT PRESERVATION

SCALE	-NA-		REVISIONS
DATE	3/17		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	4	
WBS NO. 52024.3.3			



MAP

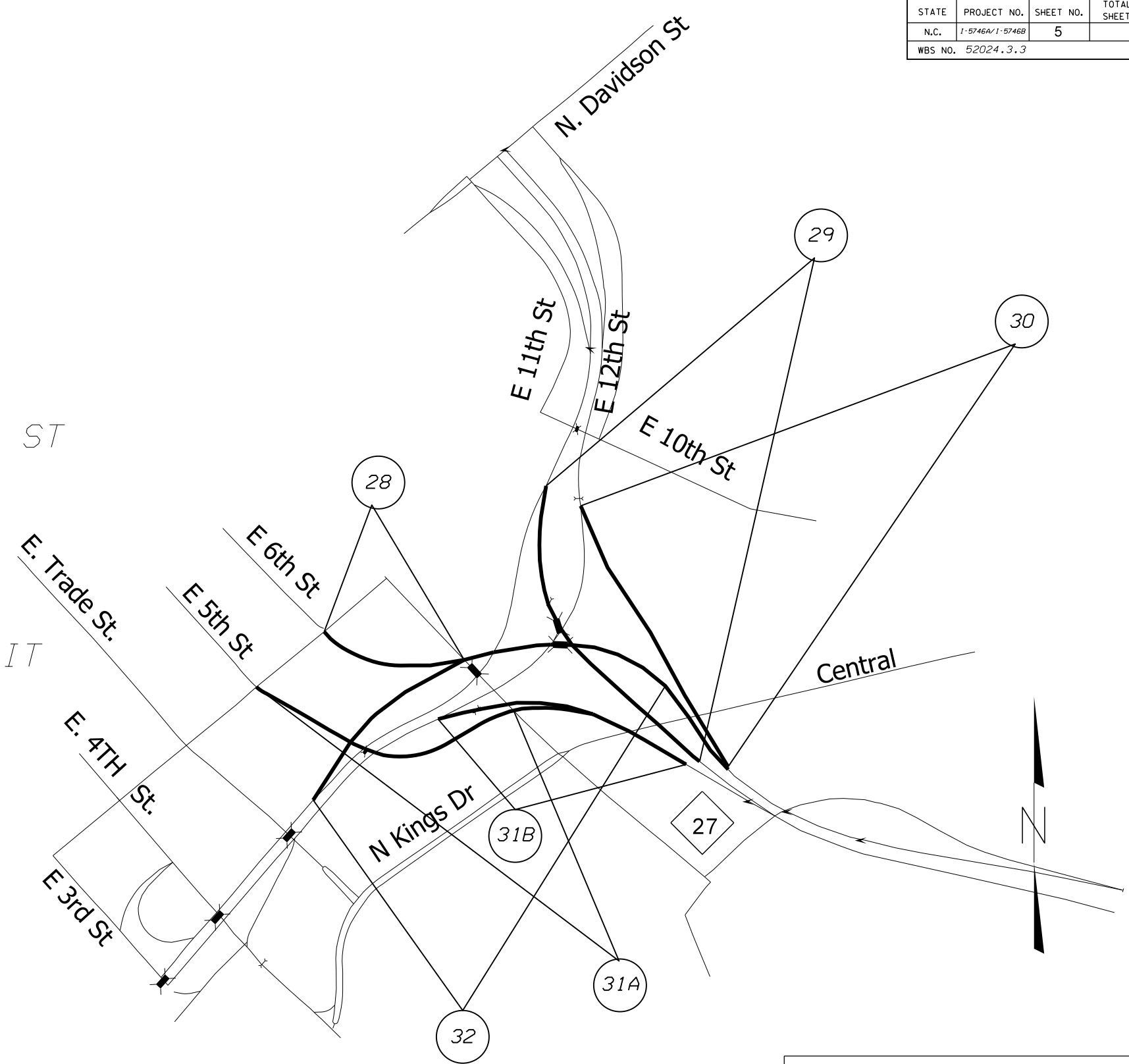
- # 21 E. STONEWALL ST TO I-277 INNER
- # 22 4TH ST TO I-277 INNER
- # 23 I-277 INNER TO 4TH ST
- # 24 4TH ST TO I-277 OUTER
- # 25 I-277 OUTER TO E. 3RD ST
- # 26 KENILWORTH AV TO I-277 OUTER
- # 27 I-277 OUTER TO KENILWORTH AV

I-5746A/I-5746B INTERSTATE 277 PAVEMENT PRESERVATION			REVISIONS	
SCALE	-NA-			
DATE	3/17			
DWG. BY	TJP			
DESIGN BY	TJP			
APPROVED	WAT			


STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	5	
WBS NO. 52024.3.3			

MAP

- # 28 I-277 INNER ON RAMP TO E. 6TH ST
- # 29 I-277 INNER TO US 74 EAST
- # 30 US 74 WEST TO I-277 OUTER
- # 31A E. 5TH ST TO I-277 OUTER EXIT RAMP TO US 74 EAST
- # 31B I-277 OUTER TO US74 EAST
- # 32 US 74 WEST TO I-277 INNER



I-5746A/I-5746B
INTERSTATE 277 PAVEMENT PRESERVATION

SCALE	-NA-		REVISIONS
DATE	3/17		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	6	
WBS NO.	52024.3.3		

Notes:

THIS CONTRACT IS INTENDED TO REPAIR AND REHABILITATE I-277 AND ITS ASSOCIATED RAMPS AND LOOPS THROUGH A COMBINATION OF PORTLAND CEMENT CONCRETE SLAB REPAIR, ASPHALT PAVEMENT REPAIR, CRACK SEALING, JOINT REPAIR, MILLING AND ASPHALT PAVING, AND FOG SEAL APPLICATION.

I-277 IS A ¾ LOOP AROUND UPTOWN CHARLOTTE THAT IS DIVIDED INTO TWO LOCALLY NAMED SECTIONS. THE SOUTHERN PORTION OF THE LOOP IS THE JOHN BELK FREEWAY. THE NORTHERN SECTION OF THE LOOP IS THE BROOKSHIRE FREEWAY. THESE TWO SECTIONS ARE DIVIDED BY THE INTERCHANGE WITH US74 (INDEPENDENCE BV) THE FOURTH QUARTER OF THE LOOP IS MADE UP OF I-77. THE LOOP IS CONSTRUCTED OF JOINTED PORTLAND CEMENT CONCRETE PAVEMENT (PCCP) WITH PREDOMINANTLY ASPHALT PAVED SHOULDERS.

THE TYPICALS IN THIS CONTRACT GENERALLY DEPICT THE FOLLOWING SCOPE OF WORK.

THE JOHN BELK FREEWAY SECTION OF I-277 FROM I-77 TO THE BRIDGE OVER 3RD ST WILL HAVE THE DISTRESSED SECTIONS OF THE PCCP REPAIRED. THE ASPHALT SHOULDERS WILL BE PRIMARILY LEFT IN PLACE AND TREATED WITH A FOG SEAL.

THE MAINLINE SECTION BETWEEN THE BRIDGE OVER 3RD ST AND THE BRIDGE OVER 10TH STREET SHALL HAVE THE DISTRESSED SECTIONS OF THE PCCP REPAIRED. THE ASPHALT SHOULDERS IN THIS SECTION WILL BE MILLED AND REPLACED.

THE TYPICALS FOR THE ON AND OFF RAMPS GENERALLY DEPICT THE SAME REPAIR/REHABILITATION METHODS AS THE SECTION OF MAINLINE I-277 THAT THEY SERVE.

THE PCCP REPAIR SHALL BE IN ACCORDANCE WITH CONTRACT DOCUMENTS, ALL APPLICABLE SECTIONS OF THE 2012 NCDOT STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES, AND THE NCDOT MANUAL FOR REPAIR OF PCC PAVEMENTS REVISED JULY 2015.

THERE IS A 650' SECTION OF FULL DEPTH ASPHALT PAVEMENT ON THE JOHN BELK FREEWAY JUST EAST OF I-77. THIS AREA WILL BE REHABILITATED BY MILLING TO A DEPTH OF 3" AND PAVING BACK WITH 3" OF ASPHALT CONCRETE SURFACE COURSE S9.5D. THE MILLING AND PAVING OF THIS SECTION SHALL BE ACCOMPLISHED IN SUCH A MANNER THAT NO TRANSVERSE PAVEMENT JOINTS WILL BE LEFT AT THE END OF EACH NIGHT'S WORK AND THERE WILL BE NO OPPORTUNITY FOR WATER TO STAND IN THE ROADWAY.

THE JOINT BETWEEN THE ASPHALT SHOULDERS AND PCCP PAVEMENT SHALL BE SEALED ALONG WITH OTHER PAVEMENT CRACKS AND JOINTS AS DIRECTED BY THE ENGINEER.

THE PAVEMENT MARKINGS ON THIS PROJECT CONSIST PRIMARILY OF POLYUREA FOR THE EDGELINES, WIDE LINES, AND GORE LINES. THE LANES SKIPS AND APPLICABLE MINISKIPS WILL BE PLACED USING COLD APPLIED PLASTIC WITH 6" WIDE WHITE SURROUNDED BY A BLACK CONTRAST BORDER.

THE CONTRACTOR'S ATTENTION IS DIRECTED TO NCDOT PAVEMENT MARKING STANDARDS FOR AUXILLARY LANES AND LANE DROPS WHICH REQUIRE THE USE OF "WIDE" MARKINGS AT SPECIFIED LOCATIONS. THERE ARE SOME AREAS OF THE PROJECT THAT IN EXISTING CONDITION DO NOT HAVE WIDE MARKINGS. THIS CONTRACT WILL REPLACE THE OLD MARKINGS WITH MARKINGS IN ACCORDANCE WITH THE NEW STANDARDS PRESENTED IN THE 2012 NCDOT ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER.

PROPOSED LANE SKIPS AND MINISKIPS SPECIFIED AS WIDE SHALL BE PLACED AS POLYUREA 12" WIDE.

THE EXISTING PAVEMENT MARKINGS SHALL BE REMOVED PRIOR APPLICATION OF NEW MARKINGS.

REMOVE EDGELINES AND VARIOUS SKIPS WITH LINE CODE "REMOVAL OF PAVEMENT MARKING LINES 6".

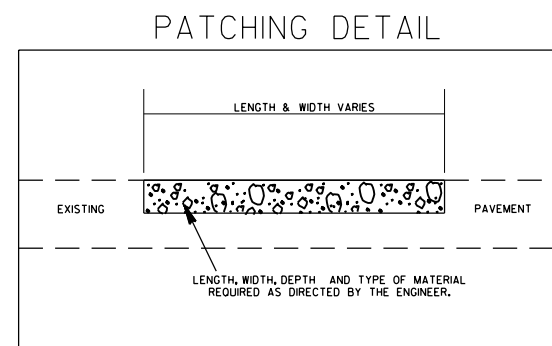
REMOVE EXISTING COLD APPLIED PLASTIC LANE LINES WITH BLACK CONTRAST BORDERS USING LINE CODE "REMOVAL OF PAVEMENT MARKING LINES 9".

REMOVE GORES AND EXISTING WIDE MARKINGS USING LINE CODE "REMOVAL OF PAVEMENT MARKINGS 12".

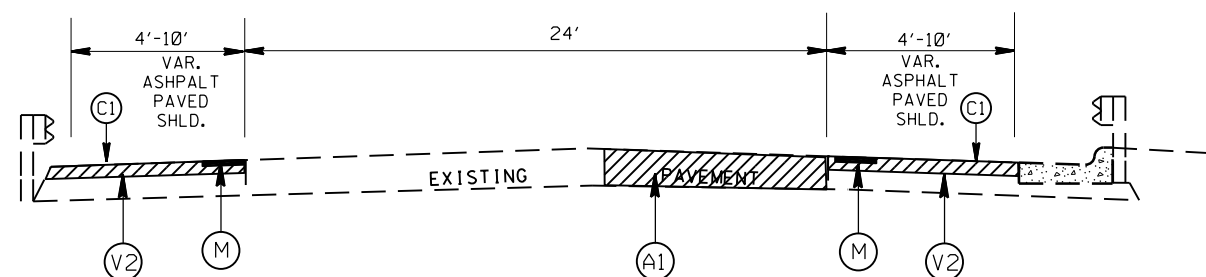
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS	
DATE	12/16			
DWG. BY	TJP			
DESIGN BY	TJP			
APPROVED	WAT			

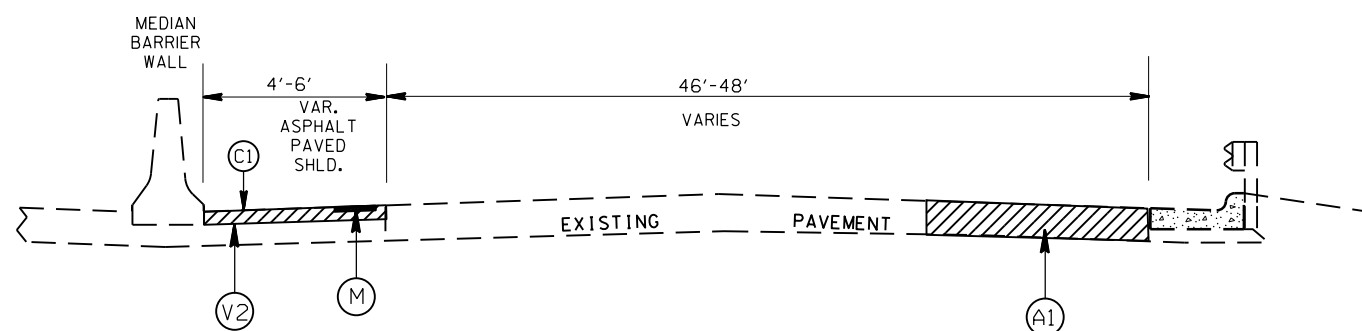
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	7	
WBS NO.		52024.3.3	



TYPICAL SECTIONS 1-6 HAVE BEEN DELETED



TYPICAL SECTION NO.7



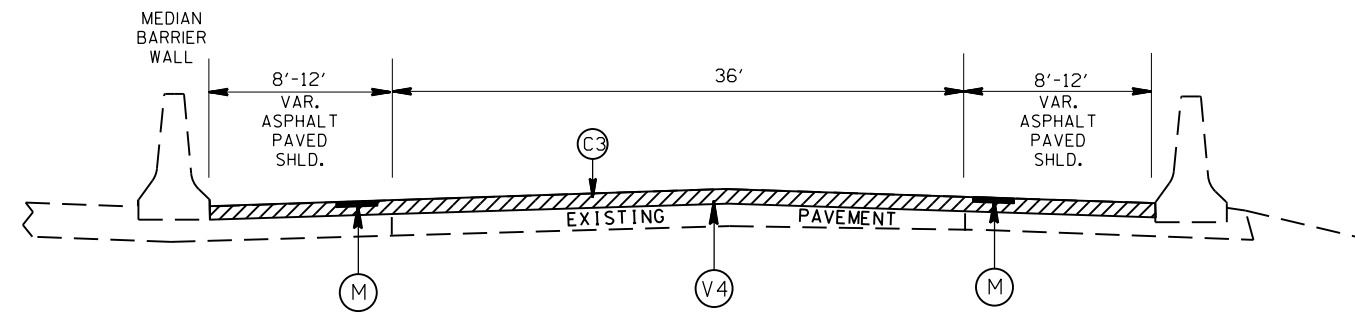
TYPICAL SECTION NO.8

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

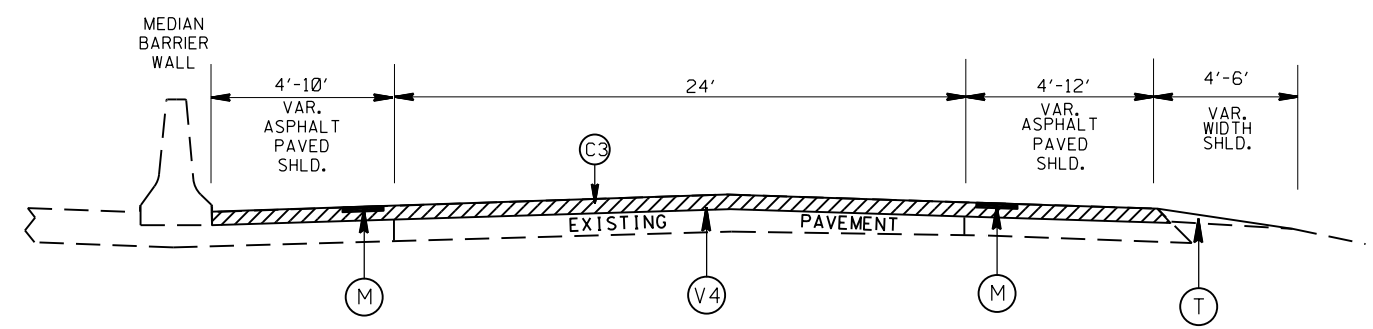
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS	
DATE	12/16			
DWG. BY	TJP			
DESIGN BY	TJP			
APPROVED	WAT			

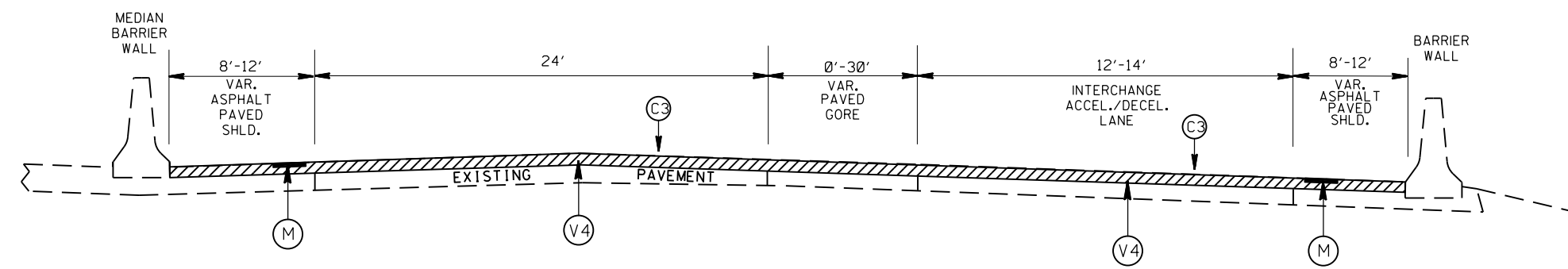
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
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WBS NO.	52024.3.3		



TYPICAL SECTION NO.9




TYPICAL SECTION NO.10



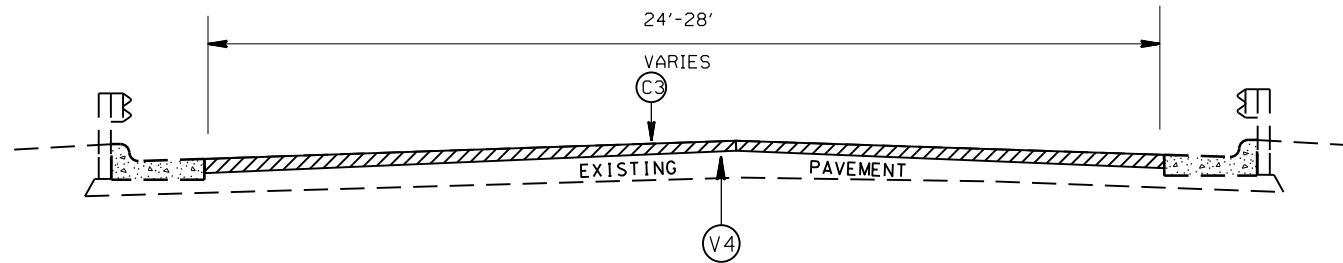
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PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

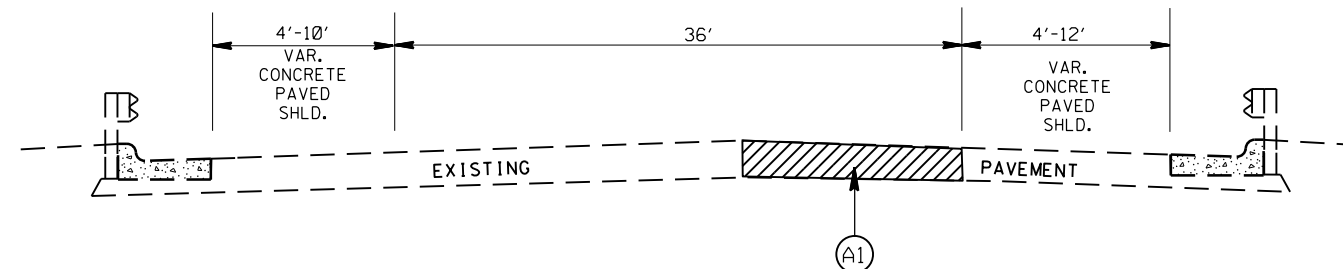
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

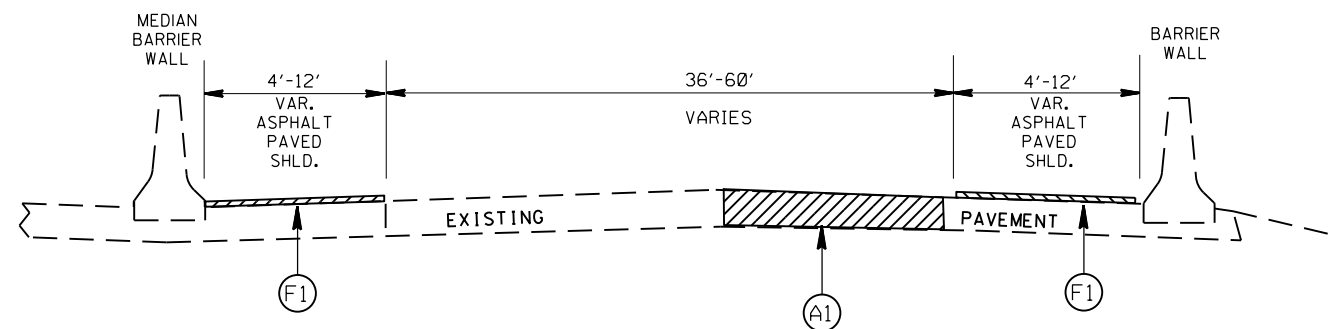
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	9	
WBS NO.	52024.3.3		



TYPICAL SECTION NO.12



TYPICAL SECTION NO.13



TYPICAL SECTION NO.14

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

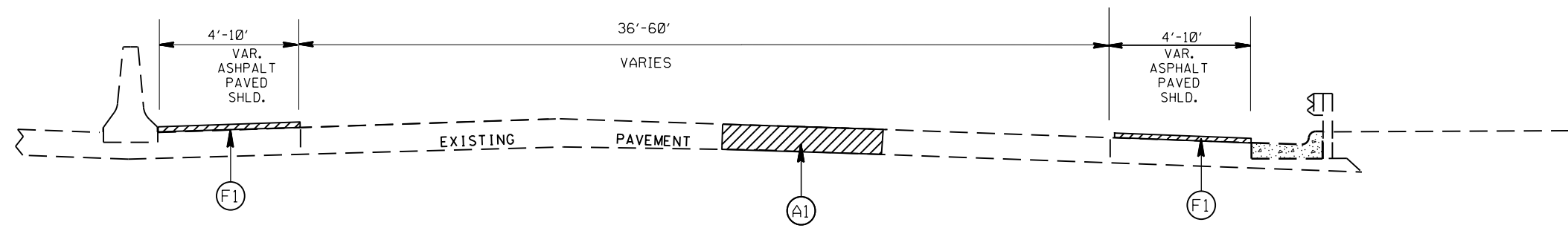
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE -NA-
DATE 12/16
DWG. BY TJP
DESIGN BY TJP
APPROVED WAT

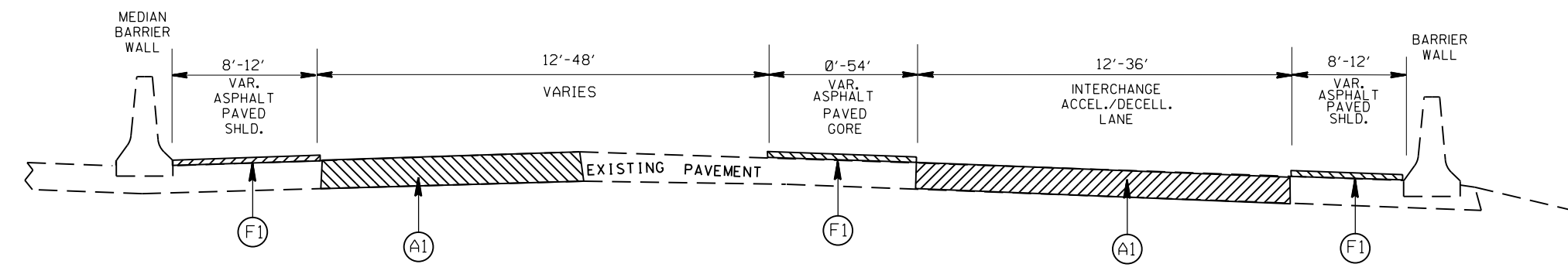


REVISIONS

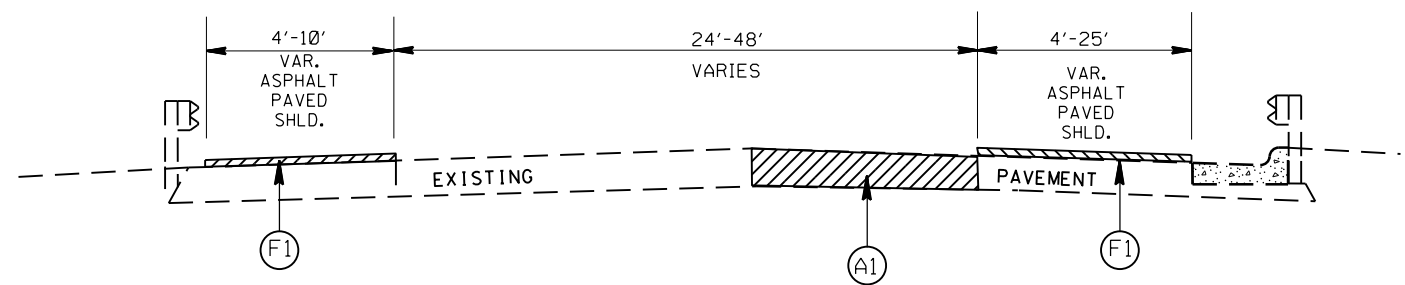
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	10	
WBS NO.		52024.3.3	



TYPICAL SECTION NO.15



TYPICAL SECTION NO.16



TYPICAL SECTION NO.17

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

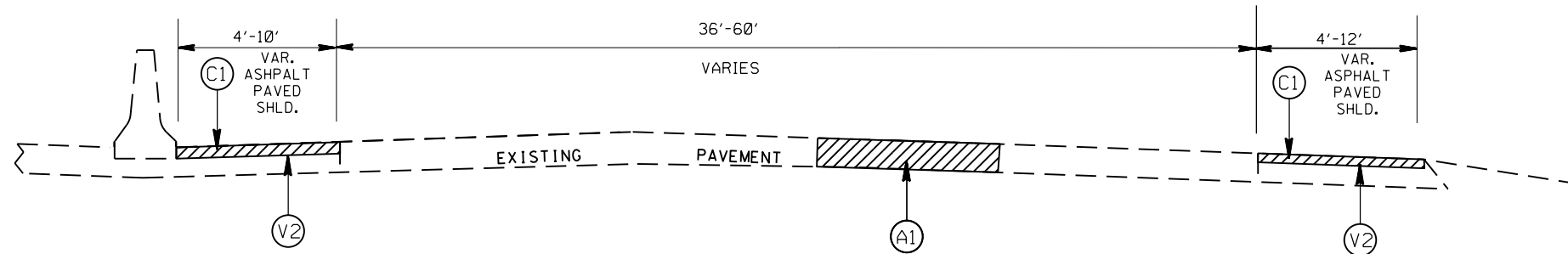
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-
DATE	12/16
DWG. BY	TJP
DESIGN BY	TJP
APPROVED	WAT

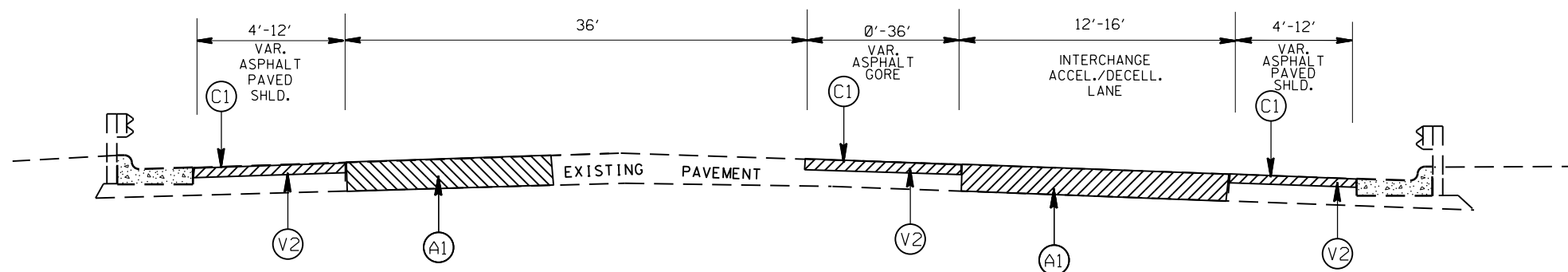


REVISIONS	

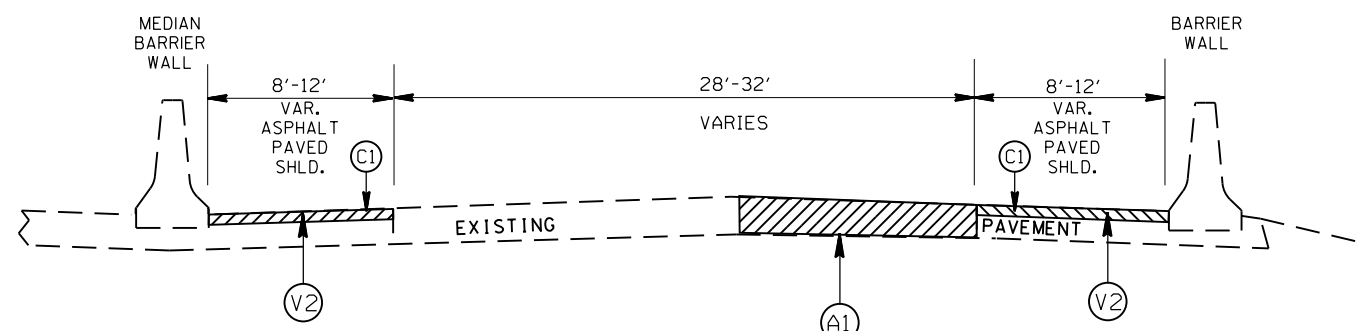
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
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WBS NO.	52024.3.3		



TYPICAL SECTION NO.18



TYPICAL SECTION NO.19



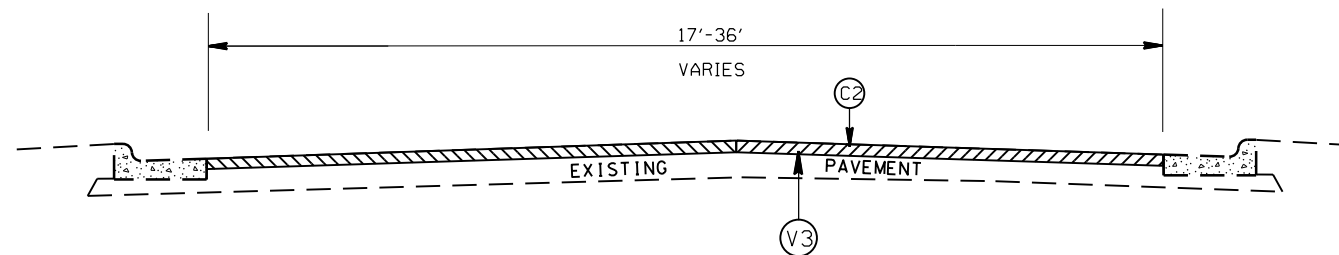
TYPICAL SECTION NO.20
150' WEST OF BRIDGE MAP 11
CARSON BV EXIT

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

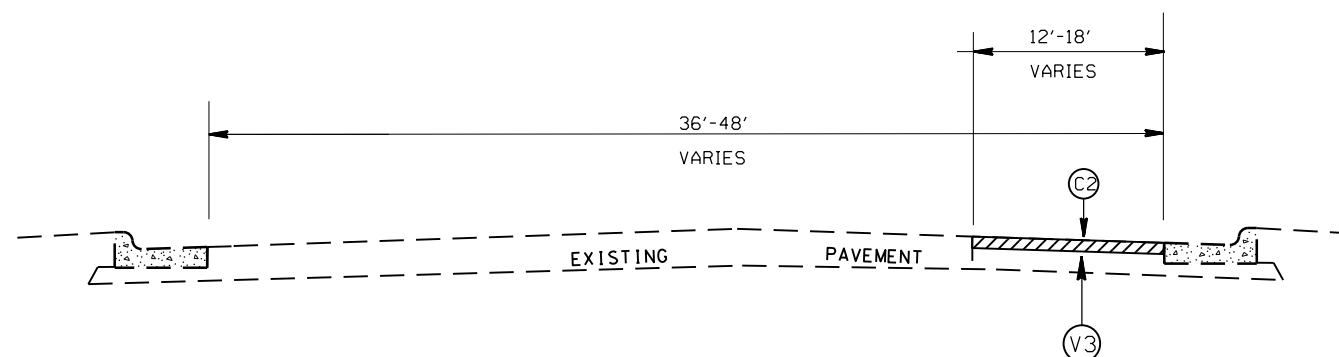
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

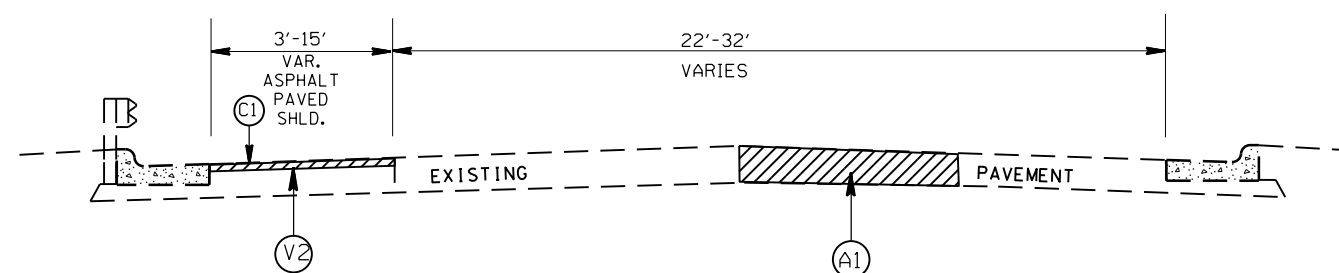
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	12	
WBS NO.	52024.3.3		



TYPICAL SECTION NO.21
PAVE TOP OF RAMP MAP 15
I-277 OUTER TO COLLEGE ST AND
MAP 22 4TH ST TO I-277 INNER



TYPICAL SECTION NO.22
PAVE TOP OF RAMP MAP 15
I-277 OUTER TO COLLEGE ST

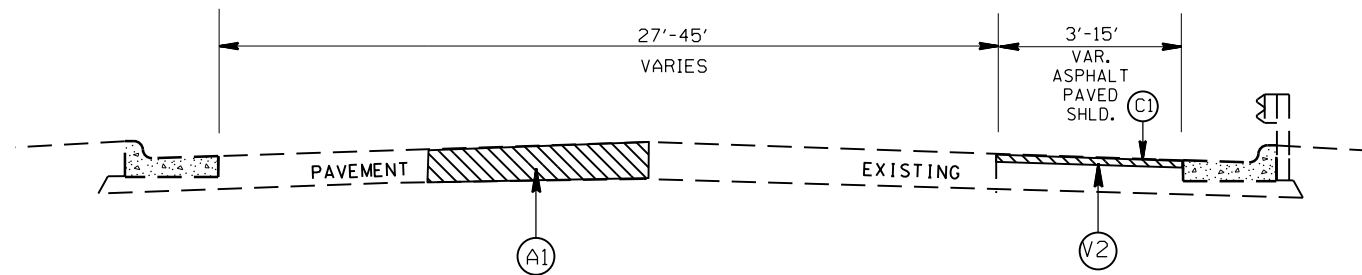


TYPICAL SECTION NO.23

I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

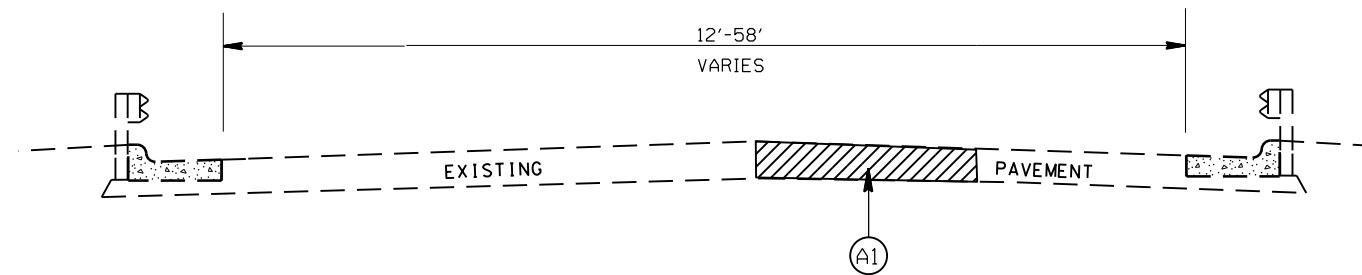
SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	13	
WBS NO.		52024.3.3	



TYPICAL SECTION NO.24

TYPICAL SECTIONS 25-26 ARE DELETED.



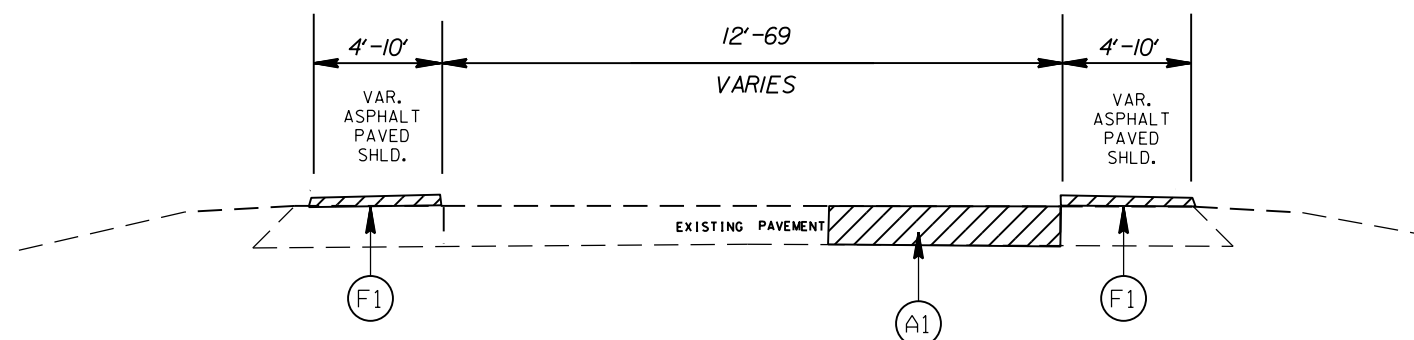
TYPICAL SECTION NO.27

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

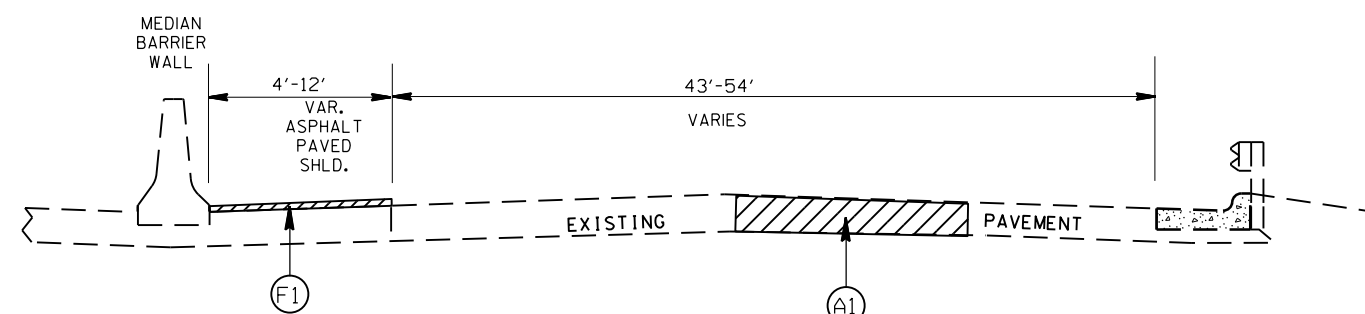
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

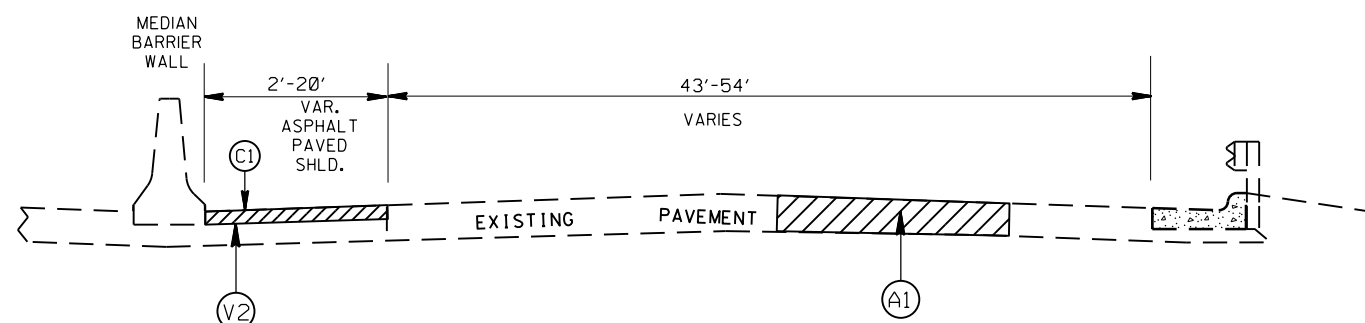
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WBS NO.	52024.3.3		



TYPICAL SECTION NO. 28



TYPICAL SECTION NO. 29



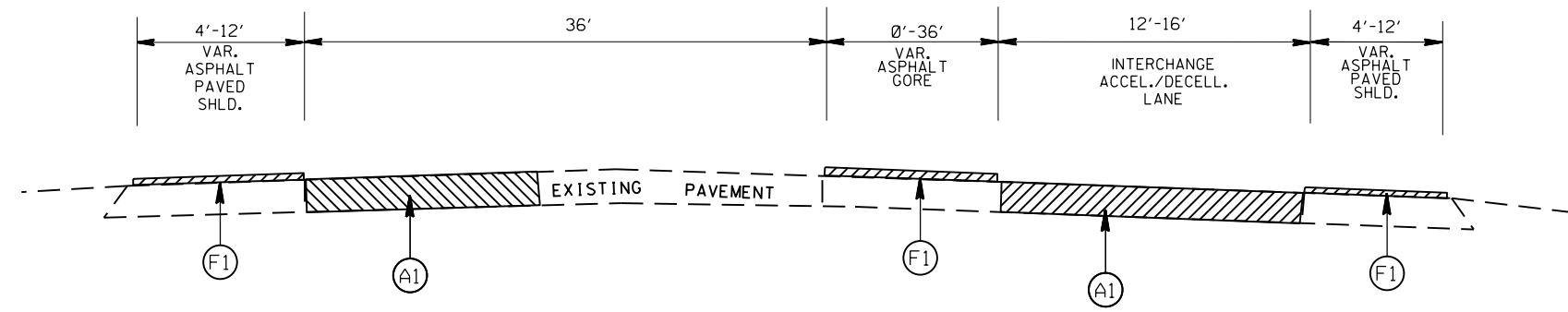
TYPICAL SECTION NO. 30

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

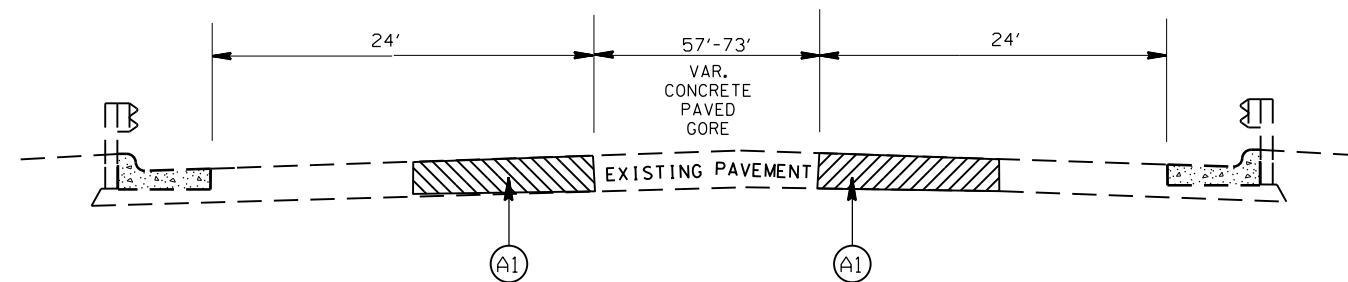
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

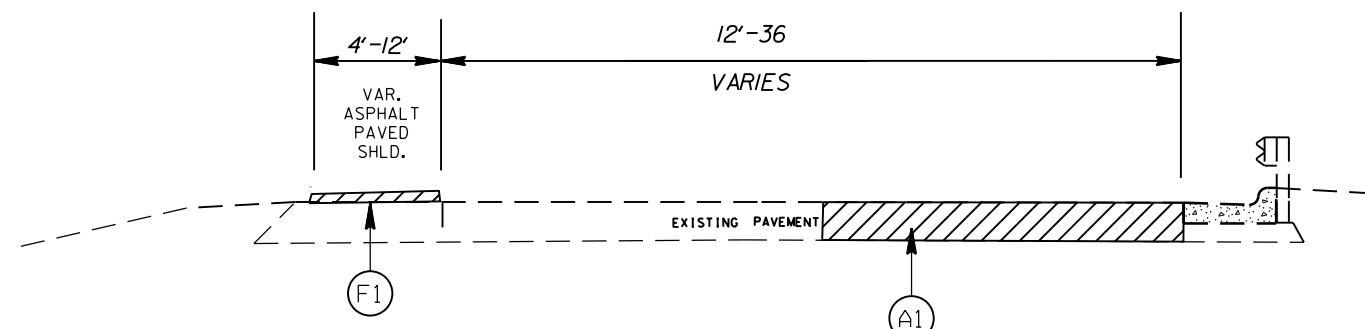
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	15	
WBS NO.	52024.3.3		



TYPICAL SECTION NO.31



TYPICAL SECTION NO.32



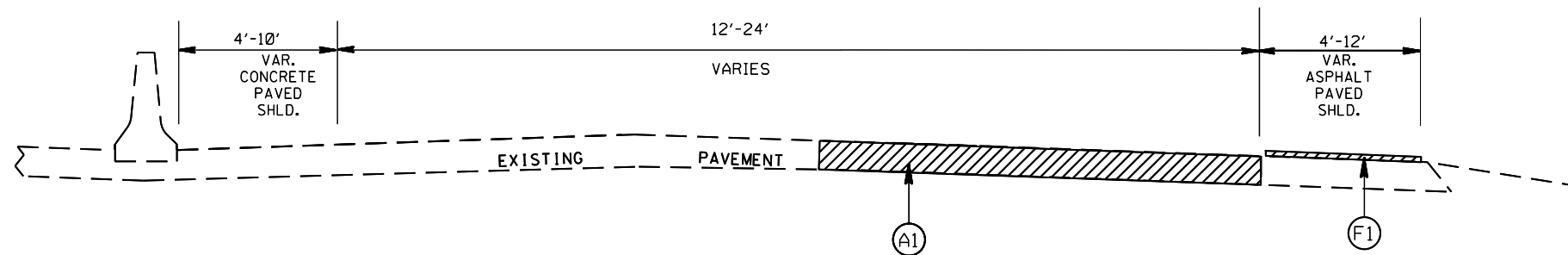
TYPICAL SECTION NO. 33

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

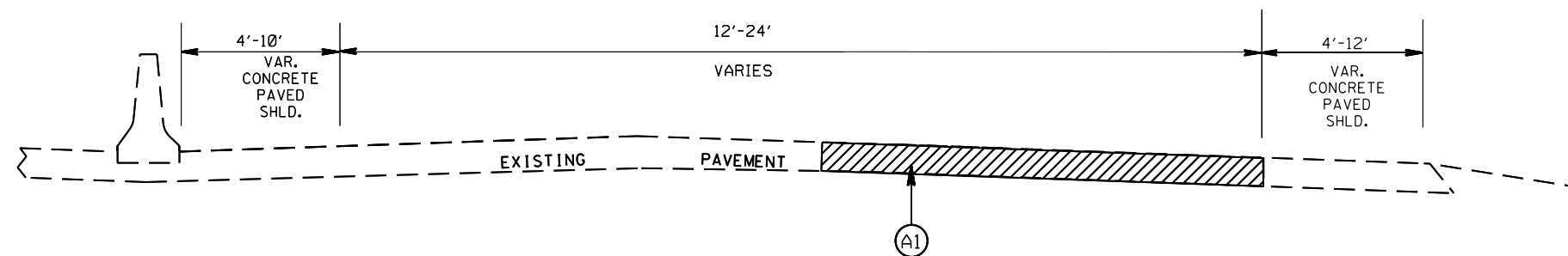
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

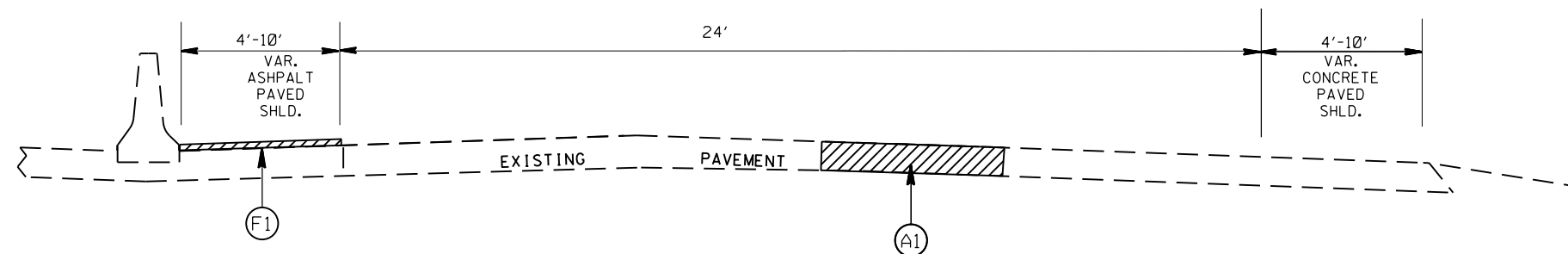
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	16	
WBS NO.	52024.3.3		



TYPICAL SECTION NO.34



TYPICAL SECTION NO.35



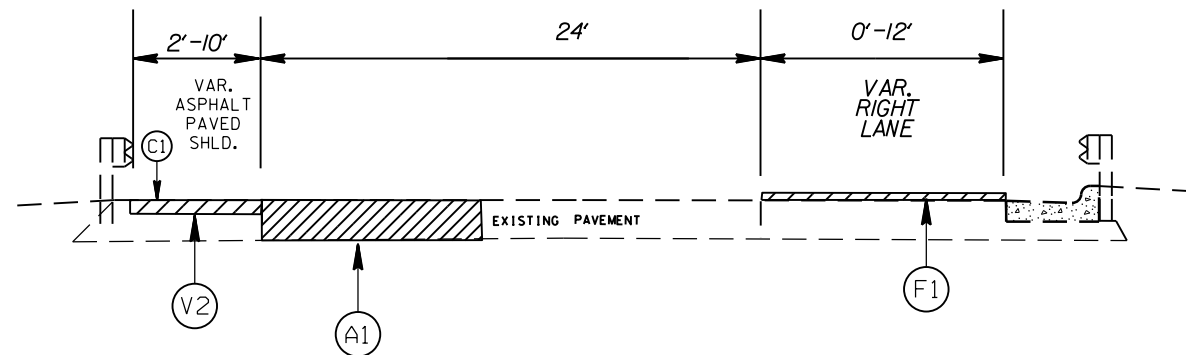
TYPICAL SECTION NO.36

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

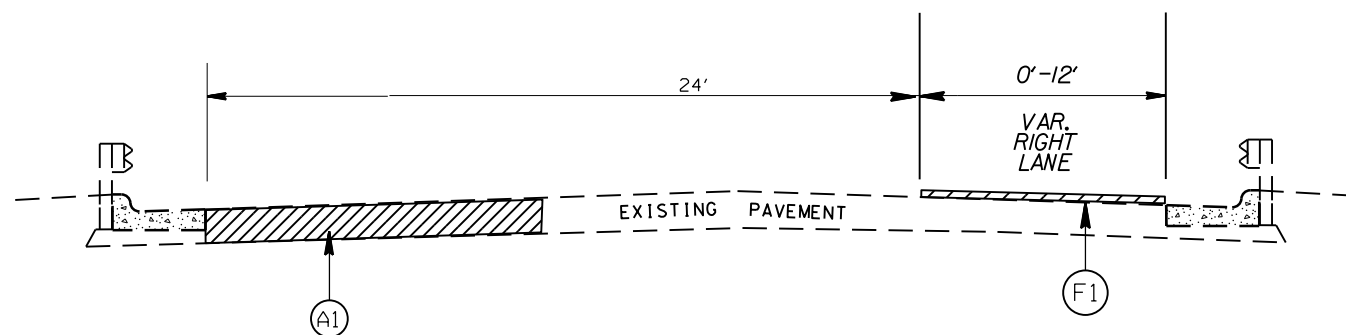
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS
DATE	12/16		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

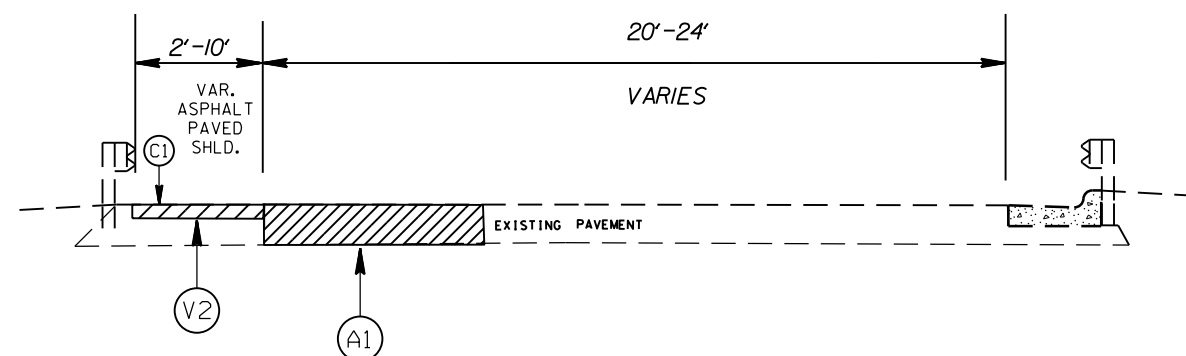
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	17	
WBS NO.		52024.3.3	



TYPICAL SECTION NO. 37



TYPICAL SECTION NO. 38



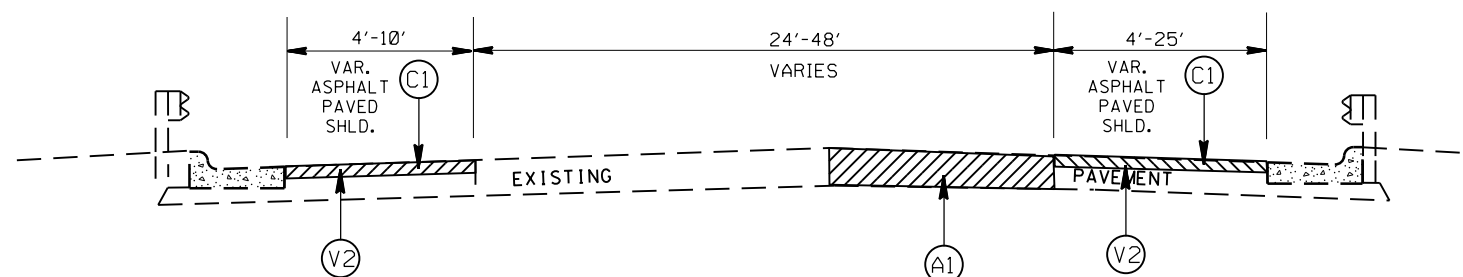
TYPICAL SECTION NO. 39

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

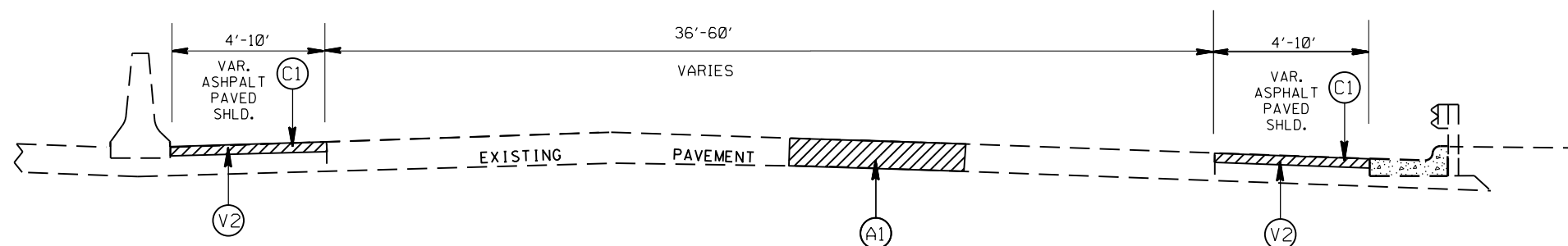
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-		REVISIONS	
DATE	12/16			
DWG. BY	TJP			
DESIGN BY	TJP			
APPROVED	WAT			

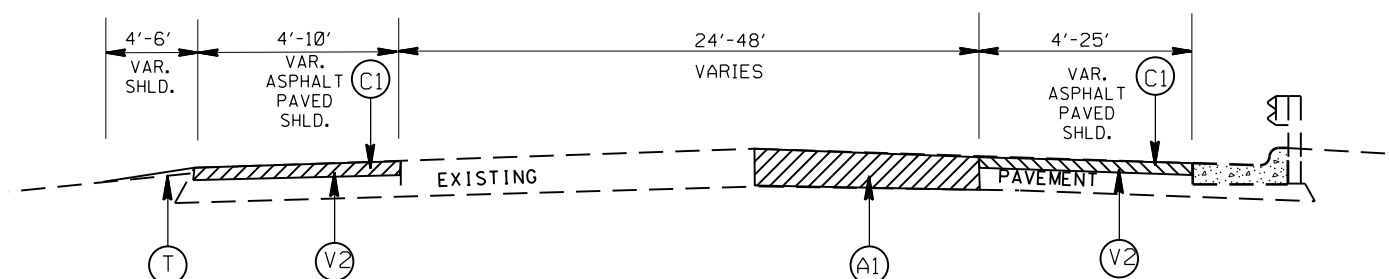
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	18	
WBS NO.	52024.3.3		



TYPICAL SECTION NO.40



TYPICAL SECTION NO.41



TYPICAL SECTION NO.42

PAVEMENT SCHEDULE	
A1	PROP. APPROX. 11" PORTLAND CEMENT CONCRETE PAVEMENT REPAIR (WITH DOWELS) AT VARIABLE LOCATIONS AS DETERMINED BY THE ENGINEER
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 3.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF 2 LAYERS
F1	FOG SEAL
V2	MILLING 2.0" DEPTH
V3	MILLING 1.5" DEPTH
V4	MILLING, 3.0" DEPTH
T	SHOULDER RECONSTRUCTION
M	MILLED RUMBLE STRIPS

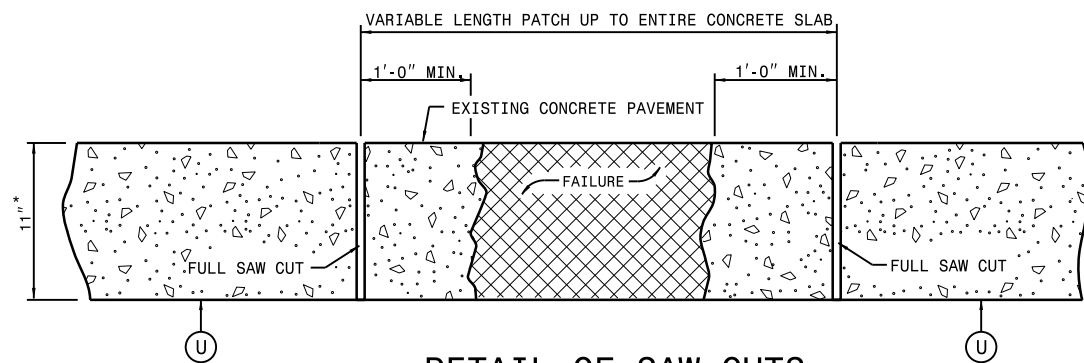
I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

SCALE	-NA-
DATE	12/16
DWG. BY	TJP
DESIGN BY	TJP
APPROVED	WAT

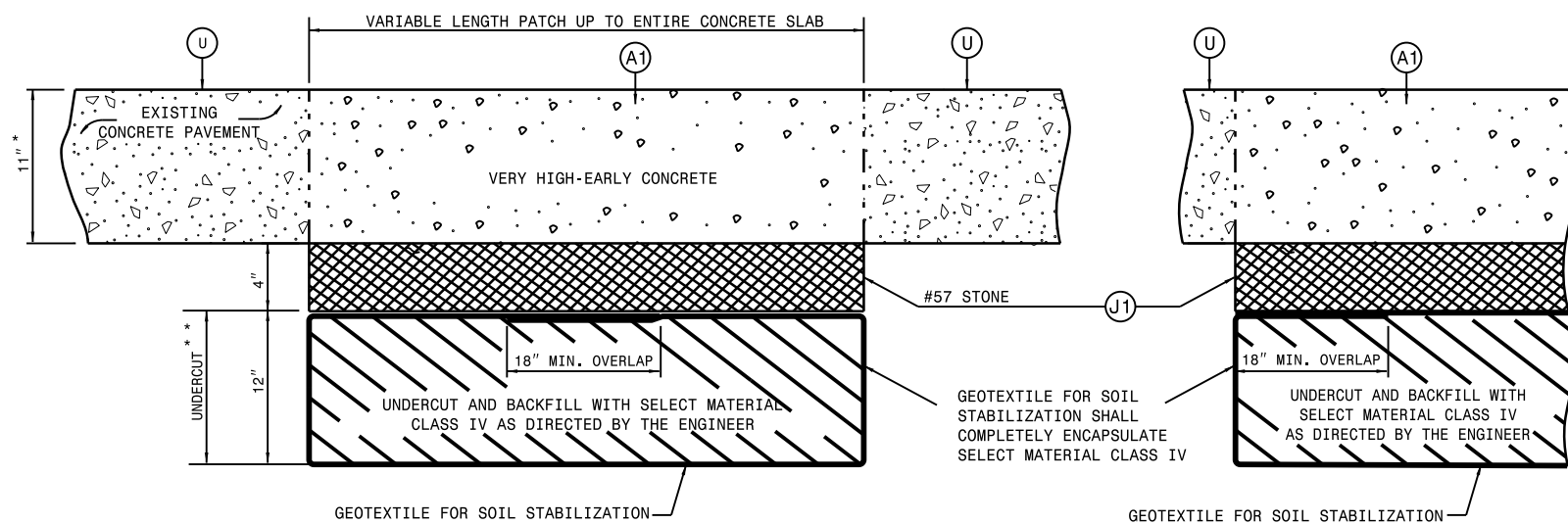


REVISIONS

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	I-5746A/I-5746B	19	
WBS NO.		52024.3.3	



DETAIL OF SAW CUTS



DETAIL OF CONCRETE PAVEMENT REPAIR

- * DIMENSIONS ARE APPROXIMATE AND SHOULD BE FIELD VERIFIED
- ** UNDERCUT REQUIRED IN AREAS AS DIRECTED BY THE ENGINEER

PAVEMENT SCHEDULE	
A1	11" VERY HIGH-EARLY CONCRETE
J1	#57 STONE
U	EXISTING PAVEMENT

Refer to the North Carolina Department of Transportation
 "Partial and Full Depth Repair Manual" when Replacing Slabs
 and when Repairing Concrete Pavement.

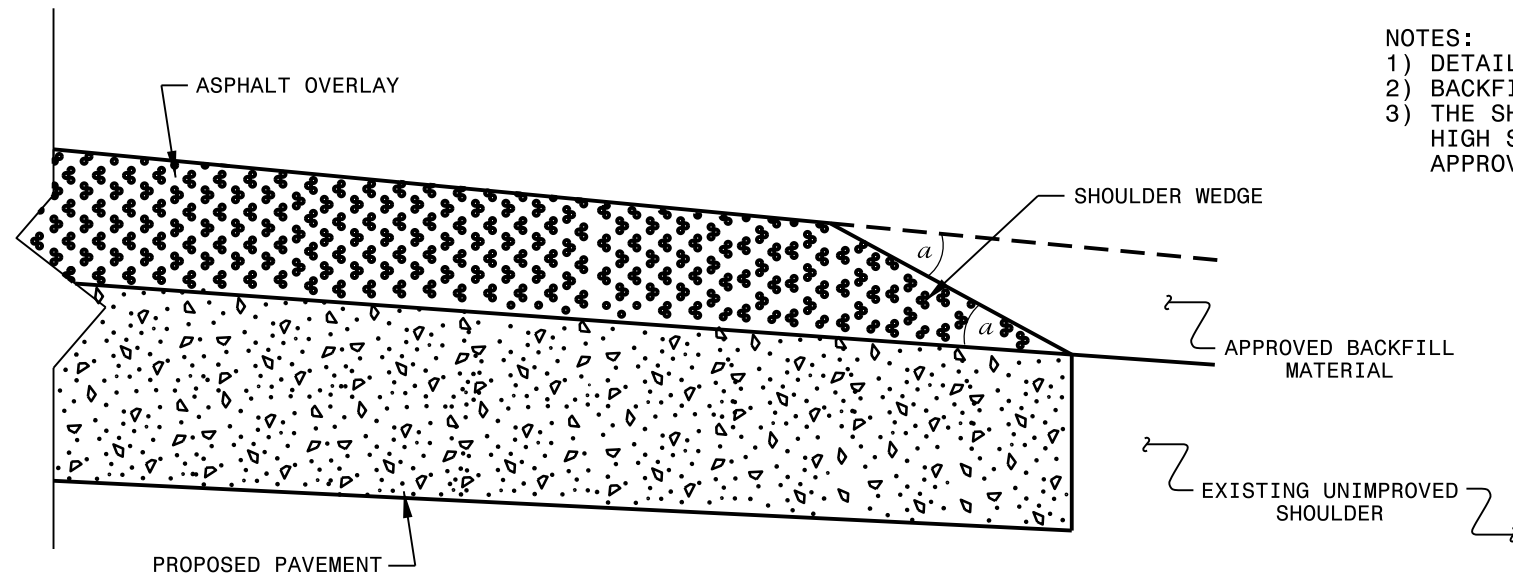
**DETAIL FOR REPAIR OF
 CONCRETE PAVEMENT**

I-5746A/I-5746B Interstate 277 Pavement Rehabilitation

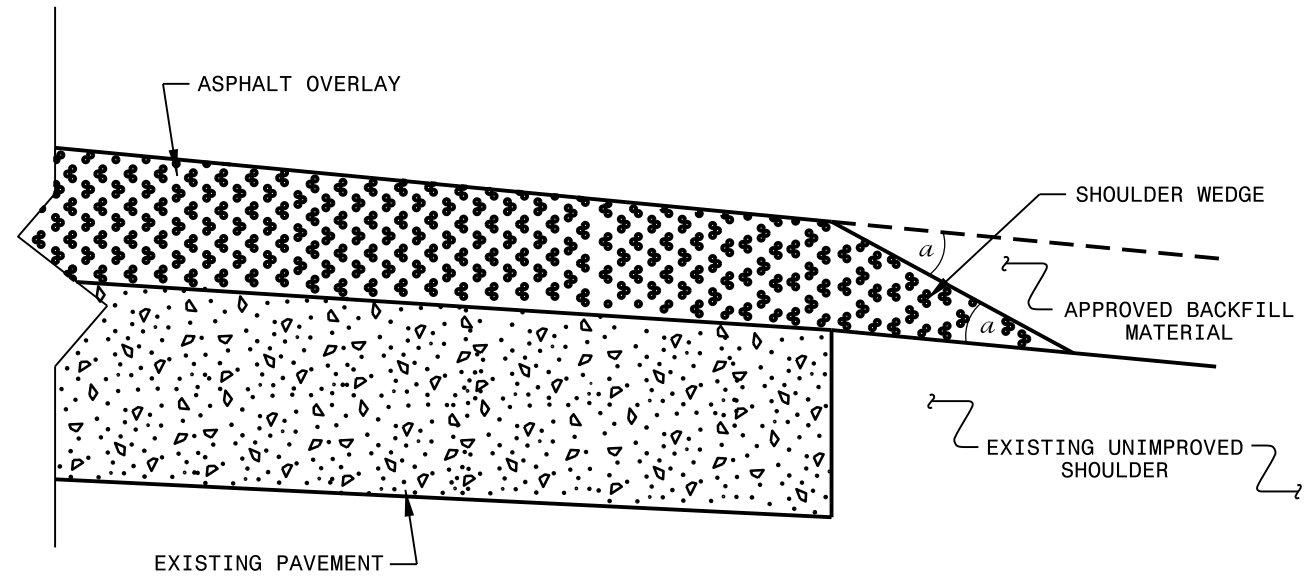
SCALE	-NA-		REVISIONS
DATE	3/17		
DWG. BY	TJP		
DESIGN BY	TJP		
APPROVED	WAT		

NOTES:

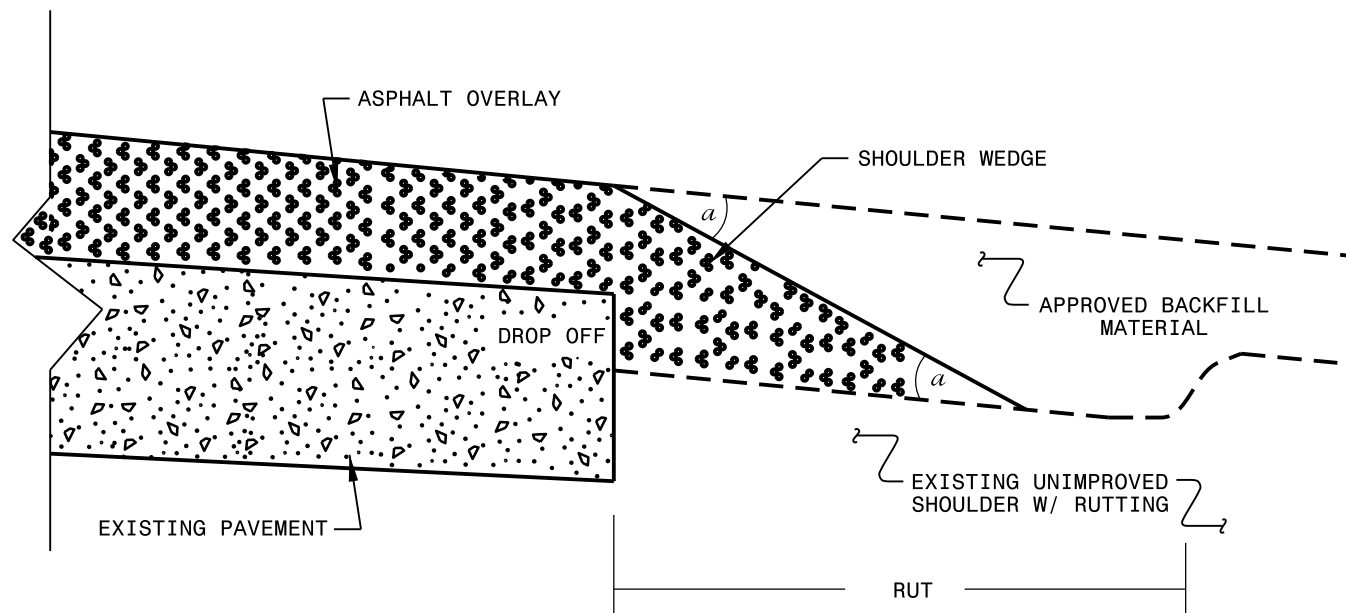
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 2/2/16
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn	

07-APR-2017 14:01
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