

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- A) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- B) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- D) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP, OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- E) DO NOT INSTALL MORE THAN 1 MILE OF LANE CLOSURE ON ANY ROAD MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- F) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

TRAFFIC PATTERN ALTERATIONS

- G) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- H) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- I) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.
 - PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.
- J) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.
 - COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.
- K) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

- L) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.
- M) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- N) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

LOCAL NOTES

LOCAL NOTE 1 - DETOUR RESURFACING REQUIREMENTS

BEFORE WORKING ON ANY ROAD, THE CONTRACTOR SHALL SUBMIT A WRITTEN CONSTRUCTION SEQUENCE FOR TRAFFIC CONTROL FOR ALL ROADS TO THE ENGINEER AT THE FIRST PRE-CONSTRUCTION MEETING FOR APPROVAL.

OBTAIN WRITTEN APPROVAL OF THE ENGINEER BEFORE WORKING IN MORE THAN ONE LOCATION.

CONTRACTOR SHALL MILL AND PAVE LANES IN AN ORDER SUCH THAT WATER SHALL NOT ACCUMULATE.

LOCAL NOTE 2 - PAVING LIFT REQUIREMENTS AND TIME LIMITATIONS FOR DETOUR RESURFACING.

BRING ALL NEWLY RESURFACED LANES TO THE SAME STATION AND ELEVATION WITHIN 72 HOURS. IF NOT BROUGHT UP TO THE SAME STATION AND ELEVATION WITHIN 72 HOURS, THE CONTRACTOR SHALL PLACE PORTABLE "UNEVEN PAVEMENT" SIGNS IN ADVANCE OF THE UNEVEN PAVEMENT AND SPACED EVERY 1/2 MILE ALONG THE SECTION OF UNEVEN PAVEMENT. ONCE MITIGATED, ALL PORTABLE "UNEVEN PAVEMENT" SIGNS SHALL BE REMOVED.

LOCAL NOTE 3 - ADVANCE WARNING SIGNS FOR DETOUR RESURFACING

INSTALL ADVANCE/GENERAL WARNING WORK ZONE SIGNS ACCORDING TO TMP-4 PRIOR TO BEGINNING OF WORK. INSTALL AND MAINTAIN SIGNING IN ACCORDANCE WITH THE ATTACHED DRAWINGS AND DIVISIONS 11 AND 12 OF THE 2012 STANDARD SPECIFICATIONS.

(A) SIGN INSTALLATION

ALL STATIONARY ADVANCE/GENERAL WARNING WORK ZONE SIGNS REQUIRE NOTIFICATION TO EXISTING UTILITY OWNERS PER ARTICLE 105-8 OF THE 2012 STANDARD SPECIFICATIONS AND SPECIAL PROVISION SP1 G115 WITHIN 3 TO 12 FULL WORKING DAYS PRIOR TO INSTALLATION.

INSTALL ALL ADVANCE/GENERAL WARNING WORK ZONE SIGNS BEFORE BEGINNING WORK ON A PARTICULAR ROAD. IF SIGNS ARE INSTALLED MORE THAN SEVEN (7) CALENDAR DAYS PRIOR TO THE BEGINNING OF WORK ON A PARTICULAR ROAD, COVER THE SIGNS UNTIL THE WORK BEGINS. INSTALL EACH WORK ZONE ADVANCE/GENERAL WARNING SIGN SEPARATELY AND NOT ON THE SAME POST OR STAND WITH ANY OTHER SIGN EXCEPT WHERE AN ADVISORY SPEED PLATE OR DIRECTIONAL ARROW IS USED.

ALL STATIONARY SIGNING IS TO BE INSTALLED AS SHOWN ON THE DETAIL DRAWING(S) UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ALL SIGN LOCATIONS TO BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. ONCE THE SIGNS HAVE BEEN INSTALLED AND ACCEPTED, ANY SIGN RELOCATIONS REQUESTED BY THE DEPARTMENT WILL BE COMPENSATED IN ACCORDANCE WITH ARTICLE 104-7. ANY ADDITIONAL SIGNS OTHER THAN THE ONES SHOWN IN THE DRAWING WILL BE COMPENSATED IN ACCORDANCE WITH ARTICLE 104-7.

NO STATIONARY -Y- LINE ADVANCE WARNING SIGNAGE IS REQUIRED UNLESS THERE'S MORE THAN 1,000 FEET OF RESURFACING ALONG THE -Y- LINE. WHENEVER WORK PROCEEDS THROUGH AN INTERSECTION, PORTABLE SIGNS SHALL BE USED FOR TRAFFIC CONTROL.

IF THERE IS A PERIOD OF CONSTRUCTION INACTIVITY LONGER THAN 14 CALENDAR DAYS, REMOVE OR COVER ADVANCE/GENERAL WARNING WORK ZONE SIGNS. UNCOVER ADVANCE/GENERAL WARNING WORK ZONE SIGNS NO MORE THAN 7 CALENDAR DAYS BEFORE WORK RESUMES. ALL OTHER OPERATIONS MAY BE SUSPENDED UPON FAILURE TO COMPLY WITH THE ABOVE REQUIREMENTS. SUCH SUSPENDED OPERATIONS WOULD NOT BE RESUMED UNTIL THE ABOVE REQUIREMENTS ARE FULFILLED.

(B) SIGN REMOVAL

ALL STATIONARY WORK ZONE SIGNS SHALL BE REMOVED ONCE THE RESURFACING IS SUBSTANTIALLY COMPLETE. THE RESURFACING IS SUBSTANTIALLY COMPLETE WHEN THE RESURFACING OPERATIONS ARE COMPLETED AND THE SHOULDERS ARE BROUGHT UP TO THE SAME ELEVATION AS THE PROPOSED PAVEMENT AND WHEN PAVEMENT MARKINGS ARE INSTALLED. THE PAVEMENT MARKING DOESN'T HAVE TO BE THE FINAL MARKING MATERIAL TO BE CONSIDERED SUBSTANTIALLY COMPLETE. ANY REMAINING PUNCH LIST ITEMS ARE TO BE COMPLETED WITH PORTABLE WORK ZONE SIGNING. THERE WILL BE NO COMPENSATION FOR ANY PORTABLE SIGNING. SIGN REMOVAL IS A CONDITION OF FINAL ACCEPTANCE.

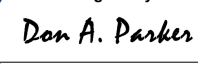
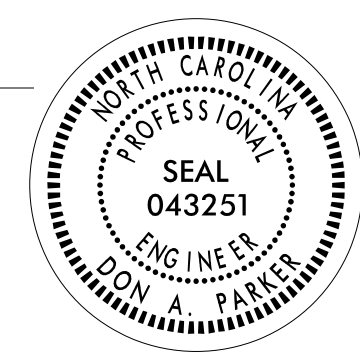

PHASING

PHASE I

- STEP 1 -- INSTALL ADVANCE WARNING SIGNS FOR DETOUR ROUTE RESURFACING. (SEE TMP-4 AND LOCAL NOTE 3).
- STEP 2 -- USING FLAGGERS, RESURFACE THE DETOUR ROUTE AND PLACE FINAL PAVEMENT MARKINGS IN ACCORDANCE WITH THE ROADWAY PLANS. (SEE RSD 1101.02, SHEET 1) AND LOCAL NOTES 1 AND 2.)
- STEP 3 -- REMOVE ADVANCE WARNING SIGNS FOR DETOUR ROUTE RESURFACING. (SEE LOCAL NOTE 3).

PHASE II

- STEP 1 -- USING RSD 1101.03, SHEET 1 OF 9, AND SHEET TMP-3, INSTALL DETOUR SIGNS, PLACE TYPE III BARRICADES TO CLOSE SR 1233 (AUTRYVILLE RD) TO THROUGH TRAFFIC, AND DETOUR TRAFFIC OFF-SITE.
- STEP 2 -- AWAY FROM TRAFFIC, COMPLETE THE FOLLOWING:
 - 1) REMOVE EXISTING STRUCTURE AND CONSTRUCT PROPOSED STRUCTURE. (SEE ROADWAY AND STRUCTURE PLANS)
 - 2) CONSTRUCT PROPOSED ROADWAY UP TO AND INCLUDING FINAL LAYER OF SURFACE COURSE. (SEE ROADWAY PLANS)
 - 3) PLACE FINAL PAVEMENT MARKINGS AND TIE INTO EXISTING PAVEMENT MARKINGS. (SEE PAVEMENT MARKING PLANS)
- STEP 3 -- REMOVE ALL TRAFFIC CONTROL DEVICES, ALL DETOUR SIGNING AND OPEN SR 1233 (AUTRYVILLE RD) TO PROPOSED TRAFFIC PATTERN.

APPROVED:  <small>40482802618410...</small> DATE: 9/26/2016 		<h1 style="margin: 0;">TRANSPORTATION OPERATIONS PLAN</h1>
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED		