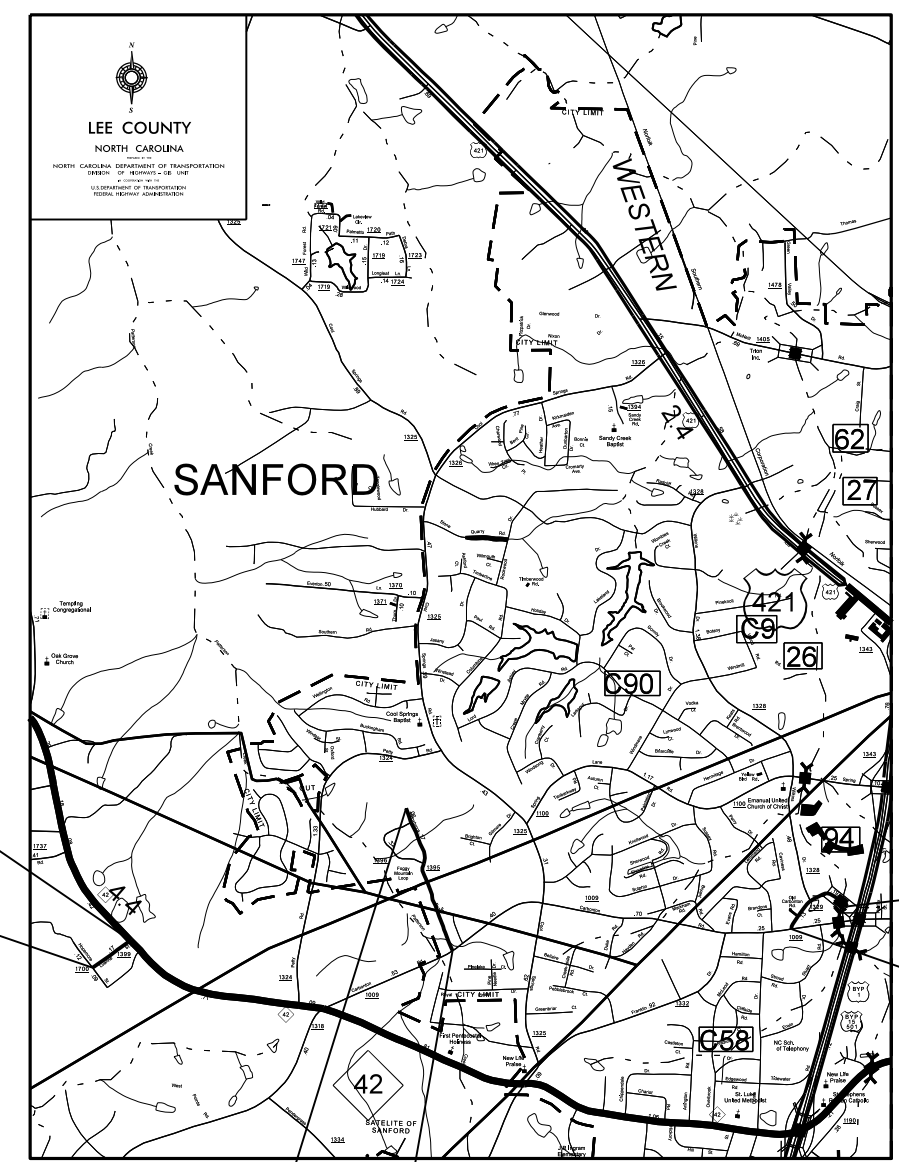
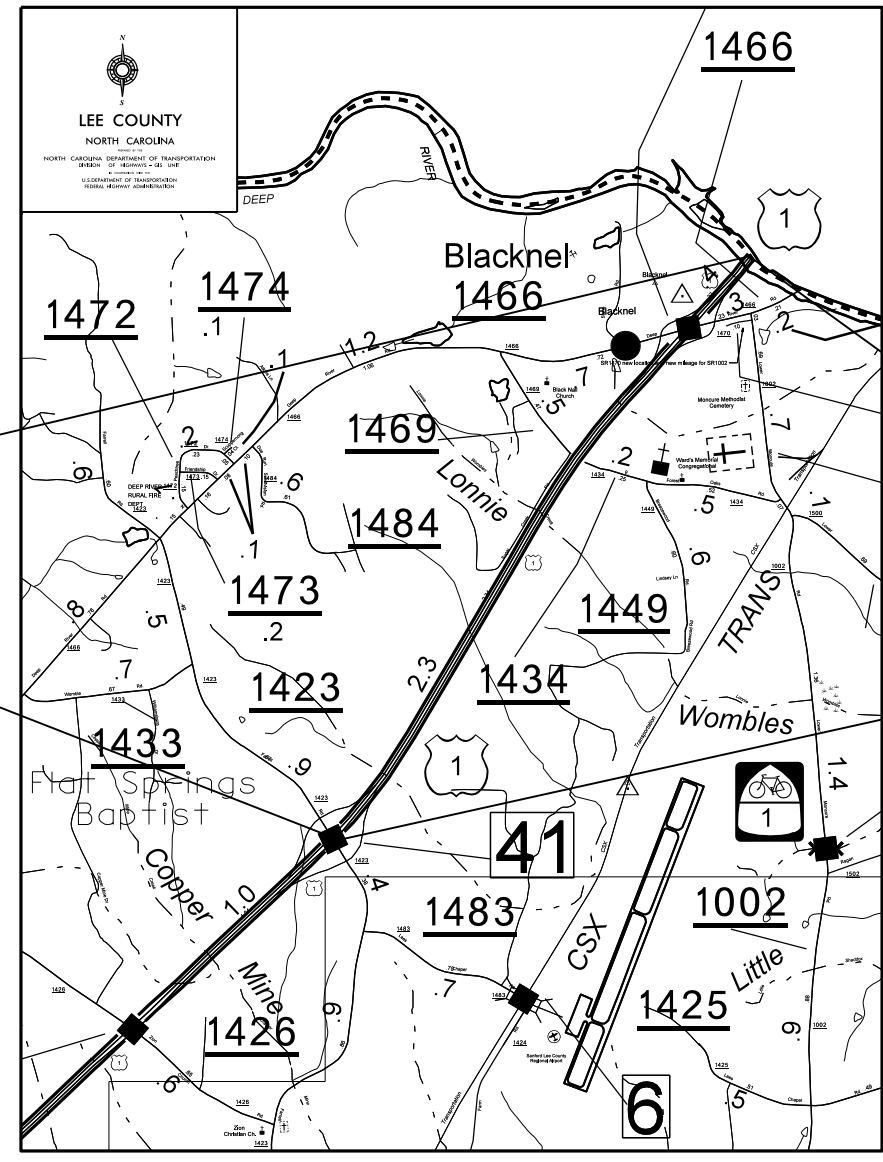


LEE COUNTY

5/28/09
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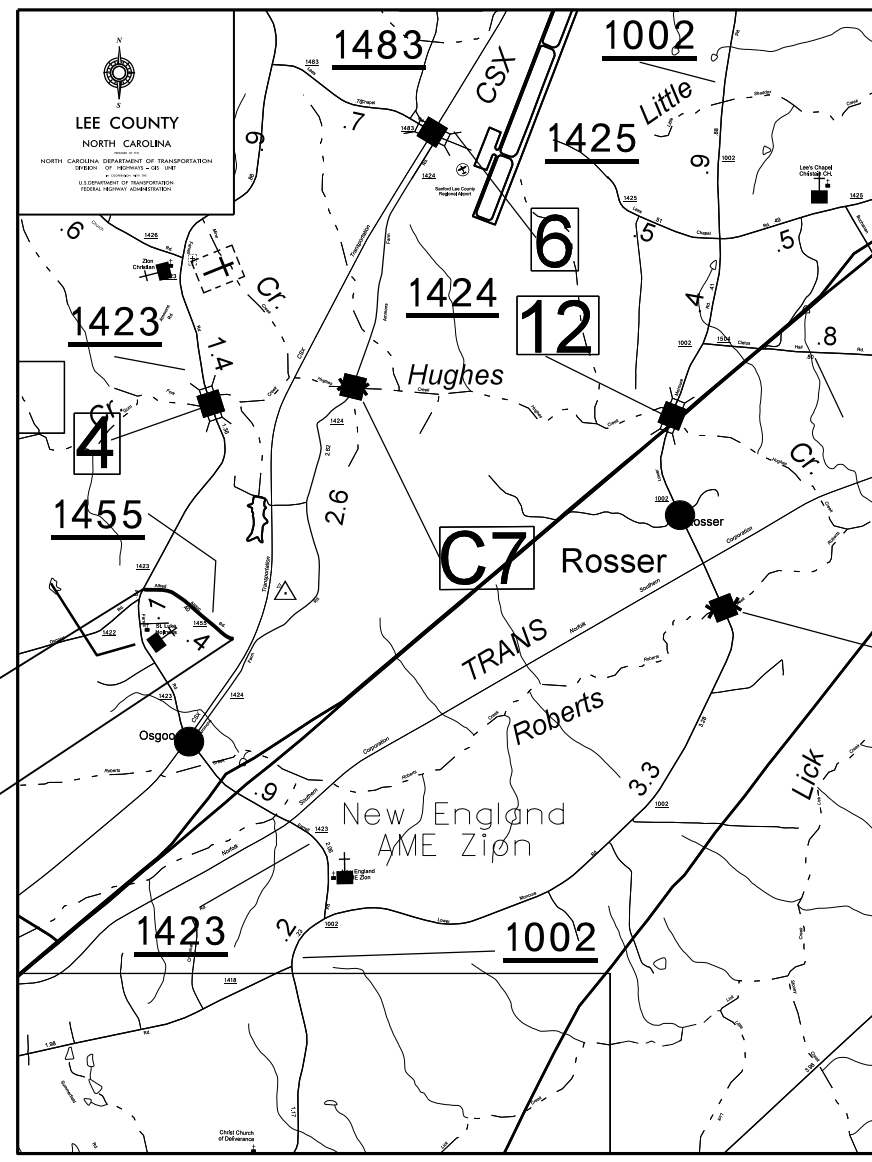
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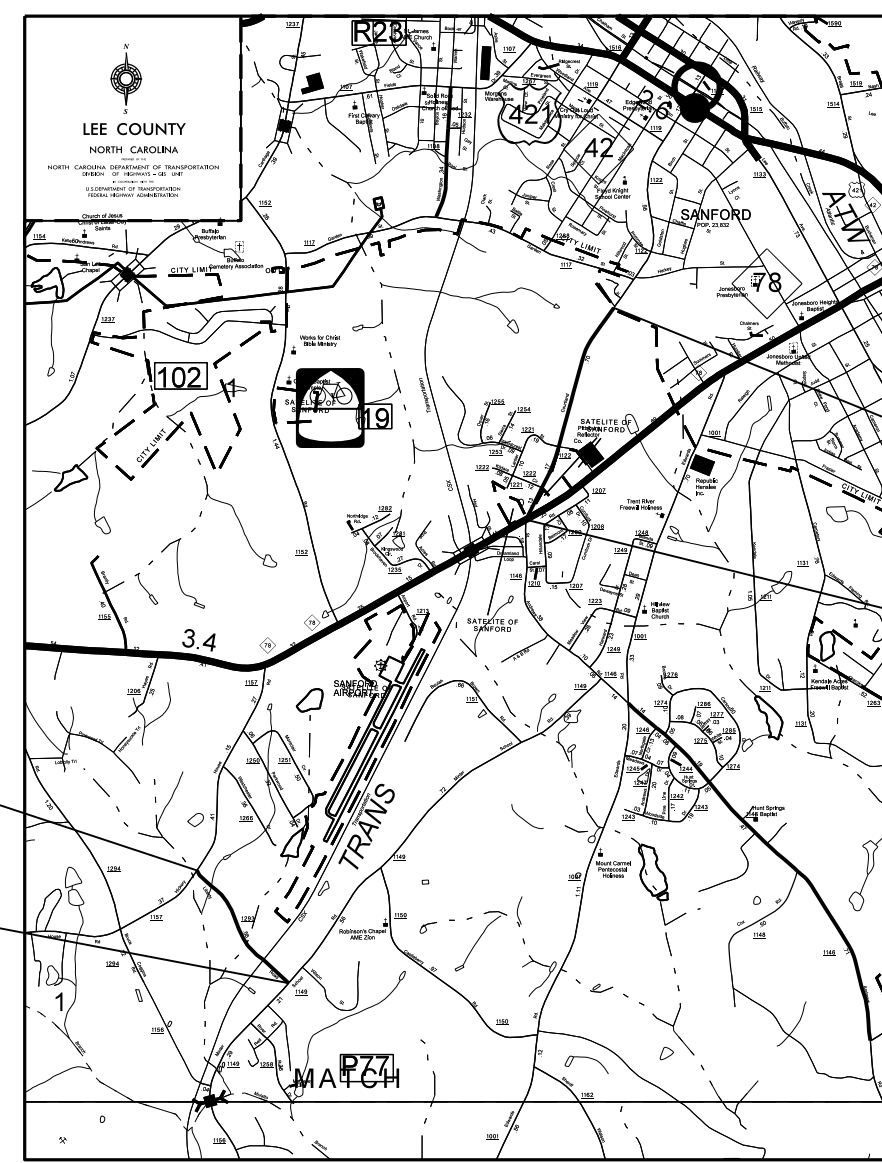
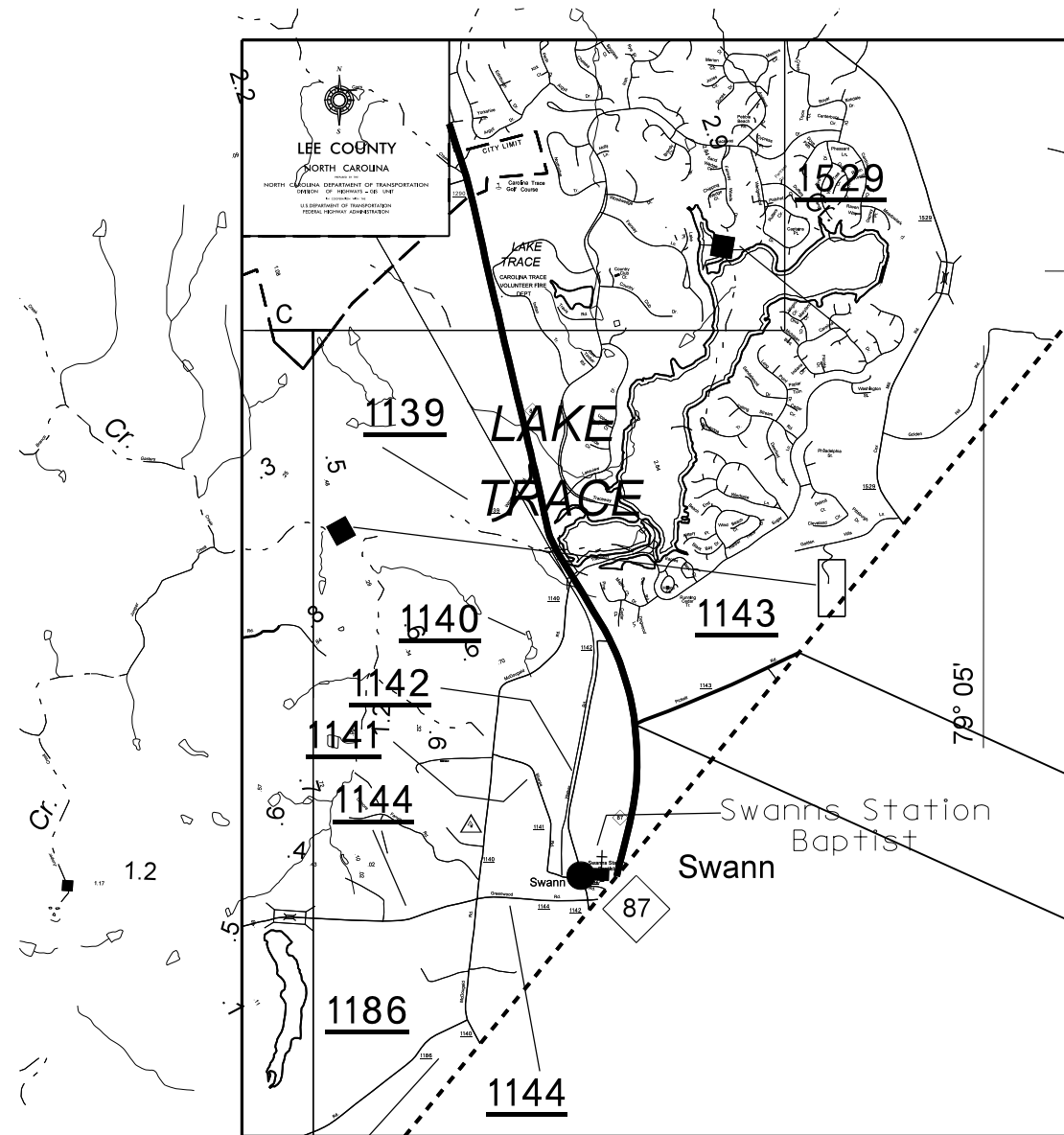
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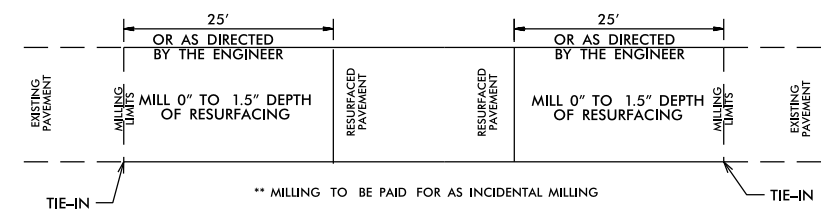
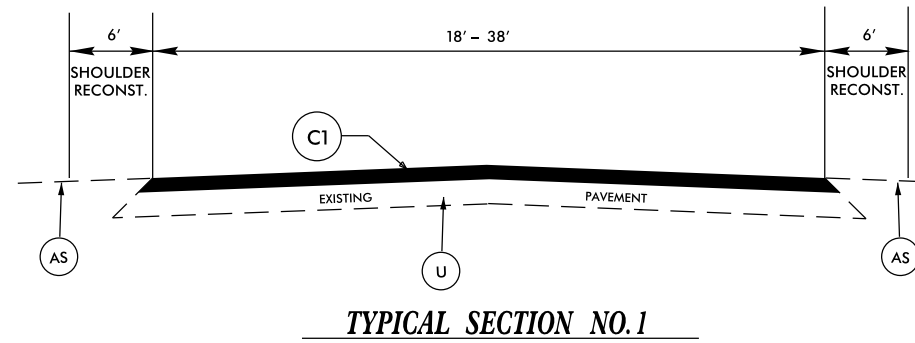
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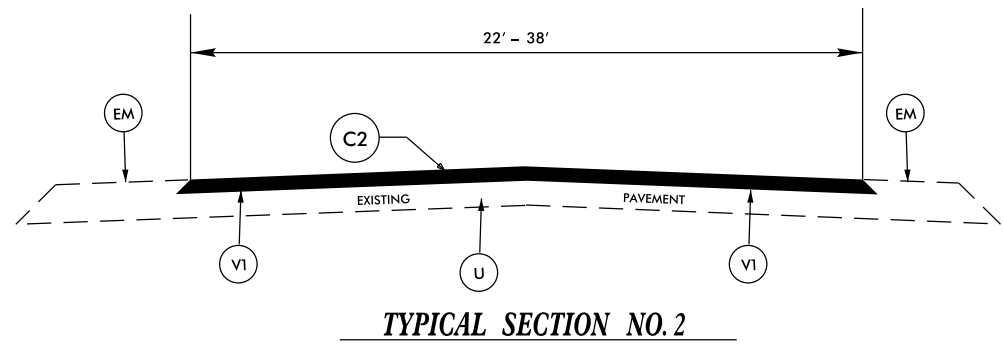
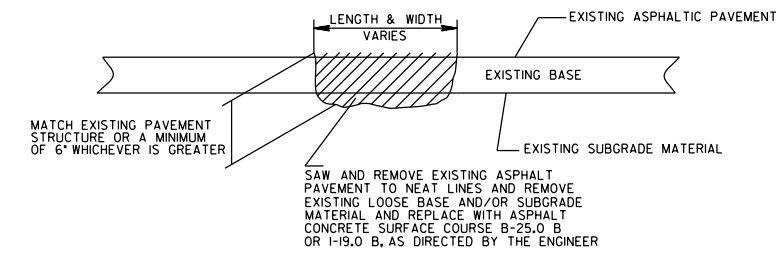
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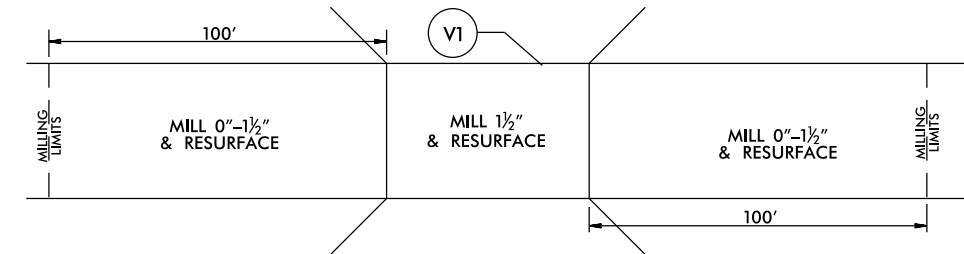
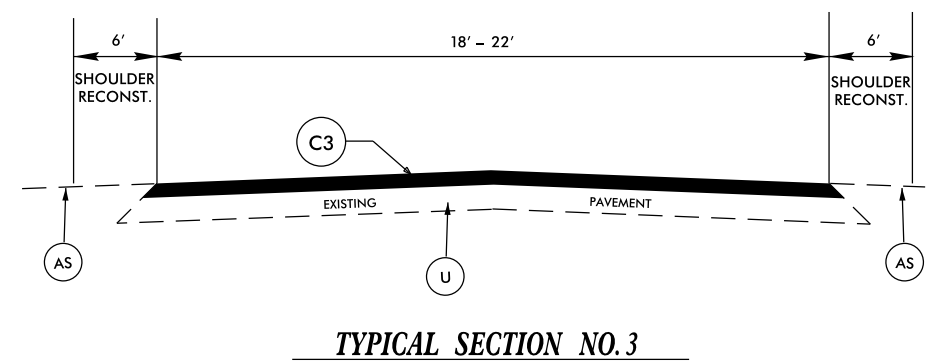


PAVEMENT TIE-IN DETAIL

DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING
DETAIL NO. 1

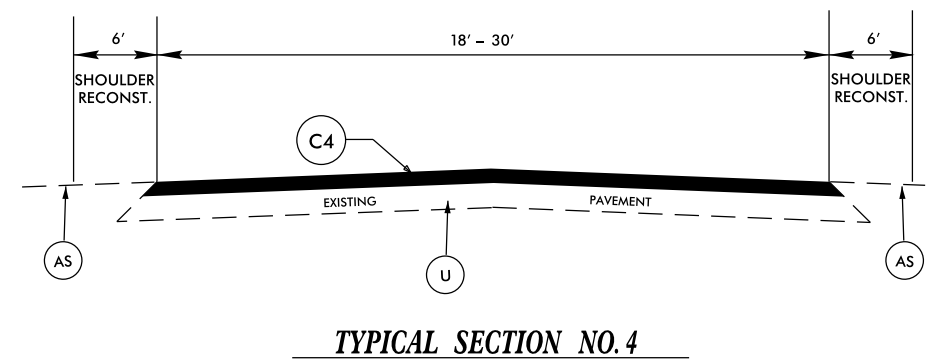


NOTE: MAP NO 3 AND NO 4 MILL UP TO RUMBLE STRIPS DO NOT MILL RUMBLE STRIPS



BRIDGE DRAWING FOR
MAP NO 9 SR 1329
(BRIDGE NO 94) &
MAP NO 16 SR 1303
(BRIDGE NO 47)

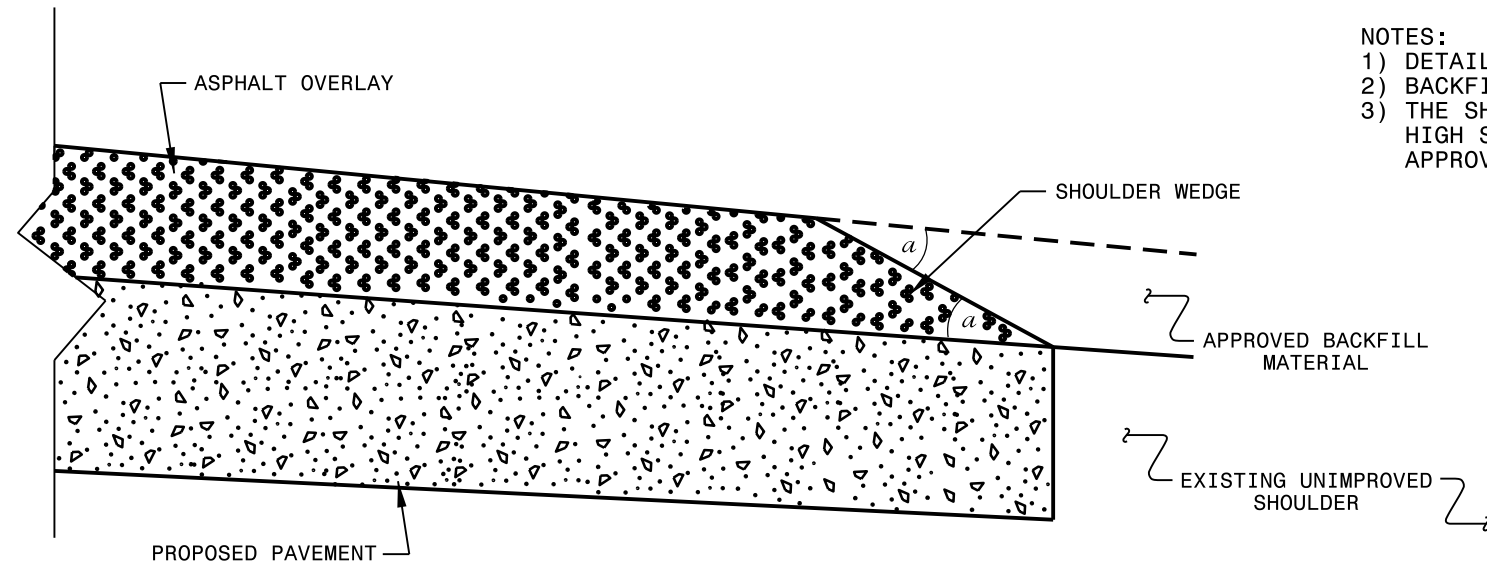
* TIE IN MILLING TO THE BRIDGE SHALL BE PAID FOR UNDER INCIDENTAL MILLING AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.



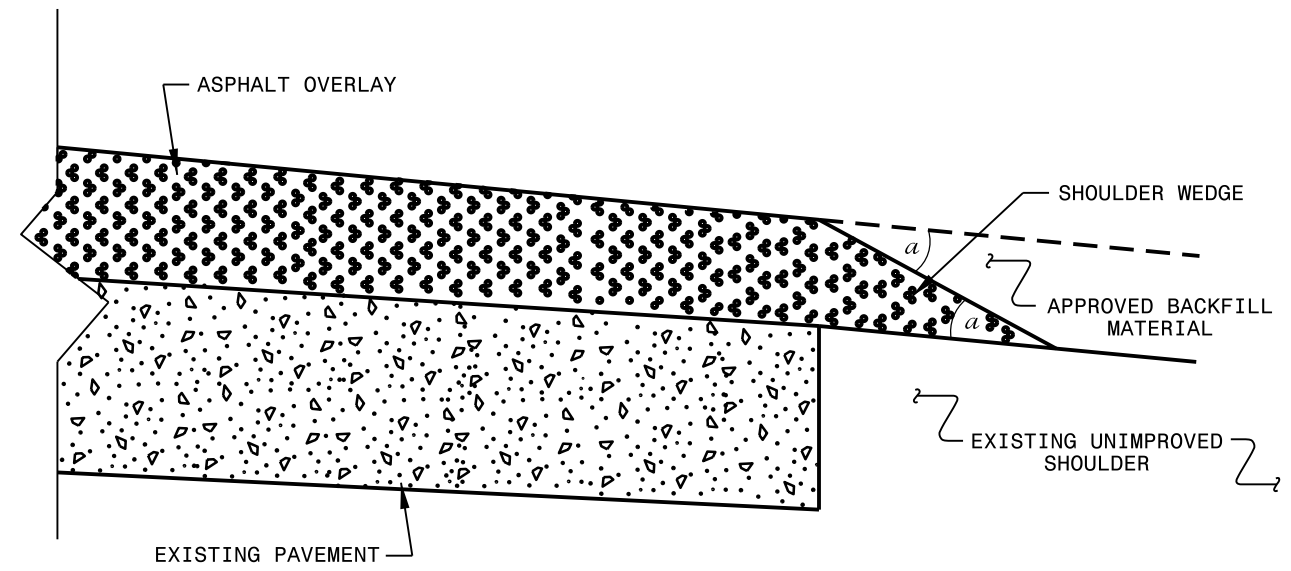
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C4	PROP. APPROX. 1.25" ASPHALT CONC. SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
EM	EXISTING MILLED RUMBLE STRIP
AS	AGGREGATE SHOULDER BORROW (ASB)
U	EXISTING PAVEMENT
VI	1.5" MILLING

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8\Lee (June 2017)\Lee.Co.2018FY.map - Copy.dgn

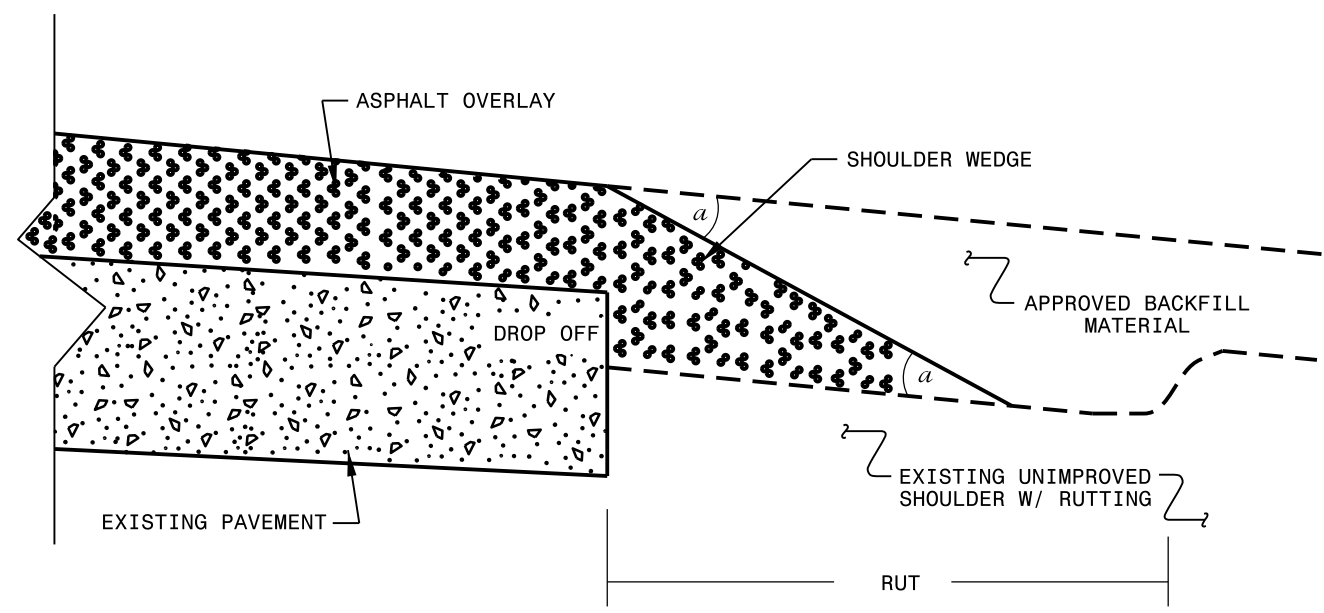
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 2/2/16
CHECKED BY:	DATE:
FILE SPEC: szusr/details/stand/shoulderwedgedetail.dgn	

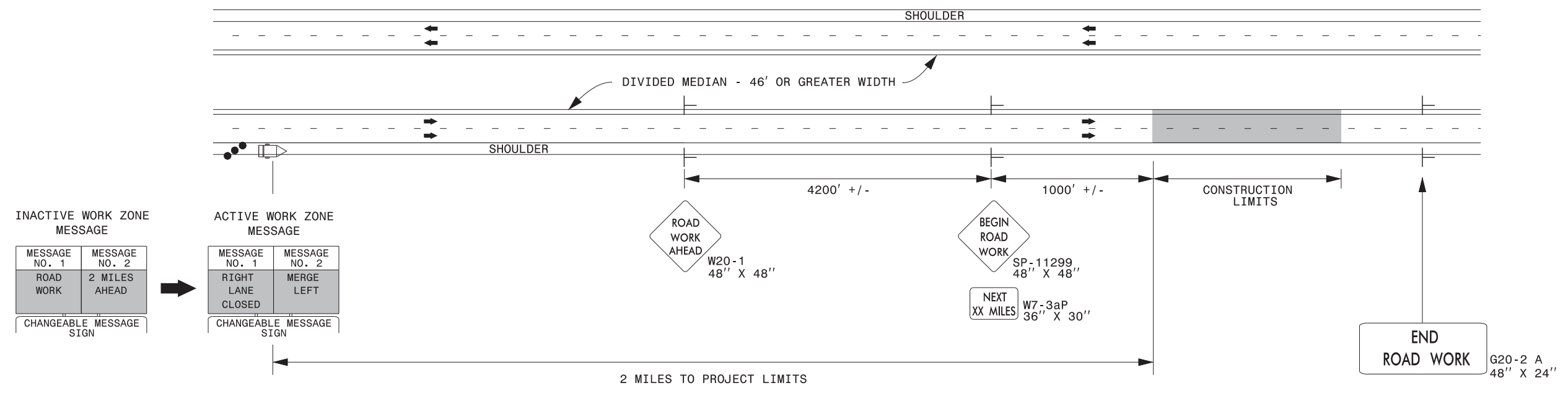
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PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.08.27.10531, 2017CPT.08.27.20531	9	

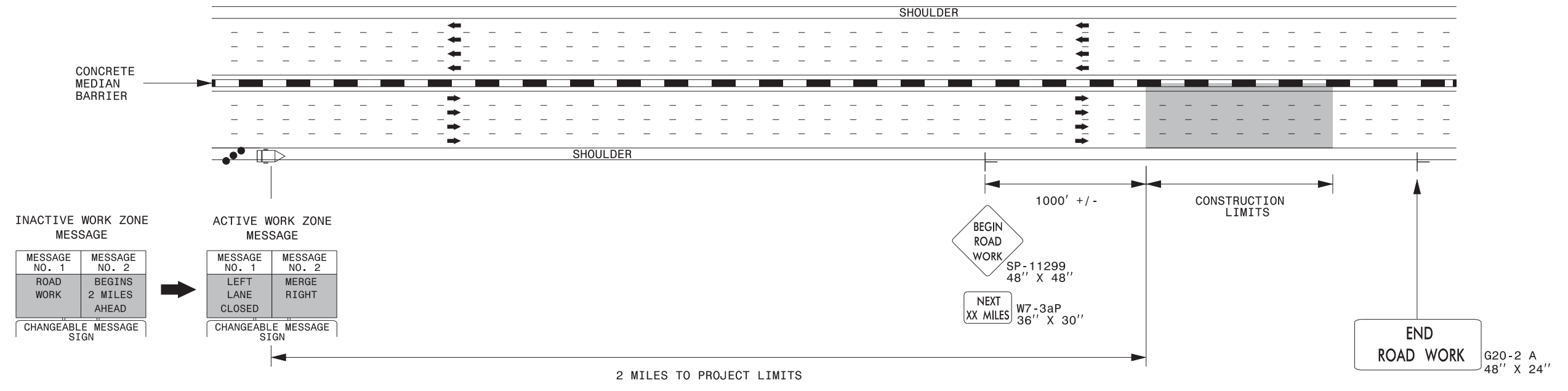
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	AGGREGATE SHOULDER BORROW TON	SHOULDER RECONSTRUCTION SMI	1.5" MILLING SY	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	SURFACE COURSE, SF9.5A TON	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ADJ. OF MANHOLES EA	ADJ. OF METER OR VALVE BOXES EA		
2017CPT.08.27.10531	Lee	1	NC 42	FROM SR 1579 (BROADWAY RD) TO SR 1535 (BERKE THOMAS RD)	1	2	2WU	NO	NO	1.987	18-38	558	3.97		440	3,290			197	2,140				
		2	US 1 BUS	FROM SR 1182 (CEDAR LN) TO MOORE COUNTY LINE	1	2	2WU	NO	NO	1.667	28	464	3.33		345	2,575			154	460				
		3	US 1 SB	FROM CHATHAM COUNTY LINE TO SR 1423 (FARRELL RD)	2	2	MD	NO	NO	2.655	22-38			38,940	350		4,575			270				
		4	US 1 NB	FROM SR 1423 (FARRELL RD) TO CHATHAM COUNTY LINE	2	2	MD	NO	NO	2.655	22-38				38,940	1,281		4,685			276			
TOTAL FOR PROJ NO. 2017CPT.08.27.10531										8.964		1,022	7.30	77,880	2,416	5,865	9,260		897	2,600				
2017CPT.08.27.20531	Lee	5	SR 1399 (LUANNE ST)	FROM NC 42 TO SR 1700 (HOPEMORE ST)	3	2	2WU	NO	NO	0.167	20	65	0.33		115			175	12	215				
		6	SR 1578 (RABBIT RIDGE RD)	FROM NC 42 TO DEAD END	3	2	2WU	NO	NO	0.19	18	57	0.38		50			175	12	1				
		7	SR 1122 (COURTLAND DR)	FROM NC 78 TO SANFORD CITY LIMIT	3	2	2WU	NO	NO	0.843	22	237	1.69		245			950	64	5				
		8	SR 1179 (ROCKY FORK CHURCH RD)	FROM SR 1144 (GREENWOOD RD) TO US 1	1	2	2WU	NO	NO	1.754	25	492	3.51		350	2,460				148	890			
		9	SR 1329 (OLD CARBANTON RD)	FROM CONST JOINT @ SR 1009 (CARBANTON RD) TO DEAD END	1	2	2WU	NO	NO	0.303	18	85	0.61	200	800	335				20	251			
		10	SR 1395 (STONEBROOK DR)	FROM SR 1009 (CARBANTON RD) TO DEAD END	3	2	2WU	NO	NO	0.577	20	160	1.16		225			660	44	290			2	
		11	SR 1540 (DOYLE COX RD)	FROM SR 1538 (BUCKHORN RD) TO DEAD END	1	2	2WU	NO	NO	0.75	20	208	1.50		60	785				47	230			
		12	SR 1143 (PICKETT RD)	FROM NC 87 TO HARNETT CO LINE	1	2	2WU	NO	NO	0.703	18	199	1.41		167	660				40	4			
		13	SR 1166 (NICHOLSON RD)	FROM SR 1175 (PILSON RD) TO SR 1001 (EDWARDS RD)	1	2	2WU	NO	NO	1.88	22	530	3.76		1,105	2,325				139	205			
		14	SR 1170 (BLACKS CHAPEL RD)	FROM SR 1166 (NICHOLSON RD) TO SR 1162 (SHERIFF WATSON RD)	1	2	2WU	NO	NO	0.904	22	256	1.81		615	1,140				68	575			
		15	SR 1293 (LIBERTY RD)	FROM SR 1157 (HICKORY HOUSE RD) TO SR 1149 (MINTER SCHOOL RD)	4	2	2WU	NO	NO	0.535	30	152	1.07		500			685	46	3				
		16	SR 1303 (CENTER CHURCH RD)	FROM SR 1302 (ARTHUR MADDOX RD) TO SR 1311 (BIG SPRINGS RD)	1	2	2WU	NO	NO	1.509	22	426	2.92	250	1,230	1,845				111	15			
		17	SR 1455 (ALFRED ALSTON RD)	FROM SR 1423 (FARRELL RD) TO DEAD END	4	2	2WU	NO	NO	0.35	18	104	0.70		170			270	18	200				
		18	SR 1531 (SWANNS STATION RD)	FROM US 421 TO SR 1579 (MAIN ST)	4	2	2WU	NO	NO	1.33	22	369	2.66		735			1,330	89	710	1	2		
		TOTAL FOR PROJ NO. 2017CPT.08.27.20531										11.795		3,340	23.51	450	6,367	9,550		4,245	858	3,594	1	4
		GRAND TOTAL										20.759		4,362	30.81	78,330	8,783	15,415	9,260	4,245	1,755	6,194	1	4

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER

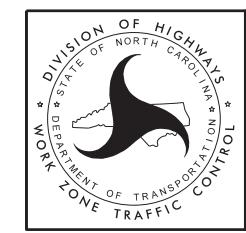


NOTES:

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

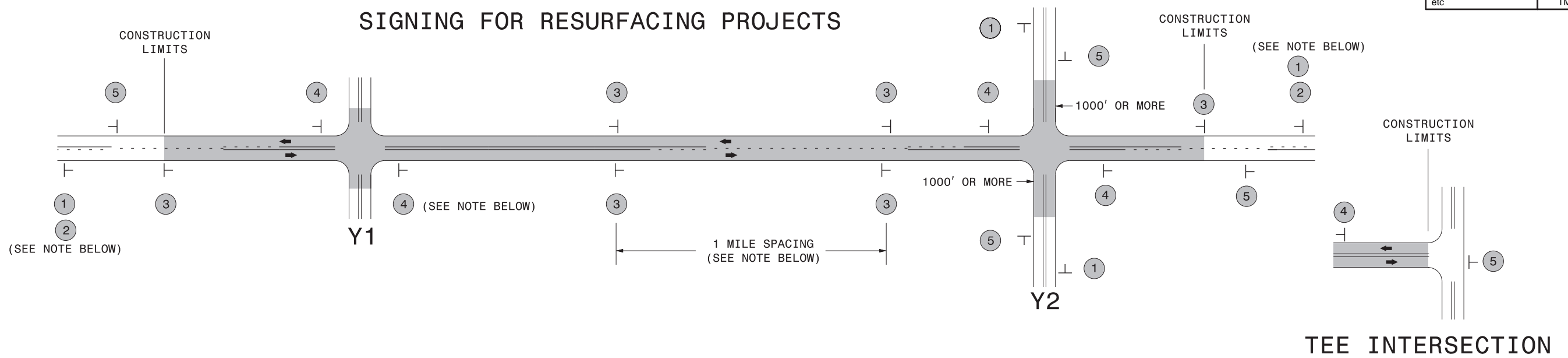
- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



**RESURFACING ADVANCE
WARNING SIGNS FOR
HIGH SPEED FACILITIES
≥ 60 MPH**

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SIGNING FOR RESURFACING PROJECTS



LEGEND	
—	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" </div> <div style="text-align: center;"> W20-7 A 48" X 48" </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 W7-3aP 24" X 18"	<p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	
	3	 SP 13107 48" X 48"	<ul style="list-style-type: none"> - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. 	
	4	 SP 13106 48" X 48"	<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE. 	
5	 G20-2 A 48" X 24"	<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>		

**RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS**